

FOREWORD

**UNIFORM STANDARD DRAWINGS FOR PUBLIC WORKS' CONSTRUCTION,
OFFSITE IMPROVEMENTS, CLARK COUNTY AREA, NEVADA
VOLUME I**

The following participating entities of the Clark County, Nevada area have adopted these standard drawings.

CITY OF LAS VEGAS Adopted by City Council action	November 4, 1987
CITY OF HENDERSON Adopted by City Council action	October 20, 1987
CITY OF NORTH LAS VEGAS Adopted by City Council action	November 4, 1987
CITY OF BOULDER CITY Adopted by City Council action	January 26, 1988
CITY OF MESQUITE Adopted by City Council action	January 26, 1988
CLARK COUNTY Adopted by Board action	April 1998
REGIONAL STREET AND HIGHWAY COMMISSION Adopted by Commission Action	October 8, 1987

The Uniform Standard Drawings for Public Works Construction may be revised by issuance of revisions or supplements to correct errors and omissions found in these drawings and to reflect advanced thinking and the changing technology of the construction industry. Each revision will supersede any previous pertinent drawing. Upon approval by the RTC, revisions will become effective and be posted on the RTC web-site, www.rtcsonthernnevada.com, by the first day of the month of January and July.

To implement this end a Specifications Committee has been established as a permanent organization to continually study and recommend changes to the standard drawings. Interested parties may address suggested changes and questions to the Regional Transportation Commission, 600 South Grand Central Parkway, Suite 350, Las Vegas, Nevada, 89106-4512.

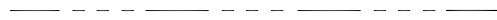
SECTION LINE



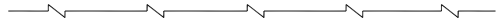
CENTERLINE OR SURVEY LINE



RIGHT-OF-WAY OR PROPERTY LINE



CUT LINE-ASPHALT CONC. OR PCC CONC.



DIRECTION OF FLOW



MISCELLANEOUS TOPOGRAPHIC SYMBOLS

VALVE (INITIALS INDICATE OWNERSHIP AND / OR TYPE)



STREET NAME SIGN



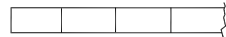
GAS CATHODE PROTECTION RECTIFIER



TEST HOLE



CONCRETE BLOCK WALL



UTILITY BOX (INITIALS INDICATE OWNERSHIP AND / OR TYPE)



FIRE ALARM BOX ON PEDESTAL



PUBLIC SERVICE UTILITY POLE, LINE TO SHOW DIRECTION OF RUN OF OVERHEAD LINE



CONCRETE MONUMENT



POLE WITH GUY ANCHOR



TREE (NO. INDICATES TRUCK DIA. IN INCHES)



NO.

FIRE HYDRANT



MISCELLANEOUS SYMBOL (ABBREVIATION INDICATES TYPE)



ABBR.

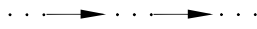
FENCE



HEDGE (NOTE SIZE & SPECIES)



FLOW LINE OF DITCH OR CHANNEL



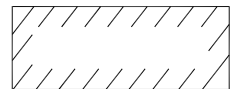
STEPS (NOTE TYPE AND NO. OF RISERS)



EXISTING UTILITY STUB-OUT



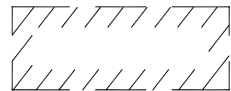
EXISTING BUILDING



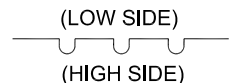
TRAFFIC CONTROL SIGN



EXISTING BUILDING FOUNDATION



RETAINING WALL



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SYMBOLS

DATE

DWG. NO.

101

PAGE NO.

1

PLAN ONLY

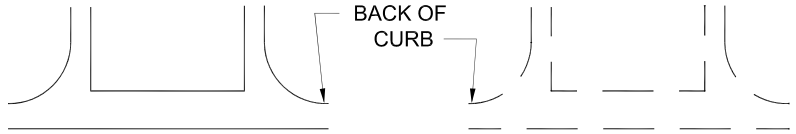
PROPOSED CONSTRUCTION

EXISTING

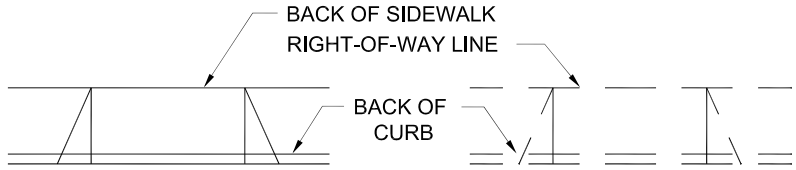
CURB AND GUTTER



VALLEY GUTTER



DRIVEWAY



WHEELCHAIR RAMP



DROP INLET



EDGE OF PAVEMENT



ELEVATIONS

0.00

(0.00)

CUT OR FILL SLOPES



NOTE: FUTURE CONSTRUCTION ITEMS ON PLANS SHALL BE INDICATED BY A DASHED LINE AND APPROPRIATE NOTE.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SYMBOLS

DATE

DWG. NO.

102

PAGE NO.

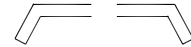
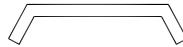
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PLAN ONLY

PROPOSED CONSTRUCTION

EXISTING

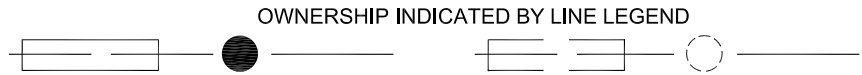
WING TYPE HEADWALL



VALLEY GUTTER



UNDERGROUND UTILITY
WITH MANHOLE AND
CASING



SIZE AND TYPE OF CONDUIT SHALL
BE PLACED ON CONDUIT WHEN
AVAILABLE

LEGEND

TSI = TRAFFIC SIGNAL INTERCONNECT
E = ELECTRIC
FA = FIRE ALARM
SL = STREET LIGHT
CATV = CABLE TELEVISION

SS = SANITARY SEWER
SD = STORM DRAIN
W = WATER
S-G = STEEL GAS
PL-G = PLASTIC GAS
T = TELEPHONE

PROFILE ONLY

CENTERLINE GRADE



TOP OF CURB OR FLOW
LINE



PIPE



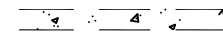
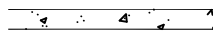
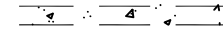
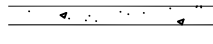
OR

OR



OR

OR



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SYMBOLS

DATE

DWG. NO.

103

PAGE NO.

3

MISCELLANEOUS ELECTRICAL SYMBOLS

PLAN ONLY

EXISTING

PROPOSED CONSTRUCTION

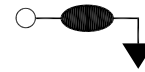
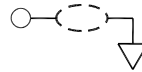
TRAFFIC SIGNAL CONTROLLER



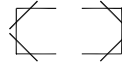
PULL BOX (INITIALS INDICATE OWNERSHIP AND NUMBER INDICATES SIZE)



TRAFFIC SIGNAL WITH LUMINAIRE



TRAFFIC SIGNAL DETECTOR LOOP



SERVICE POINT, PAD MOUNTED



7 GAGE LIGHTING STANDARD



11 GAGE LIGHTING STANDARD



100 W LUMINAIRE



150 W LUMINAIRE



200 W LUMINAIRE



250 W LUMINAIRE



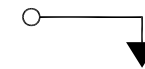
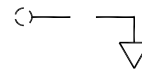
400 W LUMINAIRE



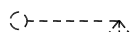
750 W LUMINAIRE



TRAFFIC SIGNAL POLE



SCHOOL FLASHER



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SYMBOLS

Aban	- Abandon	Elev	- Elevation
AIP	- Abandoned in Place	Embk	- Embankment
Agg	- Aggregate	ECR	- End of Curb Return
Alt	- Alternate	EC	- End of Curve
A	- Anode, Gas	EBS	- Cathode Protection Bond Station
ACP	- Asbestos Cement Pipe	ETS	- Cathode Protection Test Station
AC	- Asphaltic Concrete	EVC	- End of Vertical Curve
Ave	- Avenue	Exist	- Existing
BC	- Back of Curb	Ft	- Feet or Foot
BCR	- Back of Curb Radius	FG	- Finish Grade
BVC	- Beginning of Vertical Curve	FA	- Fire Alarm
BM	- Bench Mark	FH	- Fire Hydrant
Bdry Line	- Boundary Line	FL	- Flow Line
CATV	- Cable Television	Galv	- Galvanized
C-C	- Center to Center	GM	- Gas Meter
CL	- Centerline	GR	- Gas Pressure Regulator
CBC	- City of Boulder City	PL-G	- Plastic Gas
CLV	- City of Las Vegas	S-G	- Steel Gas
COH	- City of Henderson	GB	- Grade Break
CM	- City of Mesquite	Gut	- Gutter
CNLV	- City of North Las Vegas	Hdwl	- Headwall
CC	- Clark County	In	- Inch
CCSD	- Clark County Sanitation District	INT	- Intersection
CO	- Clean Out, Sewer	INV	- Invert
Comm	- Commercial	ISL	- Island
Conc, PCC	- Concrete	JB	- Junction Box
Const	- Construction or Construct	LOC	- Length of Curb
Cor	- Corner	Lt	- Left
CMP	- Corrugated Metal Pipe	LF	- Linear Feet
CSAP	- Corrugated Steel Arch Pipe	LVVWD	- Las Vegas Valley Water District
CSP	- Corrugated Steel Pipe	MH	- Manhole
C&G	- Curb and Gutter	Max	- Maximum
CIP	- Cast Iron Pipe	Min	- Minimum
Col	- Column	Mon	- Monolithic
Cu Yd, CY	- Cubic Yard	N/o	- North of
Cu Ft, CF	- Cubic Feet	NTS	- Not to Scale
Culv	- Culvert	OC	- On Center
CF	- Curb Face	OG	- Open-Graded Pavement, Original Ground
Dept	- Department	Pvmt	- Pavement
Dia	- Diameter	PI	- Point of Intersection
Dwy	- Driveway	PRC	- Point of Reverse Curve
DI	- Drop Inlet	PC	- Point of Curve
Esmt	- Easement	PT	- Point of Tangency
E/o	- East of	PCC	- Point of Compound Curve
EP	- Edge of Pavement	PCC, Conc	- Portland Cement Concrete
EO	- Edge of Oil		
E	- Electric		
EM	- Electric Meter		

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

ABBREVIATIONS

PP	- Power Pole
P	- Power
ℙ	- Property Line
Prop	- Proposed
PB	- Pull Box
RP	- Radius Point
R	- Radius
RR	- Railroad
Reinf	- Reinforced
RC	- Reinforced Concrete
RCB	- Reinforced Concrete Box
RCP	- Reinforced Concrete Pipe
Reloc	- Relocate
RT	- Right
R/W	- Right-of-way
RD	- Road
SS	- Sanitary Sewer
Sht	- Sheet
S/o	- South of
SW	- Sidewalk
SQ FT, SF	- Square foot
SQ YD, SY	- Square yard
Sta	- Station
SHP	- Steel Highpressure Pipe
SD	- Storm Drain
STD	- Standard
Struct	- Structural or Structure
Surv	- Survey
SL	- Streetlight
T	- Telephone
Temp	- Temporary
TBA	- To Be Adjusted
TBR	- To Be Removed
TC	- Top of Curb
TP	- Top of Pipe
TS	- Traffic Signal
TSI	- Traffic Signal Interconnect
Trans	- Transition
Typ	- Typical
UG	- Underground
Var	- Variable
Vert	- Vertical
VC	- Vertical Curve
VG	- Valley Gutter
VCP	- Vitrified Clay Pipe
W	- Water
WM	- Water Meter
W/o	- West of
Yd	- Yard

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

ABBREVIATIONS

R-Value	MAJOR COLLECTOR								ARTERIAL							
	Normal Traffic (8.0)				Heavy Traffic (8.5)				Normal Traffic (9.0)				Heavy Traffic (9.5)			
	SN	AC	T-II	T-I	SN	AC	T-II	T-I	SN	AC	T-II	T-I	SN	AC	T-II	T-I
16	3.35	4.0	4.0	13.5	3.65	4.5	4.0	14.5	4.25	5.0	4.0	18.5	4.55	5.5	4.0	19.5
18	3.30	4.0	4.0	13.0	3.55	4.5	4.0	14.0	4.15	5.0	4.0	17.5	4.45	5.5	4.0	19.0
20	3.20	4.0	4.0	12.0	3.45	4.5	4.0	13.0	4.05	5.0	4.0	16.5	4.35	5.5	4.0	18.0
22	3.10	4.0	4.0	11.5	3.40	4.5	4.0	12.5	3.95	5.0	4.0	16.0	4.25	5.5	4.0	17.0
24	3.05	4.0	4.0	11.0	3.30	4.5	4.0	11.5	3.85	5.0	4.0	15.0	4.15	5.5	4.0	16.0
26	2.95	4.0	4.0	10.0	3.20	4.5	4.0	10.5	3.75	5.0	4.0	14.0	4.05	5.5	4.0	15.0
28	2.90	4.0	4.0	9.5	3.15	4.5	4.0	10.0	3.65	5.0	4.0	13.0	3.95	5.5	4.0	14.0
30	2.80	4.0	4.0	8.5	3.05	4.5	4.0	9.0	3.55	5.0	4.0	12.00	3.85	5.5	4.0	13.5
32	2.75	4.0	4.0	8.0	2.95	4.5	4.0	8.5	3.45	5.0	4.0	11.5	3.75	5.5	4.0	12.5
34	2.65	4.0	4.0	7.0	2.90	4.5	4.0	8.0	3.40	5.0	4.0	11.0	3.65	5.5	4.0	11.5
36	2.60	4.0	4.0	6.5	2.80	4.5	4.0	7.0	3.30	5.0	4.0	10.0	3.55	5.5	4.0	10.5
38	2.50	4.0	4.0	6.0	2.75	4.5	4.0	6.5	3.20	5.0	4.0	9.0	3.45	5.5	4.0	9.5
40	2.45	4.0	4.0	5.5	2.65	4.5	4.0	5.5	3.15	5.0	4.0	8.5	3.35	5.5	4.0	9.0
42	2.40	4.0	4.0	5.0	2.60	4.5	4.0	5.0	3.05	5.0	4.0	7.5	3.30	5.5	4.0	8.5
44	2.35	4.0	4.0	4.5	2.55	4.5	4.0	4.5	2.95	5.0	4.0	7.0	3.20	5.5	4.0	7.5
46	2.25	4.0	4.0	4.0	2.45	4.5	4.0	4.0	2.90	5.0	4.0	6.5	3.10	5.5	4.0	6.5
48	2.20	4.0	7.0	NA	2.40	4.5	7.0	NA	2.80	5.0	4.0	5.5	3.05	5.5	4.0	6.0
50	2.15	4.0	6.5	NA	2.35	4.5	6.5	NA	2.75	5.0	4.0	5.0	2.95	5.5	4.0	5.0
52	2.10	4.0	6.0	NA	2.30	4.5	6.0	NA	2.65	5.0	4.0	4.0	2.90	5.5	4.0	4.5
54	2.05	4.0	5.5	NA	2.20	4.5	5.5	NA	2.60	5.0	7.5	NA	2.80	5.5	4.0	4.0
56	2.00	4.0	5.0	NA	2.15	4.5	5.0	NA	2.55	5.0	7.0	NA	2.75	5.5	7.0	NA
58	1.95	4.0	5.0	NA	2.10	4.5	4.5	NA	2.45	5.0	6.0	NA	2.65	5.5	6.5	NA
60	1.90	4.0	4.5	NA	2.05	4.5	4.0	NA	2.40	5.0	5.5	NA	2.60	5.5	6.0	NA
62	1.85	4.0	4.0	NA	2.00	4.0	5.0	NA	2.35	5.0	5.0	NA	2.50	5.5	5.0	NA
64	1.80	4.0	4.0	NA	1.95	4.0	5.0	NA	2.30	5.0	4.5	NA	2.45	5.5	4.5	NA
66	1.75	4.0	4.0	NA	1.90	4.0	4.5	NA	2.20	5.0	4.0	NA	2.40	5.5	4.0	NA
68	1.70	4.0	4.0	NA	1.85	4.0	4.0	NA	2.15	4.5	5.0	NA	2.35	5.0	5.0	NA
70	1.65	4.0	4.0	NA	1.80	4.0	4.0	NA	2.10	4.5	4.5	NA	2.25	5.0	4.5	NA
72	1.60	4.0	4.0	NA	1.75	4.0	4.0	NA	2.05	4.5	4.0	NA	2.20	5.0	4.0	NA
74	1.55	4.0	4.0	NA	1.70	4.0	4.0	NA	2.00	4.0	5.0	NA	2.15	4.5	5.0	NA
76	1.50	4.0	4.0	NA	1.65	4.0	4.0	NA	1.95	4.0	4.5	NA	2.10	4.5	4.5	NA
78	1.45	4.0	4.0	NA	1.60	4.0	4.0	NA	1.90	4.0	4.0	NA	2.05	4.5	4.0	NA

NOTES:

1. THIS CHART WAS CONSTRUCTED USING THE 1993 AASHTO PAVEMENT DESIGN GUIDE, 1996 NDOT MANUAL AND THE 2000 RTC DESIGN CRITERIA, SECTION 401.01.02 OF THE STANDARD SPECIFICATIONS.
2. A TRAFFIC STUDY MAY BE REQUIRED IF TI > 9.5.
3. AN AVERAGE R-VALUE MAY BE USED IF IT IS REPRESENTATIVE OF ALL PROJECT CONDITIONS.
4. ADDITIONAL DESIGN COMPENSATION IS REQUIRED IF EXPANSIVE SOILS, HYDRO-COLLAPSIBLE SOILS, OR SOLUBLE MATERIALS ARE PRESENT.
5. AC DEPTHS SHOWN ARE MINIMUMS AND 4" MINIMUM TYPE II IS REQUIRED; OTHER COMBINATIONS THAT MEET OR EXCEED THE STRUCTURAL NUMBER REQUIREMENTS ARE ACCEPTABLE.

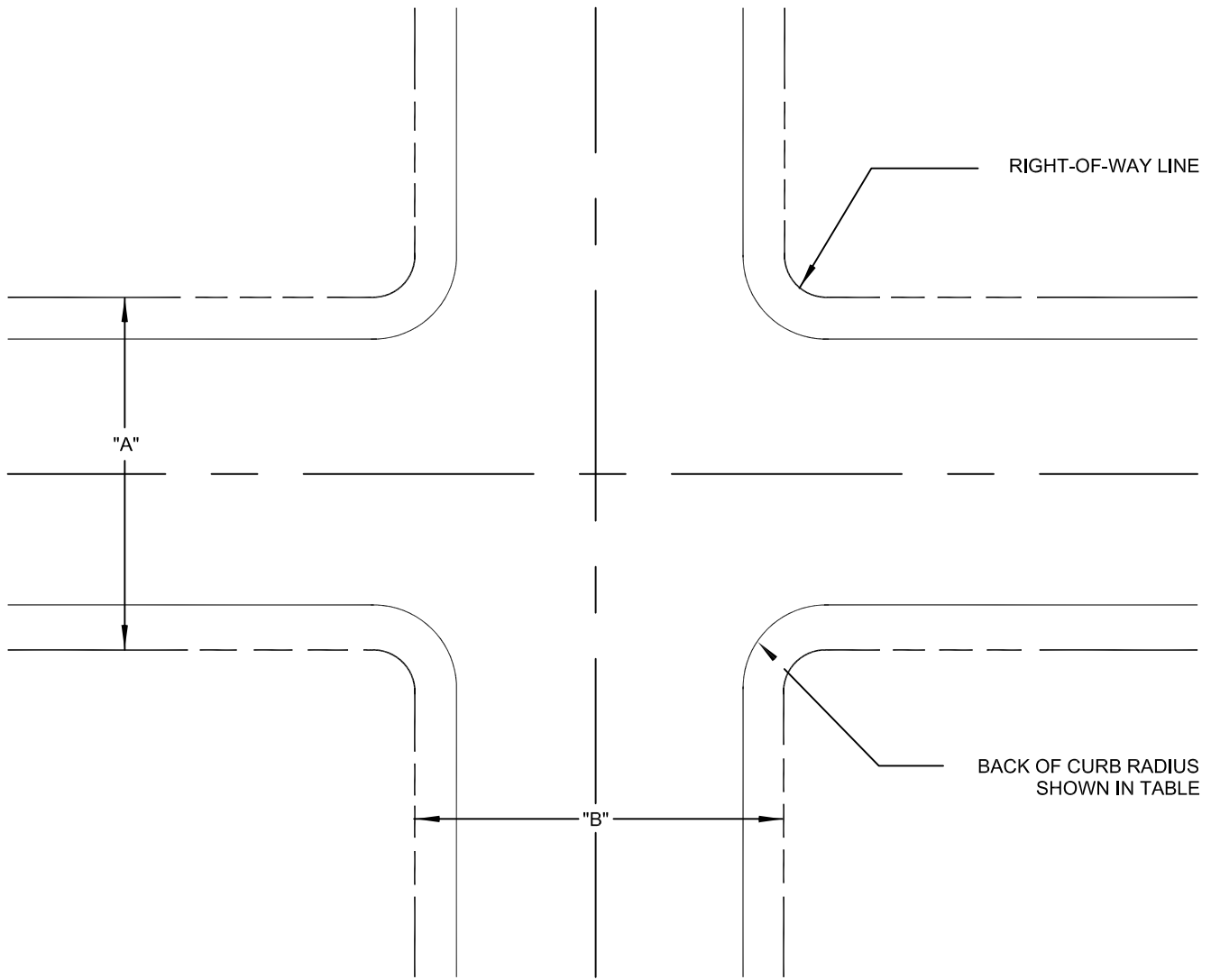
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
401	PLANTMIX BITUMINOUS PAVEMENTS					
		PAVEMENT STRUCTURE DESIGN GUIDELINE CHART FOR MAJOR COLLECTOR AND ARTERIAL ROADWAYS				
		DATE 11-10-04	DWG. NO.	200	PAGE NO.	6.1

R-Value	RESIDENTIAL								MINOR COLLECTOR							
	Normal Traffic (5.0)				Heavy Traffic (5.5)				Normal Traffic (6.0)				Heavy Traffic (6.5)			
	SN	AC	T-II	T-I	SN	AC	T-II	T-I	SN	AC	T-II	T-I	SN	AC	T-II	T-I
16	1.70	2.0	4.0	5.0	2.00	2.5	4.0	6.0	2.25	3.0	4.0	6.5	2.50	3.5	4.0	7.5
18	1.70	2.0	4.0	5.0	1.95	2.5	4.0	5.5	2.20	3.0	4.0	6.0	2.45	3.5	4.0	7.0
20	1.65	2.0	4.0	4.5	1.90	2.5	4.0	5.0	2.15	3.0	4.0	5.5	2.40	3.5	4.0	6.5
22	1.60	2.0	4.0	4.0	1.85	2.5	4.0	4.5	2.05	3.0	4.0	5.0	2.35	3.5	4.0	6.0
24	1.55	2.0	7.5	NA	1.80	2.5	4.0	4.0	2.00	3.0	4.0	4.5	2.25	3.5	4.0	5.0
26	1.50	2.0	7.0	NA	1.75	2.5	7.5	NA	1.95	3.0	4.0	4.0	2.20	3.5	4.0	4.5
28	1.45	2.0	6.5	NA	1.70	2.5	7.0	NA	1.90	3.0	7.5	NA	2.15	3.5	4.0	4.0
30	1.40	2.0	6.0	NA	1.65	2.5	6.5	NA	1.85	3.0	7.0	NA	2.10	3.5	7.5	NA
32	1.35	2.0	5.5	NA	1.60	2.5	6.0	NA	1.80	3.0	6.5	NA	2.05	3.5	7.0	NA
34	1.35	2.0	5.5	NA	1.55	2.5	6.0	NA	1.75	3.0	6.0	NA	2.00	3.5	6.5	NA
36	1.30	2.0	5.0	NA	1.50	2.5	5.5	NA	1.70	3.0	5.5	NA	1.95	3.5	6.0	NA
38	1.25	2.0	5.0	NA	1.45	2.5	5.0	NA	1.65	3.0	5.0	NA	1.85	3.5	5.5	NA
40	1.20	2.0	4.5	NA	1.40	2.5	4.5	NA	1.60	3.0	5.0	NA	1.80	3.5	5.0	NA
42	1.15	2.0	4.0	NA	1.35	2.5	4.0	NA	1.55	3.0	4.5	NA	1.75	3.5	4.5	NA
44	1.15	2.0	4.0	NA	1.35	2.5	4.0	NA	1.55	3.0	4.5	NA	1.70	3.5	4.0	NA
46	1.10	2.0	4.0	NA	1.30	2.0	5.0	NA	1.50	3.0	4.0	NA	1.70	3.5	4.0	NA
48	1.05	2.0	4.0	NA	1.25	2.0	5.0	NA	1.45	3.0	4.0	NA	1.65	3.0	5.0	NA
50	1.05	2.0	4.0	NA	1.20	2.0	4.5	NA	1.40	3.0	4.0	NA	1.60	3.0	5.0	NA
52	1.00	2.0	4.0	NA	1.20	2.0	4.5	NA	1.35	3.0	4.0	NA	1.55	3.0	4.5	NA
54	0.95	2.0	4.0	NA	1.15	2.0	4.0	NA	1.30	3.0	4.0	NA	1.50	3.0	4.0	NA
56	0.95	2.0	4.0	NA	1.10	2.0	4.0	NA	1.30	3.0	4.0	NA	1.45	3.0	4.0	NA
58	0.90	2.0	4.0	NA	1.05	2.0	4.0	NA	1.25	3.0	4.0	NA	1.40	3.0	4.0	NA
60	0.85	2.0	4.0	NA	1.05	2.0	4.0	NA	1.20	3.0	4.0	NA	1.35	3.0	4.0	NA
62	0.85	2.0	4.0	NA	1.00	2.0	4.0	NA	1.15	3.0	4.0	NA	1.35	3.0	4.0	NA
64	0.80	2.0	4.0	NA	0.95	2.0	4.0	NA	1.15	3.0	4.0	NA	1.30	3.0	4.0	NA
66	0.75	2.0	4.0	NA	0.95	2.0	4.0	NA	1.10	3.0	4.0	NA	1.25	3.0	4.0	NA
68	0.75	2.0	4.0	NA	0.90	2.0	4.0	NA	1.05	3.0	4.0	NA	1.20	3.0	4.0	NA
70	0.70	2.0	4.0	NA	0.85	2.0	4.0	NA	1.00	3.0	4.0	NA	1.15	3.0	4.0	NA
72	0.70	2.0	4.0	NA	0.80	2.0	4.0	NA	1.00	3.0	4.0	NA	1.15	3.0	4.0	NA
74	0.65	2.0	4.0	NA	0.75	2.0	4.0	NA	0.95	3.0	4.0	NA	1.10	3.0	4.0	NA
76	0.65	2.0	4.0	NA	0.75	2.0	4.0	NA	0.90	3.0	4.0	NA	1.05	3.0	4.0	NA
78	0.65	2.0	4.0	NA	0.70	2.0	4.0	NA	0.85	3.0	4.0	NA	1.05	3.0	4.0	NA

NOTES:

1. THIS CHART WAS CONSTRUCTED USING THE 1993 AASHTO PAVEMENT DESIGN GUIDE, 1996 NDOT MANUAL AND THE 2000 RTC DESIGN CRITERIA, SECTION 401.01.02 OF THE STANDARD SPECIFICATIONS.
2. AN AVERAGE R-VALUE MAY BE USED IF IT IS REPRESENTATIVE OF ALL PROJECT CONDITIONS.
3. ADDITIONAL DESIGN COMPENSATION IS REQUIRED IF EXPANSIVE SOILS, HYDRO-COLLAPSIBLE SOILS, OR SOLUBLE MATERIALS ARE PRESENT.
4. AC DEPTHS SHOWN ARE MINIMUMS AND 4" MINIMUM TYPE II IS REQUIRED; OTHER COMBINATIONS THAT MEET OR EXCEED THE STRUCTURAL NUMBER REQUIREMENTS ARE ACCEPTABLE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
401	PLANTMIX BITUMINOUS PAVEMENTS					
		PAVEMENT STRUCTURE DESIGN GUIDELINE CHART FOR MINOR COLLECTOR AND RESIDENTIAL ROADWAYS				
		DATE 11-10-04	DWG. NO.	200A	PAGE NO.	6.1A



NOTES

PROPERTY LINES SHALL BE PARALLEL AND RADIAL TO THE BACK OF CURB AT A DISTANCE CONSISTENT WITH THE STANDARD STREET SECTIONS DRAWING NUMBERS.

* PROPERTY LINE RADIUS SHALL BE A MINIMUM OF 54 FEET.

** PROPERTY LINE RADIUS SHALL BE A MINIMUM OF 40 FEET.

"B" \ "A"	60' OR LESS	80'	100' OR MORE
60' OR LESS	20'	25'	30'
80'	25'	** 30'	* 30'
100' OR MORE	30'	* 30'	* 30'

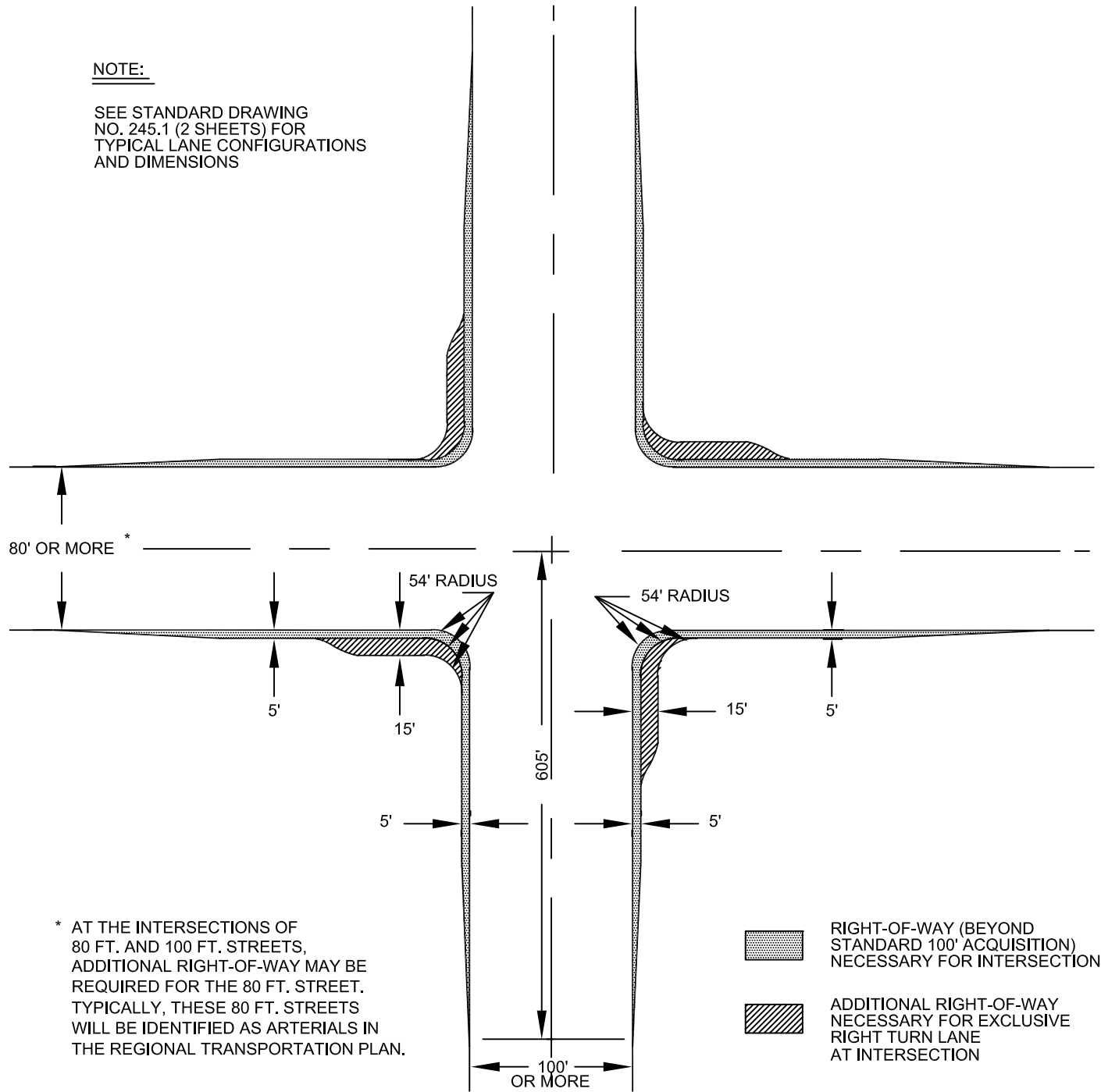
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

MINIMUM BACK OF CURB RADIUS

NOTE:

SEE STANDARD DRAWING
NO. 245.1 (2 SHEETS) FOR
TYPICAL LANE CONFIGURATIONS
AND DIMENSIONS



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

ADDITIONAL RIGHT-OF-WAY REQUIRED
AT MAJOR INTERSECTIONS

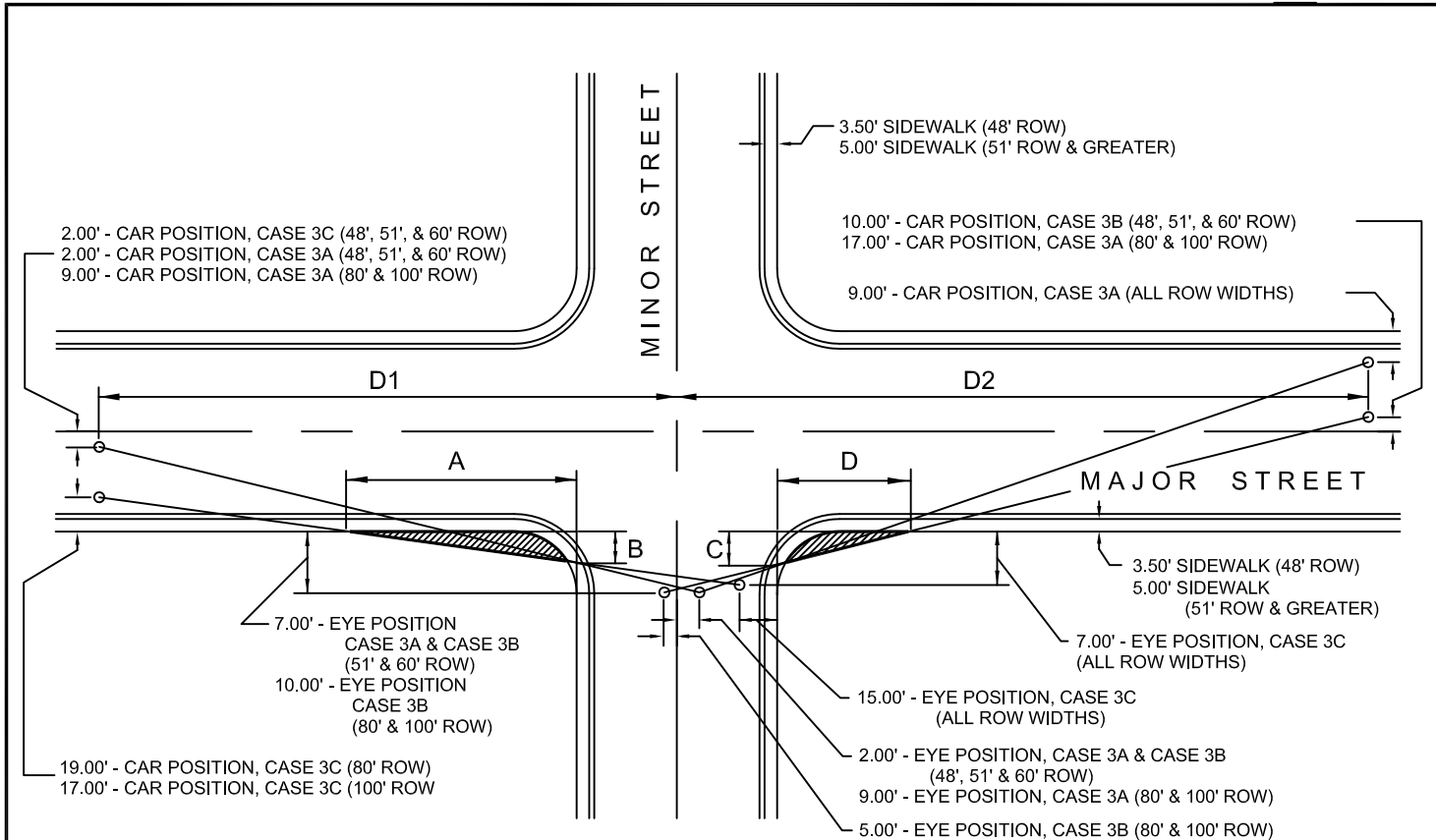
DATE 7-10-03

DWG. NO.

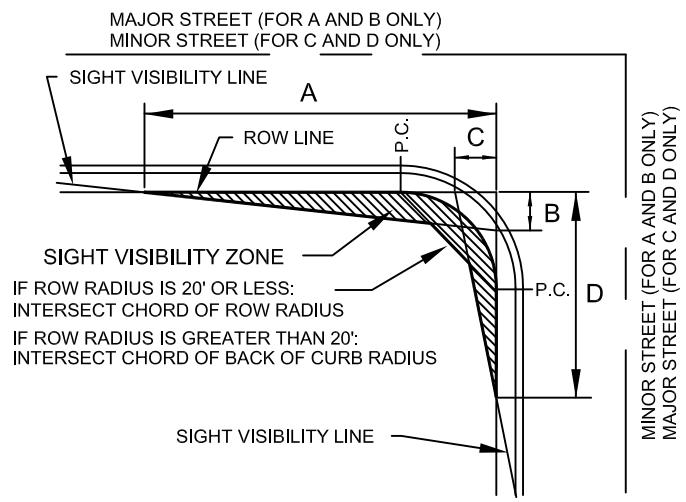
201.1

PAGE NO.

7.1



INTERSECTION SIGHT VISIBILITY ZONE



TYPICAL INTERSECTION CORNER

NOTE: FOR SIGHT ZONE DIMENSIONS, SEE SETBACK TABLE ON SHEET 2 OF THIS STANDARD DRAWING.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
		SIGHT VISIBILITY ZONES AT INTERSECTIONS			
		DATE	8-21-97	DWG. NO.	201.2 (1 OF 2)
		PAGE	7.2		

MAJOR ROW	48'	51'	60'	80'	100'
MINOR ROW	D1=278'(3A) D1=300'(3C) D2=309'(3A) D2=319'(3B)	D1=278'(3A) D1=300'(3C) D2=309'(3A) D2=319'(3B)	D1=388'(3A) D1=486'(3C) D2=442'(3A) D2=510'(3B)	D1=394'(3A) D1=486'(3C) D2=491'(3A) D2=527'(3B)	D1=545'(3A) D1=736'(3C) D2=693'(3A) D2=787'(3B)
48' D1=278'(3A) D1=300'(3C) D2=309'(3A) D2=319'(3B)	A = 53' B = 6' C = 6' D = 42'	A = 39' B = 5' C = 5' D = 31'	A = 66' B = 5' C = 5' D = 55'	A = 100' B = 6' C = 5' D = 36'	A = 184' B = 6' C = 5' D = 53'
51' D1=278'(3A) D1=300'(3C) D2=309'(3A) D2=319'(3B)	A = 51' B = 6' C = 6' D = 40'	A = 37' B = 4' C = 4' D = 29'	A = 64' B = 5' C = 5' D = 53'	A = 98' B = 6' C = 5' D = 34'	A = 182' B = 6' C = 5' D = 51'
60' D1=388'(3A) D1=486'(3C) D2=442'(3A) D2=510'(3B)	A = 46' B = 6' C = 6' D = 50'	A = 33' B = 4' C = 4' D = 37'	A = 56' B = 5' C = 5' D = 48'	A = 90' B = 5' C = 4' D = 30'	A = 174' B = 6' C = 5' D = 47'
80' D1=394'(3A) D1=486'(3C) D2=491'(3A) D2=527'(3B)	N/A	N/A	A = 56' B = 6' C = 6' D = 58'	A = 73' B = 6' C = 6' D = 35'	A = 157' B = 6' C = 6' D = 58'
100' D1=545'(3A) D1=736'(3C) D2=693'(3A) D2=787'(3B)	N/A	N/A	N/A	A = 56' B = 4' C = N/A D = N/A	A = 140' B = 5' C = N/A D = N/A

SETBACK TABLE

GENERAL NOTES

- EACH CORNER OF EVERY INTERSECTION SHALL HAVE A SIGHT VISIBILITY EASEMENT REGARDLESS OF RIGHT-OF-WAY WIDTH.
- NO WALLS, FENCES, TREES, SHRUBS, UTILITY APPURTENANCES OR ANY OTHER OBJECT, OTHER THAN TRAFFIC CONTROL DEVICES AND STREET LIGHT POLES, MAY BE CONSTRUCTED OR INSTALLED WITHIN THE SIGHT VISIBILITY ZONE UNLESS SAID OBJECT IS MAINTAINED AT LESS THAN 24 INCHES IN HEIGHT, MEASURED FROM TOP OF CURB, OR WHERE NO CURB EXISTS, A HEIGHT OF 27 INCHES MEASURED FROM THE TOP OF ADJACENT ASPHALT, GRAVEL OR PAVEMENT STREET SURFACE.
- AT INTERSECTIONS WHERE THE CLASSIFICATION OF MAJOR AND MINOR STREETS CANNOT BE PERMANENTLY ESTABLISHED, EACH LEG OF THE INTERSECTION MUST BE ANALYZED AS IF THE APPROACH LEG IS A MINOR STREET INTERSECTING A MAJOR STREET. THE PORTIONS OF THE SIGHT VISIBILITY ZONE LABELED "N/A" IN THE SETBACK TABLE ARE NOT REQUIRED. AT "T" INTERSECTIONS, THE TERMINATING LEG WILL ALWAYS BE THE MINOR STREET.
- CURVING ROADWAYS AND ROADWAYS WITH INTERSECTING ANGLES GREATER THAN 10 DEGREES MUST BE ANALYZED USING D1, D2, THE EYE POSITION, AND THE CAR POSITION AS SHOWN IN THE INFORMATION ABOVE.
- USE OF A SIGHT VISIBILITY ZONE DIFFERENT THAN THAT SHOWN HEREIN SHALL REQUIRE A SIGHT VISIBILITY ANALYSIS PREPARED AND SUBMITTED FOR APPROVAL TO THE LOCAL ENTITY ENGINEER BY A CIVIL ENGINEER REGISTERED IN THE STATE OF NEVADA.
- THE AREA WITHIN THE LIMITS OF THE ARC AND THE CHORD AT THE CURB RETURN SHALL BE ADDED TO THE SIGHT VISIBILITY ZONE AT EACH CORNER OF EVERY INTERSECTION, EXCEPT FOR 100' x 100' INTERSECTIONS OR GREATER.
- ON-STREET PARKING SHALL BE PROHIBITED WITHIN AREAS DESIGNATED BY DIMENSIONS "A" AND "D" ON SHEET 1 OF THIS DRAWING, SUBJECT TO THE APPROVAL OF THE TRAFFIC ENGINEER OR DESIGNATED REPRESENTATIVE OF THE ENTITY HAVING JURISDICTION.

BASIS FOR ANALYSIS

THE FOLLOWING CRITERIA WAS AND SHALL BE USED AS THE BASIS FOR DESIGN OF SIGHT VISIBILITY ZONES:

AASHTO PUBLICATION OF "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS", 1990 EDITION, CHAPTER IX, USING THE MOST RESTRICTIVE SIGHT LINE DERIVED FROM EACH OF THE THREE POSSIBLE CROSSING MANEUVERS (STOPPED CONDITION):

- CASE 3A - CROSSING MANEUVER
- CASE 3B - LEFT TURN MANEUVER ONTO A MAJOR STREET
- CASE 3C - RIGHT TURN MANEUVER ONTO A MAJOR STREET

THE ANALYSIS SHOULD USE THE GREATER OF THE FOLLOWING:

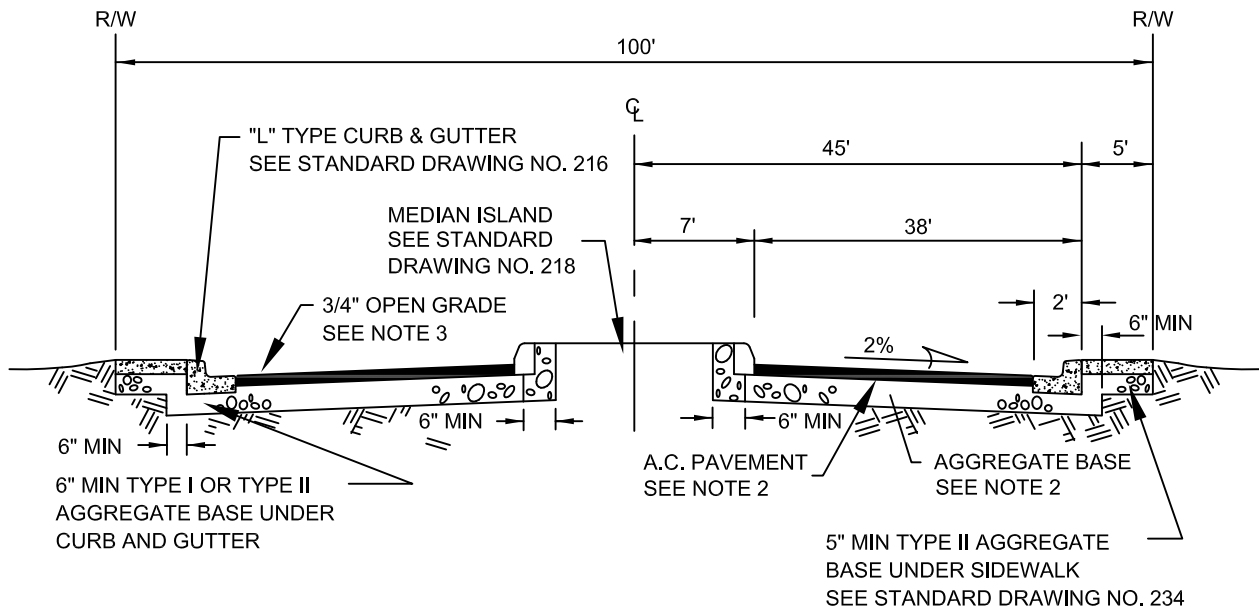
- DESIGN SPEED = POSTED SPEED LIMIT PLUS FIVE
- DESIGN SPEED = POSTED SPEED LIMIT DIVIDED BY 0.85

CAR AND EYE POSITIONS ARE AS SHOWN ON SHEET 1 OF THIS DRAWING.

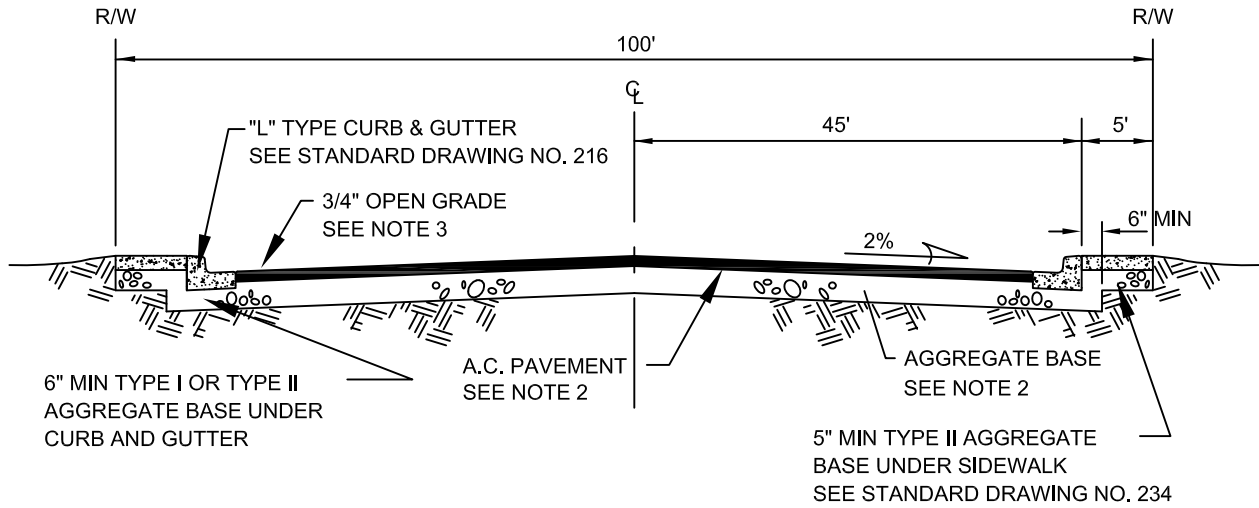
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SIGHT VISIBILITY ZONES
AT INTERSECTIONS



**ARTERIAL WITH
MEDIAN ISLAND**

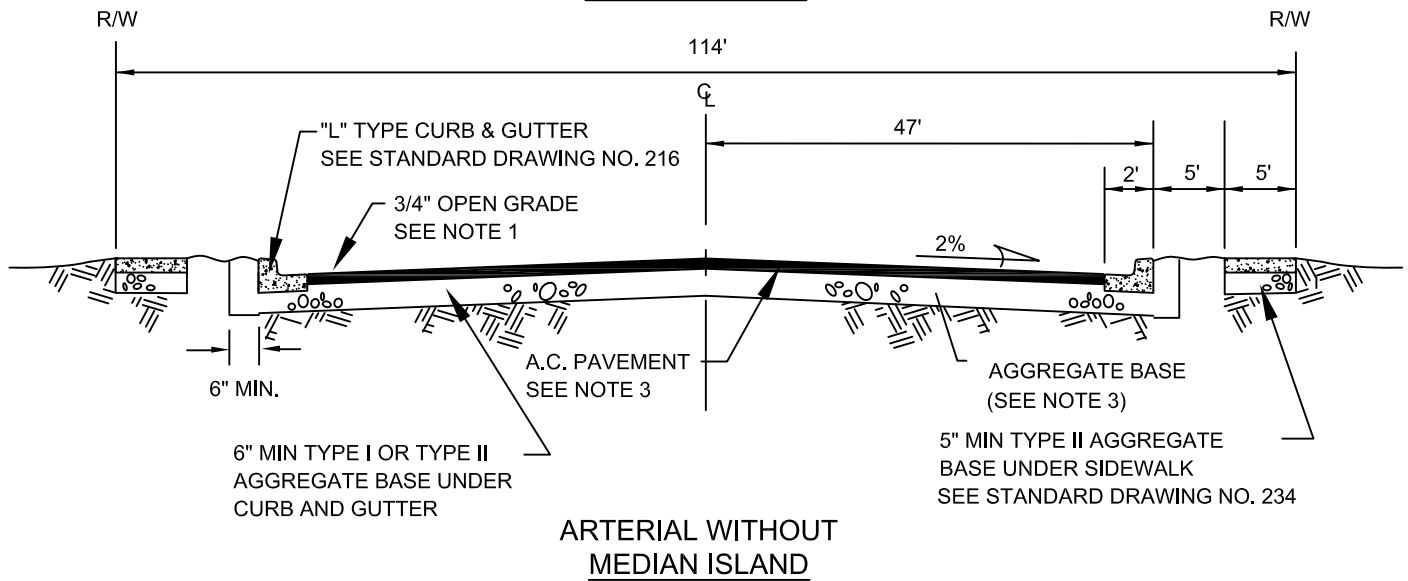
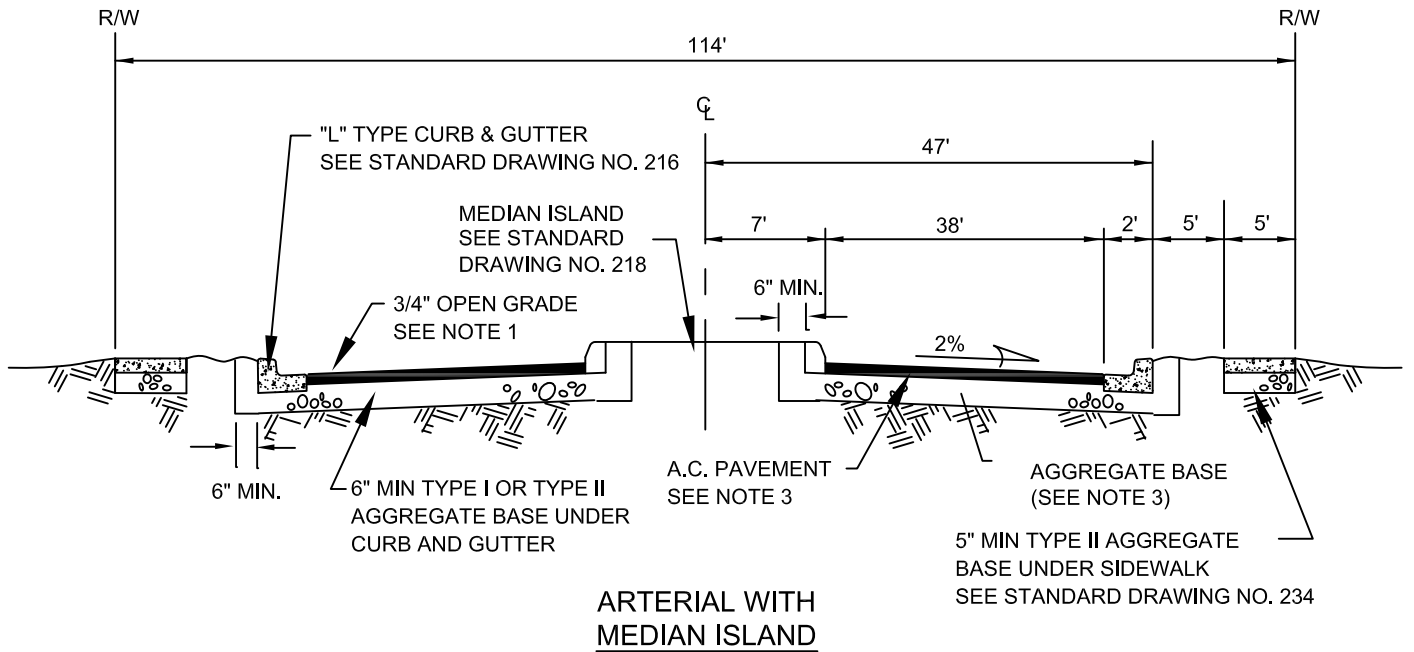


**ARTERIAL WITHOUT
MEDIAN ISLAND**

NOTES:

1. FINAL A.C. PAVEMENT SURFACE SHALL BE 1/2" MAXIMUM ABOVE LIP OF GUTTER. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING 200.
3. A 3/4" OPEN GRADE IS REQUIRED ON CLARK COUNTY ROADWAYS AND MAY BE REQUIRED IN OTHER JURISDICTIONS AS DETERMINED BY THE ENTITY'S ENGINEER.
4. IF OPEN GRADE IS REQUIRED DENSE GRADE SHALL BE FLUSH WITH LIP OF GUTTER AND FINAL A.C. PAVEMENT SURFACE SHALL BE 3/4" MAXIMUM ABOVE LIP OF GUTTER. FINAL A.C. PAVEMENT SURFACE SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
5. FOG SEAL AND PRIME COAT REQUIRED IN THE CITIES OF LAS VEGAS AND NORTH LAS VEGAS.

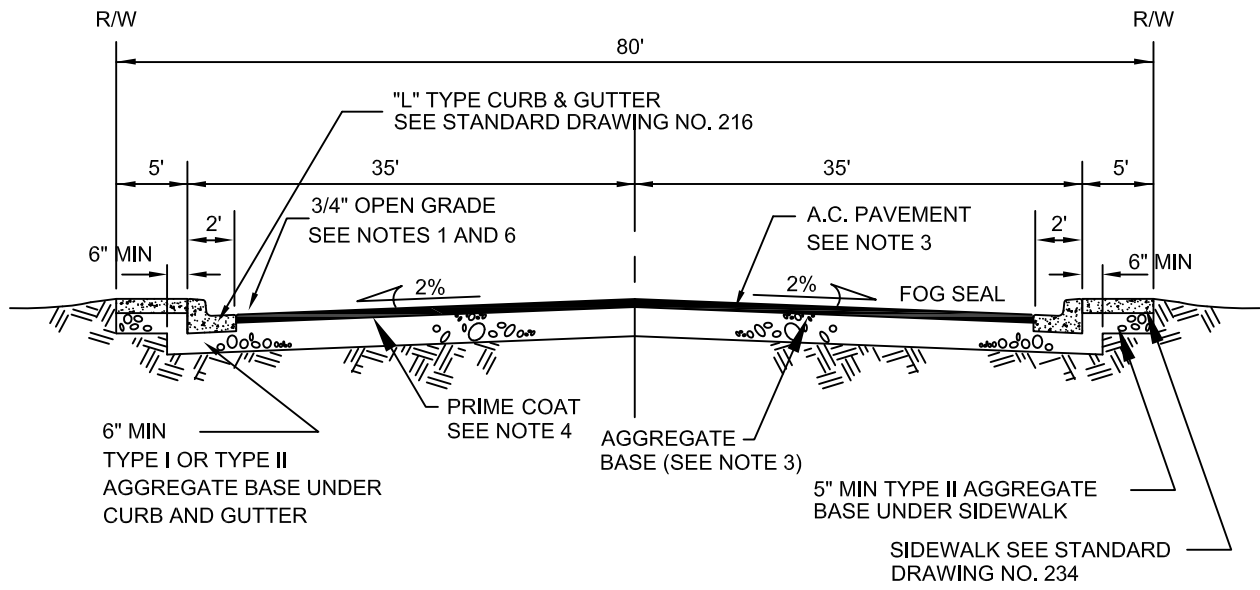
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	ARTERIAL URBAN AREA STREET SECTIONS WITH CURBSIDE SIDEWALK		
401	BITUMINOUS PAVEMENT			
403	OPEN GRADE			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 202	PAGE NO. 8



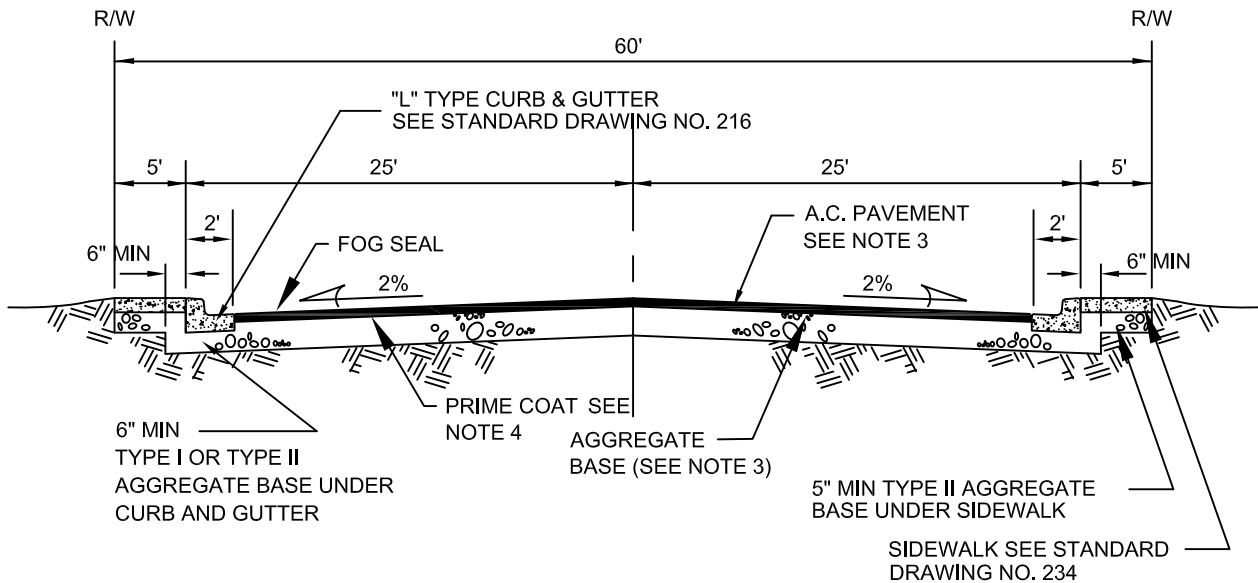
NOTES:

1. FINAL A.C. PAVEMENT SURFACE (INCLUDING OPEN GRADE) SHALL BE 3/4" MAXIMUM ABOVE LIP OF GUTTER. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. DENSE GRADE SHALL BE FLUSH WITH LIP OF GUTTER.
3. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING NO. 200.
4. THIS STANDARD IS AN ALTERNATE STREET SECTION TO BE USED AT LOCATIONS DETERMINED BY EACH LOCAL JURISDICITON. NO ABOVE GROUND OBJECTS SHALL BE PLACED WITHIN THE 5 FOOT SIDEWALK.
5. UNDERGROUND DRY UTILITIES SHOULD BE PLACED IN A UTILITY CORRIDOR UNDER THE SIDEWALK.
6. INCREASE PAVEMENT WIDTH BY 11 FEET ON EACH SIDE OF ROADWAY FOR AN 8 LANE CROSS SECTION.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	ARTERIAL ALTERNATE URBAN AREA STREET SECTIONS WITH OFFSET SIDEWALK		
401	BITUMINOUS PAVEMENT			
403	OPEN GRADE			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 202 ALT	PAGE NO. 8.1ALT



MAJOR COLLECTOR

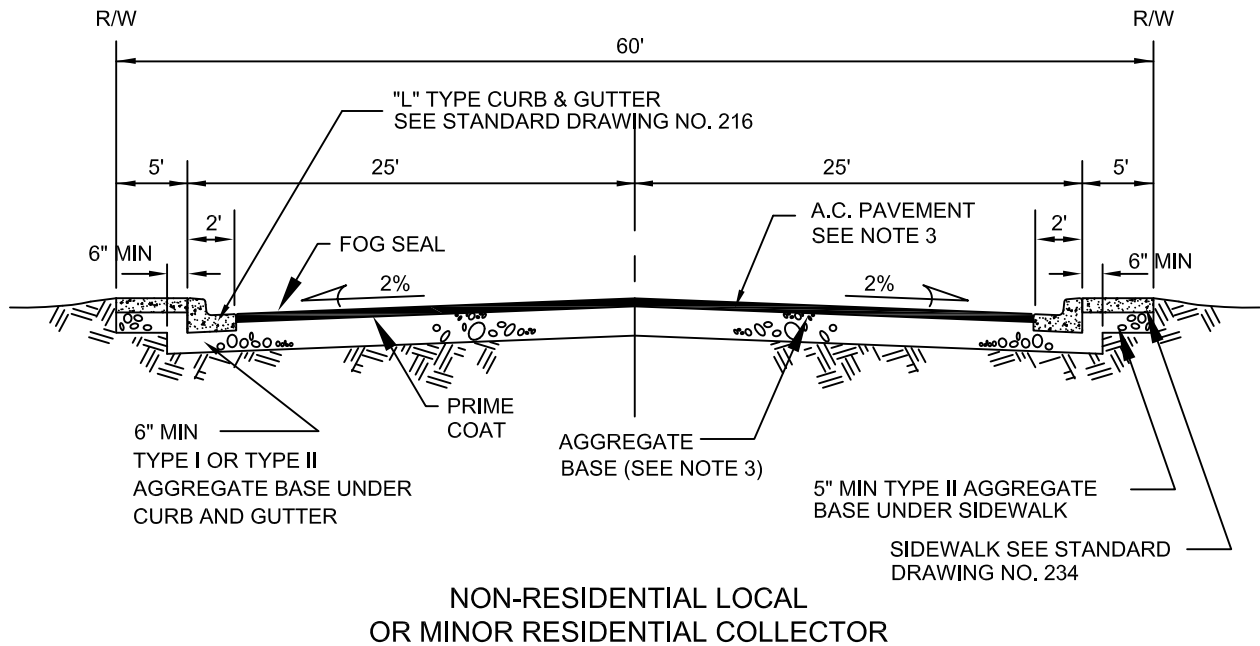
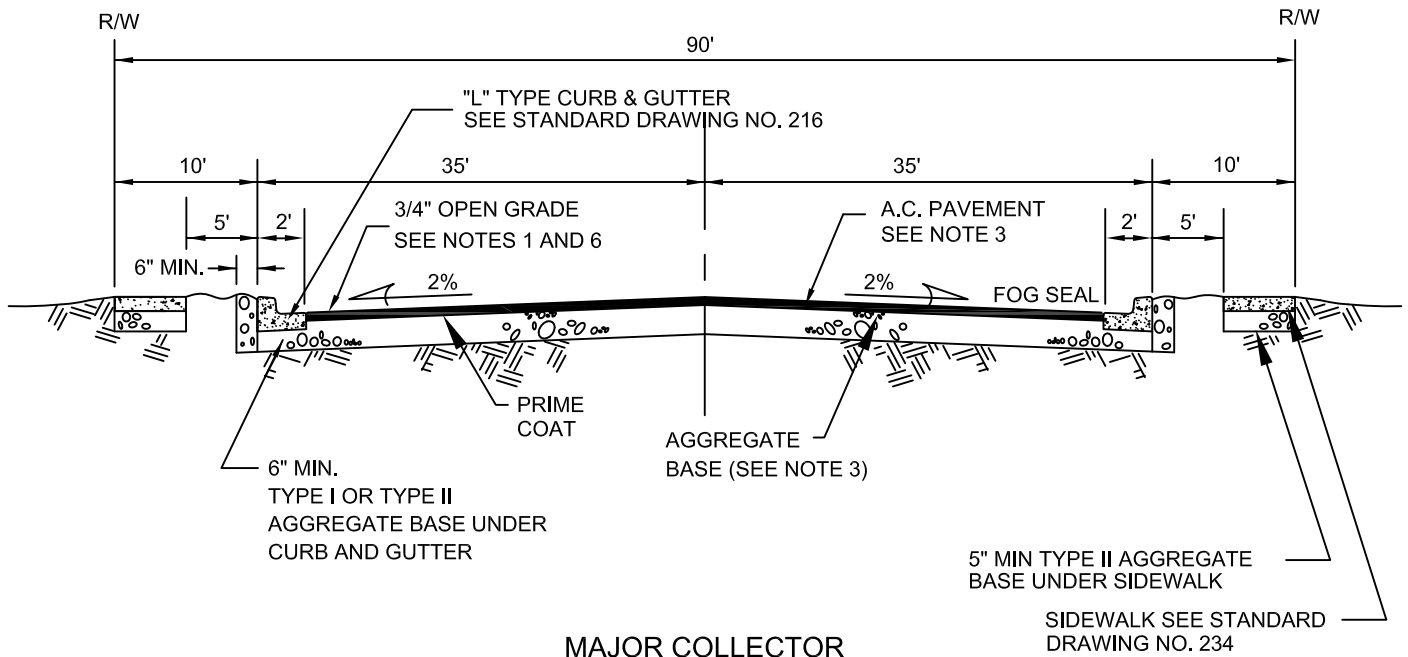


**NON-RESIDENTIAL LOCAL
OR MINOR RESIDENTIAL COLLECTOR**

NOTES:

1. A.C. PAVEMENT TO BE 1/2" MAXIMUM ABOVE LIP OF GUTTER AFTER COMPACTION. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. THE GRADE BREAK OCCURRING IN THE CROSS SECTION SHALL FALL BETWEEN DRIVING LANES.
3. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING NOS. 200 AND 200A.
4. PRIME COAT IS NOT REQUIRED IN HENDERSON, MESQUITE OR BOULDER CITY WHEN A.C. THICKNESS \geq 5 IN.
5. 4 INCH MINIMUM THICKNESS REQUIRED IN HENDERSON, MESQUITE AND BOULDER CITY.
6. 3/4" OPEN GRADE REQUIRED IN CLARK COUNTY. OPEN GRADE IN OTHER JURISDICTIONS AS REQUIRED BY THE ENGINEER.

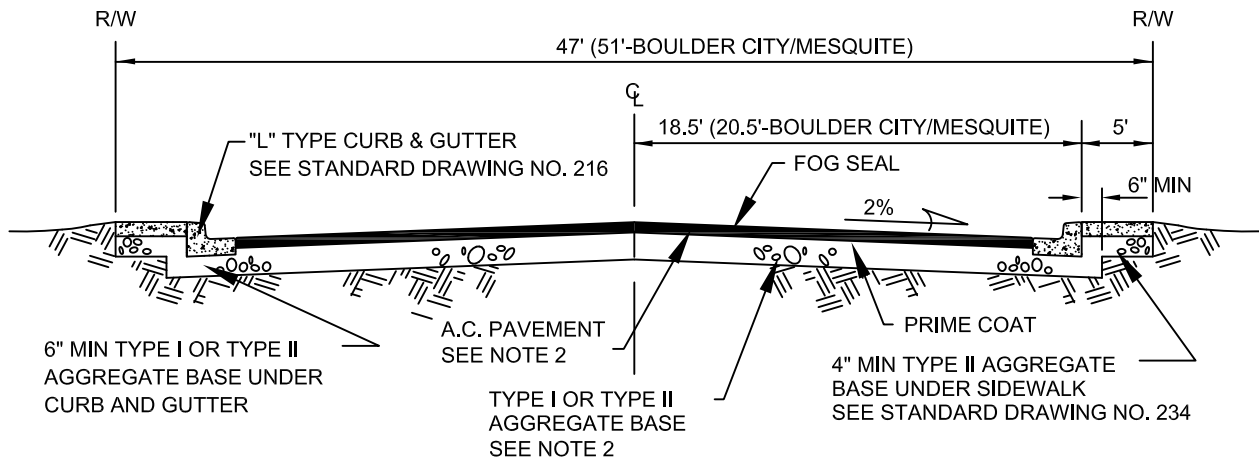
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	COLLECTOR URBAN AREA STREET SECTIONS WITH CURBSIDE SIDEWALK		
401	BITUMINOUS PAVEMENT			
406	PRIME COAT			
407	FOG SEAL			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 205	PAGE NO. 11



NOTES:

1. A.C. PAVEMENT TO BE 1/2" MAXIMUM ABOVE LIP OF GUTTER AFTER COMPACTION. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. THE GRADE BREAK OCCURRING IN THE CROSS SECTION SHALL FALL BETWEEN DRIVING LANES.
3. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING NOS. 200 AND 200A.
4. THIS STANDARD IS AN ALTERNATE STREET SECTION TO BE USED AT LOCATIONS DETERMINED BY EACH LOCAL JURISDICITON. NO ABOVE GROUND OBJECTS SHALL BE PLACED WITHIN THE 5 FOOT SIDEWALK.
5. UNDERGROUND DRY UTILITIES SHOULD BE PLACED IN A UTILITY CORRIDOR UNDER THE SIDEWALK.
6. 3/4" OPEN GRADE REQUIRED IN CLARK COUNTY. OPEN GRADE IN OTHER JURISDICTIONS AS REQUIRED BY THE ENGINEER.

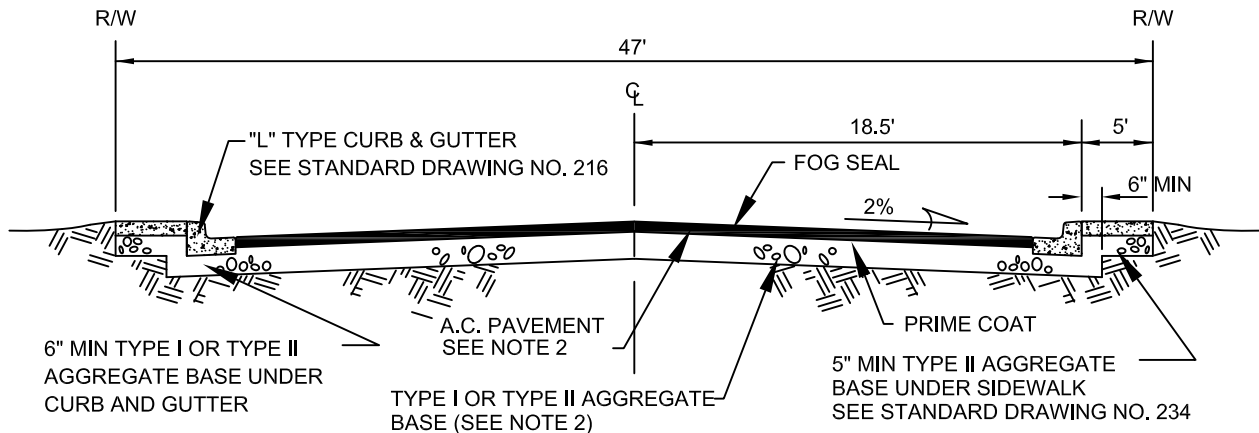
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	CLARK COUNTY, LAS VEGAS AND NORTH LAS VEGAS COLLECTOR ALTERNATE URBAN AREA STREET SECTIONS WITH OFFSET SIDEWALK		
401	BITUMINOUS PAVEMENT			
406	PRIME COAT			
407	FOG SEAL			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 205 ALT	PAGE NO. 11ALT.



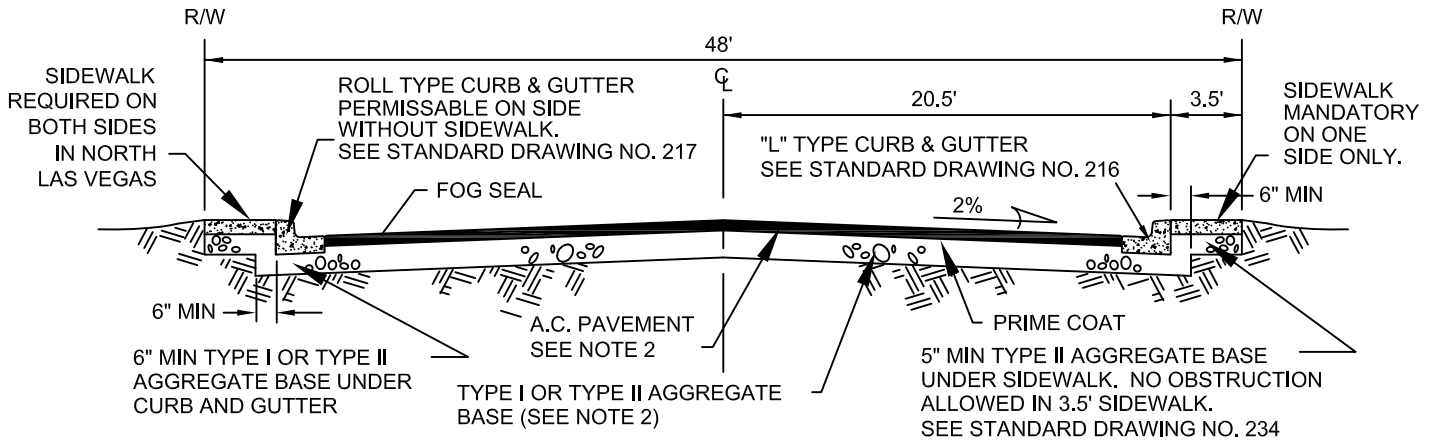
NOTES:

1. FINAL A.C. PAVEMENT SURFACE SHALL BE 1/4" MAXIMUM ABOVE LIP OF GUTTER. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING NO. 200A.

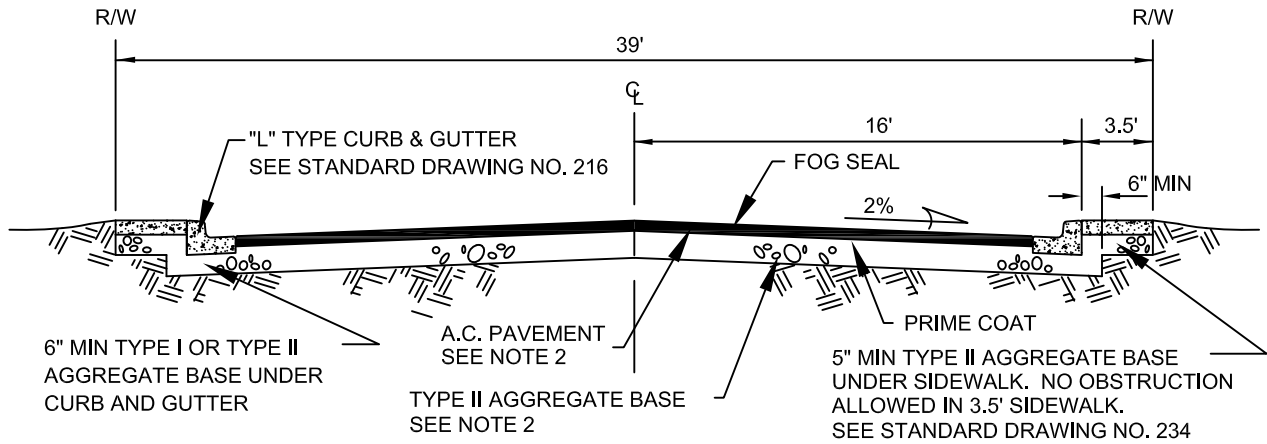
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	HENDERSON, BOULDER CITY, MESQUITE LOCAL RESIDENTIAL URBAN AREA STREET SECTION		
401	BITUMINOUS PAVEMENT			
406	PRIME COAT			
407	FOG SEAL			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 206	PAGE NO. 12



**RESIDENTIAL TWO-WAY LOCAL OR CUL-DE-SAC
(OPTION "A")**



**RESIDENTIAL TWO-WAY LOCAL, CUL-DE-SAC (OPTION "B")
(NOT ALLOWED IN CLV)**



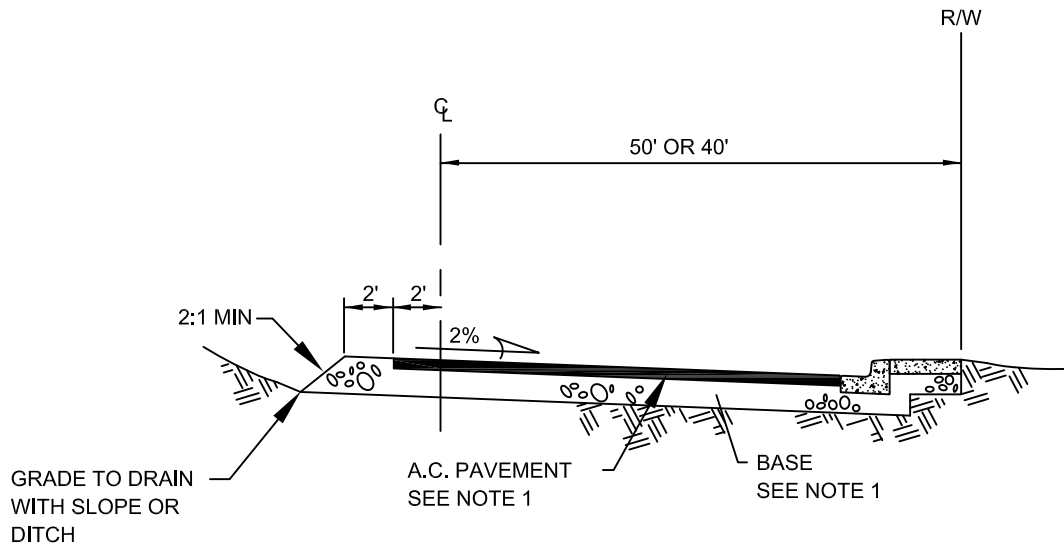
RESIDENTIAL ONE-WAY (NOT ALLOWED IN CLV)

NOTES:

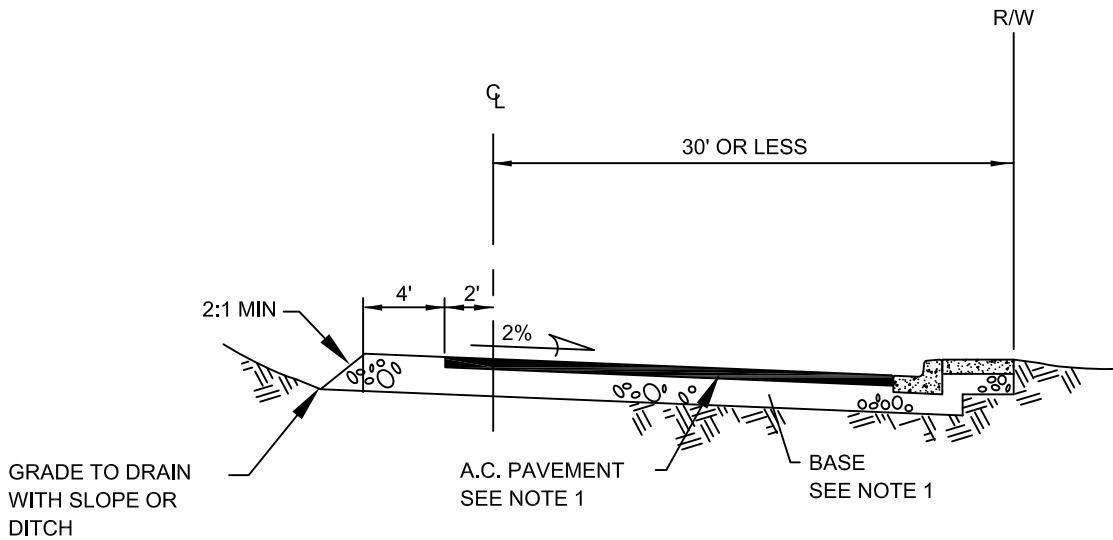
1. FINAL A.C. PAVEMENT SURFACE SHALL BE 1/4" MAXIMUM ABOVE LIP OF GUTTER. PAVEMENT SHALL BE FLUSH WITH LIP AT SIDEWALK RAMPS.
2. STRUCTURAL SECTION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 401 AND STANDARD DRAWING NO. 200A.
3. RESIDENTIAL ONE-WAY STREET SHALL NOT EXCEED ONE THOUSAND FEET OR TWENTY RESIDENTIAL LOTS IN LENGTH WHICHEVER IS LESS.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	CLARK COUNTY, LAS VEGAS AND NORTH LAS VEGAS LOCAL RESIDENTIAL URBAN AREA STREET SECTIONS		
401	BITUMINOUS PAVEMENT			
406	PRIME COAT			
407	FOG SEAL			
501	CONCRETE			
		DATE 11-10-04	DWG. NO. 207	PAGE NO. 13

Effective 07/01/10 - 12/30/10



ARTERIAL OR MAJOR COLLECTOR

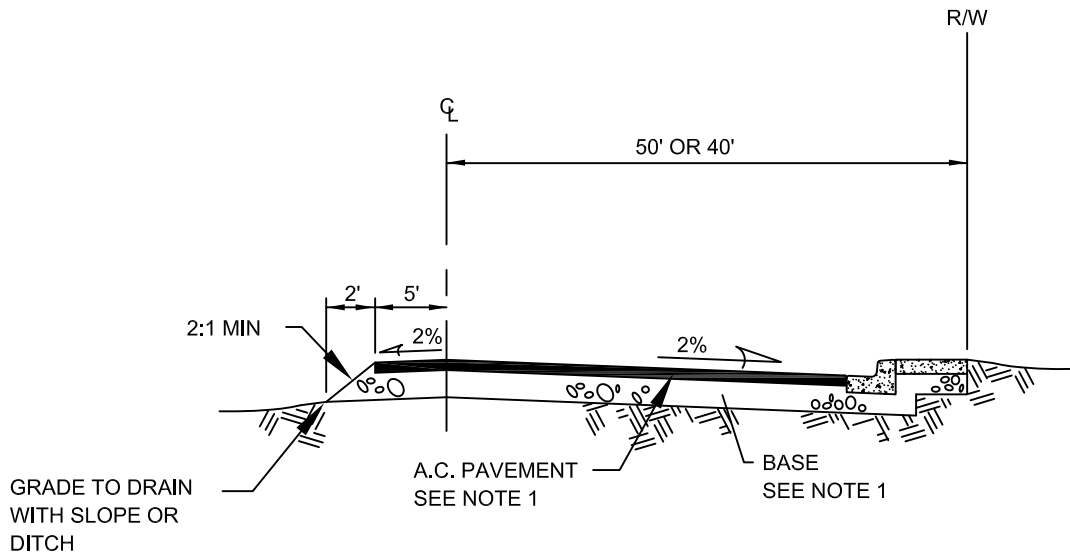


COMMERCIAL/INDUSTRIAL LOCAL
OR MINOR RESIDENTIAL COLLECTOR
OR LOCAL RESIDENTIAL

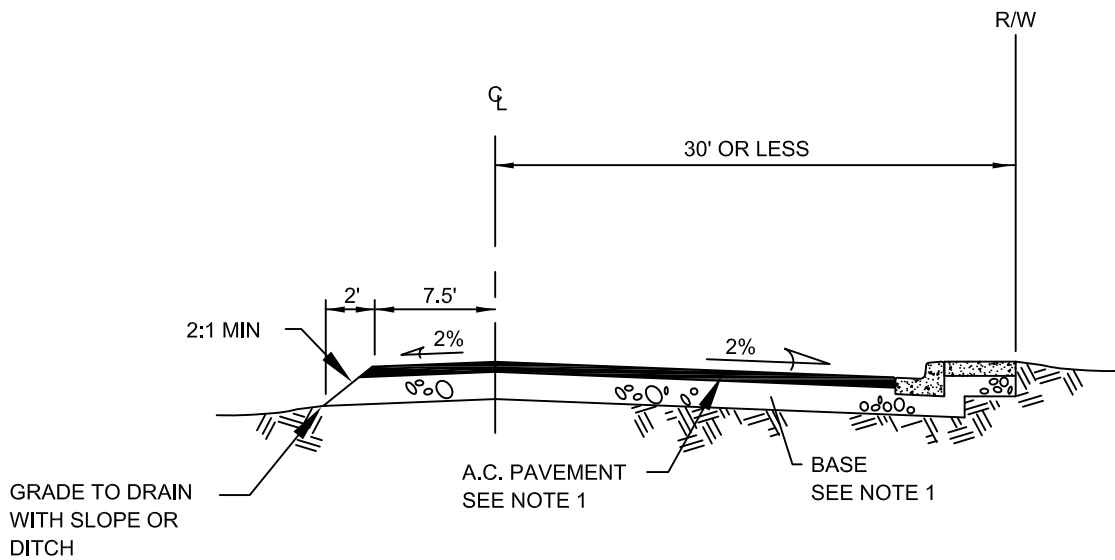
NOTES:

1. A.C. PAVEMENT AND BASE THICKNESS SHALL BE IN ACCORDANCE TO STANDARD DRAWINGS NUMBER 202 THROUGH 207, WHICHEVER IS APPLICABLE.
2. GREATER WIDTHS MAY BE REQUIRED IF TRAFFIC WARRANTS, AS DETERMINED BY THE ENGINEER.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	HALF STREET CONSTRUCTION SECTIONS	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE	DWG. NO. 208
			PAGE 14



ARTERIAL OR MAJOR COLLECTOR

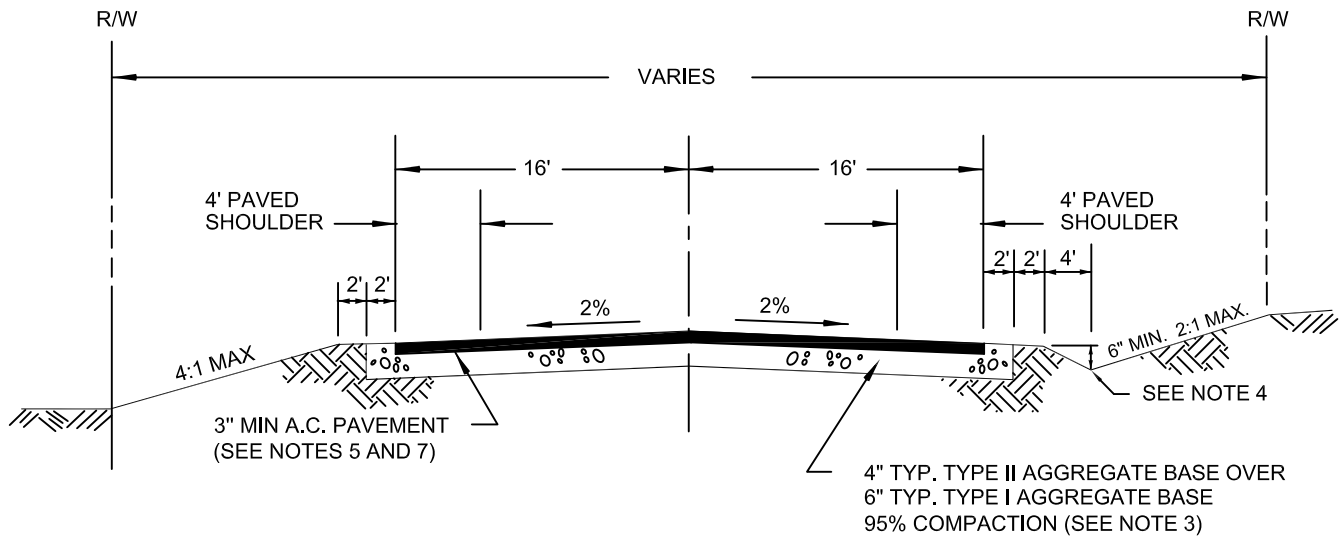


COMMERCIAL/INDUSTRIAL LOCAL
OR MINOR RESIDENTIAL COLLECTOR
OR LOCAL RESIDENTIAL

NOTES:

1. A.C. PAVEMENT AND BASE THICKNESS SHALL BE IN ACCORDANCE TO STANDARD DRAWINGS NUMBER 202 THROUGH 207, WHICHEVER IS APPLICABLE.
2. GREATER WIDTHS MAY BE REQUIRED IF TRAFFIC WARRANTS, AS DETERMINED BY THE ENGINEER.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
302	AGGREGATE BASE	CLARK COUNTY ONLY HALF STREET CONSTRUCTION SECTIONS					
401	BITUMINOUS PAVEMENT						
406	PRIME COAT						
407	FOG SEAL						
501	CONCRETE						
		DATE	8-12-99	DWG. NO.	208.1	PAGE	14.1

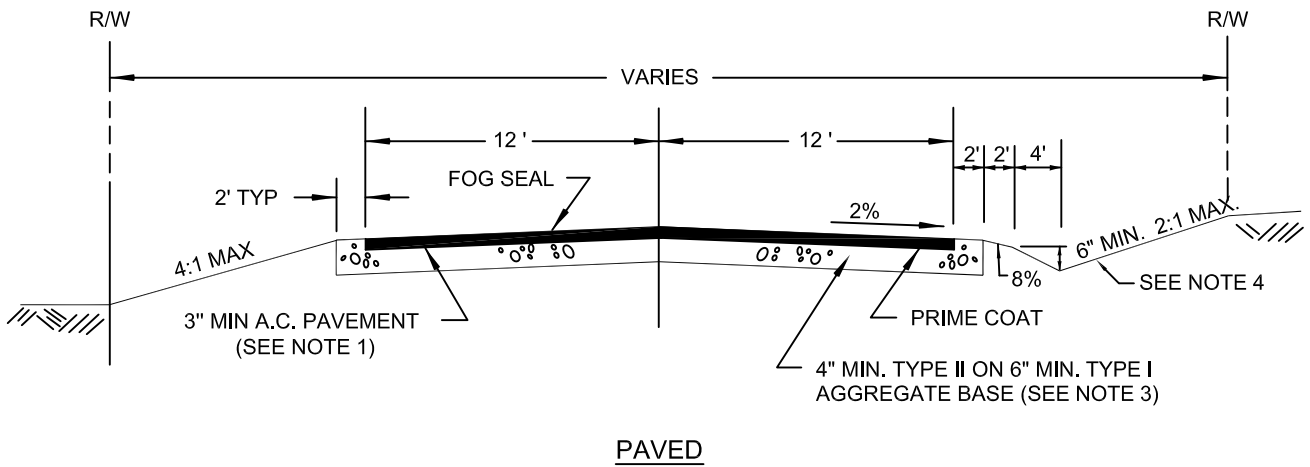
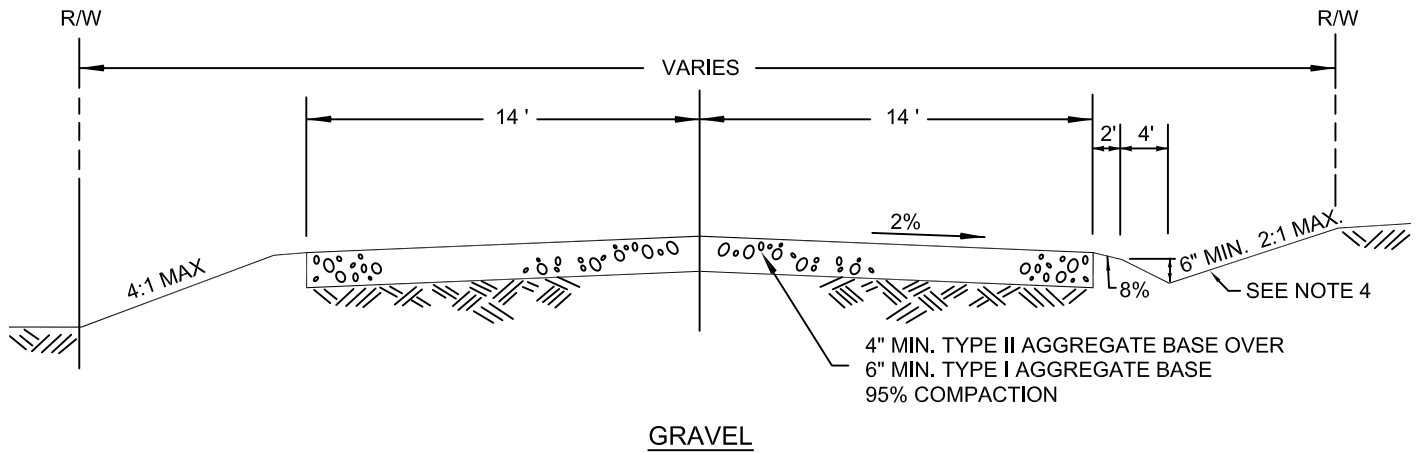


NOTES:

1. INTERSECTIONS SHALL HAVE 34 FOOT MINIMUM EDGE OF A.C. RETURN RADII.
2. COMPACTION OF AGGREGATE BASE AND SUBGRADE PREPARATION SHALL BE IN ACCORDANCE WITH THE UNIFORM STANDARD SPECIFICATIONS.
3. STRUCTURAL SECTION SHOWN IS BASED ON A SUBGRADE "R" VALUE OF 20. OTHER STRUCTURAL SECTIONS MAY BE APPROVED IF BASED ON ENGINEERING ANALYSIS BASED ON "R" OR "CBR" VALUES DETERMINED BY SOIL TESTING.
4. CULVERTS MAY BE REQUIRED AT DRIVEWAYS.
5. A.C. PAVEMENT SHALL BE IN ACCORDANCE WITH SECTION 401 OF THE UNIFORM STANDARD SPECIFICATIONS. ALTERNATE PAVING MATERIALS MAY BE USED AT THE DISCRETION OF THE ENTITY.
6. PAVEMENT MARKINGS MAY BE REQUIRED AND INCLUDE DOUBLE YELLOW CENTERLINE, RAISED PAVEMENT MARKERS OR YELLOW PAINT, AND 4' OFFSET WHITE PAINTED EDGELINES.
7. PAVEMENT WIDTH AND PAVEMENT THICKNESS MAY BE REDUCED TO 28 FEET (14 FEET EACH DIRECTION) AND 2 INCHES RESPECTIVELY BASED UPON A DETERMINATION BY THE LOCAL ENTITY THAT THE REDUCED WIDTH AND THICKNESS WILL PROVIDE SATISFACTORY LIFE AND A SAFE ROADWAY.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
302	AGGREGATE BASE	ACCESS ROADS (FOR USE IN HYDROGRAPHIC BASIN NO. 212) (PM-10 NON-ATTAINMENT AREAS)					
401	BITUMINOUS PAVEMENT						
		DATE	5-20-04	DWG. NO.	209	PAGE NO.	15



NOTES:

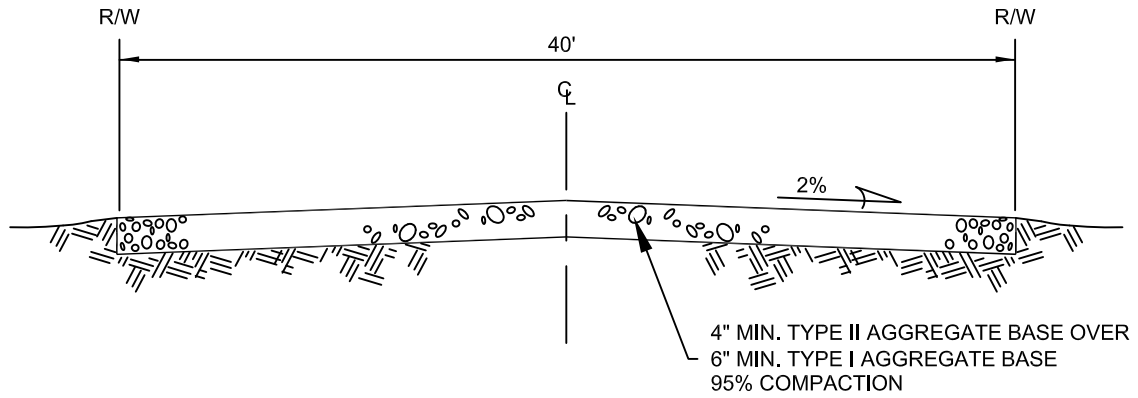
1. INTERSECTIONS SHALL HAVE 25 FOOT MINIMUM EDGE OF OIL RADII.
2. COMPACTION OF AGGREGATE BASE AND SUBGRADE PREPARATION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS".
3. STRUCTURAL SECTION SHOWN IS BASED ON A SUBGRADE "R" VALUE OF 20. OTHER STRUCTURAL SECTIONS MAY BE APPROVED IF BASED ON ENGINEERING ANALYSIS BASED ON "R" OR "CBR" VALUES DETERMINED BY SOIL TESTING. IN NO CASE SHALL THE A.C. THICKNESS BE LESS THAN THAT SHOWN, NOR SHALL THE BASE BE LESS THAN 4".
4. CULVERTS MAY BE REQUIRED AT DRIVEWAYS.

SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

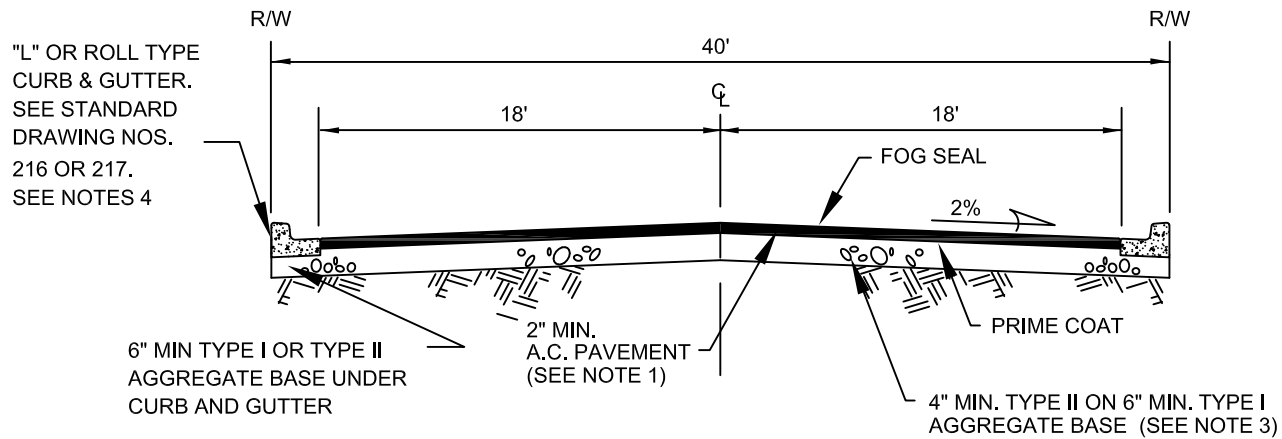
302	AGGREGATE BASE
401	BITUMINOUS PAVEMENT
406	PRIME COAT
407	FOG SEAL

**ACCESS ROADS
(FOR USE IN PM-10 COMPLIANT AREAS)**



GRAVEL

(THIS SECTION NOT FOR USE IN PM-10 NON-ATTAINMENT AREAS)

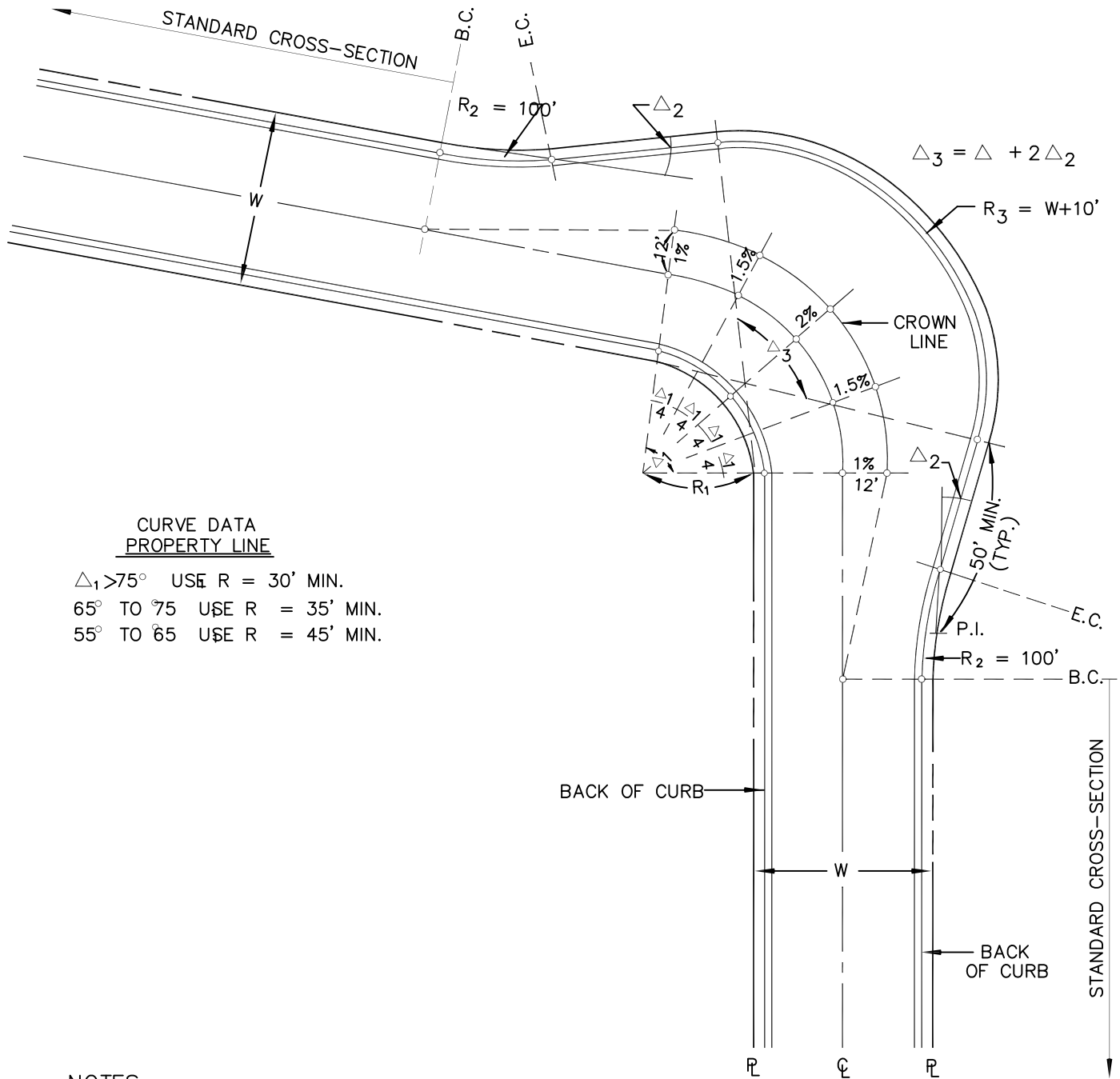


PAVED

NOTES:

1. INTERSECTIONS SHALL HAVE 25 FOOT MINIMUM EDGE OF OIL RADII OR 20 FOOT MINIMUM BACK OF CURB RADII.
2. COMPACTION OF AGGREGATE BASE AND SUBGRADE PREPARATION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATION".
3. STRUCTURAL SECTION SHOWN IS BASED ON A SUBGRADE "R" VALUE OF 20. OTHER STRUCTURAL SECTIONS MAY BE APPROVED IF BASED ON ENGINEERING ANALYSIS BASED ON "R" OR "CBR" VALUES DETERMINED BY SOIL TESTING. IN NO CASE SHALL THE A.C. THICKNESS BE LESS THAN THAT SHOWN, NOR SHALL THE BASE BE LESS THAN 4" EXCEPT THAT THE BASE SHALL NOT BE LESS THAN 10" IN NORTH LAS VEGAS.
4. ALLOW IN CITY OF NORTH LAS VEGAS ONLY WITH EXPRESS WRITTEN PERMISSION FROM THE CITY ENGINEER.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
302	AGGREGATE BASE	PRIVATE STREET SECTIONS					
401	BITUMINOUS PAVEMENT						
406	PRIME COAT						
407	FOG SEAL						
501	CONCRETE						
		DATE	12-14-00	DWG. NO.	210	PAGE	16



**CURVE DATA
PROPERTY LINE**

- $\Delta_1 > 75^\circ$ USE $R = 30'$ MIN.
- 65° TO 75° USE $R = 35'$ MIN.
- 55° TO 65° USE $R = 45'$ MIN.

NOTES

1. USE NORMAL SECTION FROM INNER CURB TO CENTER LINE.
2. FROM CROWN LINE TO OUTER CURB, THE STANDARD SLOPE IS 2%.
3. SUPERELEVATION PERCENTAGES SHOWN ARE A STRAIGHT GRADE FROM CENTER LINE TO CROWN LINE.
4. ELEVATIONS ARE REQUIRED WHERE CIRCLES () ARE SHOWN.
5. KNUCKLES ARE NOT ALLOWED ON MAJOR COLLECTOR OR ARTERIAL STREETS.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

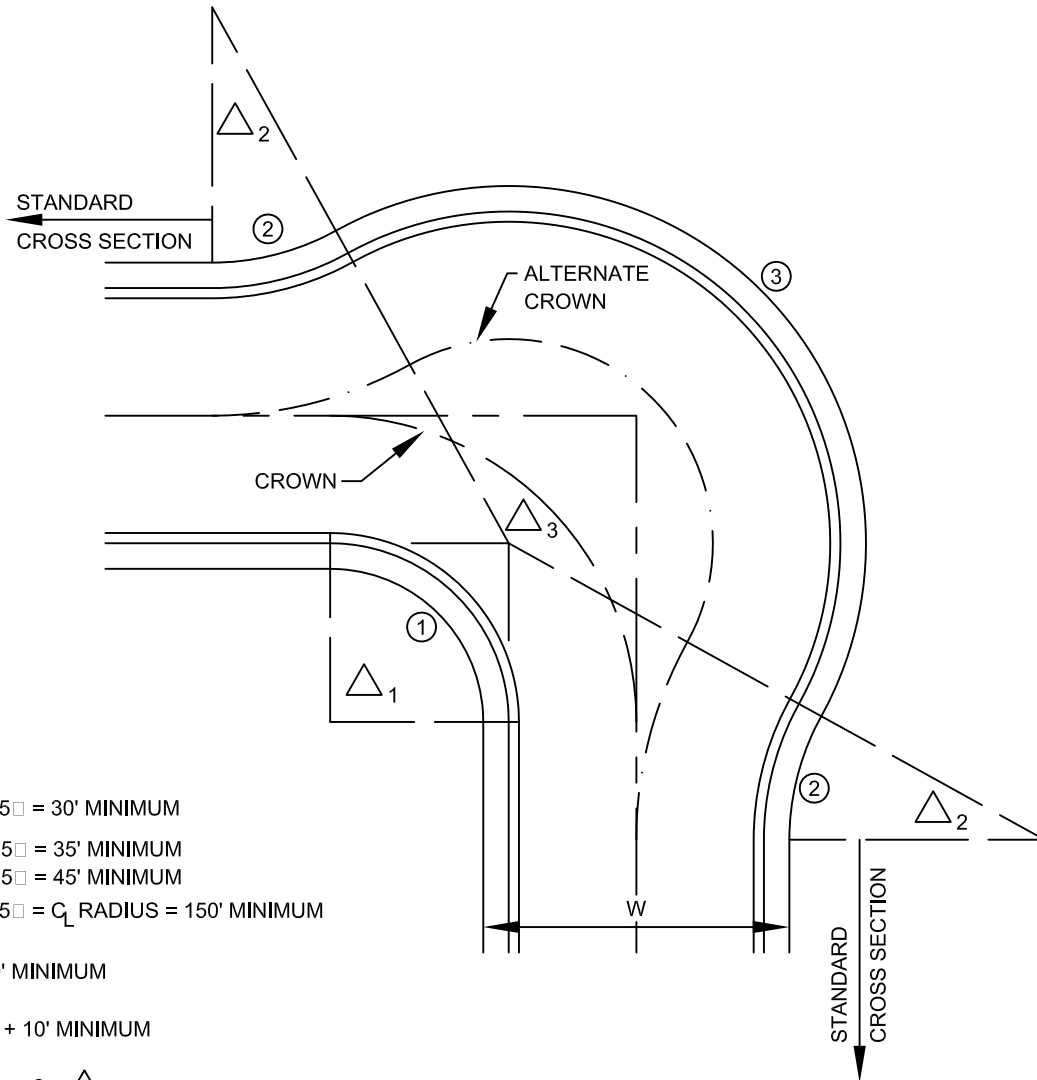
KNUCKLE

DATE

DWG. NO.

211

PAGE NO. 17



- ① BCR
 - $\Delta_1 > 75' = 30'$ MINIMUM
 - 65' TO 75' = 35' MINIMUM
 - 45' TO 65' = 45' MINIMUM
 - $\Delta_1 < 45' = C_L$ RADIUS = 150' MINIMUM
 - ② BCR = 50' MINIMUM
 - ③ BCR = W + 10' MINIMUM
- $$\Delta_3 = \Delta_1 + 2 \Delta_2$$

NOTES:

1. USE 2% SLOPE FROM INNER CURB TO CROWN LINE.
2. FROM CROWN LINE TO OUTER CURB, THE STANDARD SLOPE IS 0.90% (MIN).
3. ELEVATIONS REQUIRED ALONG CURBS (3) AND CROWN EVERY 1/4 (MIN).
4. KNUCKLES ARE ALLOWED ON RESIDENTIAL STREETS ONLY.
5. MINIMUM SLOPE ALONG THE BACK OF CURB OF CURVES (2) AND (3) SHALL BE 0.60% (MIN).
6. SPECIAL KNUCKLE DESIGNS INCLUDING LANDSCAPED MEDIAN ISLAND MAY BE PERMITTED, IF APPROVED BY THE COUNTY ENGINEER.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

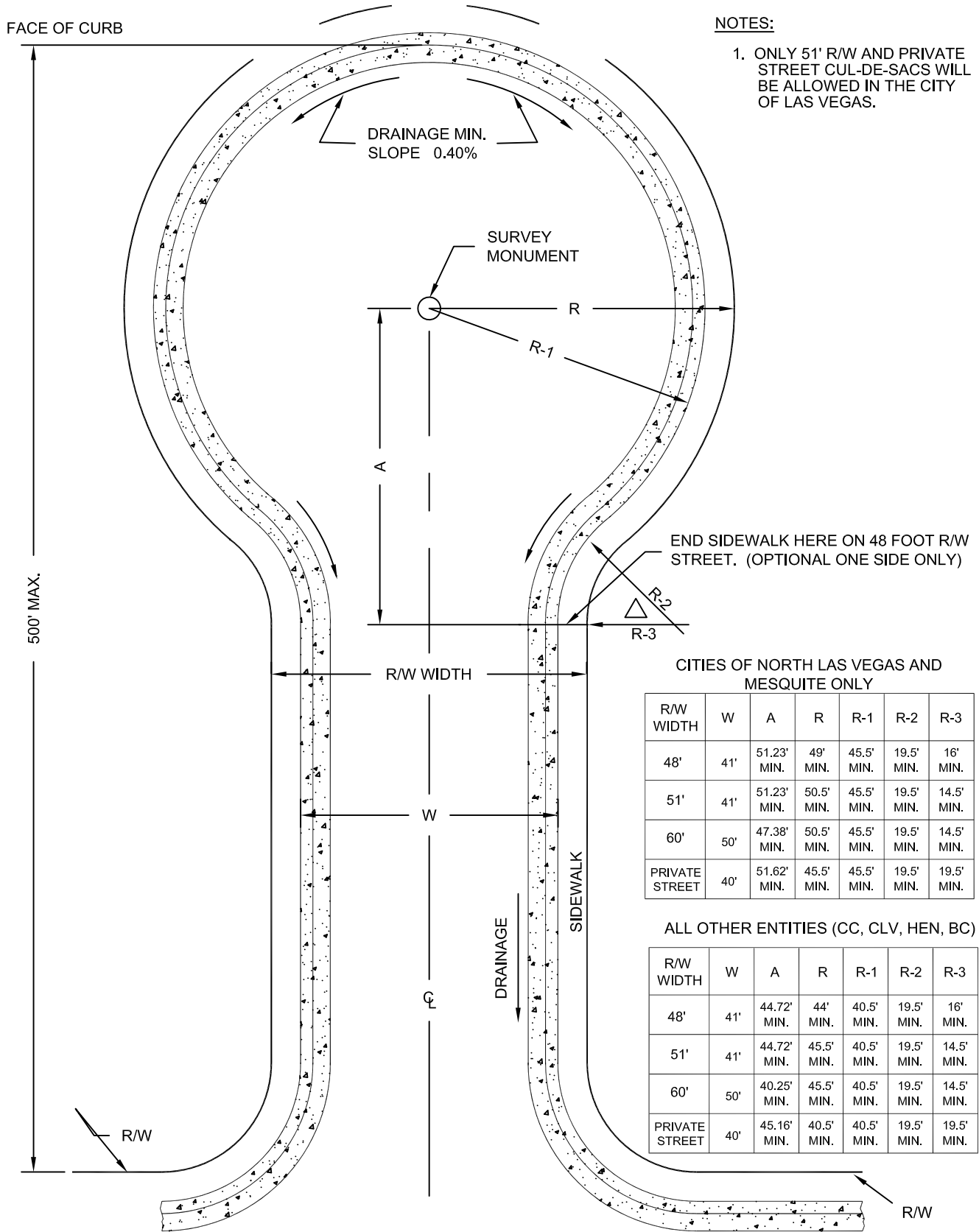
CLARK COUNTY ONLY

KNUCKLE - TYPE I

FACE OF CURB

NOTES:

1. ONLY 51' R/W AND PRIVATE STREET CUL-DE-SACS WILL BE ALLOWED IN THE CITY OF LAS VEGAS.



CITIES OF NORTH LAS VEGAS AND MESQUITE ONLY

R/W WIDTH	W	A	R	R-1	R-2	R-3
48'	41'	51.23' MIN.	49' MIN.	45.5' MIN.	19.5' MIN.	16' MIN.
51'	41'	51.23' MIN.	50.5' MIN.	45.5' MIN.	19.5' MIN.	14.5' MIN.
60'	50'	47.38' MIN.	50.5' MIN.	45.5' MIN.	19.5' MIN.	14.5' MIN.
PRIVATE STREET	40'	51.62' MIN.	45.5' MIN.	45.5' MIN.	19.5' MIN.	19.5' MIN.

ALL OTHER ENTITIES (CC, CLV, HEN, BC)

R/W WIDTH	W	A	R	R-1	R-2	R-3
48'	41'	44.72' MIN.	44' MIN.	40.5' MIN.	19.5' MIN.	16' MIN.
51'	41'	44.72' MIN.	45.5' MIN.	40.5' MIN.	19.5' MIN.	14.5' MIN.
60'	50'	40.25' MIN.	45.5' MIN.	40.5' MIN.	19.5' MIN.	14.5' MIN.
PRIVATE STREET	40'	45.16' MIN.	40.5' MIN.	40.5' MIN.	19.5' MIN.	19.5' MIN.

SPECIFICATION REFERENCE

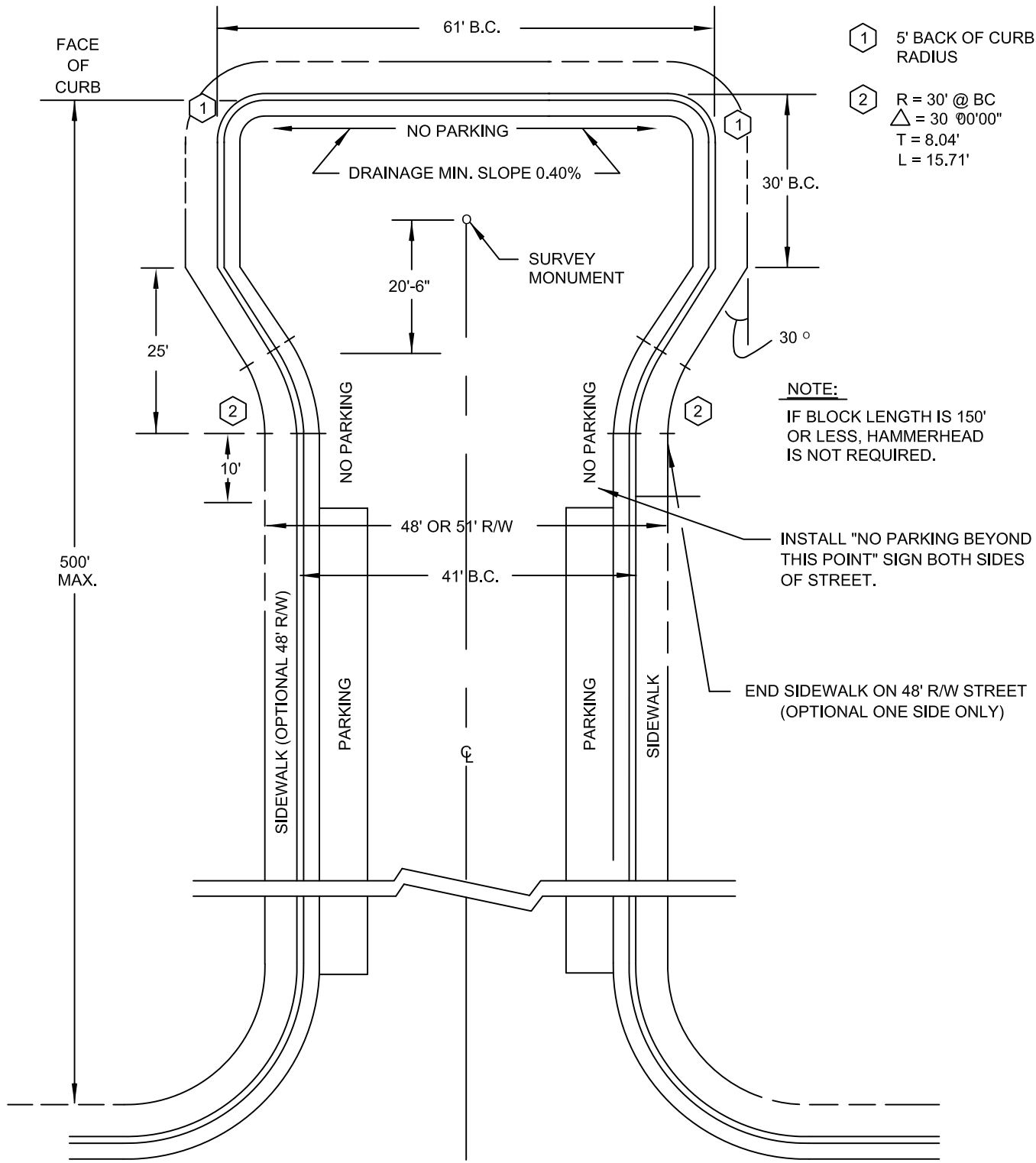
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

CUL-DE-SAC

DATE 6-8-00

DWG. NO. 212

PAGE 18

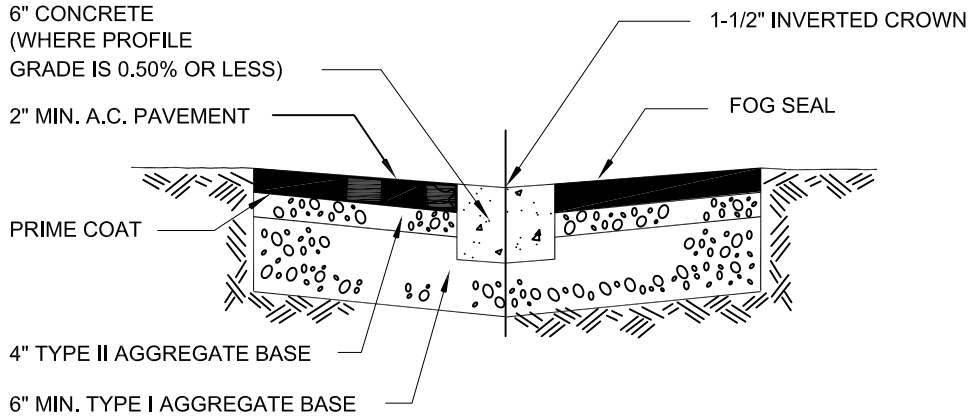


- 1 5' BACK OF CURB RADIUS
- 2 $R = 30' @ BC$
 $\Delta = 30^\circ 00' 00''$
 $T = 8.04'$
 $L = 15.71'$

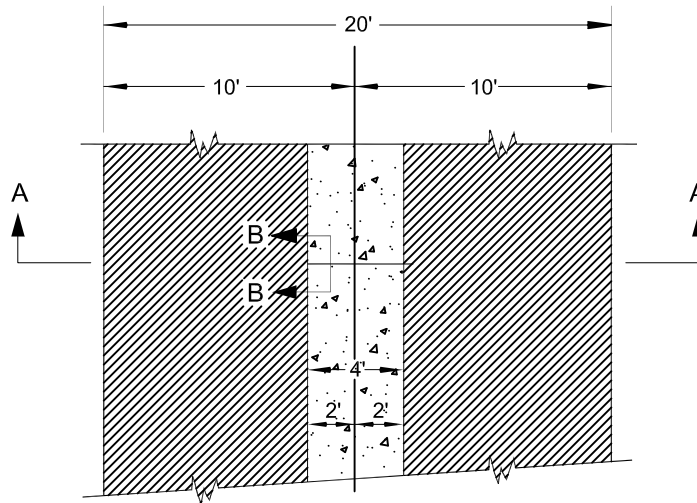
NOTE:
USE OF THE HAMMERHEAD WILL BE ALLOWED IN SINGLE FAMILY RESIDENTIAL DWELLING AREAS ONLY.

Effective 07/01/10 - 12/30/10

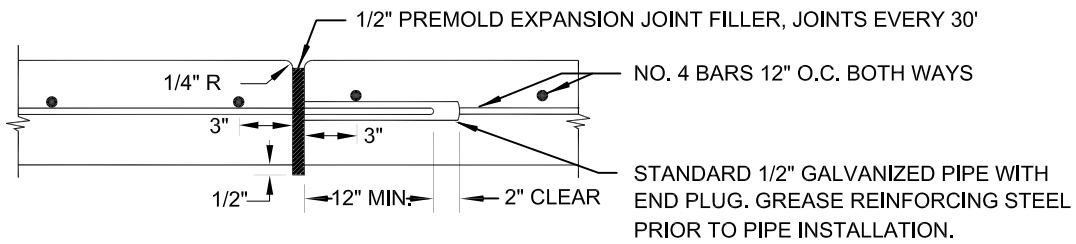
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
		CLARK COUNTY ONLY	
		HAMMERHEAD	
DATE	11-10-04	DWG. NO.	213
PAGE NO.	19		



SECTION A-A



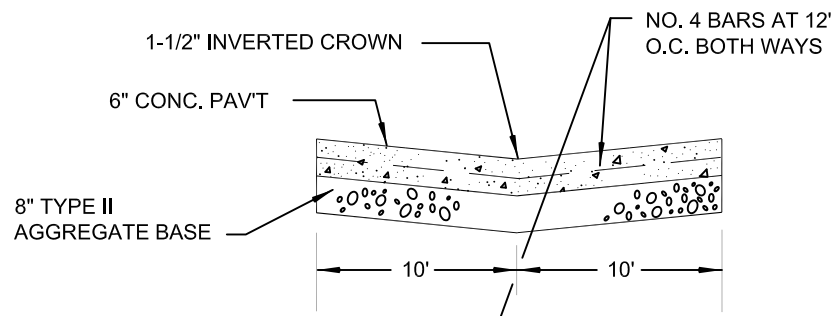
PLAN



SECTION B-B

Effective 07/01/10 - 12/30/10

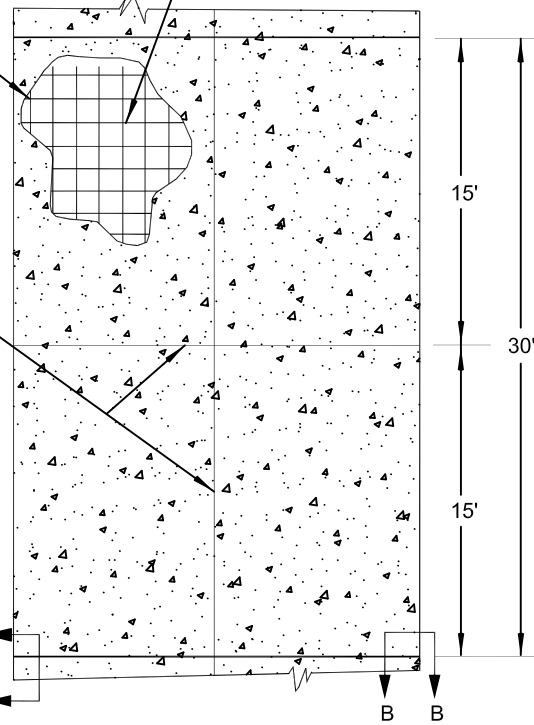
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	EXCEPT CLARK COUNTY ALLEY		
501	CONCRETE			
505	REINFORCING STEEL			
707	JOINT MATERIAL			
		DATE 12-14-00	DWG. NO. 214	PAGE NO. 20



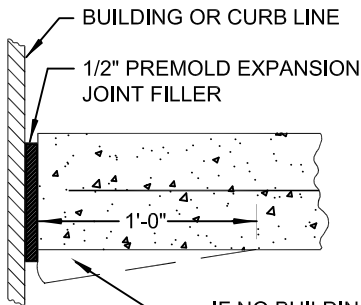
SECTION

NO. 4 BARS TO DISCONTINUE WITHIN 2" OF JOINT MATERIAL

WEAKENED PLANE JOINTS 1/4" MAX. WIDTH BY 2" DEPTH SAWCUT

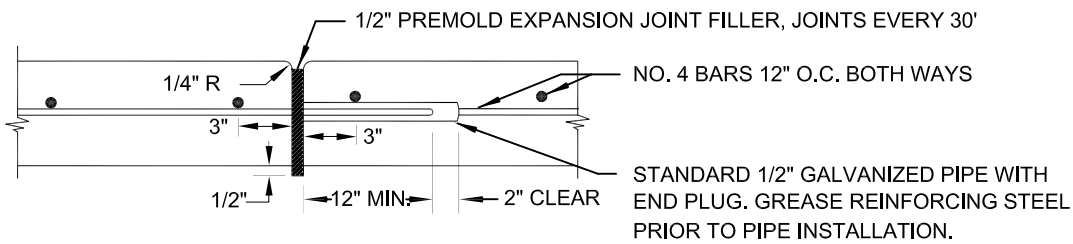


PLAN



IF NO BUILDING OR CURB EXISTS THICKEN EDGE TO 8" TOTAL DEPTH

SECTION B-B



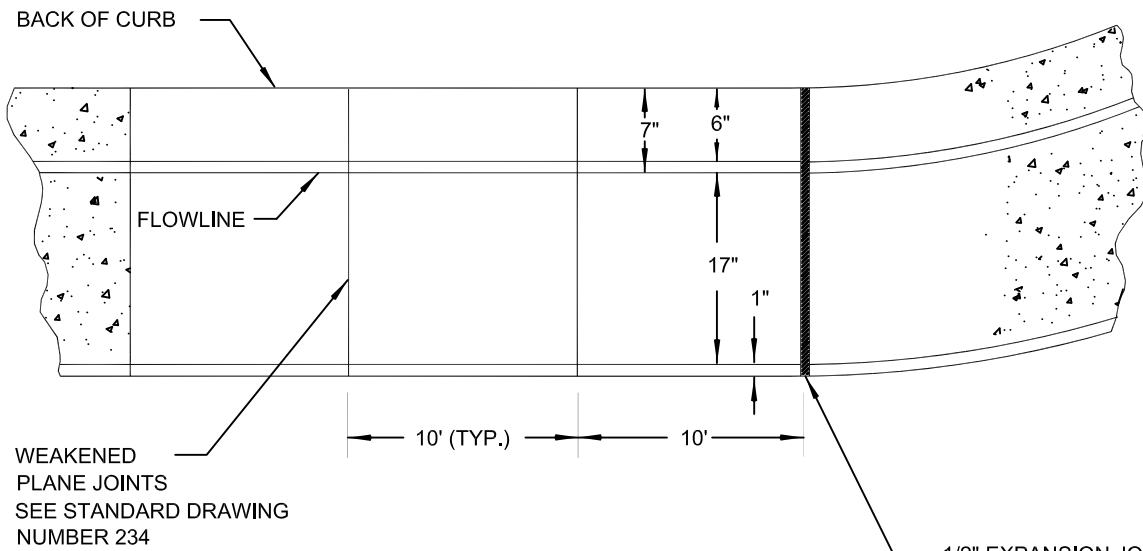
SECTION A-A

SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

302	AGGREGATE BASE
501	CONCRETE
505	REINFORCING STEEL
707	JOINT MATERIAL

EXCEPT CLARK COUNTY
ALLEY, CONCRETE

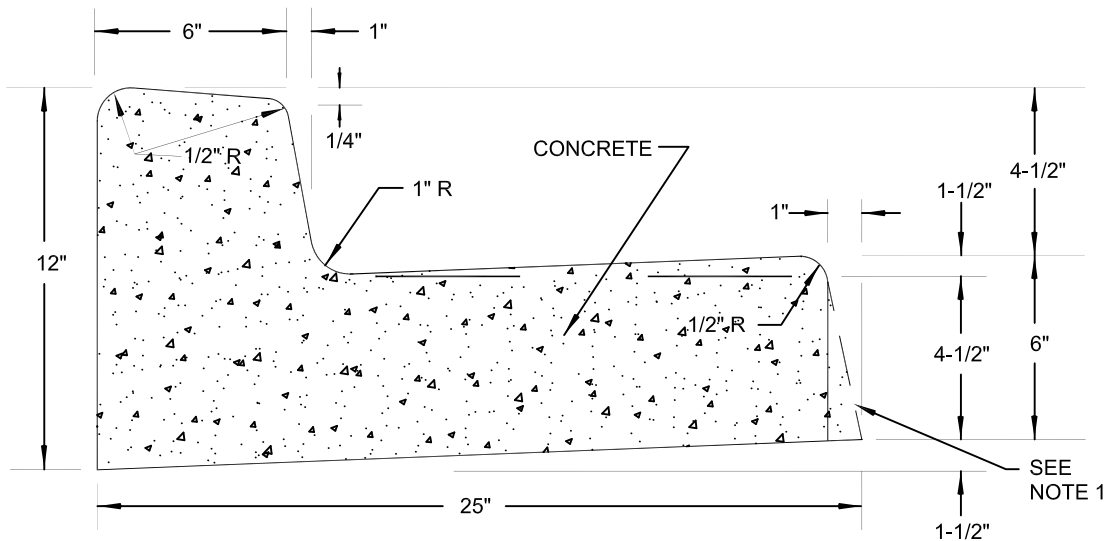


PLAN

NOTES:

1. 1" BATTER ON GUTTER FACE OPTIONAL.
2. WHERE LONGITUDINAL SLOPE IS LESS THAN 0.4%, THE FLOW LINE SHALL BE WATER TESTED.

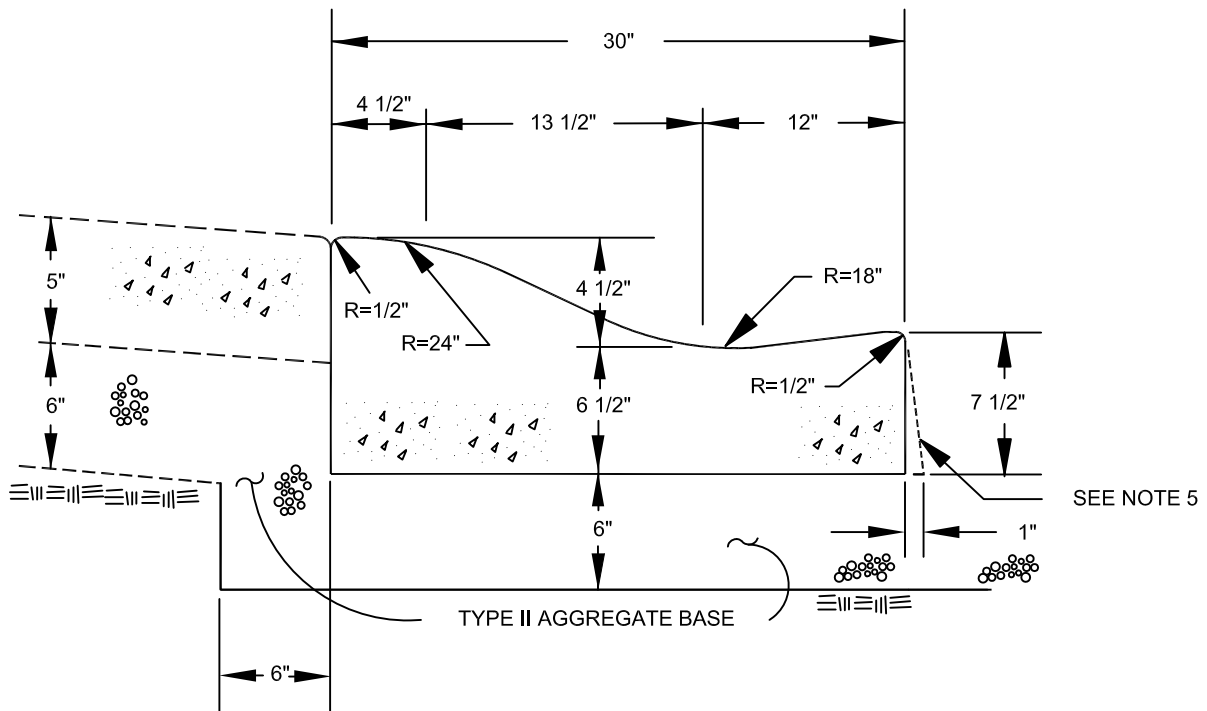
1/2" EXPANSION JOINT AT ALL COLD JOINTS, AT BEGINNING AND END OF RETURN AND AT 300' MAX. INTERVALS FOR EXTRUDED CURB AND 30' MAX. INTERVALS FOR FORMED CURB. FOR JOINT DETAIL SEE STANDARD DRAWING NUMBER 234



TYPICAL SECTION

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	"L" TYPE CURB AND GUTTER		
502	CONCRETE STRUCTURES			
707	JOINT MATERIAL			
		DATE 12-14-00	DWG. NO. 216	PAGE NO. 22



NOTES:

1. USE OF ROLL CURB MAY BE RESTRICTED BY SURFACE DRAINAGE CONSIDERATIONS.
2. SIDEWALK CONSTRUCTED CONTIGUOUS TO ROLL CURB SHALL BE 5 INCHES THICK (MIN).
3. WHERE LONGITUDINAL SLOPE IS LESS THAN 0.4% THE FLOW LINE SHALL BE WATER TESTED.
4. CONSTRUCT 1/2" EXPANSION JOINT AT ALL COLD JOINTS, AT BEGINNING AND END OF CURB RETURNS, AND AT 300 FT. MAX. INTERVALS FOR EXTRUDED CURB AND 30 FT. MAX. INTERVALS FOR FORMED CURB. WEAKENED PLANE JOINTS SHALL BE FORMED AT THE REMAINING 15 FT. INTERVALS. SEE STD. DWG. NO. 234 FOR JOINT DETAILS.
5. ONE INCH BATTER AT CURB FACE IS OPTIONAL.
6. CITY OF LAS VEGAS COUNCIL APPROVAL REQUIRED FOR USE OF 30" ROLL CURB IN THE CITY OF LAS VEGAS.
7. IN NORTH LAS VEGAS, ROLL CURBS ARE PROHIBITED IN AREAS WHERE FLOW LINE GRADIENT IS LESS THAN 0.8% UNLESS OTHERWISE APPROVED BY CITY ENGINEER.
8. ALL UTILITY BOXES AND COVERS ADJACENT TO ROLL CURB SHALL BE HS-20 RATED "TRAFFIC BEARING" TYPE

SPECIFICATION REFERENCE

320	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES
707	JOINT MATERIAL

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

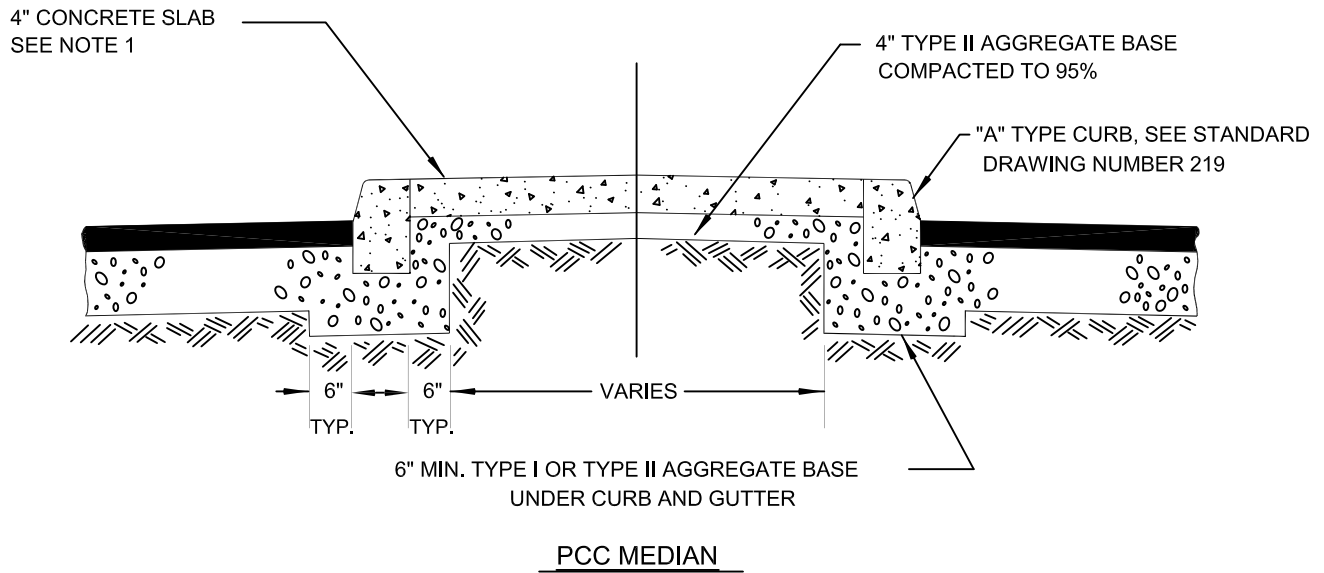
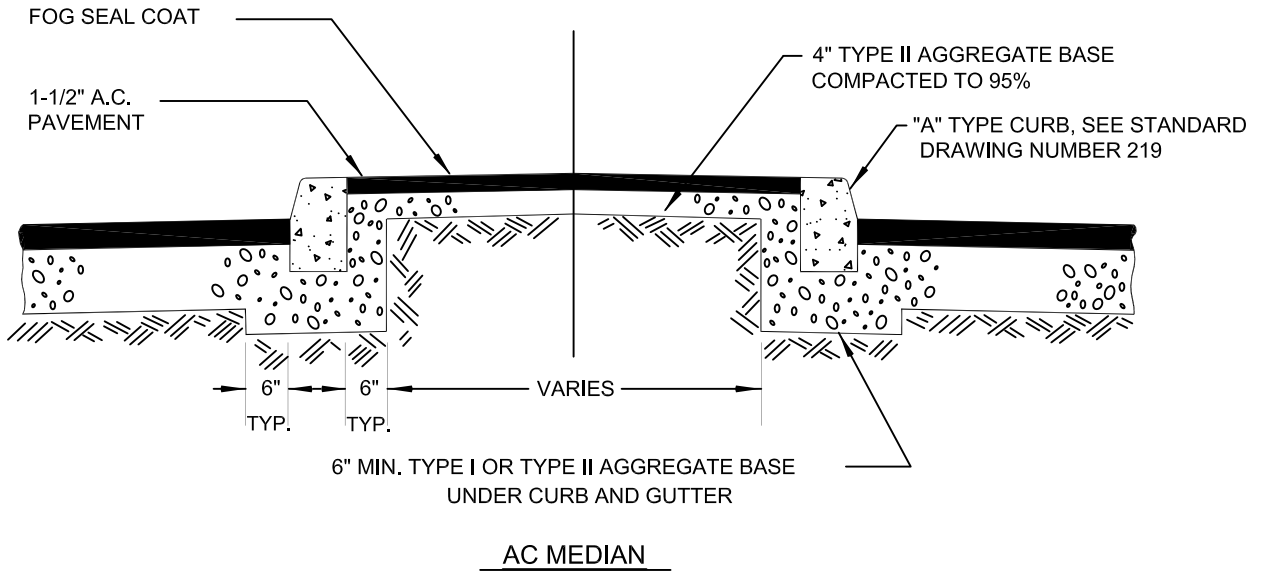
CLARK COUNTY, LAS VEGAS, NORTH LAS VEGAS,
AND HENDERSON ONLY

30" ROLL CURB
RESIDENTIAL AREA

DATE 11-10-04

DWG. NO. 217A

PAGE NO. 23A



NOTES:

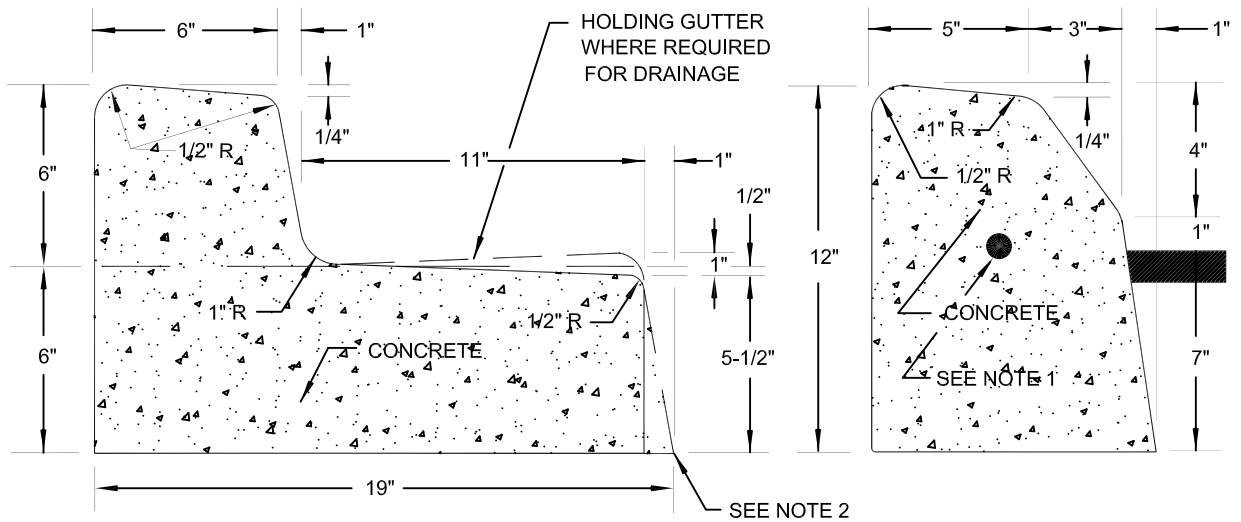
1. CONSTRUCT WEAKENED PLANE JOINT IN CURB AND SLAB AT SAME LOCATION EVERY 10'; CONSTRUCT EXPANSION JOINTS EVERY 300' FOR CONCRETE SLAB TO MATCH CURB JOINTS. FOR JOINT DETAILS SEE STANDARD DRAWING NUMBER 234.
2. "L" -TYPE CURB AND GUTTER PER STANDARD DRAWING NUMBER 221 IS REQUIRED IN THE CITY OF HENDERSON AND MAY BE REQUIRED FOR DRAINAGE CONSIDERATIONS.
3. WHEN CURB MACHINE IS USED TO PLACE CURB, A 2" MINIMUM LEVELING COURSE OF TYPE II AGGREGATE BASE IS REQUIRED.

SPECIFICATION REFERENCE

302	AGGREGATE BASE
501	CONCRETE

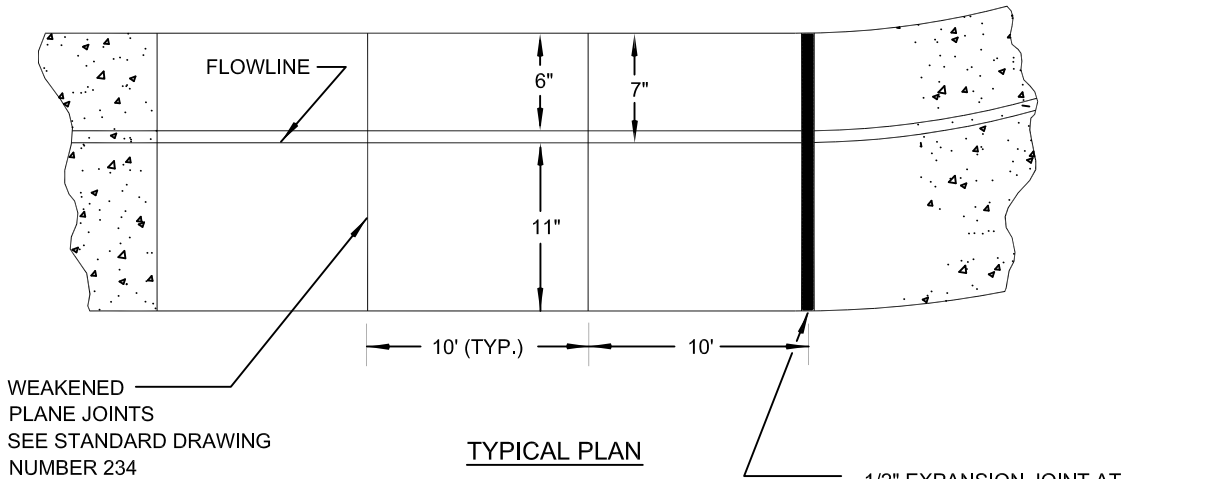
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

MEDIAN ISLAND
TYPICAL SECTION



"L" CURB SECTION

"A" CURB SECTION



TYPICAL PLAN

WEAKENED PLANE JOINTS
SEE STANDARD DRAWING
NUMBER 234

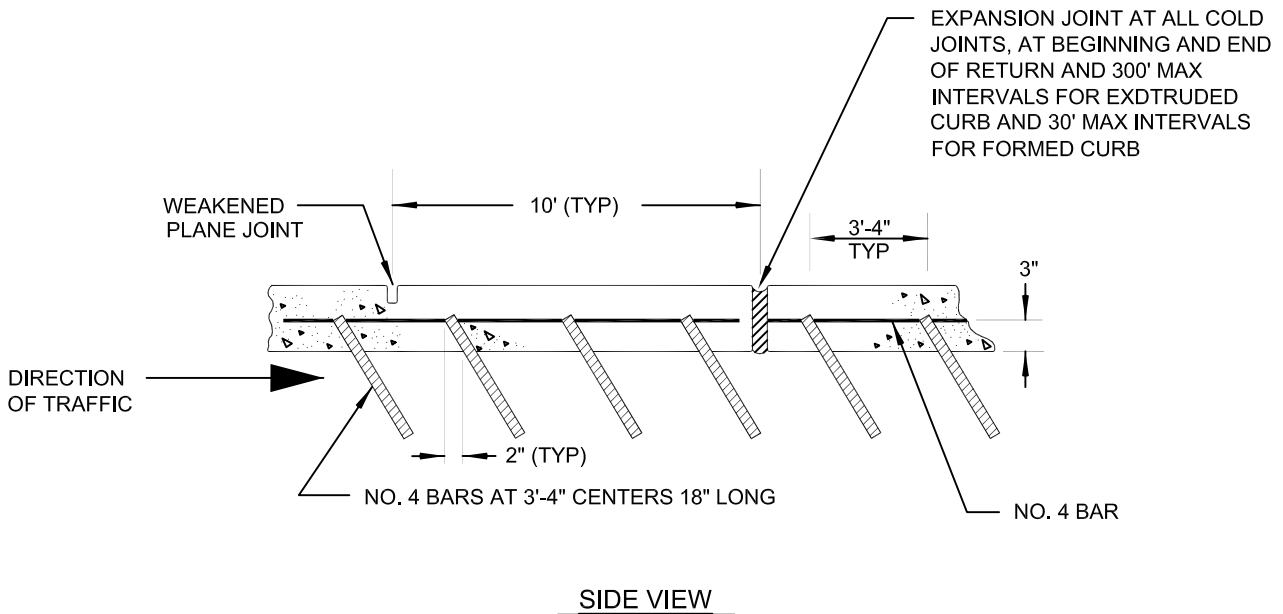
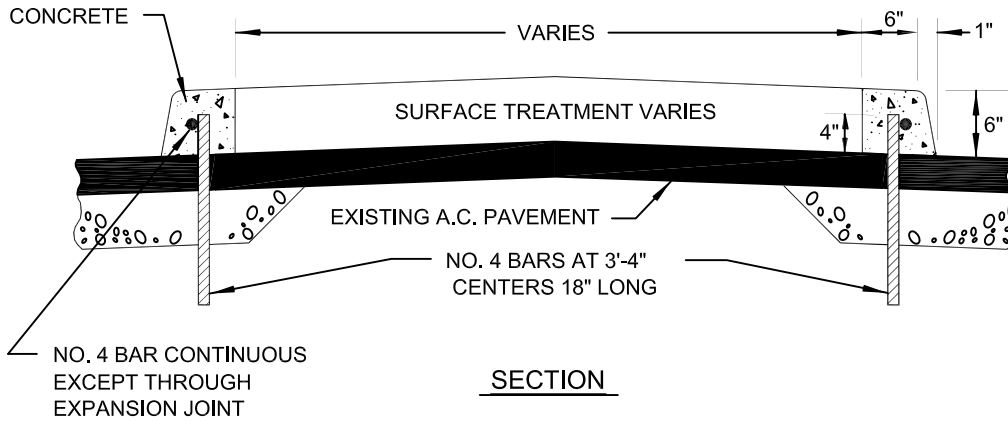
1/2" EXPANSION JOINT AT ALL COLD JOINTS, AT BEGINNING AND END OF RETURN AND AT 300' MAX. INTERVALS FOR EXTRUDED CURB AND 30' MAX. INTERVALS FOR FORMED CURB. FOR JOINT DETAIL SEE STANDARD DRAWING NUMBER 234

NOTES:

1. CONTINUOUS NO. 4 BAR REQUIRED IN NOSE OF MEDIAN ONLY.
2. 1" BATTER ON GUTTER FACE OPTIONAL.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE COURSE	"A" AND "L" TYPE ISLAND CURB	
501	CONCRETE		
707	JOINT MATERIAL		
		DATE 12-14-00	DWG. NO. 219
		PAGE NO. 25	

1/2" RADIUS ROUNDED
EDGE ON ALL EXPOSED
CORNERS



NOTES:

1. FOR EXPANSION JOINT AND WEAKENED PLANE JOINT DETAIL, SEE STANDARD DRAWING NO. 234.
2. WEAKENED PLANE JOINTS EVERY 10' STAGGER WITH NO. 4 BARS.
3. ALL REINFORCING STEEL SHALL HAVE 2" CLEAR COVER UNLESS OTHERWISE SHOWN.
4. WHEN APPROVED BY THE ENGINEER/ENTITY, STRUCTURAL EPOXY ADHESIVE MAY BE USED IN LIEU OF NUMBER 4 DOWEL BAR EXCEPT AT CURB NOSE AND WITHIN 2 FEET OF ANY POINT OF CURVATURE.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501 CONCRETE

505 REINFORCING STEEL

707 JOINT MATERIAL

TACK ON ISLAND CURB

DATE 01-13-05

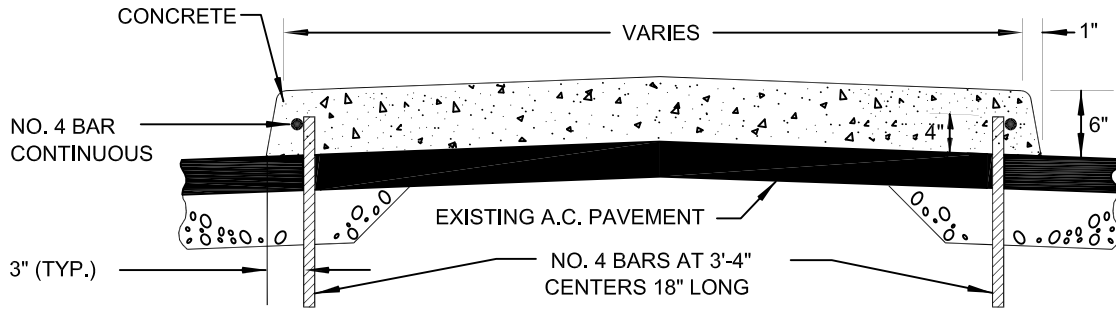
DWG. NO.

220

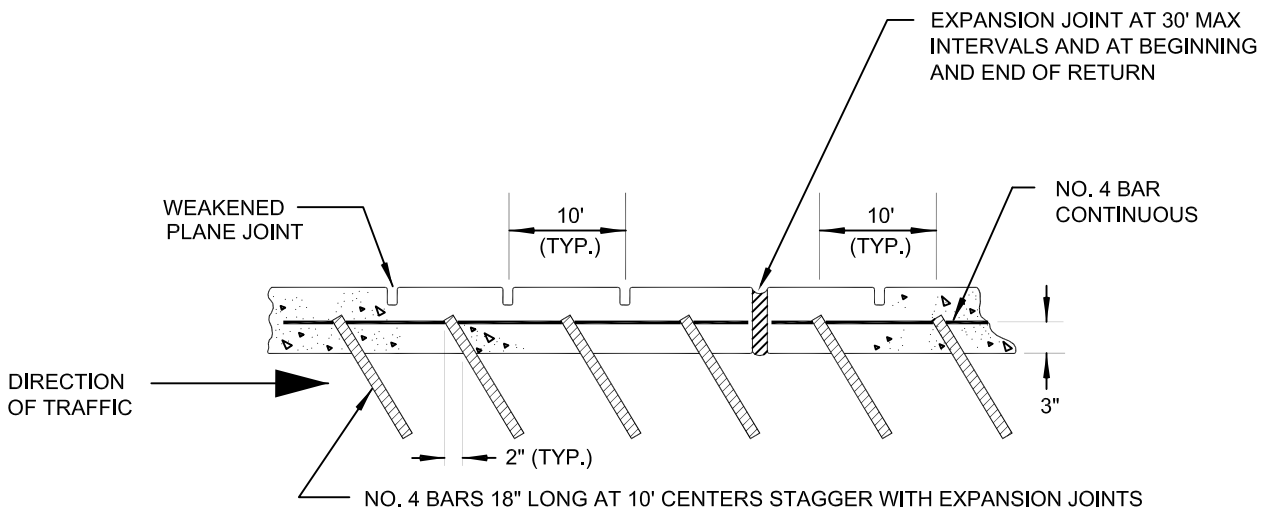
PAGE NO.

26

1/2" RADIUS ROUNDED
EDGE ON ALL EXPOSED
CORNERS



SECTION



SIDE VIEW

NOTES:

1. FOR EXPANSION JOINT AND WEAKENED PLANE JOINT DETAIL, SEE STANDARD DRAWING NO. 234.
2. WHEN APPROVED BY THE ENGINEER/ENTITY, STRUCTURAL EPOXY ADHESIVE MAY BE USED IN LIEU OF NUMBER 4 DOWEL BAR EXCEPT AT CURB NOSE AND WITHIN 2 FEET OF ANY POINT OF CURVATURE.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501 CONCRETE

505 REINFORCING STEEL

707 JOINT MATERIAL

EXCEPT CITY OF HENDERSON
TACK ON ISLAND

DATE 01-13-05

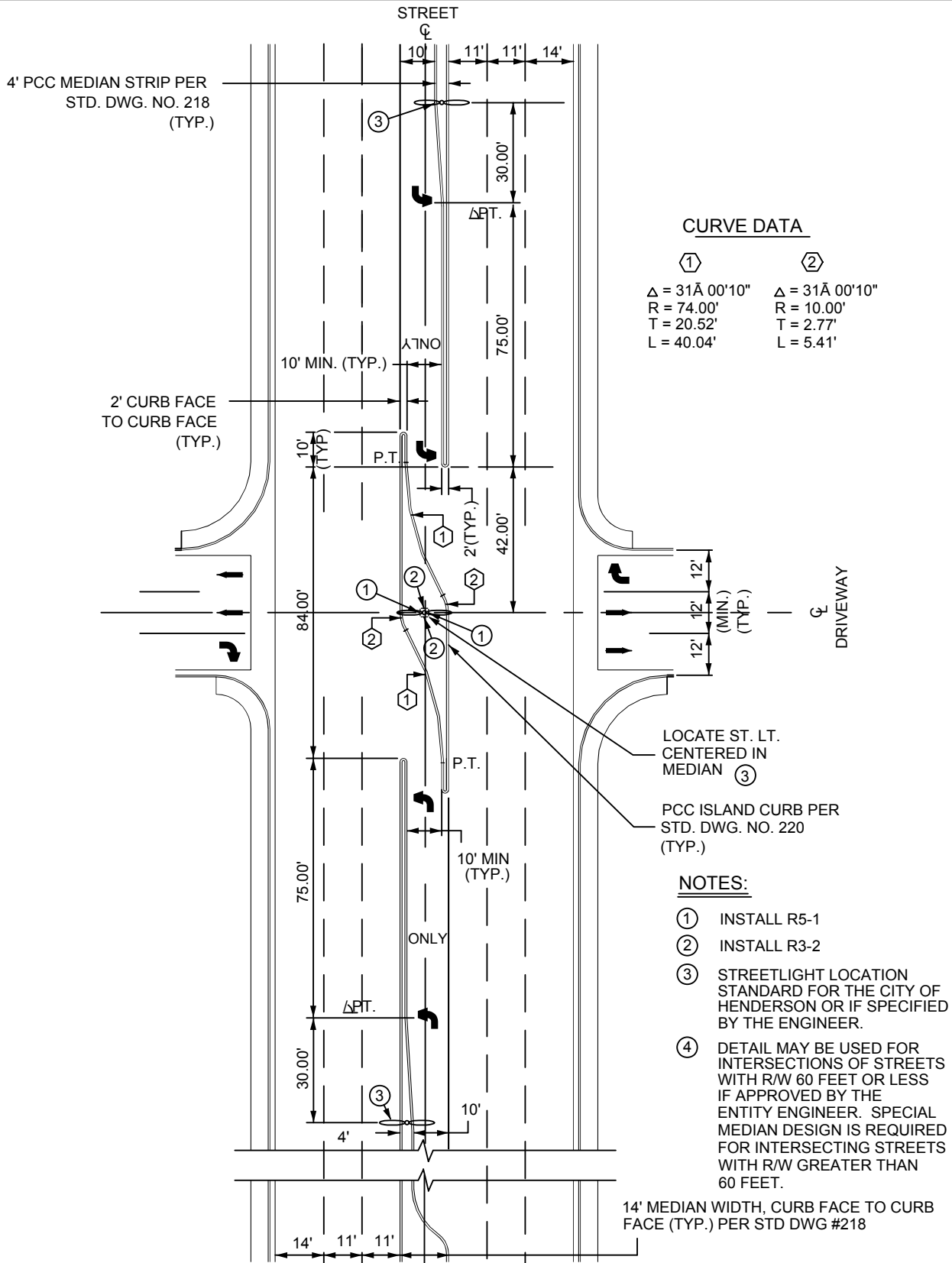
DWG. NO.

221

PAGE NO.

27

Effective 07/01/10 - 12/30/10

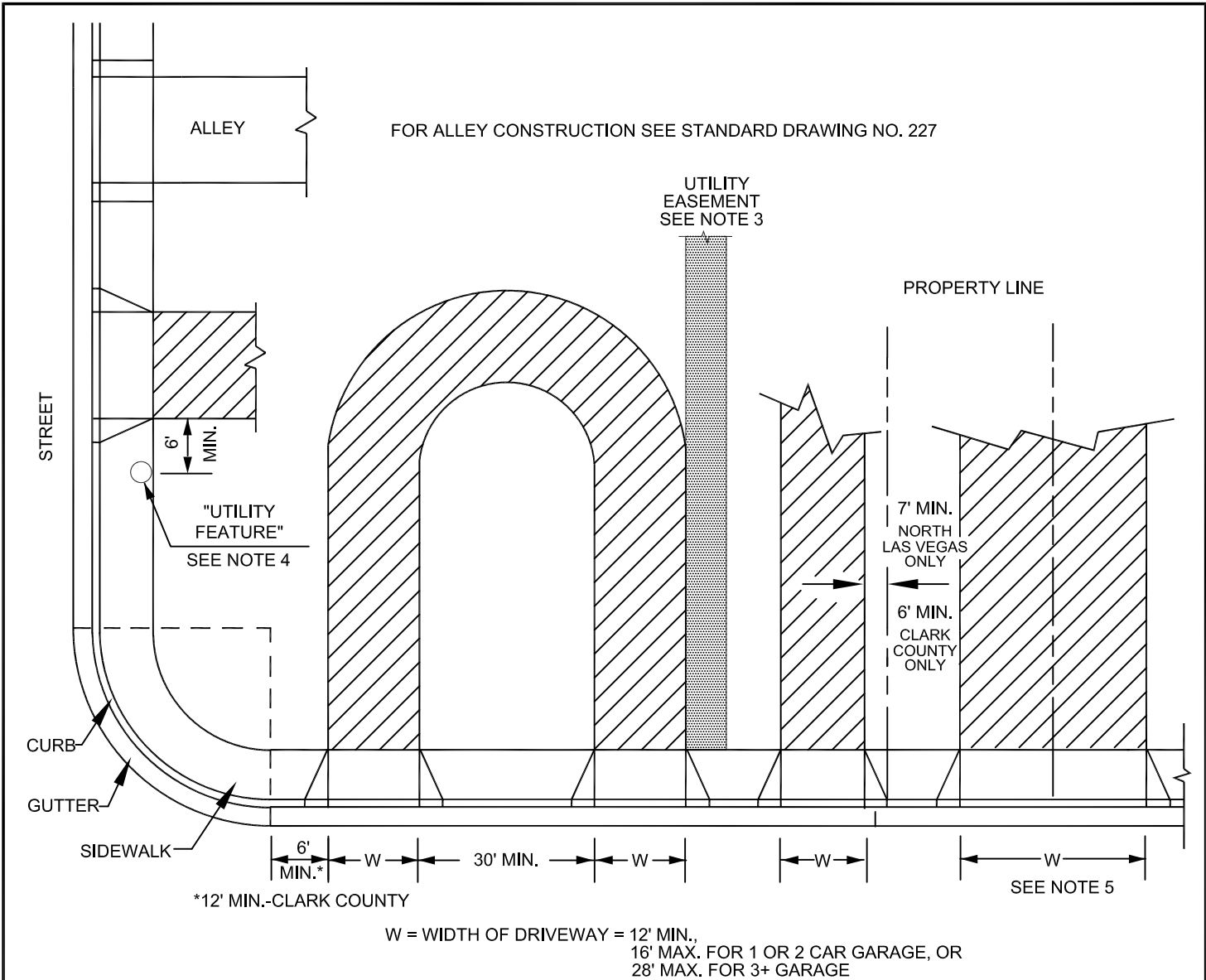


SPECIFICATION REFERENCE	

**UNIFORM STANDARD DRAWINGS
 CLARK COUNTY AREA**

**CHANNELIZED MEDIAN DETAIL
 FOR COMMERCIAL DRIVEWAYS OR
 INTERSECTING STREETS**

DATE 6-8-06	DWG. NO. 221.1	PAGE 27.1
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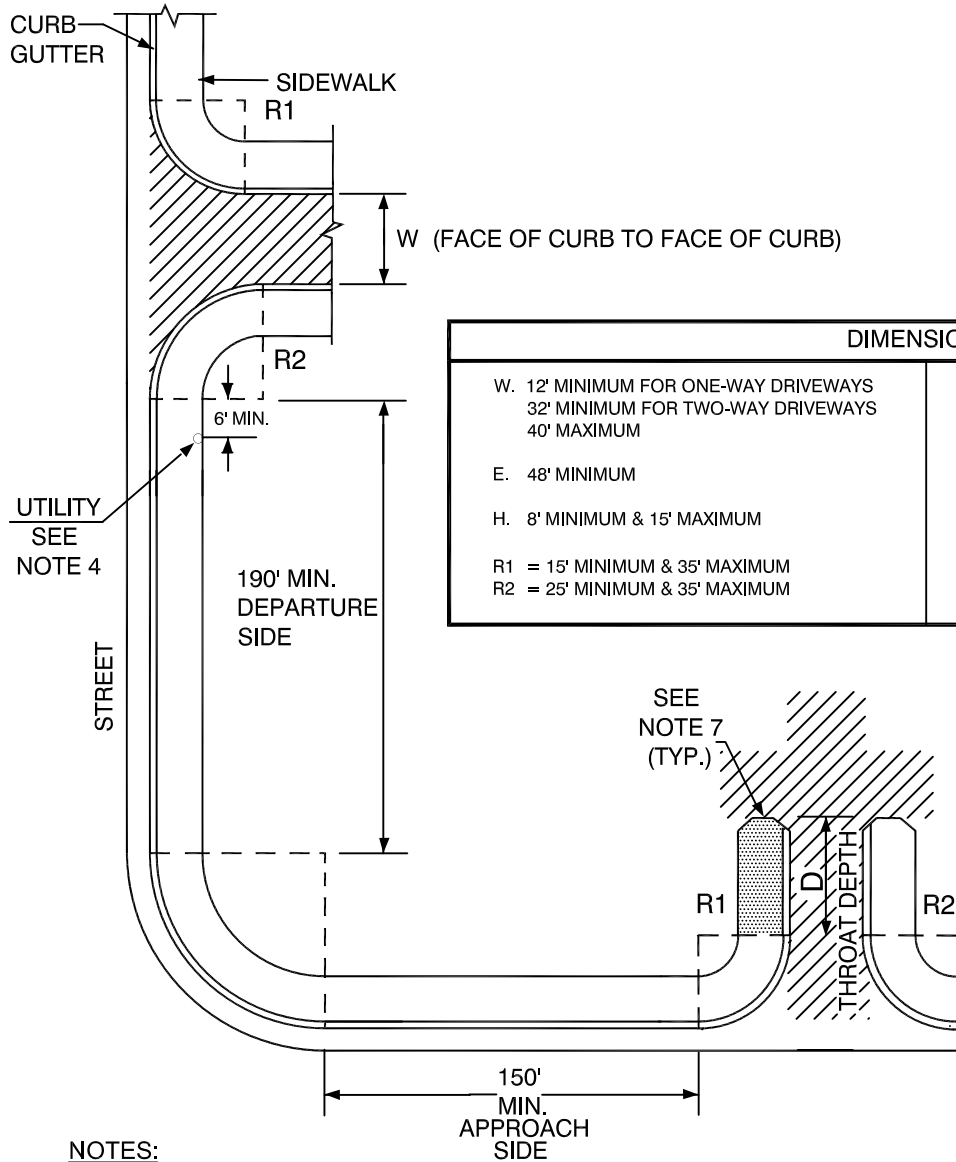


NOTES:

1. ALL RESIDENTIAL PROPERTIES MAY HAVE ONLY ONE CURB CUT EXCEPT CIRCULAR DRIVEWAYS AS SHOWN.
2. LOCAL ORDINANCES MAY APPLY AND SHALL HAVE PREFERENCE.
3. NO DRIVEWAY SHALL BE LOCATED WHOLLY OR PARTIALLY, ON OR OVER A UTILITY EASEMENT WHICH RUNS PERPENDICULAR TO THE CURB LINE.
4. NO DRIVEWAY SHALL BE LOCATED WITHIN 6 FEET OF A LIGHT POLE (UNLESS ACCEPTED BY THE ENTITY TRAFFIC ENGINEER), FIRE HYDRANT, MAIL BOX, ABOVE-GROUND ELECTRICAL TRANSFER BOX, BLOCK WALL HIGHER THAN 2 FEET, OR THE CURB RETURN AT A STREET INTERSECTION OR ALLEY.
5. COMMON DRIVEWAY CONSTRUCTION MAY BE PERMITTED AT ANY TWO RESIDENTIAL PROPERTIES OF 60 FEET IN WIDTH OR LESS. THE WIDTH OF THE JOINT DRIVEWAY SHALL BE A MAXIMUM OF 24 FEET. A JOINT DRIVEWAY AGREEMENT SHALL BE REQUIRED. (EXCEPT CLARK COUNTY)
6. GEOMETRICS APPLY TO NEW CONSTRUCTION ONLY, AND MAY VARY IN EXISTING SUBDIVISIONS SUBJECT TO APPROVAL OF THE ENGINEER.
7. MULTI-FAMILY RESIDENTIAL AND ALL NON-RESIDENTIAL DRIVEWAYS SHALL CONFORM TO THE COMMERCIAL DRIVEWAY STANDARDS.
8. ALL DRIVEWAY LOCATIONS SHALL BE SUBJECT TO REVIEW AND APPROVAL BY THE ENGINEER.
9. FOR CURB DEPRESSION AND DRIVEWAY APRON DETAIL, SEE STD. DWG. NO. 223.

SPECIFICATION REFERENCE	UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
	RESIDENTIAL DRIVEWAY GEOMETRICS		
	DATE 8-12-99	DWG. NO. 222	PAGE NO. 28

Effective 07/01/10 - 12/30/10



DIMENSIONS	
W. 12' MINIMUM FOR ONE-WAY DRIVEWAYS 32' MINIMUM FOR TWO-WAY DRIVEWAYS 40' MAXIMUM	D. THROAT DEPTH 25' MINIMUM 50' MINIMUM FOR PARKING LOTS > 50 PARKING SPACES 75' MINIMUM FOR PARKING LOTS 51 TO 100 PARKING SPACES 100' MINIMUM FOR PARKING LOTS 101 TO 200 PARKING SPACES 150' MINIMUM FOR PARKING LOTS > 201 PARKING SPACES
E. 48' MINIMUM	
H. 8' MINIMUM & 15' MAXIMUM	
R1 = 15' MINIMUM & 35' MAXIMUM R2 = 25' MINIMUM & 35' MAXIMUM	

NOTES:

1. COMMERCIAL AND MULTI-FAMILY DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DRAWING NUMBERS 224, 225, 228, 235 AND 235.1.
2. LOCAL ORDINANCES AND POLICIES MAY APPLY AND SHALL HAVE PRECEDENCE. SEE NDOT ACCESS POLICY FOR STATE ROADWAYS.
3. THE TOTAL WIDTH "W" OF DRIVEWAY CURB OPENINGS SHALL NOT EXCEED 65% OF FRONT FOOTAGE.
4. NO DRIVEWAY SHALL BE LOCATED WITHIN 6 FEET OF A LIGHT POLE (UNLESS APPROVED BY THE ENTITY TRAFFIC ENGINEER), FIRE HYDRANT, MAIL BOX, ABOVE-GROUND ELECTRICAL TRANSFER BOX, OR BLOCK WALL HIGHER THAN 2 FEET.
5. THE CENTERLINES OF THE DRIVEWAYS ON OPPOSITE SIDES OF THE STREET AT A MEDIAN OPENING SHOULD BE WITHIN 10' FROM EACH OTHER AT THE MEDIAN OPENING.
6. GEOMETRICS APPLY TO NEW CONSTRUCTION ONLY, AND EXCEPTIONS MAY BE GRANTED BY THE APPROVAL OF THE AGENCY TRAFFIC ENGINEER BASED ON SITE CONSTRAINTS.
7. HANDICAPPED ACCESSIBLE SIDEWALKS SHALL BE PROVIDED ADJACENT TO DRIVEWAYS TO THE P.C. OF THE ONSITE CURB RETURN, MINIMUM, OR AT AN ALTERNATE LOCATION.
8. WHEN A PROPERTY LINE FALLS IN A MEDIAN OPENING A JOINT DRIVEWAY AGREEMENT SHALL BE REQUIRED OR NO DRIVEWAY WILL BE ALLOWED.

SPECIFICATION REFERENCE	UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
	COMMERCIAL AND MULTI-FAMILY DRIVEWAY GEOMETRICS		
	DATE 02-09-06	DWG. NO. 222A	SHEET 1 OF 2

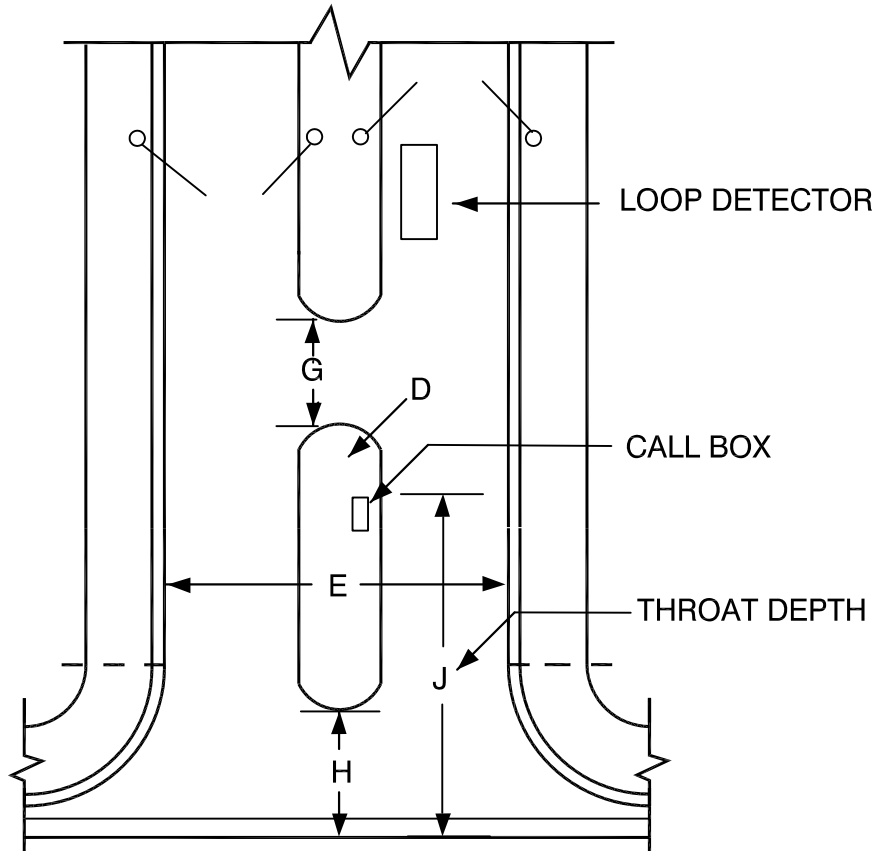
DIMENSIONS

J. THROAT DEPTH FOR SECURITY GATE

50' MINIMUM FOR 1 TO 49 HOMES OR APT. UNITS TO VISITOR CALL BOX.
 100' MINIMUM FOR 50 TO 100 HOMES OR APT. UNITS TO VISITOR CALL BOX.
 GREATER THAN 100 HOMES OR APT. UNITS REQUIRE TRAFFIC STUDY

DIMENSIONS FOR SECURITY GATE
 CONTROLLED DRIVEWAY DETAIL

- D. ISLAND : LENGTH-20' MINIMUM
 WIDTH- 4' MINIMUM
- G. 15' MINIMUM
- E. 48' MINIMUM
- H. 8' MINIMUM & 15' MAXIMUM



DETAIL FOR SECURITY GATE
 CONTROLLED DRIVEWAYS

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
 CLARK COUNTY AREA

COMMERCIAL AND MULTI-FAMILY
 SECURITY GATE GEOMETRICS

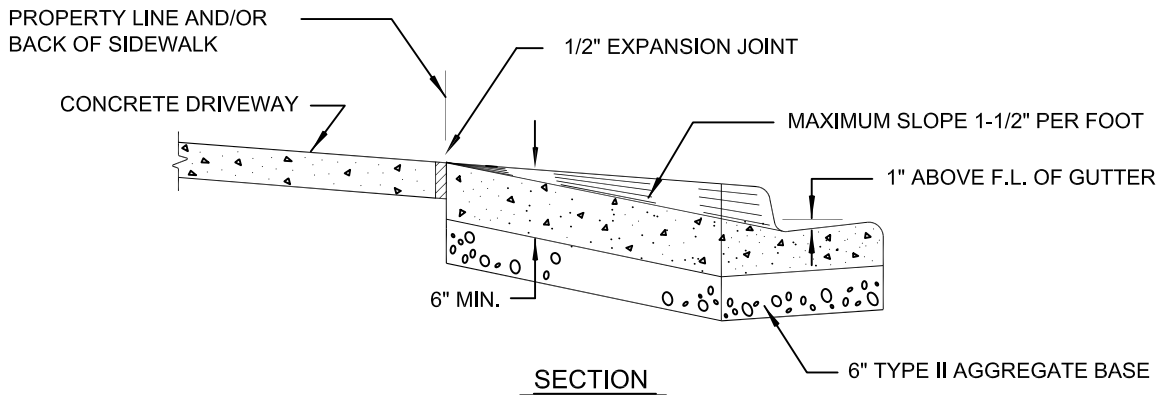
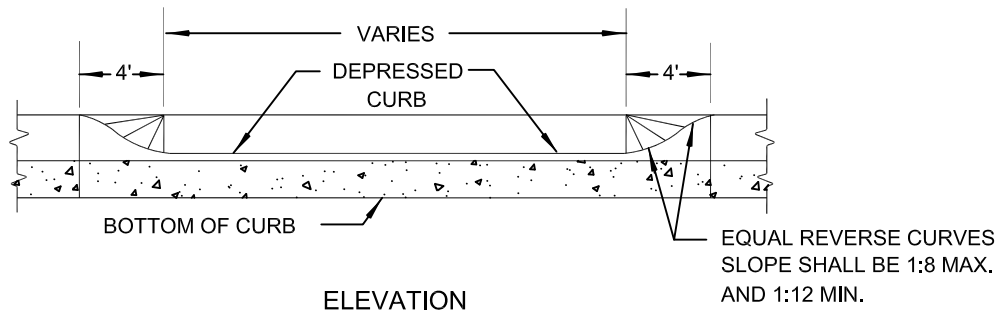
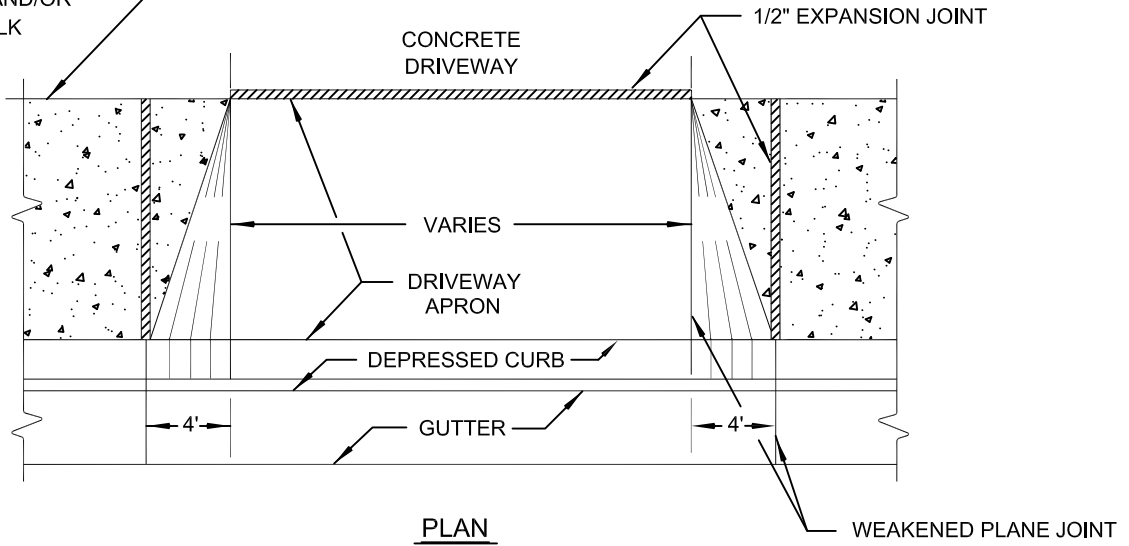
DATE 02-09-06

DWG. NO. 222A

SHEET

2 OF 2

PROPERTY LINE AND/OR
BACK OF SIDEWALK



NOTES:

1. WHEN CONSTRUCTING DRIVEWAY WHERE CURB AND GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB AND GUTTER. DRIVEWAYS MAY BE MONOLITHIC TO A.C. LINE.
2. WEAKENED PLANE JOINTS SHALL BE UNIFORMLY PLACED BETWEEN 5' AND 7' INTERVALS, SEE STANDARD DRAWING NO. 234.

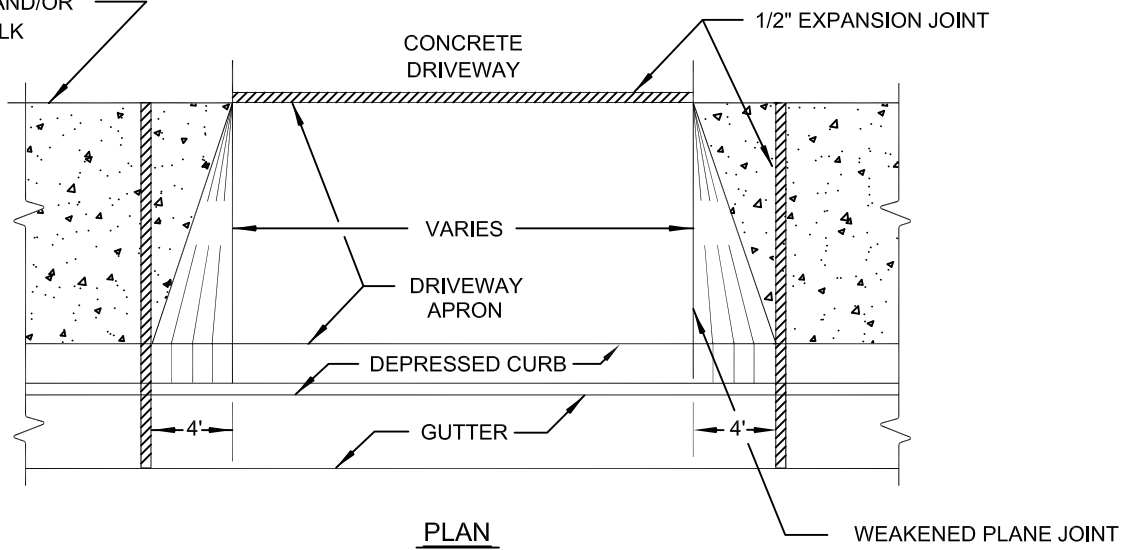
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

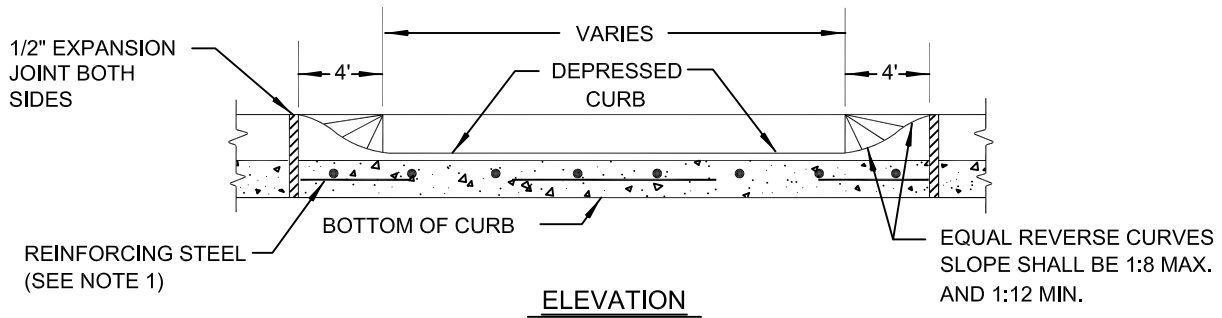
302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES
707	JOINT MATERIAL

RESIDENTIAL DRIVEWAY

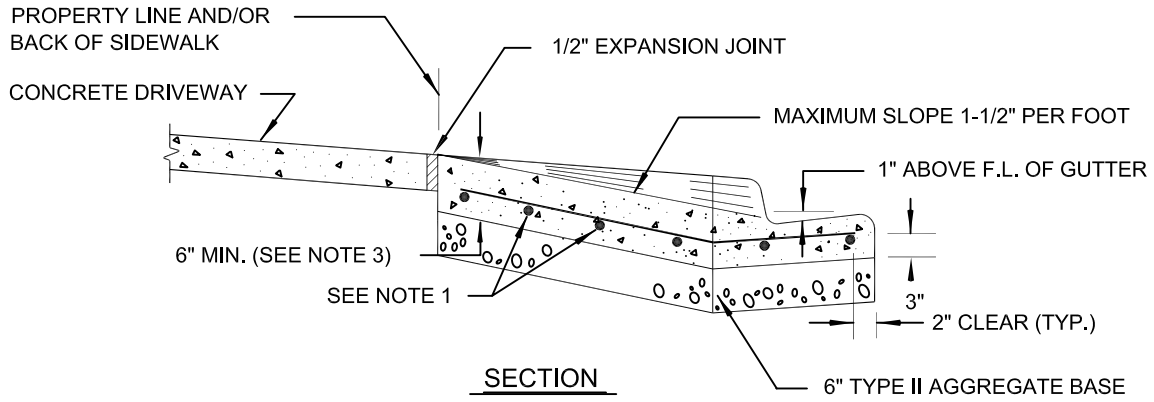
PROPERTY LINE AND/OR
BACK OF SIDEWALK



PLAN



ELEVATION



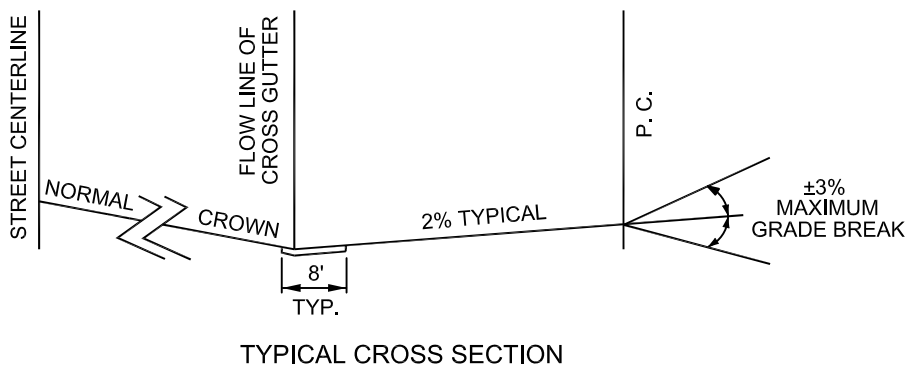
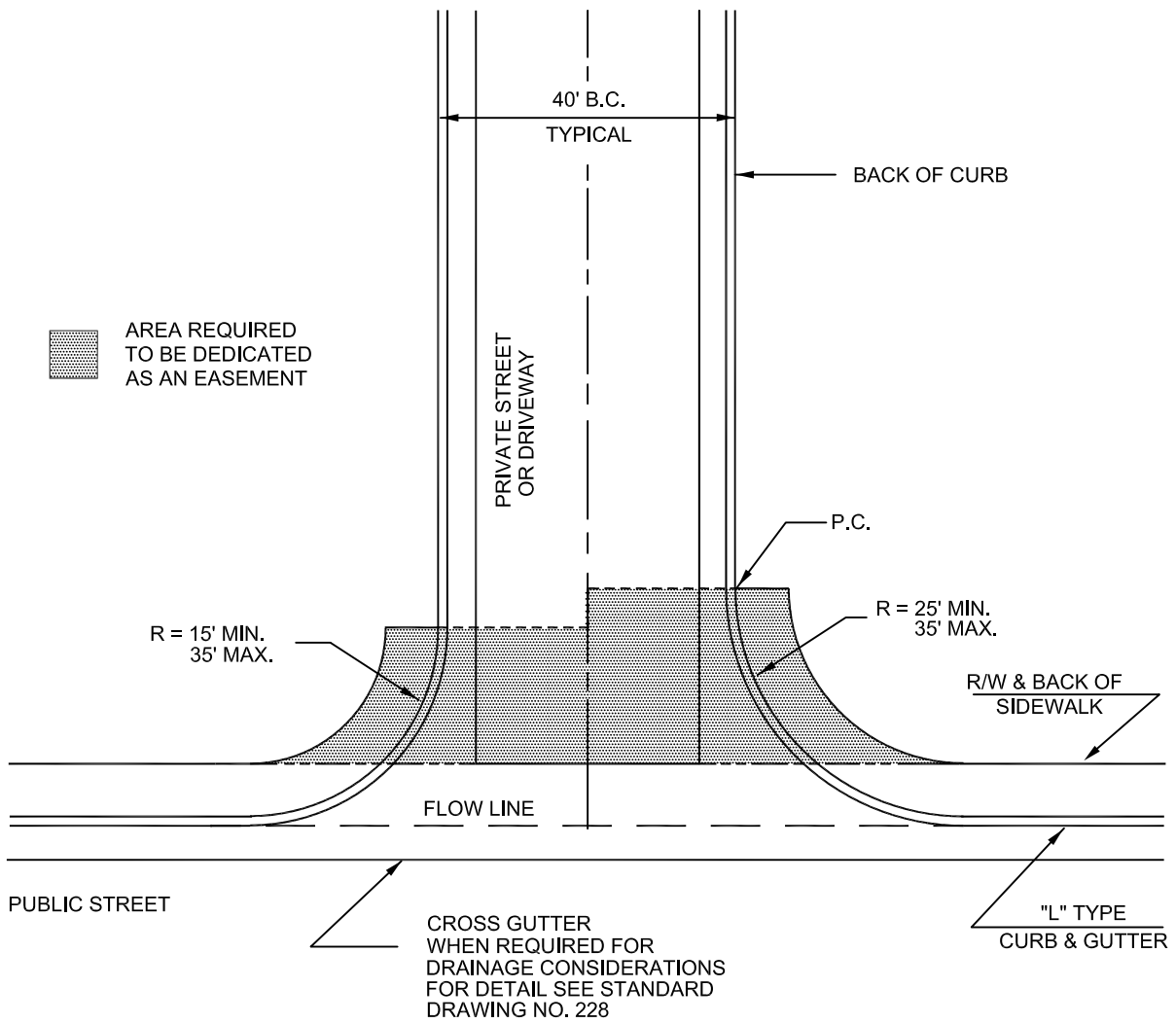
SECTION

NOTES:

1. NO. 4 BARS AT 16" O.C. BOTH WAYS EXTENDING INTO GUTTER. NO. 4 BARS SHALL BE PLACED 3" ABOVE BOTTOM OF CONCRETE SUPPORTED BY NON-FERROUS CHAIRS APPROVED BY THE ENGINEER.
2. WHEN CONSTRUCTING DRIVEWAY WHERE CURB AND GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB AND GUTTER. DRIVEWAY SHALL BE MONOLITHIC TO A.C. LINE.
3. DRIVEWAY THICKNESS FOR INDUSTRIAL USE SHALL BE 8" MIN.
4. WEAKENED PLANE JOINTS SHALL BE EQUALLY SPACED AT 15' MAX. INTERVALS, SEE STANDARD DRAWING NO. 234.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	COMMERCIAL AND INDUSTRIAL DRIVEWAY (OPTION A)		
501	CONCRETE			
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
707	JOINT MATERIAL			
		DATE 12-14-00	DWG. NO. 224	PAGE NO. 30

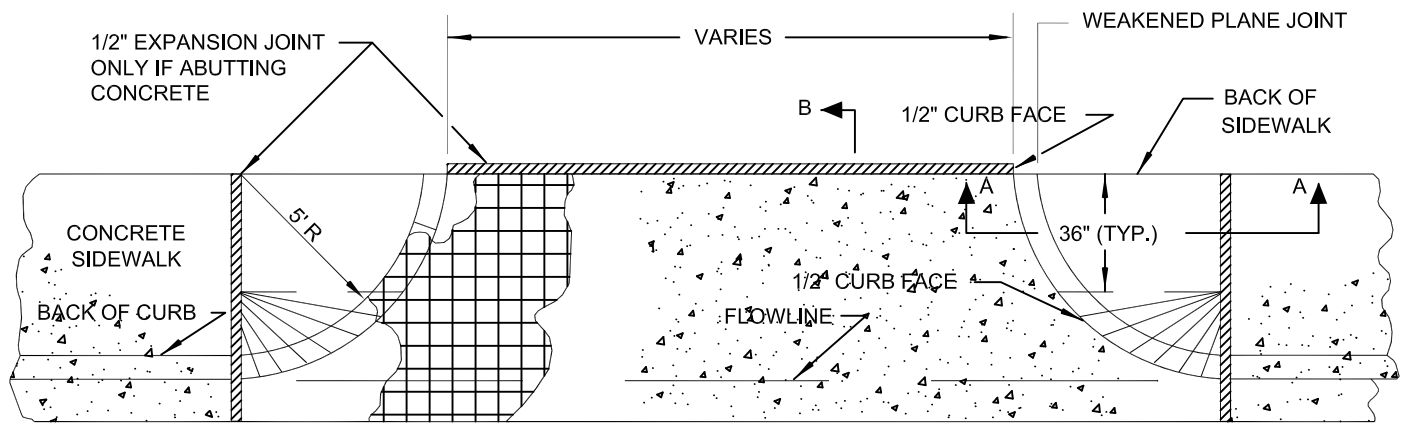


NOTES:

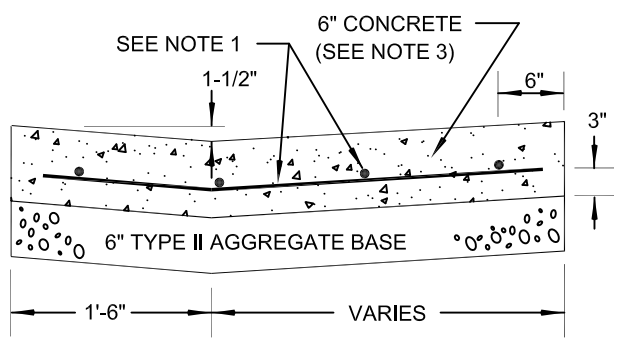
1. SEPARATION OF PEDESTRIAN AND VEHICLE TRAFFIC MUST BE MAINTAINED ON SITE.
2. FOR GRADE CHANGES GREATER THAN 3%, VERTICAL CURVES OF AT LEAST 10 FEET MUST BE USED.
3. WHEELCHAIR RAMPS SHALL BE CONSTRUCTED IN THE CURB RETURN IN ACCORDANCE WITH STANDARD DRAWING NO. 235.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
		COMMERCIAL AND INDUSTRIAL DRIVEWAY (OPTION B) OR PRIVATE STREET ACCESS	
DATE	2-11-93	DWG. NO.	225
		PAGE NO.	31

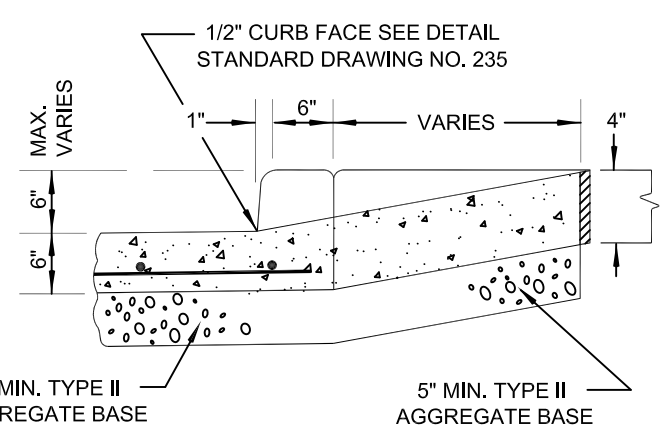
Effective 07/01/10 - 12/30/10



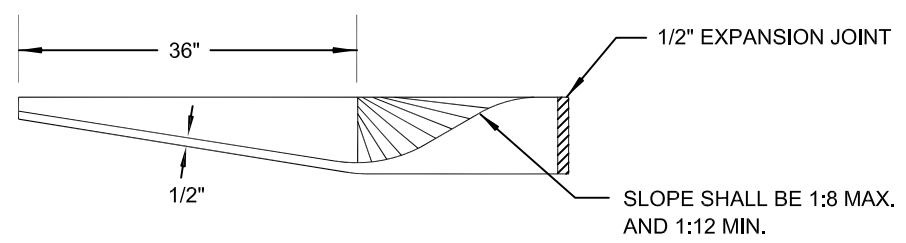
PLAN



SECTION B-B



SECTION A-A

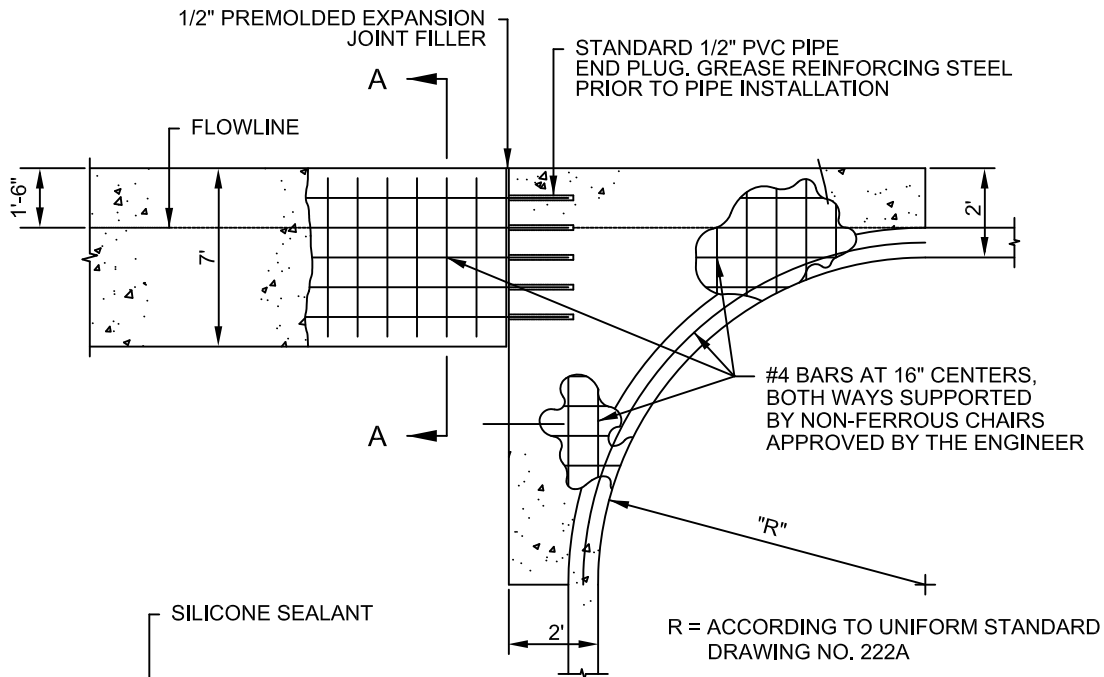


WHEELCHAIR RAMP CURB FACE PROFILE

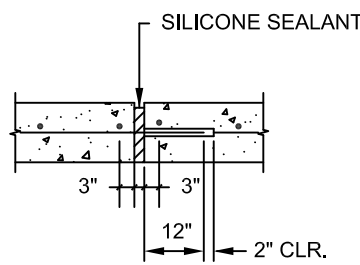
1. NO. 4 BARS AT 16" O.C. BOTH WAYS EXTENDING INTO GUTTER. NO. 4 BARS SHALL BE PLACED 3" ABOVE BOTTOM OF CONCRETE SUPPORTED BY NON-FERROUS CHAIRS APPROVED BY THE ENGINEER.
2. WHEN CONSTRUCTING DRIVEWAY WHERE CURB AND GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB AND GUTTER. DRIVEWAY SHALL BE MONOLITHIC TO A.C. LINE.
3. DRIVEWAY THICKNESS FOR INDUSTRIAL USE SHALL BE 8" MIN.
4. WEAKENED PLANE JOINTS SHALL BE EQUALLY SPACED AT 15' MAX. INTERVALS.
5. THIS DRIVEWAY DESIGN SHALL ALSO BE USED FOR ALLEY INTERSECTIONS, 8" MIN. THICKNESS.
6. SPECIAL DESIGNS SUBJECT TO APPROVAL OF THE ENGINEER.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA (ALLOWABLE IN NORTH LAS VEGAS ONLY) COMMERCIAL AND INDUSTRIAL DRIVEWAY (OPTION C)		
302	AGGREGATE BASE			
501	CONCRETE			
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
707	JOINT MATERIAL			
		DATE 12-14-00	DWG. NO. 226	PAGE NO. 32

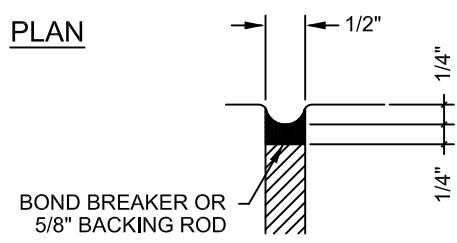


R = ACCORDING TO UNIFORM STANDARD DRAWING NO. 222A

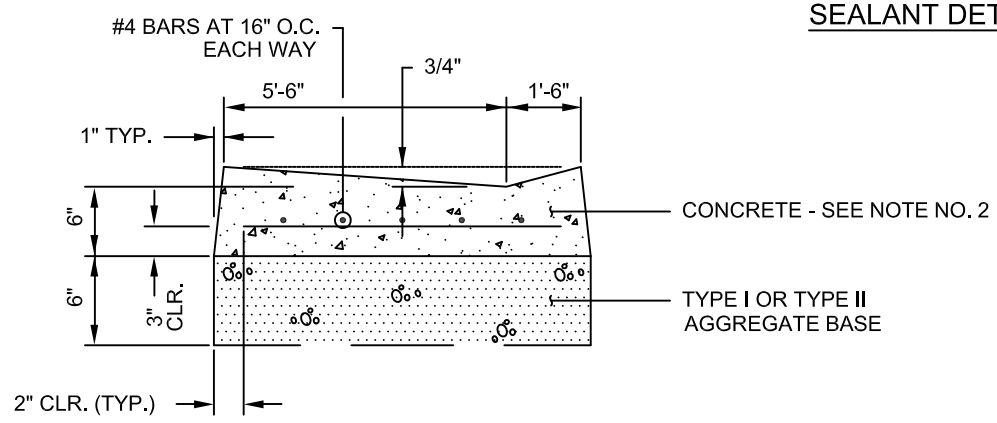


JOINT DETAIL

PLAN



SEALANT DETAIL



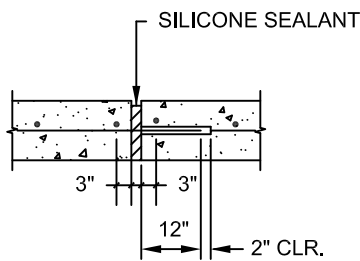
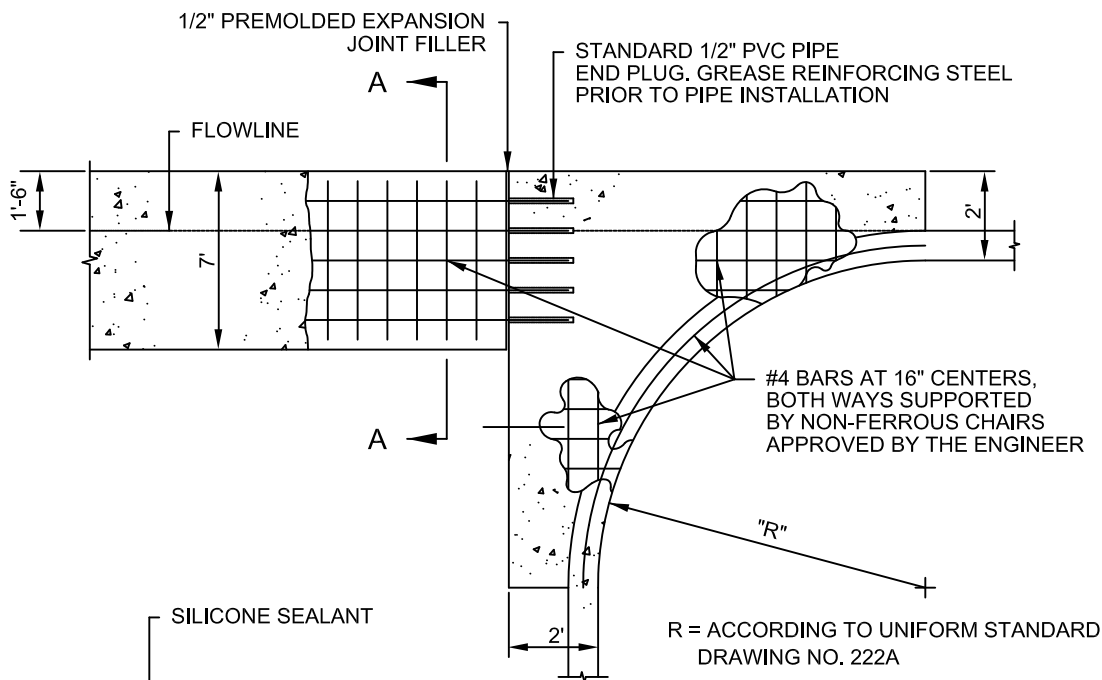
SECTION A-A

NOTES:

1. FINISHED ASPHALT CONCRETE SURFACE TO BE FLUSH WITH CROSS GUTTER LIP.
2. ADJACENT SPANDREL SHALL BE 6" THICK P.C.C.

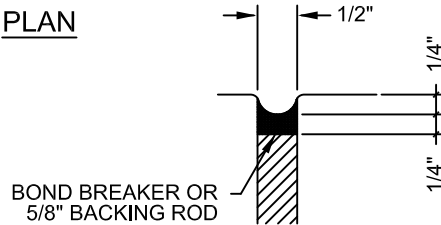
Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
302	AGGREGATE BASE	(HENDERSON ONLY)			
501	CONCRETE				
502	CONCRETE STRUCTURES	LIGHT DUTY COMMERCIAL DRIVEWAY			
505	REINFORCING STEEL				
707	EXPANSION JOINT MATERIAL	(OFFICES, CHURCHES, SCHOOLS, RESTAURANTS, ETC.)			
TT-S-00153A CLASS A SEALANT					
DATE	12-14-00	DWG. NO.	226.1	PAGE	32.1

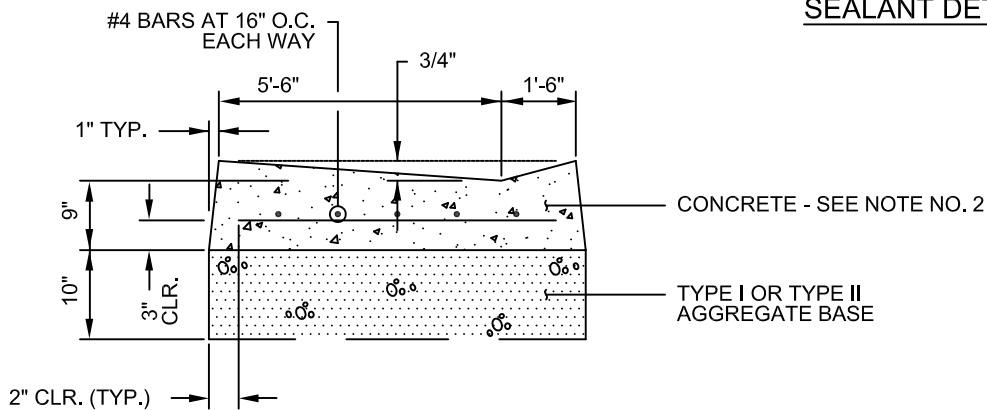


JOINT DETAIL

PLAN



SEALANT DETAIL



SECTION A-A

NOTES:

1. FINISHED ASPHALT CONCRETE SURFACE TO BE FLUSH WITH CROSS GUTTER LIP.
2. ADJACENT SPANDREL SHALL BE 9" THICK P.C.C.

SPECIFICATION REFERENCE

302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES
505	REINFORCING STEEL
707	EXPANSION JOINT MATERIAL

TT-S-00153A CLASS A SEALANT

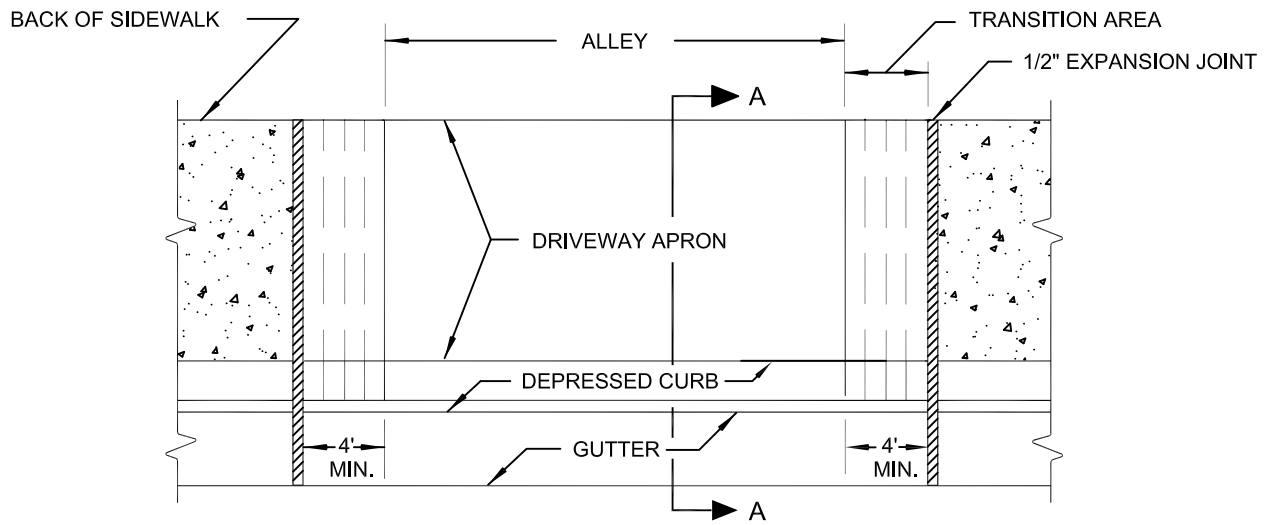
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

(HENDERSON ONLY)

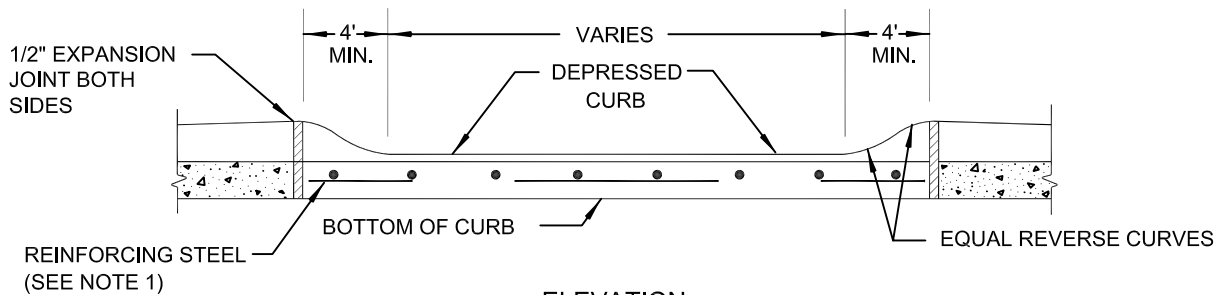
HEAVY DUTY COMMERCIAL
DRIVEWAY

(SERVICE STATIONS, INDUSTRIAL, LOADING DOCKS, ETC.)

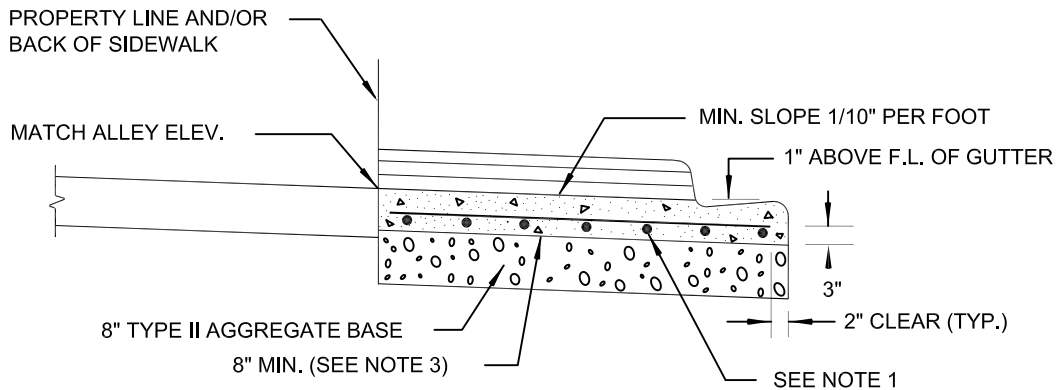
DATE 12-14-00 | DWG. NO. 226.2 | PAGE 32.2



PLAN



ELEVATION



SECTION A-A

NOTES:

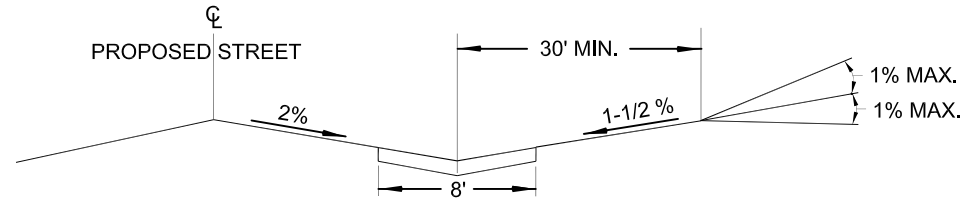
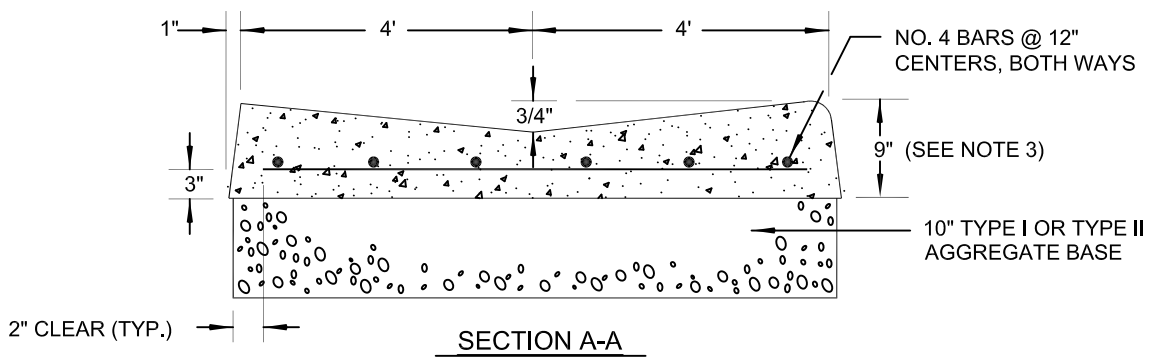
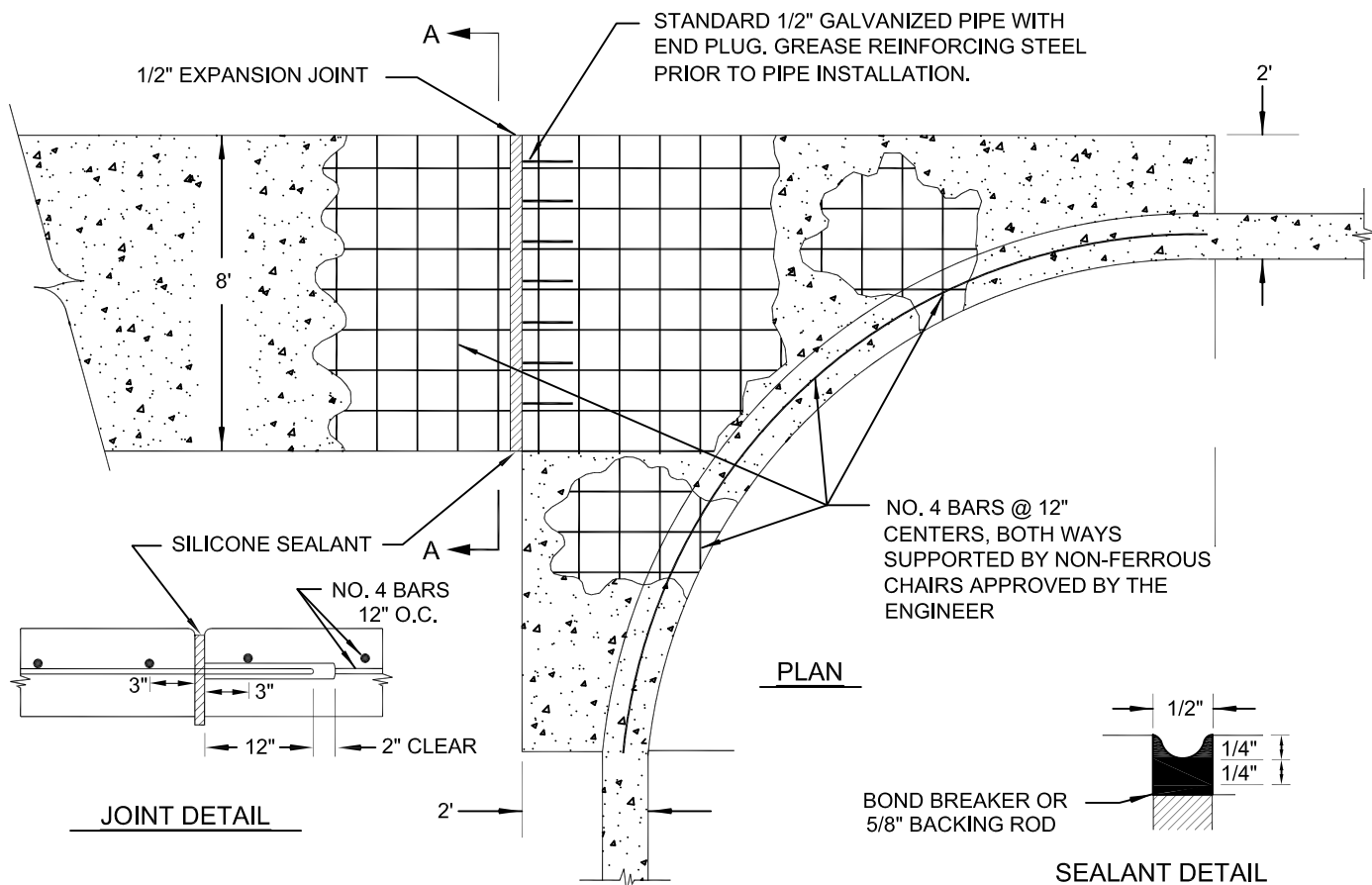
1. NO. 4 BARS AT 16" O.C. BOTH WAYS CONTINUOUS THROUGH GUTTER. NO. 4 BARS SHALL BE PLACED 3" ABOVE BOTTOM OF CONCRETE.
2. WHEN CONSTRUCTING DRIVEWAY WHERE CURB AND GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB AND GUTTER. DRIVEWAY SHALL BE MONOLITHIC TO A.C. LINE.
3. DRIVEWAY THICKNESS SHALL BE 8" MIN.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

CITY OF LAS VEGAS ONLY

DEPRESSED ALLEY DRIVEWAY

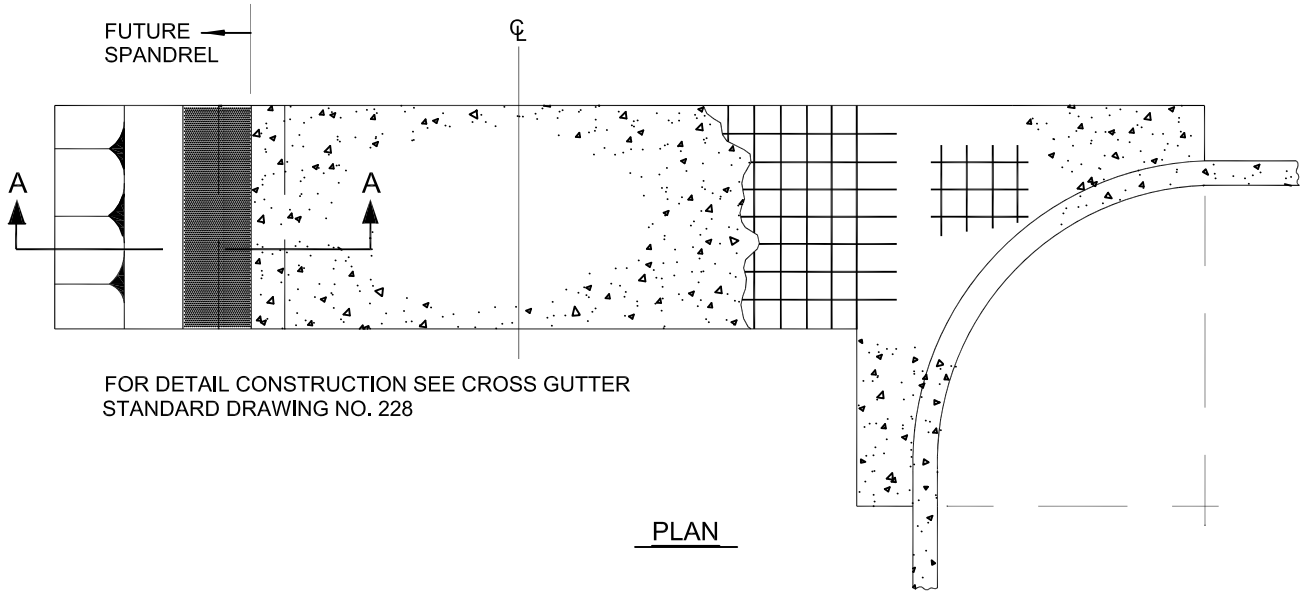


NOTES:

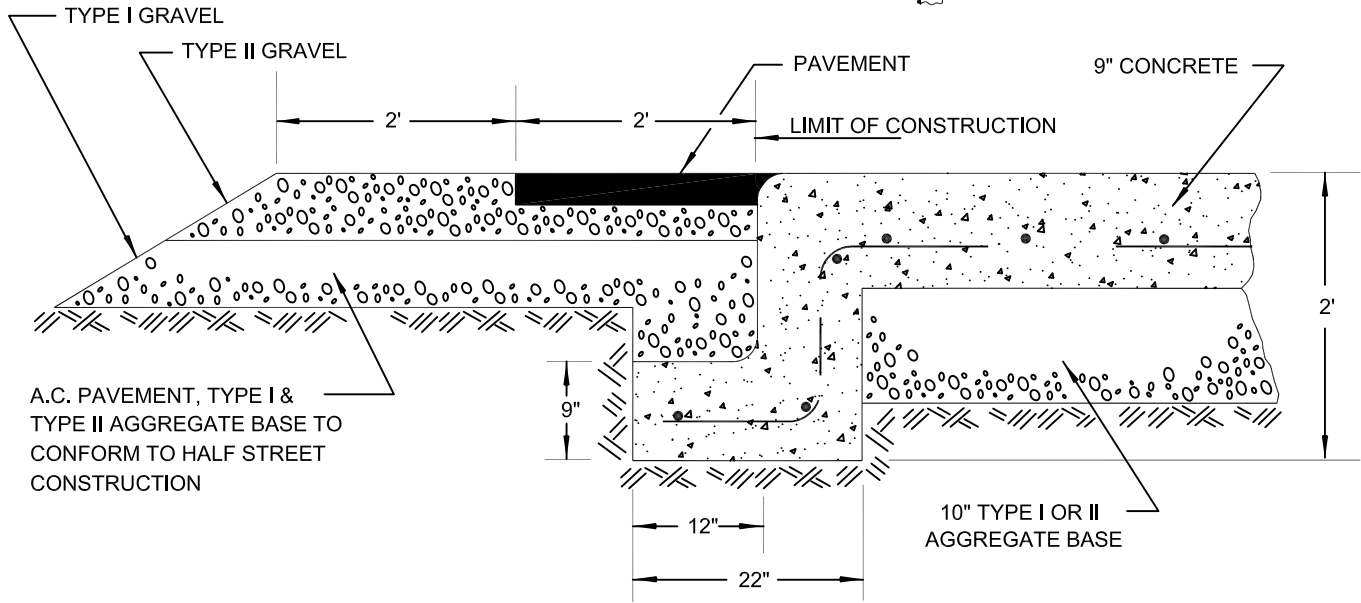
1. FINISHED ASPHALT CONCRETE SURFACE TO BE FLUSH WITH CROSS GUTTER LIP.
2. CONSTRUCTION OF CROSS GUTTER IS NOT ALLOWED ACROSS MAJOR COLLECTOR OR ARTERIAL STREETS.
3. ADJACENT SPANDREL SHALL BE 9" THICK P.C.C.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	CROSS GUTTER		
501	CONCRETE			
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
707	EXPANSION JOINT MATERIAL			
TT-S-00153A CLASS A SEALANT		DATE 12-14-00	DWG. NO. 228	PAGE NO. 34

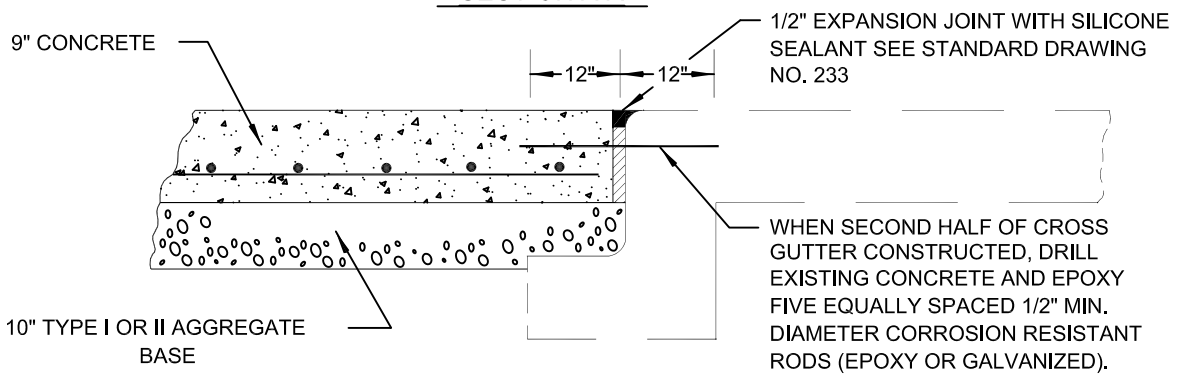
Effective 07/01/10 - 12/30/10



PLAN



SECTION A-A

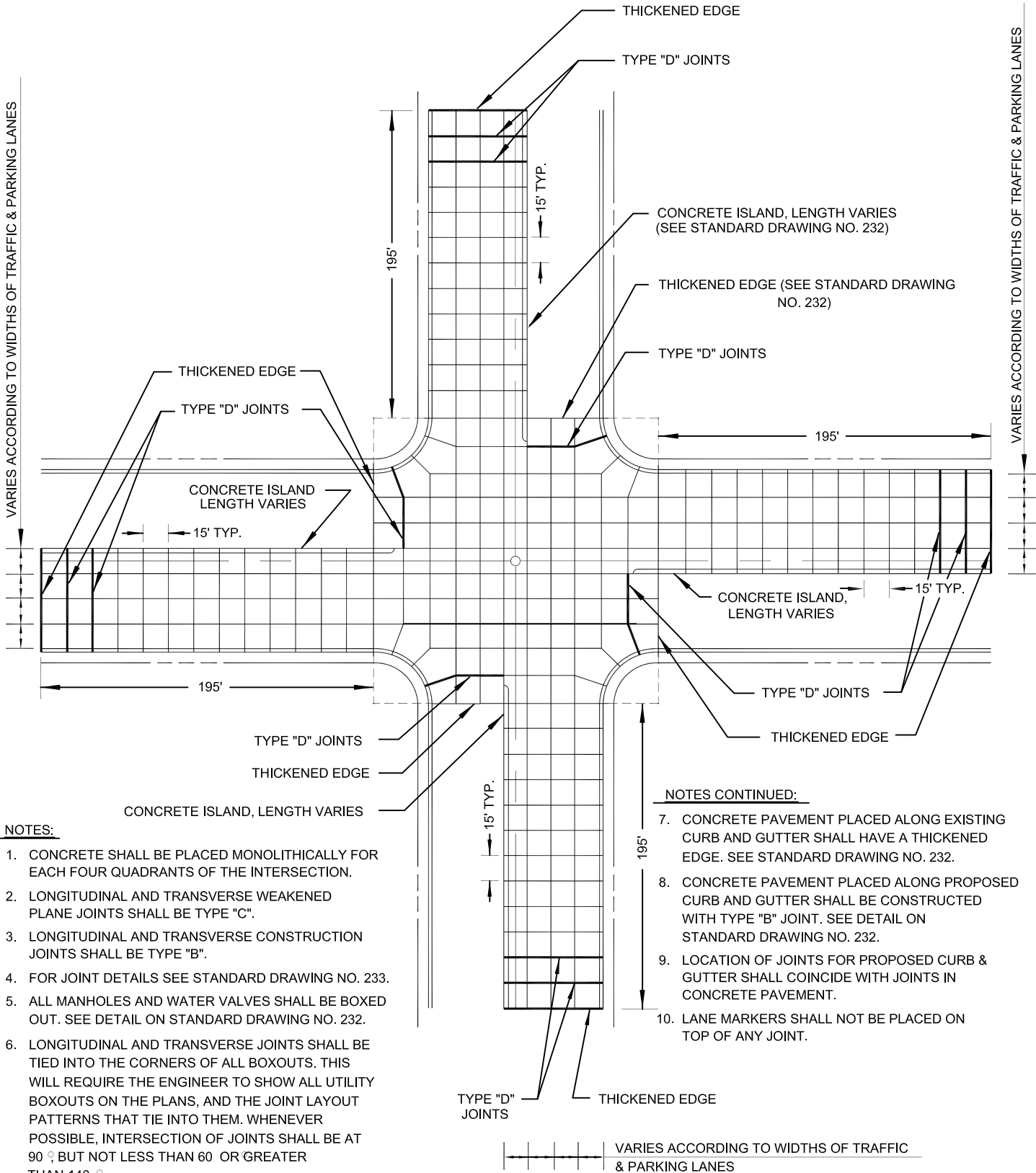


DETAIL FOR FUTURE CONSTRUCTION

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	HALF STREET CROSS GUTTER		
501	CONCRETE			
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
707	EXPANSION JOINT MATERIAL			
TT-S-00153A CLASS A SEALANT		DATE	DWG. NO. 229	PAGE NO. 35

VARIES ACCORDING TO WIDTHS OF TRAFFIC & PARKING LANES



NOTES:

1. CONCRETE SHALL BE PLACED MONOLITHICALLY FOR EACH FOUR QUADRANTS OF THE INTERSECTION.
2. LONGITUDINAL AND TRANSVERSE WEAKENED PLANE JOINTS SHALL BE TYPE "C".
3. LONGITUDINAL AND TRANSVERSE CONSTRUCTION JOINTS SHALL BE TYPE "B".
4. FOR JOINT DETAILS SEE STANDARD DRAWING NO. 233.
5. ALL MANHOLES AND WATER VALVES SHALL BE BOXED OUT. SEE DETAIL ON STANDARD DRAWING NO. 232.
6. LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE TIED INTO THE CORNERS OF ALL BOXOUTS. THIS WILL REQUIRE THE ENGINEER TO SHOW ALL UTILITY BOXOUTS ON THE PLANS, AND THE JOINT LAYOUT PATTERNS THAT TIE INTO THEM. WHENEVER POSSIBLE, INTERSECTION OF JOINTS SHALL BE AT 90 ° BUT NOT LESS THAN 60 ° OR GREATER THAN 140 °

NOTES CONTINUED:

7. CONCRETE PAVEMENT PLACED ALONG EXISTING CURB AND GUTTER SHALL HAVE A THICKENED EDGE. SEE STANDARD DRAWING NO. 232.
8. CONCRETE PAVEMENT PLACED ALONG PROPOSED CURB AND GUTTER SHALL BE CONSTRUCTED WITH TYPE "B" JOINT. SEE DETAIL ON STANDARD DRAWING NO. 232.
9. LOCATION OF JOINTS FOR PROPOSED CURB & GUTTER SHALL COINCIDE WITH JOINTS IN CONCRETE PAVEMENT.
10. LANE MARKERS SHALL NOT BE PLACED ON TOP OF ANY JOINT.

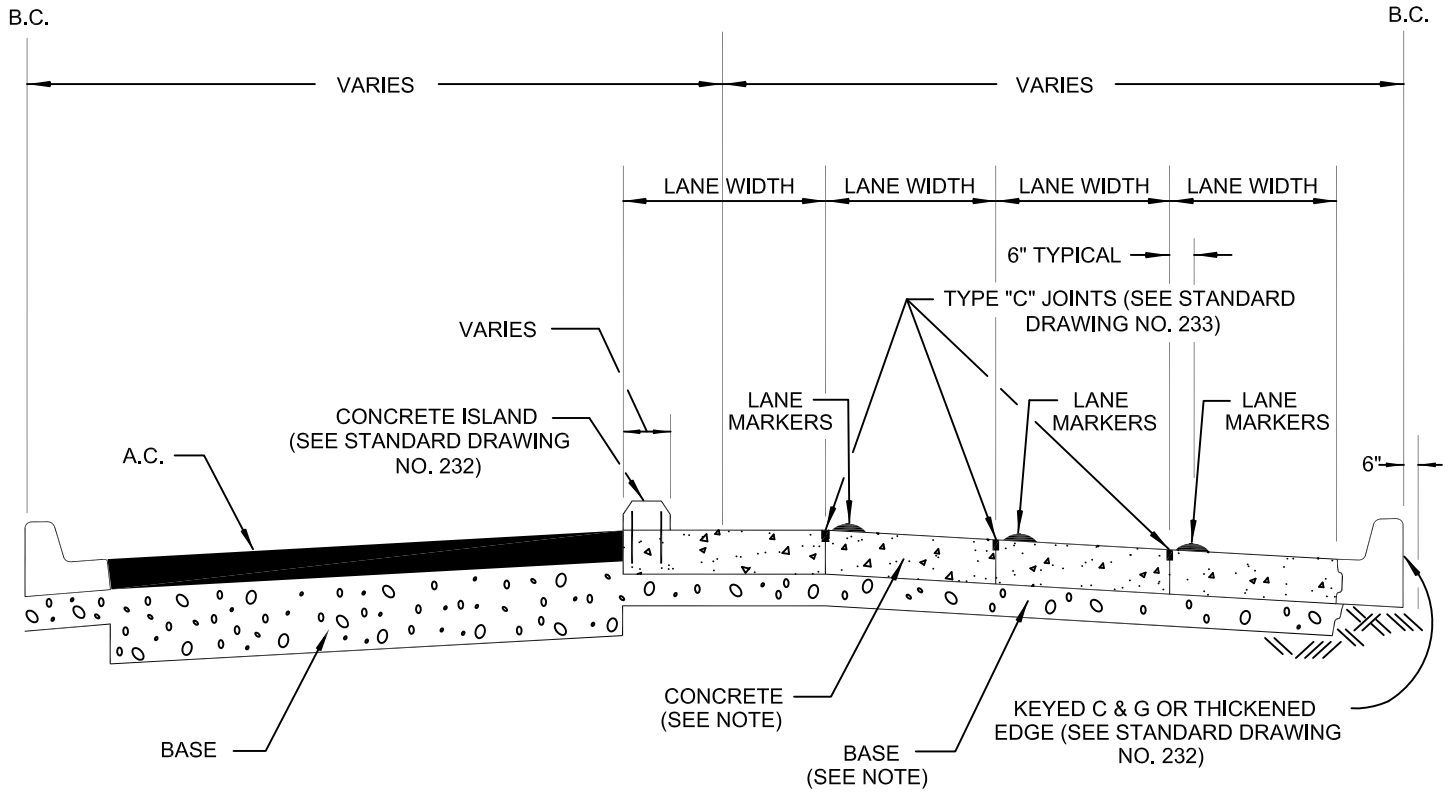
SPECIFICATION REFERENCE

409	CONCRETE PAVEMENT
501	CONCRETE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**CONCRETE PAVEMENT INTERSECTION
TYPICAL LAYOUT**

Effective 07/01/10 - 12/30/10

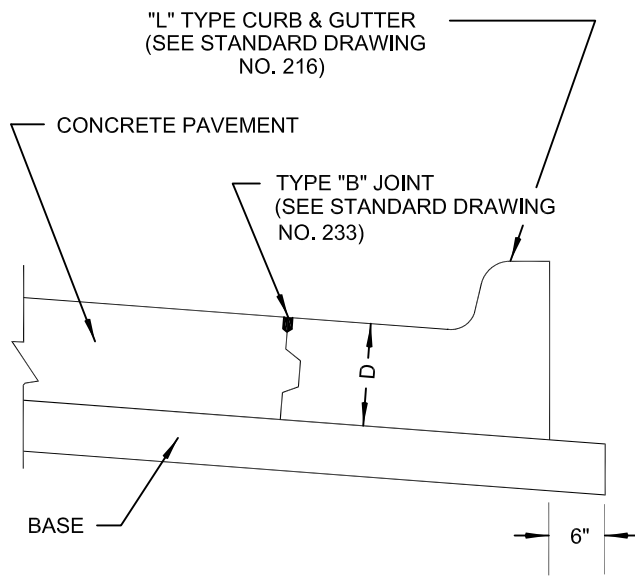


NOTE:

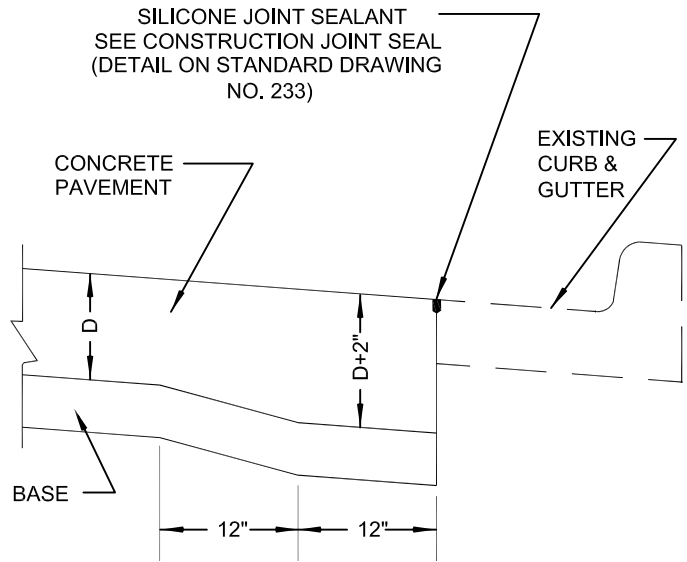
CONCRETE AND BASE THICKNESS TO BE DETERMINED BY ENGINEERING ANALYSIS BASED ON TRAFFIC CONDITIONS, SUBGRADE STRENGTH, QUALITY OF BASE, AND FLEXURAL STRENGTH OF CONCRETE.

Effective 07/01/10 - 12/30/10

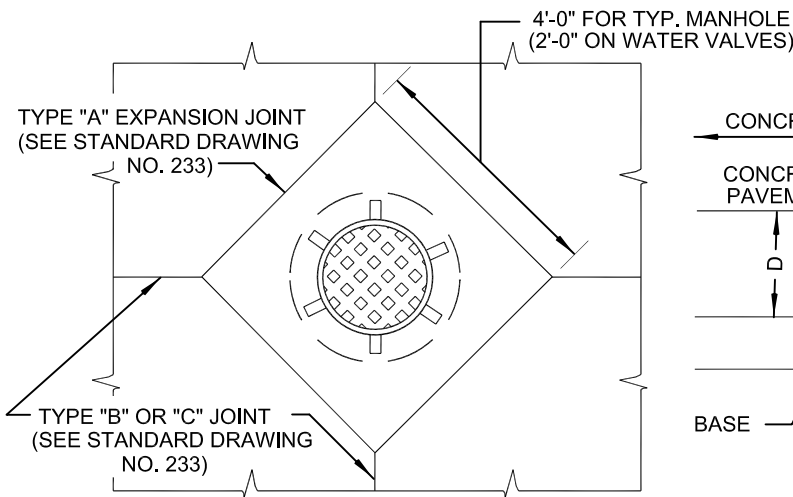
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
302	AGGREGATE BASE	CONCRETE PAVEMENT TYPICAL ROADWAY SECTION				
409	CONCRETE PAVEMENT					
501	CONCRETE					
633	PAVEMENT MARKERS					
707	JOINT MATERIAL					
		DATE	DWG. NO.	231	PAGE NO.	37



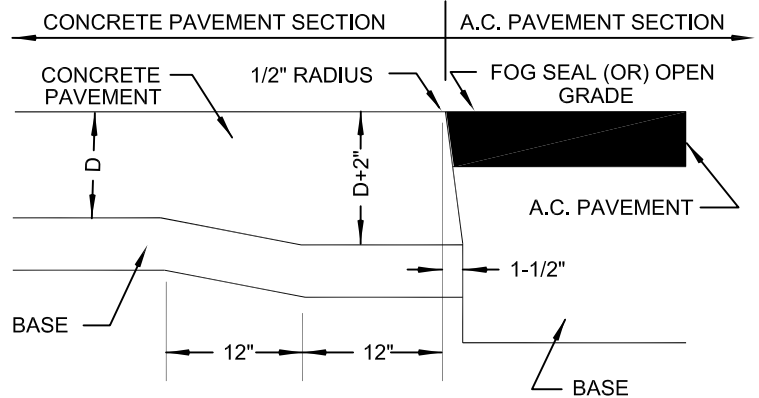
**CURB & GUTTER JOINT DETAIL
PROPOSED CURB & GUTTER**



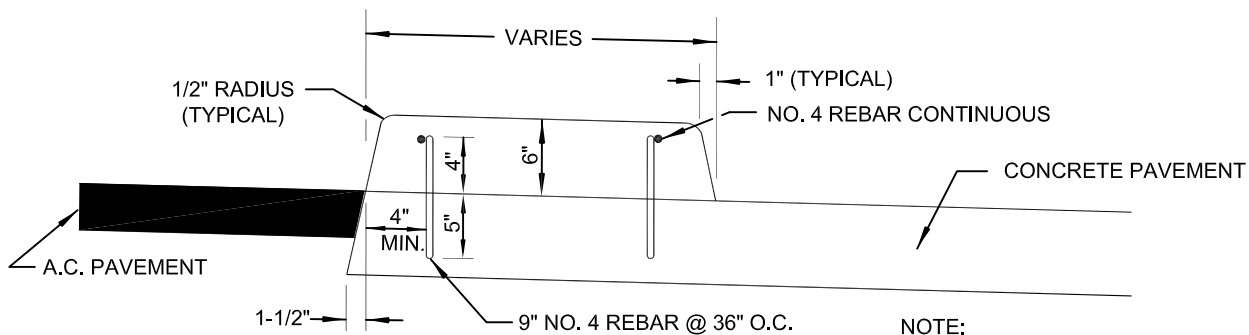
**CURB & GUTTER JOINT DETAIL
EXISTING CURB & GUTTER**



BOXOUT DETAIL



THICKENED EDGE DETAIL



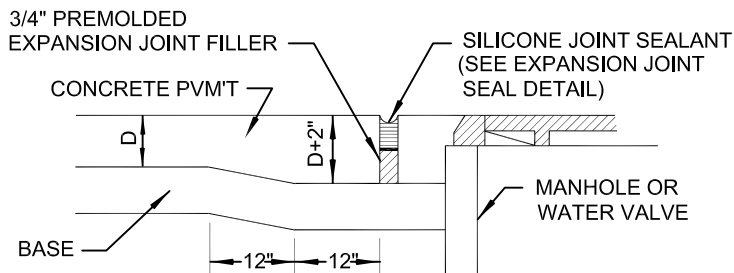
TACK ON CONCRETE ISLAND DETAIL

NOTE:

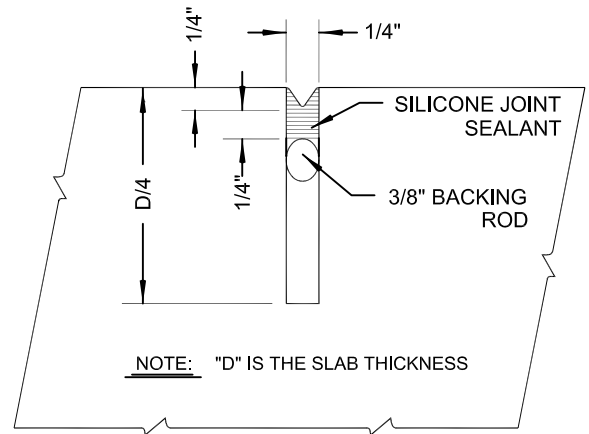
TRANSVERSE WEAKENED PLANE JOINTS TO MATCH JOINTS IN CONCRETE PAVEMENT (SEE DETAIL STANDARD DRAWING NO. 234)

Effective 07/01/10 - 12/30/10

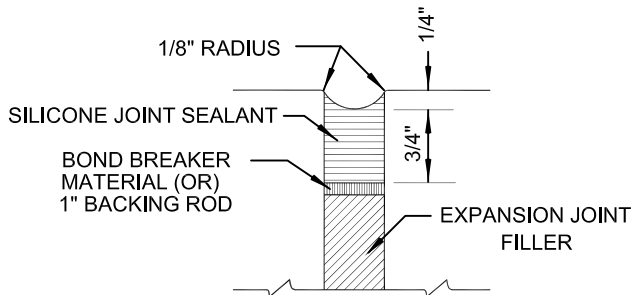
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
409	CONCRETE PAVEMENT	CONCRETE PAVEMENT CONSTRUCTION DETAILS		
501	CONCRETE			
505	REINFORCING STEEL			
TT-S-00153A CLASS A SEALANT				
DATE	DWG. NO.	232	PAGE NO.	38



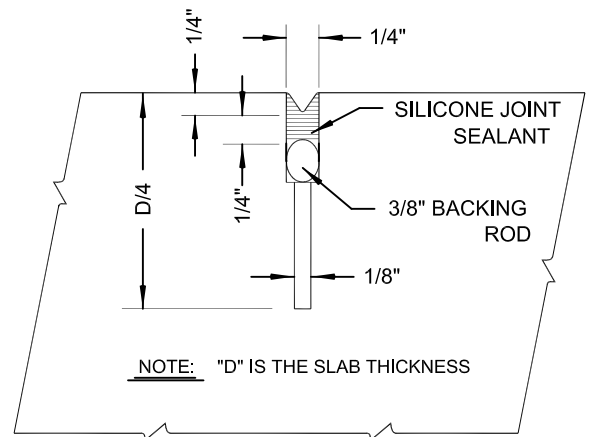
**TYPE "A" EXPANSION JOINT DETAIL
BOXOUT**



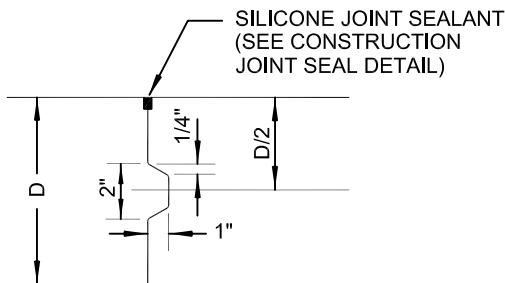
**TYPE "C" WEAKENED PLANE JOINT DETAIL
SINGLE SAW-CUT**



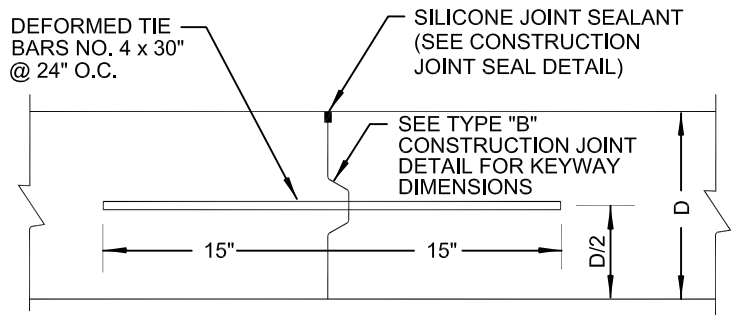
EXPANSION JOINT SEAL DETAIL



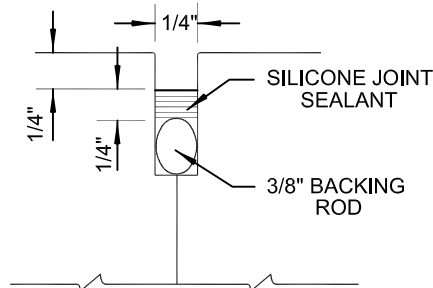
**TYPE "C" WEAKENED PLANE JOINT DETAIL
DOUBLE SAW-CUT**



**TYPE "B" CONSTRUCTION JOINT DETAIL
KEYWAY**



TYPE "D" TIED CONSTRUCTION JOINT DETAIL



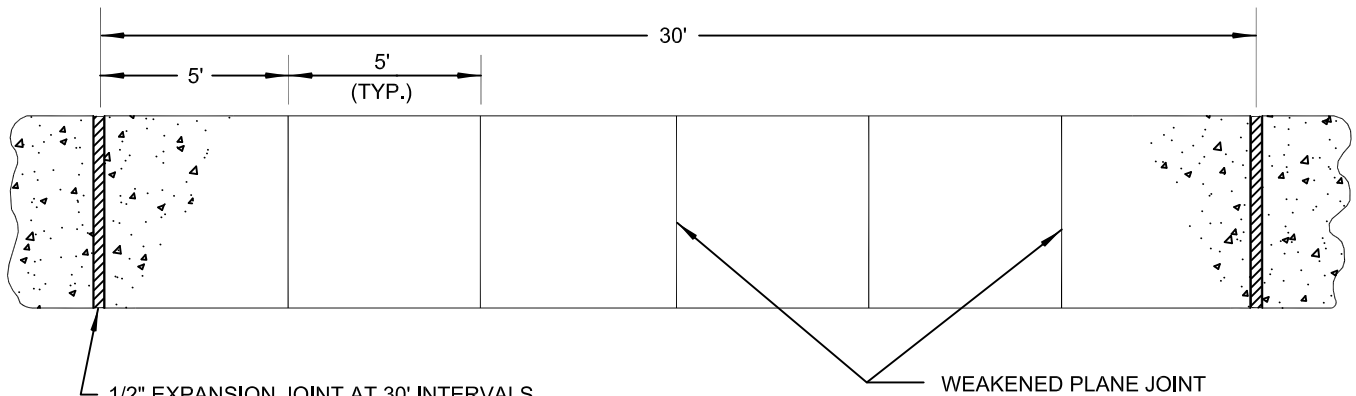
CONSTRUCTION JOINT SEAL DETAIL

SPECIFICATION REFERENCE

409	CONCRETE PAVEMENT
707	JOINT MATERIAL
TT-S-00153A CLASS A SEALANT	

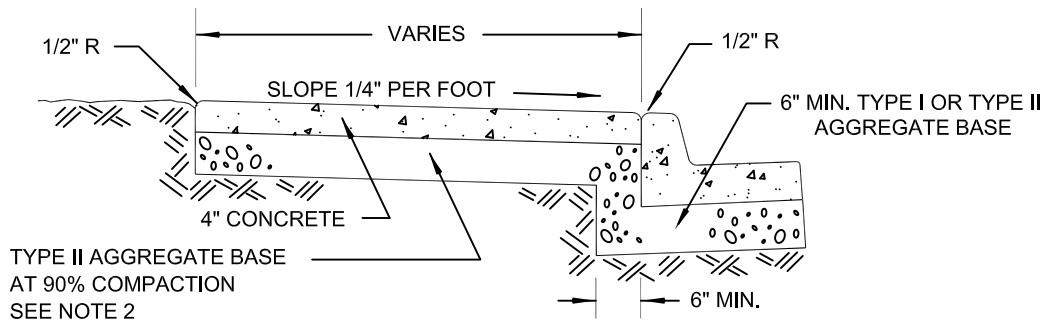
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**CONCRETE PAVEMENT
JOINT DETAILS**

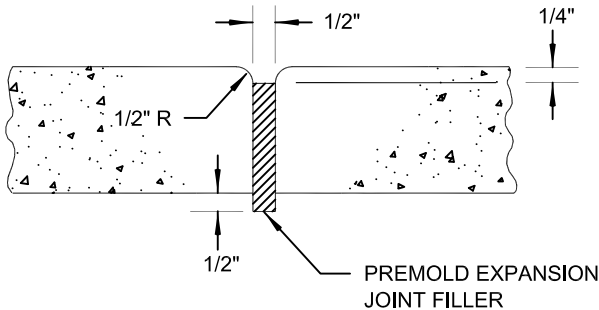


1/2" EXPANSION JOINT AT 30' INTERVALS, AT COLD JOINTS AND AT BEGINNING AND END OF RETURN.
 EXPANSION JOINTS TO MATCH LOCATION MATCH LOCATION OF CURB AND GUTTER EXPANSION JOINT.

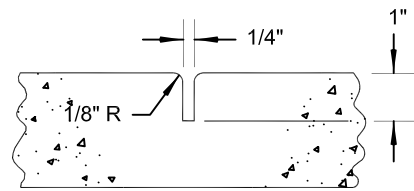
PLAN



TYPICAL SECTION



EXPANSION JOINT



WEAKENED PLANE JOINT

NOTES:

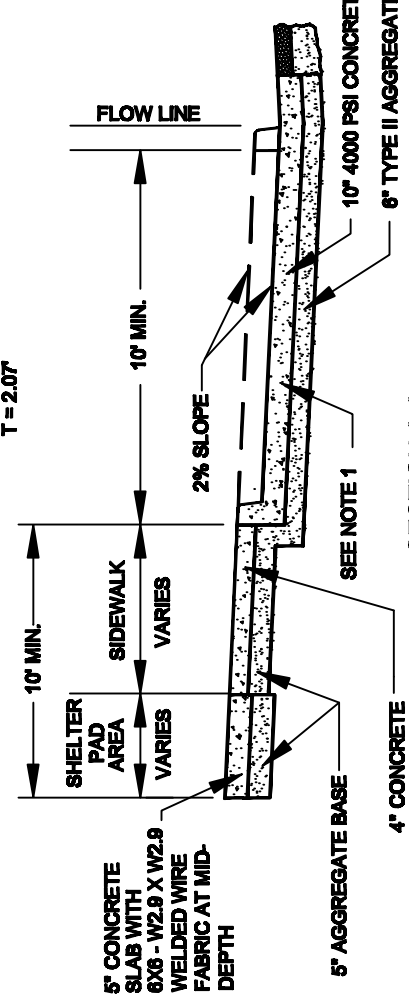
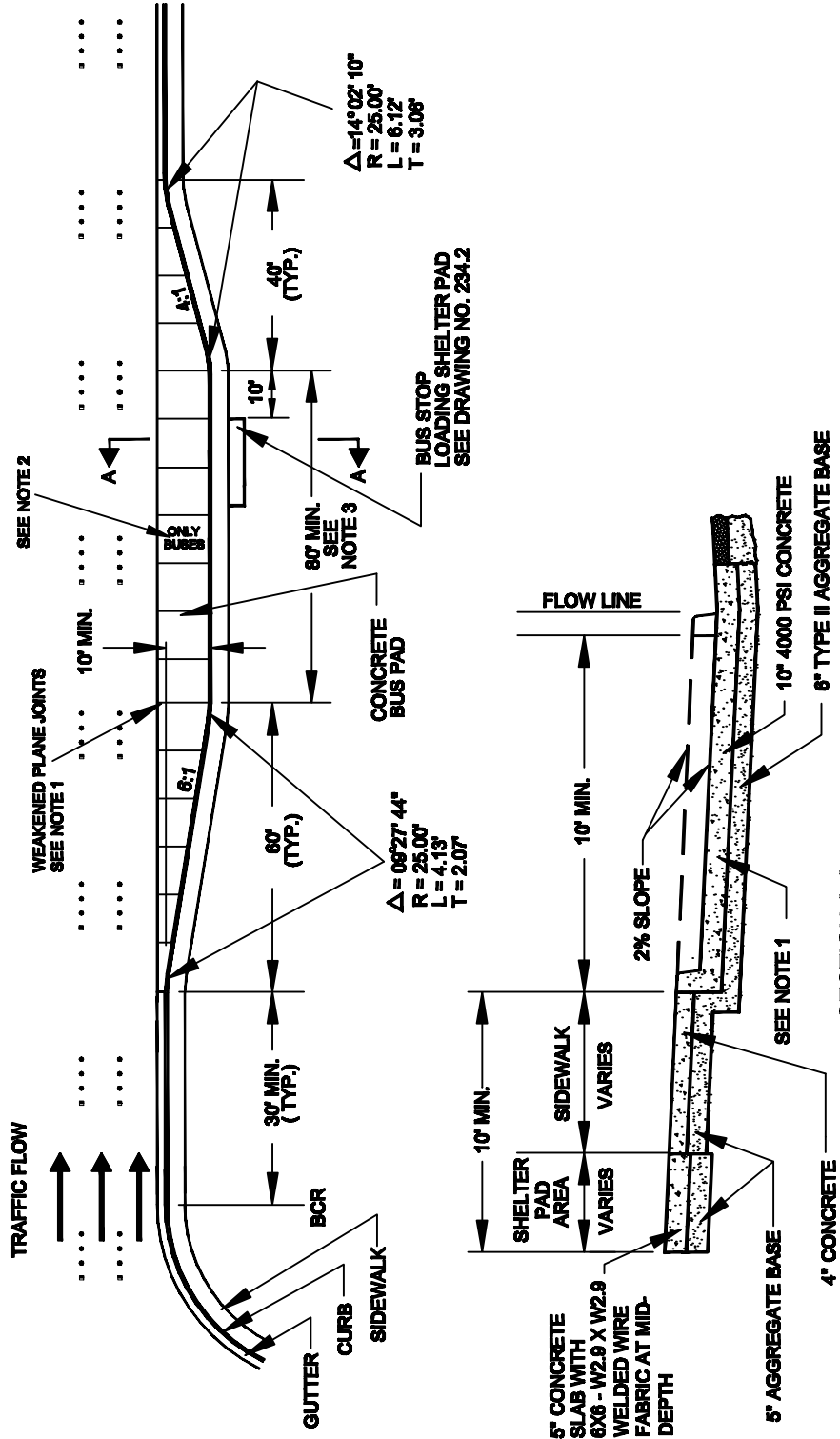
1. ON ALL CURB RETURNS A 1/2" EXPANSION JOINT SHALL BE CONSTRUCTED BETWEEN THE BACK OF CURB AND THE SIDEWALK FOR THE ENTIRE LENGTH OF THE RETURN.
2. THE TYPE II AGGREGATE BASE THICKNESS IS SHOWN ON THE TYPICAL SECTION DRAWINGS 202 - 207.
3. LONGITUDINAL WEAKENED PLANE JOINT REQUIRED AT MIDPOINT OF SIDEWALK 10' OR WIDER.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES
707	JOINT MATERIAL

SIDEWALK

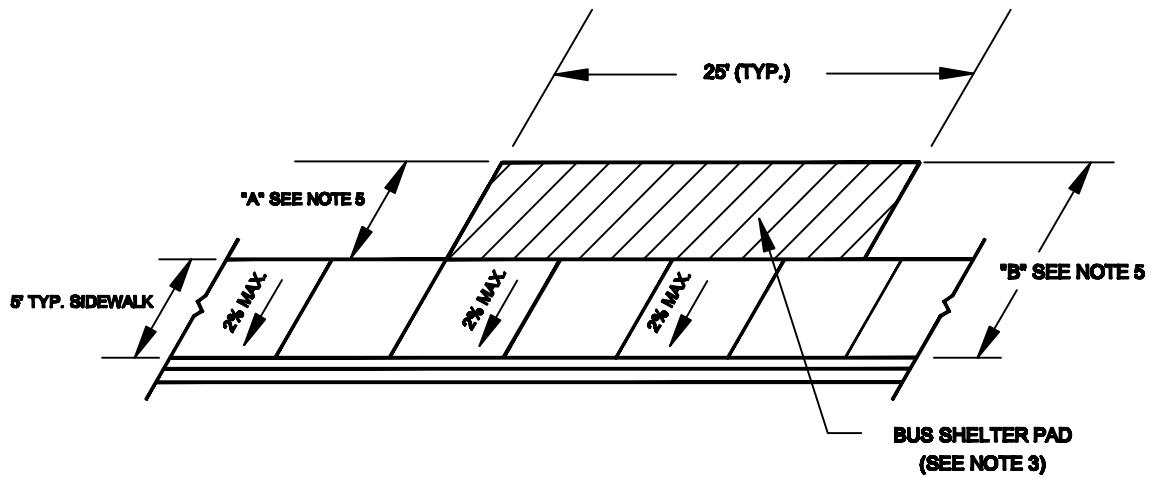


NOTES:

1. CONCRETE BUS PAD SHALL BE MONOLITHIC. TRANSVERSE WEAKENED PLANE JOINTS SHALL BE INSTALLED AT 10' INTERVALS AND AS DETAILED IN STANDARD DRAWING NO. 233, TYPE 'C'.
2. A MINIMUM OF ONE SET OF PAVEMENT MARKINGS CONTAINING THE "BUSES ONLY" SYMBOL SHALL BE PLACED IN THE TURN-OUT AREA. EXACT LOCATION TO BE DETERMINED BY THE ENGINEER.
3. ADDITIONAL STORAGE AREA WILL BE REQUIRED WHEN MORE THAN ONE BUS IS EXPECTED TO OCCUPY THE TURN-OUT AT THE SAME TIME.
4. ALTERNATE CONCRETE AND BASE THICKNESS MAY BE SUBSTITUTED, BUT MUST BE SUPPORTED BY ENGINEERING ANALYSIS AND APPROVED BY THE ENGINEER.
5. TURN-OUT SURFACE SHALL BE TEXTURED IN ACCORDANCE WITH UNIFORM STANDARD SPECIFICATION NO. 408.03.08. FLOW LINE SHALL NOT BE TEXTURED, BUT SHALL BE A TROWELED SURFACE.

SPECIFICATION REFERENCE	
302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES

UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
TYPICAL BUS TURN-OUT		
DATE 6-10-10	DWG. NO.	234.1

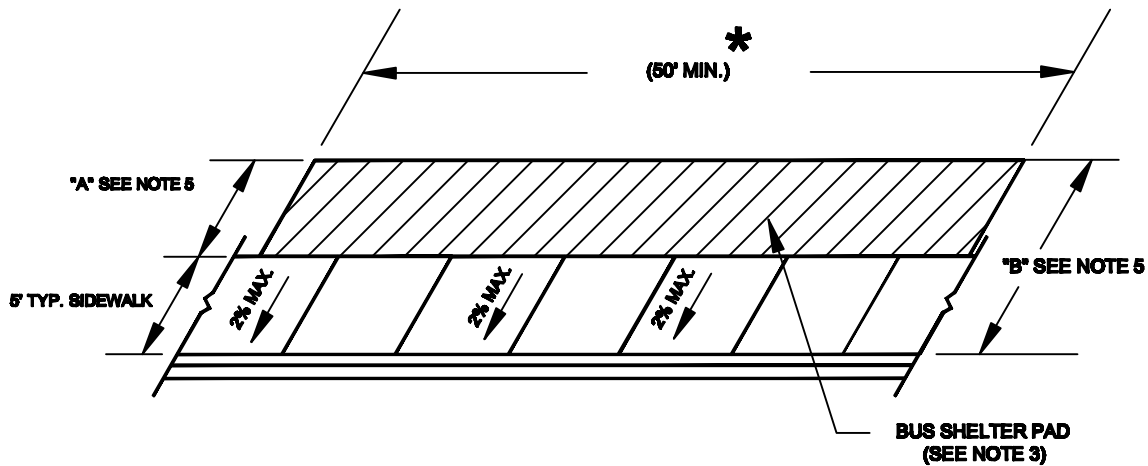


 ADDITIONAL AREA REQUIRED BEHIND TYPICAL 5 FT. SIDEWALK FOR BUS SHELTER PAD

NOTES:

1. SIDEWALK RAMP MAY BE REQUIRED TO BE CONSTRUCTED IN THOSE LOCATIONS WHERE THE BUS STOP WOULD OTHERWISE BE INACCESSIBLE AS DEFINED BY THE AMERICANS WITH DISABILITIES ACT. SEE DRAWING NO. 235, SHEET 4 OF 4 FOR SIDEWALK RAMP DETAILS.
2. ADDITIONAL RIGHT-OF-WAY OR EASEMENT IS REQUIRED FOR BUS SHELTER PAD AND VARIABLE HEIGHT CURB AT BACK OF SIDEWALK RAMP AND SHALL BE DEDICATED TO THE LOCAL ENTITY.
3. CONCRETE FOR BUS SHELTER PAD SHALL BE 5 INCHES THICK SLAB WITH 6X6 - W2.9 X W2.9 WELDED WIRE FABRIC AT MID-POINT AND TYPE II AGGREGATE BASE SHALL BE 5 INCHES THICK.
4. BUS SHELTER PAD CONNECTION TO DETACHED SIDEWALK CONDITION SHALL BE DETERMINED BY THE ENTITIES.
5. "A" = 10', "B" = 15' UNLESS BUS TURNOUT IS CONSTRUCTED PER STANDARD DRAWINGS 234.1 OR 234.3, THEN "A" = 5', "B" = 10'.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	TYPICAL BUS STOP PASSENGER LOADING AND SHELTER PADS	
501	CONCRETE		
502	CONCRETE STRUCTURES		
		DATE 6-10-10	DWG. NO. 234.2



 ADDITIONAL AREA REQUIRED BEHIND TYPICAL 5 FT. SIDEWALK FOR BUS SHELTER PAD

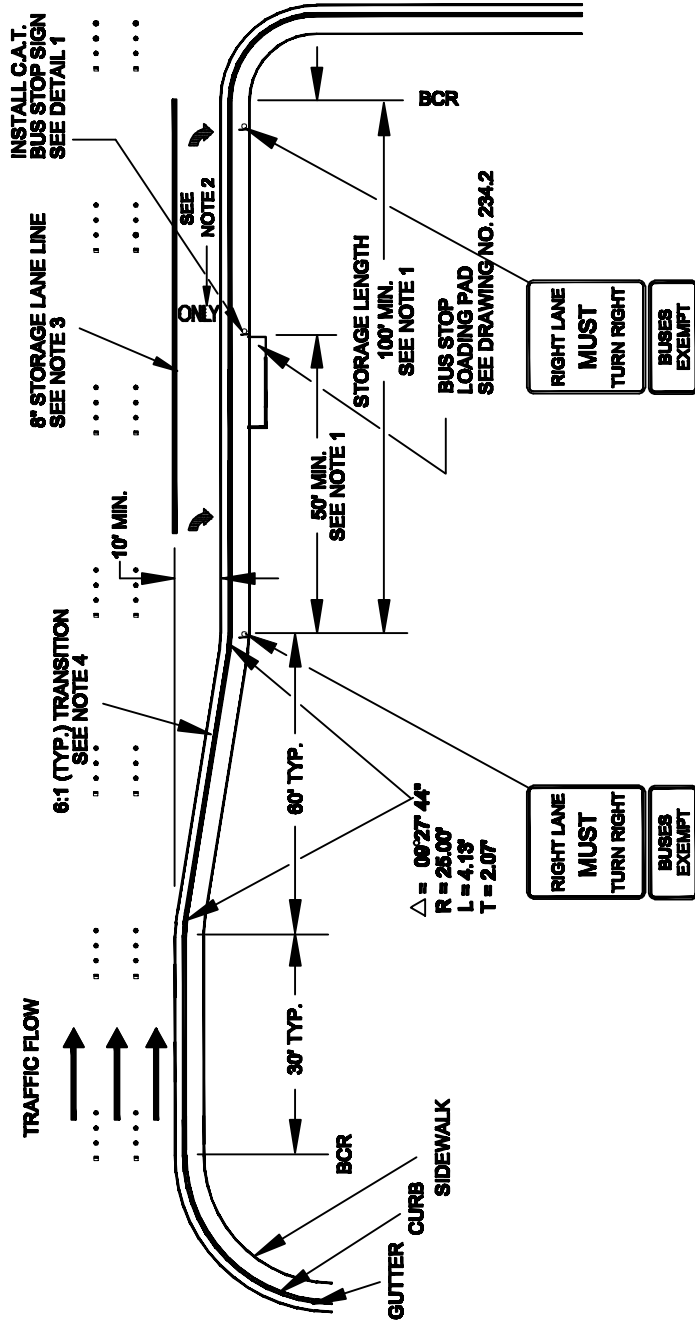
 ADDITIONAL 25 FEET MAY BE REQUIRED BY RTC.

NOTES:

1. SIDEWALK RAMP MAY BE REQUIRED TO BE CONSTRUCTED IN THOSE LOCATIONS WHERE THE BUS STOP WOULD OTHERWISE BE INACCESSIBLE AS DEFINED BY THE AMERICANS WITH DISABILITIES ACT. SEE DRAWING NO. 235, SHEET 4 OF 4 FOR SIDEWALK RAMP DETAILS.
2. ADDITIONAL RIGHT-OF-WAY OR EASEMENT IS REQUIRED FOR BUS SHELTER PAD AND VARIABLE HEIGHT CURB AT BACK OF SIDEWALK RAMP AND SHALL BE DEDICATED TO THE LOCAL ENTITY.
3. CONCRETE FOR BUS SHELTER PAD SHALL BE 5 INCHES THICK SLAB WITH 6X6 - W2.9 X W2.9 WELDED WIRE FABRIC AT MID-POINT AND TYPE II AGGREGATE BASE SHALL BE 5 INCHES THICK.
4. BUS SHELTER PAD CONNECTION TO DETACHED SIDEWALK CONDITION SHALL BE DETERMINED BY THE ENTITIES.
5. "A" = 10', "B" = 15' UNLESS BUS TURNOUT IS CONSTRUCTED PER STANDARD DRAWINGS 234.1 OR 234.3, THEN "A" = 5', "B" = 10'.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	TYPICAL DOUBLE BUS STOP PASSENGER LOADING AND SHELTER PADS	
501	CONCRETE		
502	CONCRETE STRUCTURES		
		DATE 6-10-10	DWG. NO. 234.2A

COMMERCIAL DRIVEWAY INTO PROPERTY



	(ROUTE #)

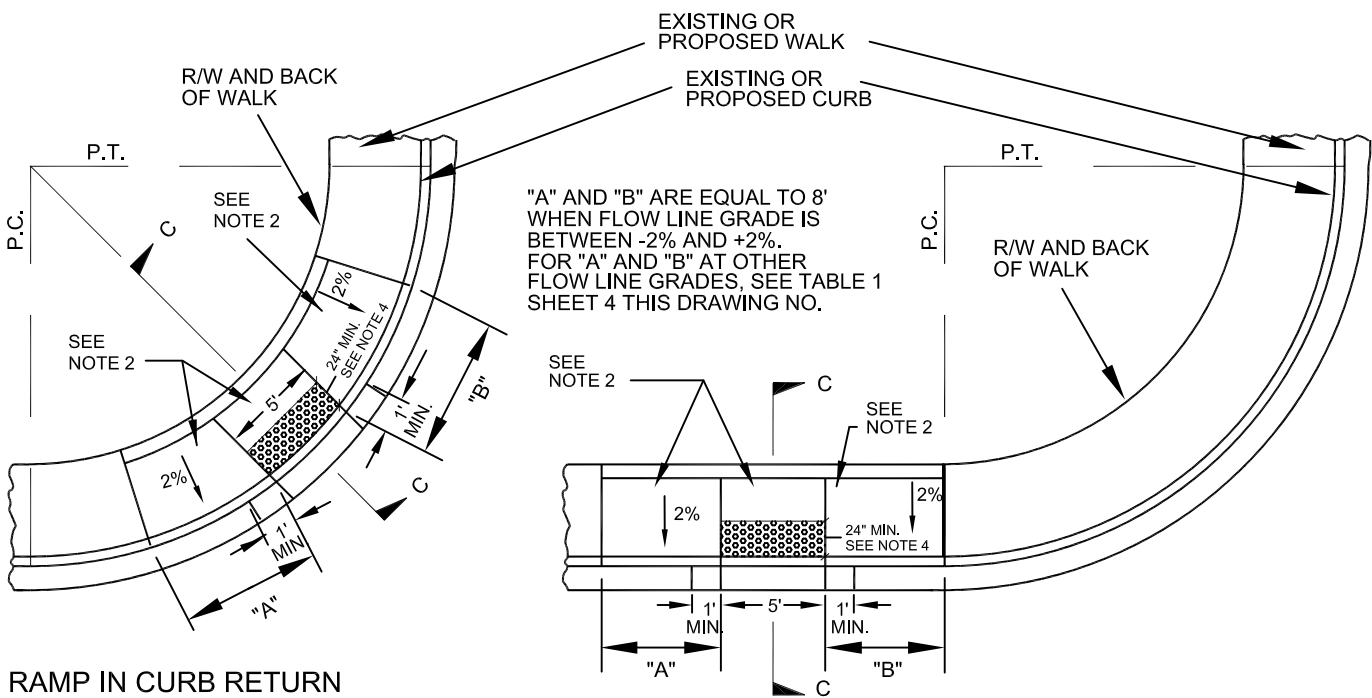
DETAIL 1
BUS ROUTE
SIGN

NOTES:

1. IF ARTICULATED BUSES ARE EXPECTED TO SERVICE BUS STOP, DISTANCE FROM END OF ENTRY TAPER TO THE END OF THE BUS STOP LOADING PAD SHALL BE INCREASED TO 70 FT. MIN. AND THE RIGHT TURN STORAGE LANE LENGTH SHALL BE INCREASED TO 120 FT. MIN.
2. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, INSTALL ARROW AND "ONLY" SYMBOL PAVEMENT MARKINGS FOR THE LENGTH OF THE STORAGE LANE. SYMBOLS SHALL BE APPROVED TYPE II PAVEMENT MARKING FILM.
3. STORAGE LANE LINE SHALL BE APPROVED TYPE I PAVEMENT MARKING FILM, OR IF APPROVED BY THE ENGINEER, RAISED PAVEMENT MARKERS MAY BE USED.
4. REVERSE CURVE TRANSITION MAY BE USED SUBJECT TO THE APPROVAL OF THE ENGINEER.

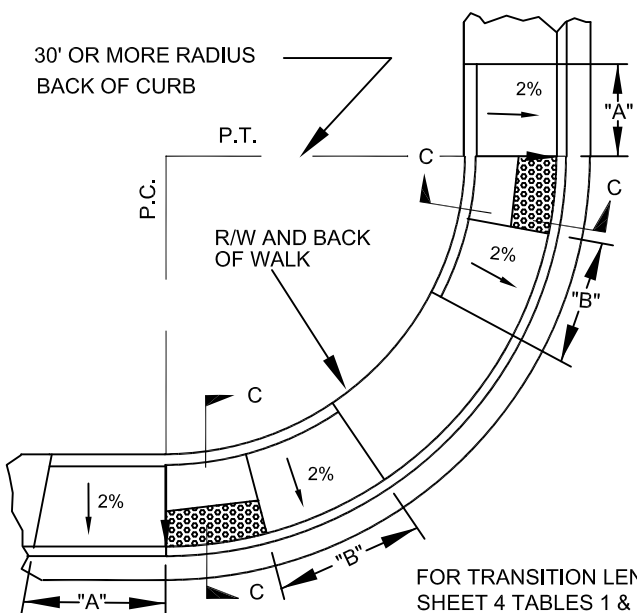
SPECIFICATION REFERENCE	
302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES
628	PAINTING TRAFFIC STRIPING, PAVEMENT MARKINGS...

UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
BUS STOP PLACEMENT WITHIN EXCLUSIVE RIGHT TURN LANE FOR COMMERCIAL PROPERTIES	
DATE 12-08-09	DWG. NO. 234.3

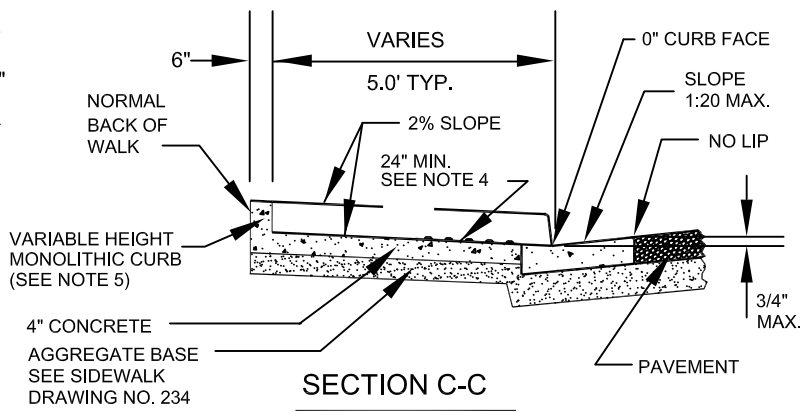


RAMP IN CURB RETURN

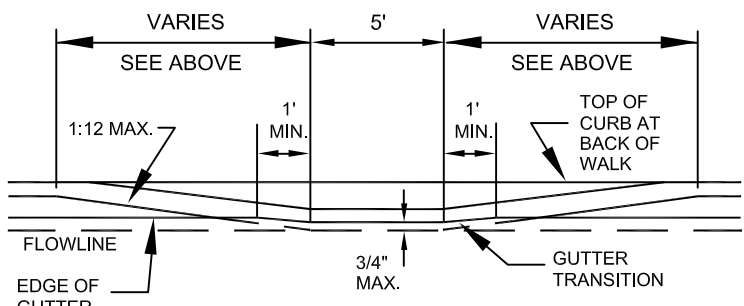
RAMP OUTSIDE CURB RETURN



PAIRED RAMP IN CURB RETURN



SECTION C-C



PROFILE

NOTES:

1. SIDEWALK RAMPS OUTSIDE OF THE CURB RETURN SHALL BE LOCATED ADJACENT TO THE RETURN UNLESS OTHERWISE APPROVED.
2. RAMPS SHALL BE CONSTRUCTED WITH A ROUGH BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
3. WHEN CONSTRUCTING RAMP WHERE CURB & GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB & GUTTER.
4. DETECTABLE WARNING CONSISTING OF RAISED TRUNCATED DOMES WHICH COMPLY WITH DETAILS ON SHEET 4 OF THIS DRAWING NO. AND CONTRASTING VISUALLY WITH ADJOINING SURFACES SHALL BE PLACED ON BOTTOM PORTION OF RAMP EXTENDING THE FULL WIDTH OF THE RAMP AND TO A MINIMUM DEPTH OF 24 INCHES. PAVER BLOCKS PERMITTED ONLY IN THE CITY OF BOULDER CITY FOR DETECTABLE WARNING AREAS.
5. CURB MAY BE PLACED AND IS PREFERRED BEHIND BACK OF WALK IF SUFFICIENT RIGHT-OF-WAY OR EASEMENTS EXIST AND AS APPROVED BY THE ENGINEER.

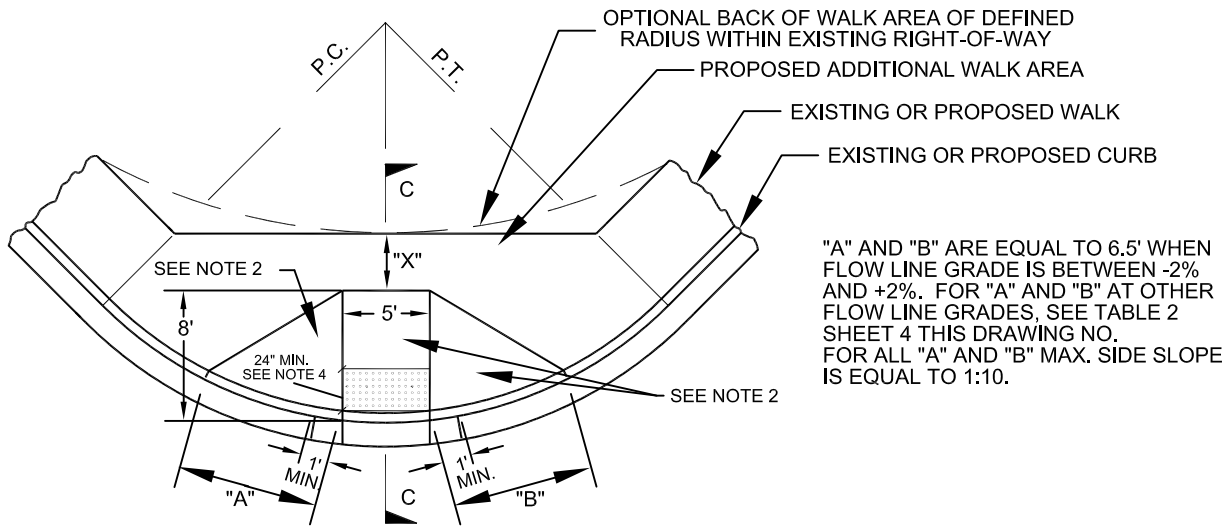
SPECIFICATION REFERENCE

302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES

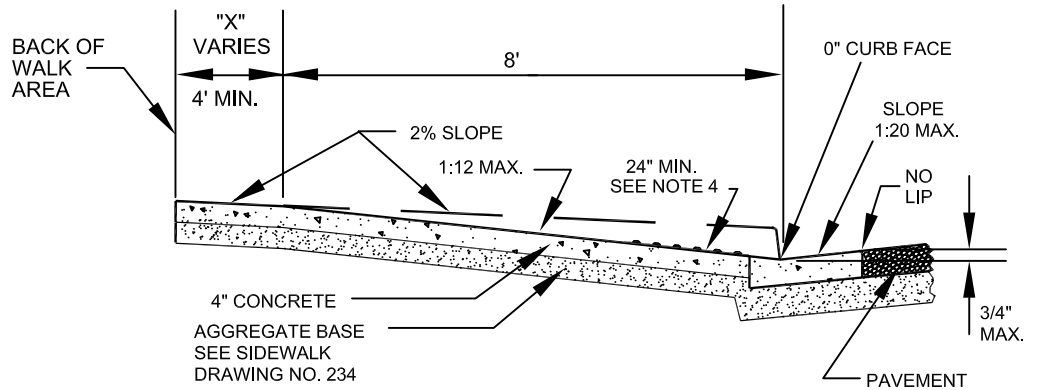
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**SIDEWALK RAMP
CASE I**

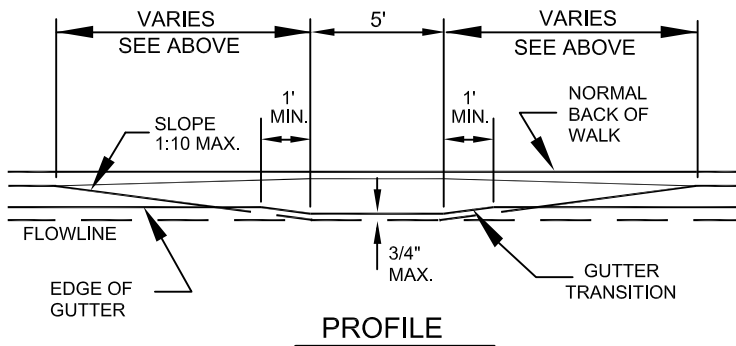
Effective 07/01/10 - 12/30/10



RAMP IN CURB RETURN
(NO BACK OF WALK DEPRESSION)



SECTION C-C



PROFILE

CASE II SHALL BE USED WHERE R/W AND FIELD CONDITIONS PERMIT.

NOTES:

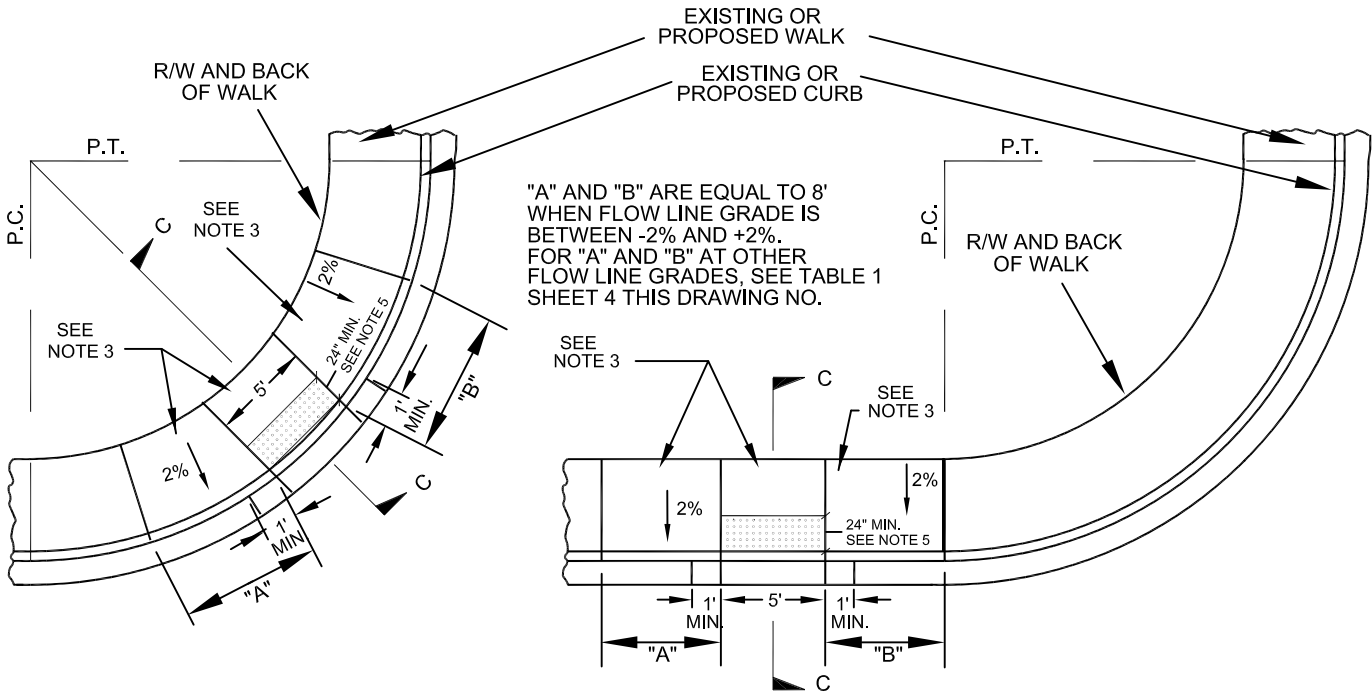
1. SIDEWALK RAMP WITHIN CURB RETURN SHALL BE LOCATED AT THE MIDPOINT OF CURB RETURN UNLESS OTHERWISE APPROVED.
2. RAMPS SHALL BE CONSTRUCTED WITH A ROUGH BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
3. WHEN CONSTRUCTING RAMP WHERE CURB & GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB & GUTTER.
4. DETECTABLE WARNING CONSISTING OF RAISED TRUNCATED DOMES WHICH COMPLY WITH DETAILS ON SHEET 4 OF THIS DRAWING NO. AND CONTRASTING VISUALLY WITH ADJOINING SURFACES SHALL BE PLACED ON BOTTOM PORTION OF RAMP EXTENDING THE FULL WIDTH OF THE RAMP AND TO A MINIMUM DEPTH OF 24 INCHES. PAVER BLOCKS PERMITTED ONLY IN THE CITY OF BOULDER CITY FOR DETECTABLE WARNING AREAS.

SPECIFICATION REFERENCE

302	AGGREGATE BASE
501	CONCRETE
502	CONCRETE STRUCTURES

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

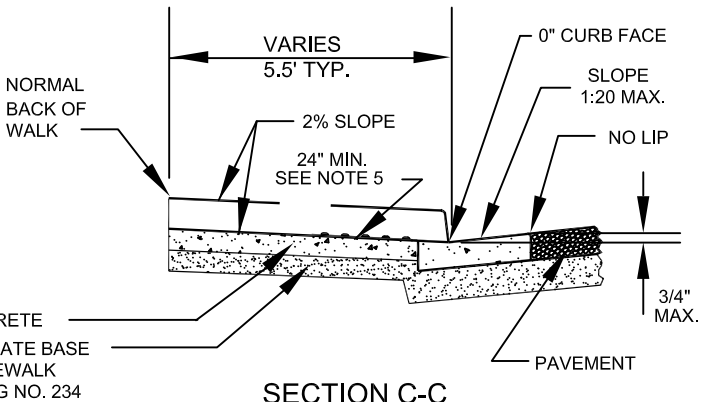
**SIDEWALK RAMP
CASE II**



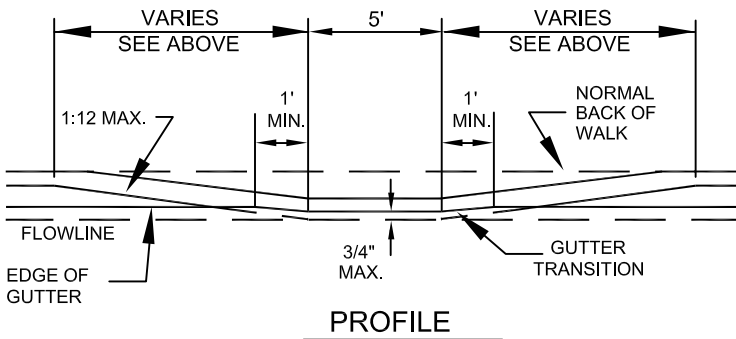
RAMP IN CURB RETURN

RAMP OUTSIDE CURB RETURN

"A" AND "B" ARE EQUAL TO 8' WHEN FLOW LINE GRADE IS BETWEEN -2% AND +2%. FOR "A" AND "B" AT OTHER FLOW LINE GRADES, SEE TABLE 1 SHEET 4 THIS DRAWING NO.



SECTION C-C



PROFILE

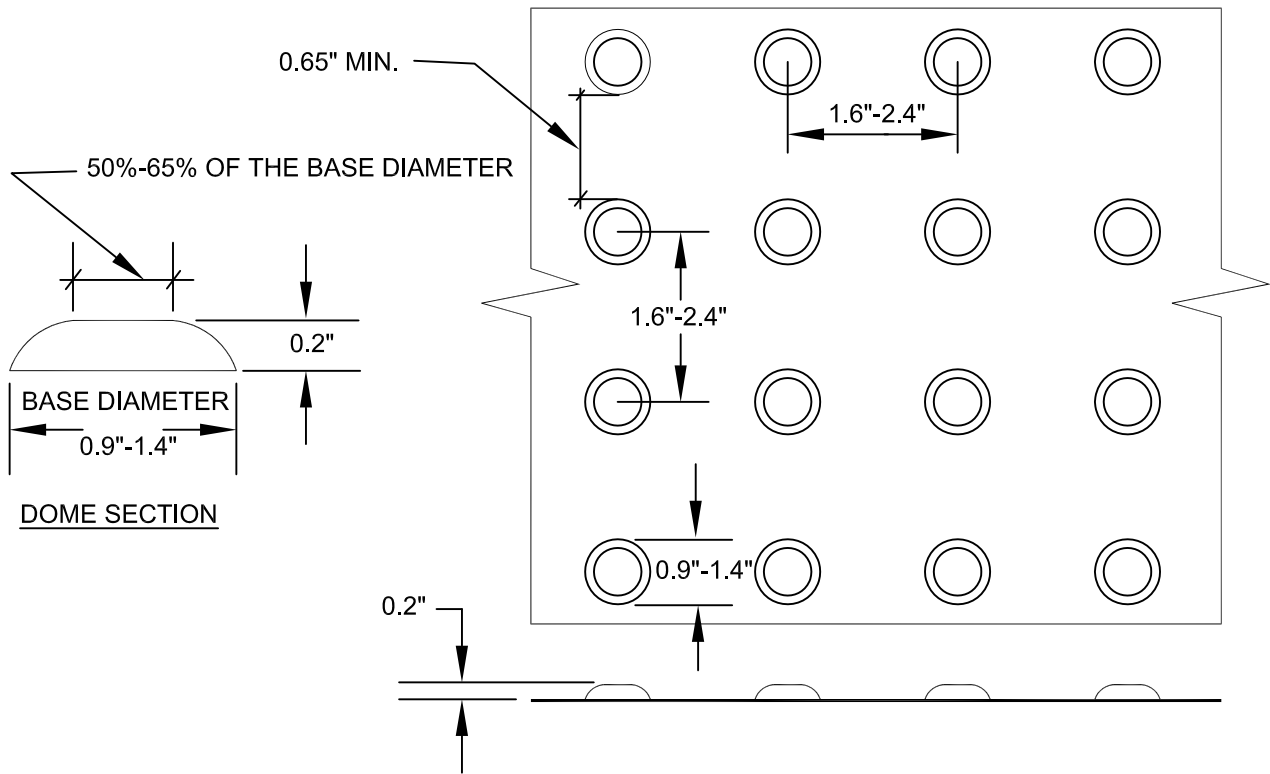
CASE III TO BE USED FOR AREAS WHERE OBSTRUCTION (I.E. BLOCK WALL) EXISTS AT BACK OF WALK ONLY WHEN APPROVED BY THE ENGINEER.

NOTES:

1. SIDEWALK RAMP WITHIN CURB RETURN SHALL BE LOCATED AT THE MIDPOINT OF CURB RETURN UNLESS OTHERWISE APPROVED.
2. SIDEWALK RAMPS OUTSIDE OF THE CURB RETURN SHALL BE LOCATED ADJACENT TO THE RETURN UNLESS OTHERWISE APPROVED.
3. RAMPS SHALL BE CONSTRUCTED WITH A ROUGH BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
4. WHEN CONSTRUCTING RAMP WHERE CURB & GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB & GUTTER.
5. DETECTABLE WARNING CONSISTING OF RAISED TRUNCATED DOMES WHICH COMPLY WITH DETAILS ON SHEET 4 OF THIS DRAWING NO. AND CONTRASTING VISUALLY WITH ADJOINING SURFACES SHALL BE PLACED ON BOTTOM PORTION OF RAMP EXTENDING THE FULL WIDTH OF THE RAMP AND TO A MINIMUM DEPTH OF 24 INCHES. PAVER BLOCKS PERMITTED ONLY IN THE CITY OF BOULDER CITY FOR DETECTABLE WARNING AREAS.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	SIDEWALK RAMP CASE III	
501	CONCRETE		
502	CONCRETE STRUCTURES		
		DATE 11-10-04	DWG. NO. 235 (3 OF 4) PAGE 41B



DETECTABLE WARNING DETAILS (TRUNCATED DOMES)

GRADE (%) "B" TO "A"	"A" (FT) MIN.	"B" (FT) MIN.
-6 TO -5.01	4.5	21.5
-5 TO -4.01	4.5	15.0
-4 TO -3.01	4.5	12.0
-3 TO -2.01	4.5	9.5
-2 TO 2	8.0	8.0
2.01 TO 3	9.5	4.5
3.01 TO 4	12.0	4.5
4.01 TO 5	15.0	4.5
5.01 TO 6	21.5	4.5

TABLE 1. TRANSITION LENGTHS FOR 1:12 SIDE SLOPES

GRADE (%) "B" TO "A"	"A" (FT) MIN.	"B" (FT) MIN.
-6 TO -5.01	4.0	12.5
-5 TO -4.01	4.0	10.0
-4 TO -3.01	4.0	8.5
-3 TO -2.01	4.0	7.5
-2 TO 2	6.5	6.5
2.01 TO 3	7.5	4.0
3.01 TO 4	8.5	4.0
4.01 TO 5	10.0	4.0
5.01 TO 6	12.5	4.0

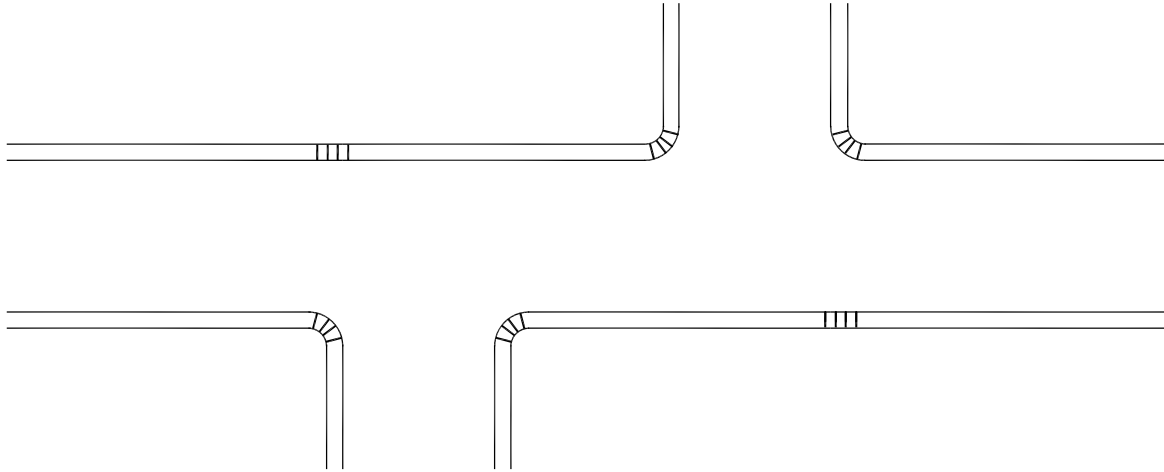
TABLE 2. TRANSITION LENGTHS FOR 1:10 SIDE SLOPES

NOTE:

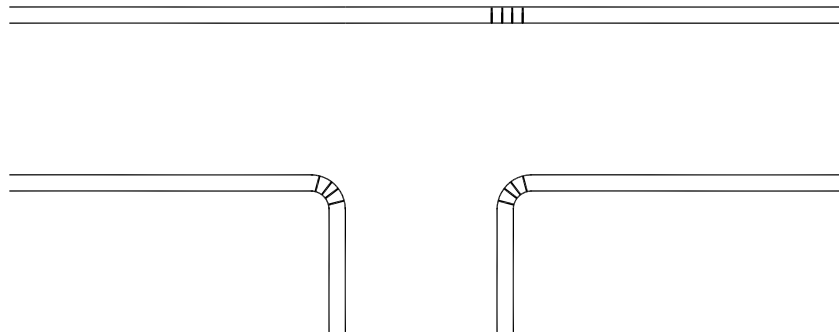
CHARTS APPLY TO CURB WITH 6" CURB FACE. IF CURB HAS GREATER THAN A 6" CURB FACE, A SPECIAL DESIGN IS REQUIRED.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
302	AGGREGATE BASE	SIDEWALK RAMP DETAILS		
501	CONCRETE			
502	CONCRETE STRUCTURES			
		DATE 6-8-06	DWG. NO. 235 (4 OF 4)	PAGE 41C



OFFSET "T"



ISOLATED "T"

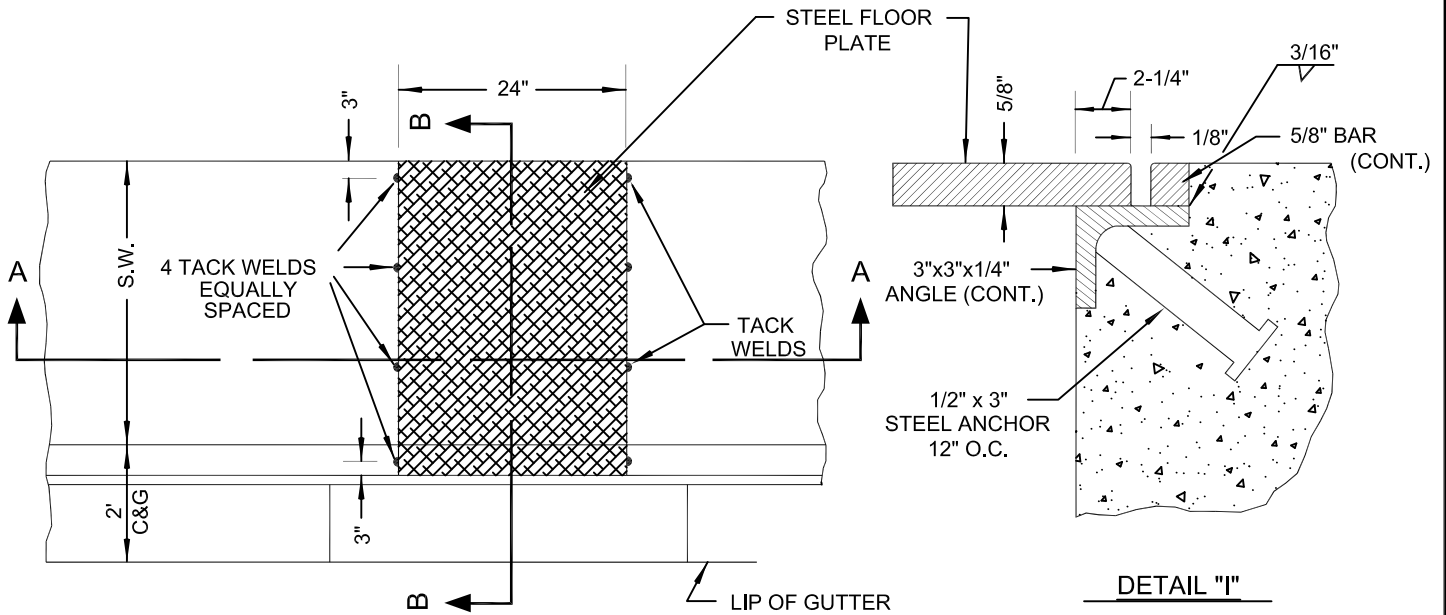
NOTES:

1. THE TYPICAL LOCATIONS OF SIDEWALK RAMPS SHOWN ABOVE ARE INTENDED TO MEET THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA). AT LEAST ONE SIDEWALK RAMP SHALL BE CONSTRUCTED OPPOSITE THE INTERSECTING ROADWAY. ADDITIONAL SIDEWALK RAMPS MAY BE REQUIRED BY THE ENGINEER TO PROVIDE A CONTINUOUS UNOBSTRUCTED PEDESTRIAN CIRCULATION PATH AS DEFINED BY THE ADA.
2. SIDEWALK RAMP LOCATIONS SHOWN ARE FOR INTERSECTIONS WITH UNMARKED CROSSWALKS. IF A PEDESTRIAN CROSSING AREA IS MARKED, SIDEWALK RAMPS SHALL BE LOCATED WITHIN THE MARKED CROSSWALKS AS APPROVED BY THE ENGINEER.

SPECIFICATION REFERENCE

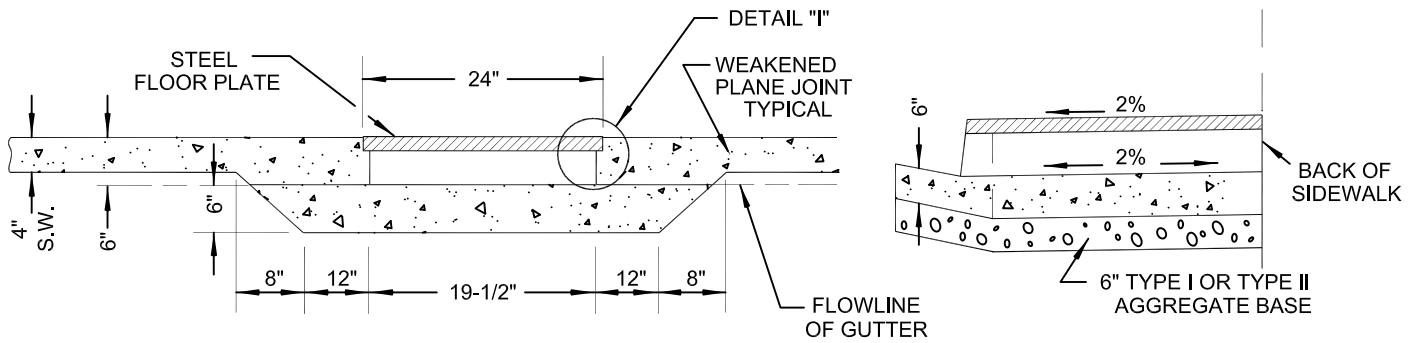
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SIDEWALK RAMP LOCATION
"T" INTERSECTIONS



PLAN

DETAIL "I"



SECTION A-A

SECTION B-B

NOTES:

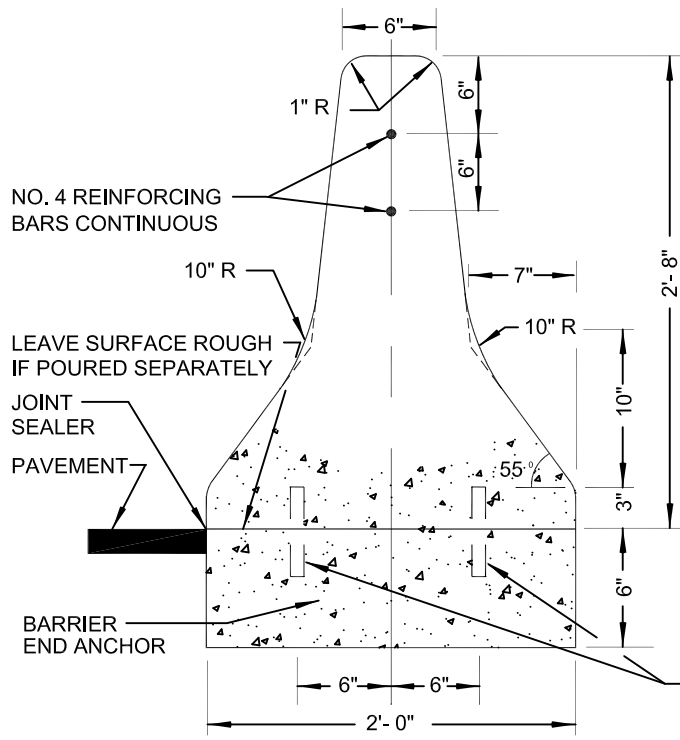
1. IF WIDTH OF PLATE IS GREATER THAN 24", A SPECIAL DESIGN IS REQUIRED.
2. ALL EXPOSED METAL PARTS SHALL BE GALVANIZED AND ALL GALVANIZING DAMAGED BY FABRICATION OR INSTALLATION SHALL RECEIVE TWO COATS OF ALUMINUM PAINT (GALVONOX OR EQUAL).

SPECIFICATION REFERENCE

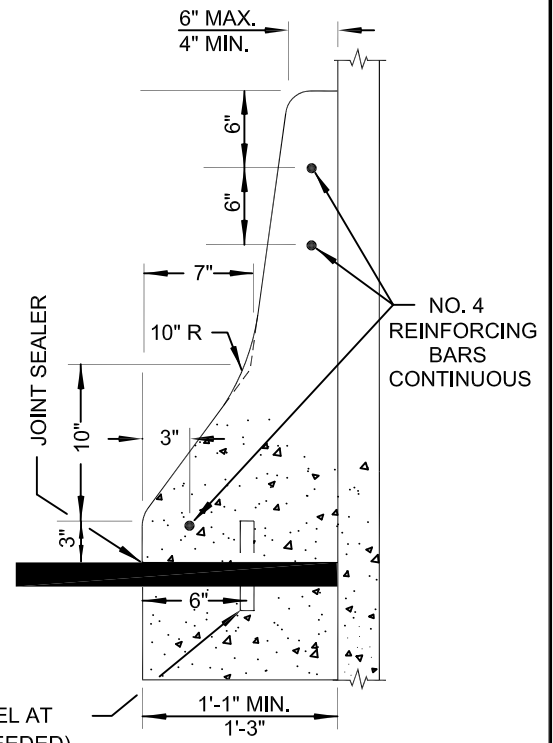
302	BASE AGGREGATES
501	CONCRETE
710	STRUCTURAL STEEL

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

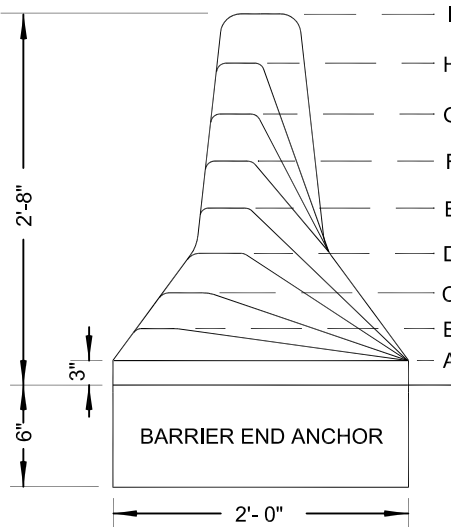
SIDEWALK DRAIN



TYPE A



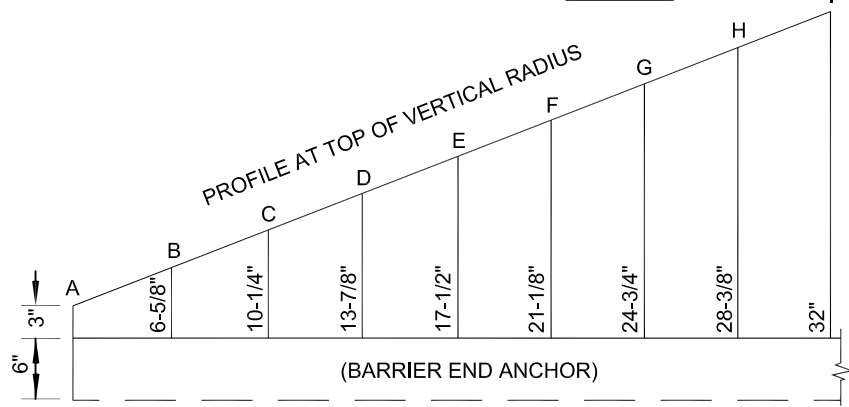
TYPE B



TRANSITION DETAIL

**CONCRETE BARRIER RAIL
FLARE RATES**

OPERATING SPEED	FLARE RATE
60	17:1 MAX
50	14:1
40	11:1



**TRANSITION OF END OF BARRIER
TO BE USED ONLY IF END IS FLARED**

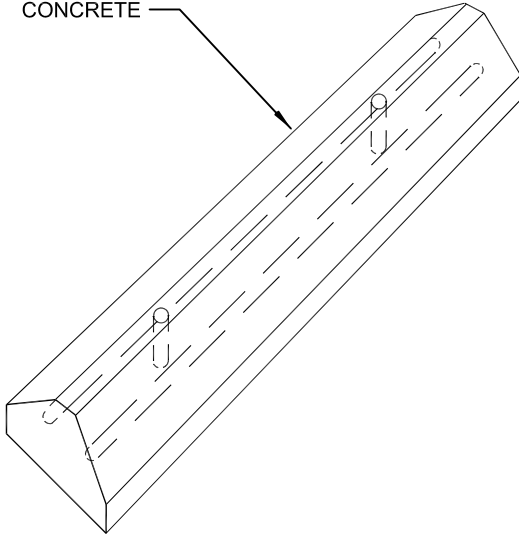
NOTES:

1. TRANSVERSE JOINTS WITH 1" PREMOLDED EXPANSION JOINT FILLER OR 1" OPEN TRANSVERSE JOINTS SHALL BE PLACED AT STRUCTURES. JOINTS IN BARRIER RAIL OVER A STRUCTURE SHALL BE AT THE SAME LOCATION AND OF THE SAME DIMENSION AS THOSE IN THE STRUCTURE.
2. BITUMINOUS PAVING REQUIRED: PAVING SHALL BUTT AGAINST THE BARRIER RAIL END ANCHOR SECTION AND SHALL EXTEND FULL WIDTH UNDER THE NORMAL BARRIER RAIL SECTION PLUS 6" MINIMUM 6-INCH DEEP BARRIER. END ANCHORS SHALL BE CONSTRUCTED IN THE FIRST AND LAST 10 LINEAR FEET OF THE FULL HEIGHT BARRIER RAIL RUN. IF TRANSITIONS ARE USED, THE ANCHOR SHALL BE EXTENDED UNDER THE TRANSITION.

Effective 07/01/10 - 12/30/10

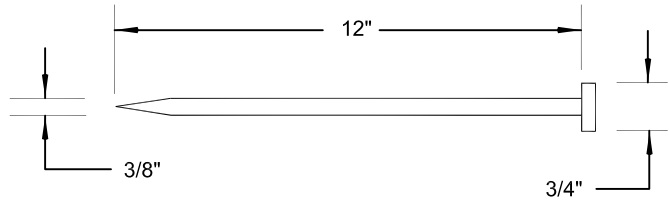
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	CONCRETE BARRIER RAIL		
502	CONCRETE STRUCTURES			
		DATE 12-14-00	DWG. NO. 237	PAGE NO. 43

CONCRETE



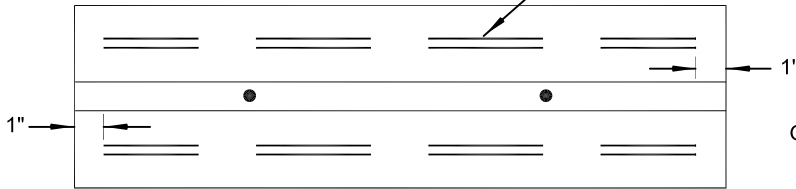
NOTES:

1. PRECAST BUMPER BLOCK TO BE USED IN PARKING LOTS ONLY.
2. GROUT HOLE BEFORE DRIVING SPIKE. AFTER DRIVING SPIKE, FILL HOLE WITH CONCRETE MORTAR AND FINISH FLUSH WITH TOP.

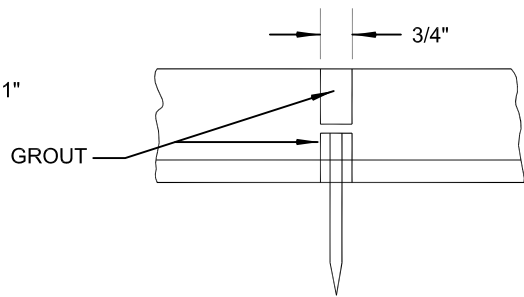


STEEL WIRE BRIDGE SPIKE

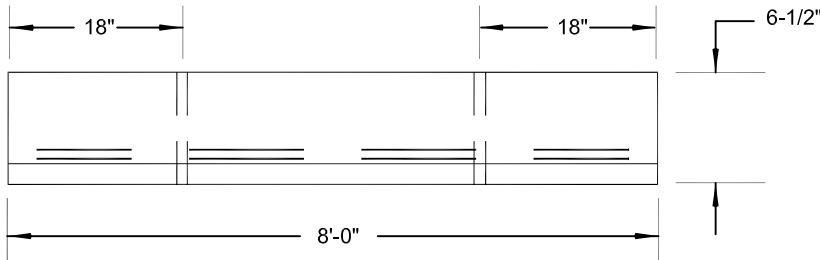
1/2" DEFORMED BAR TO STAY 1" MIN. INSIDE CONCRETE



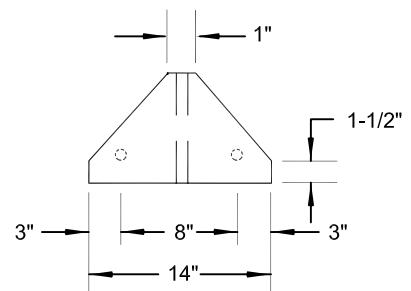
TOP VIEW



HOLE DETAIL



SIDE VIEW



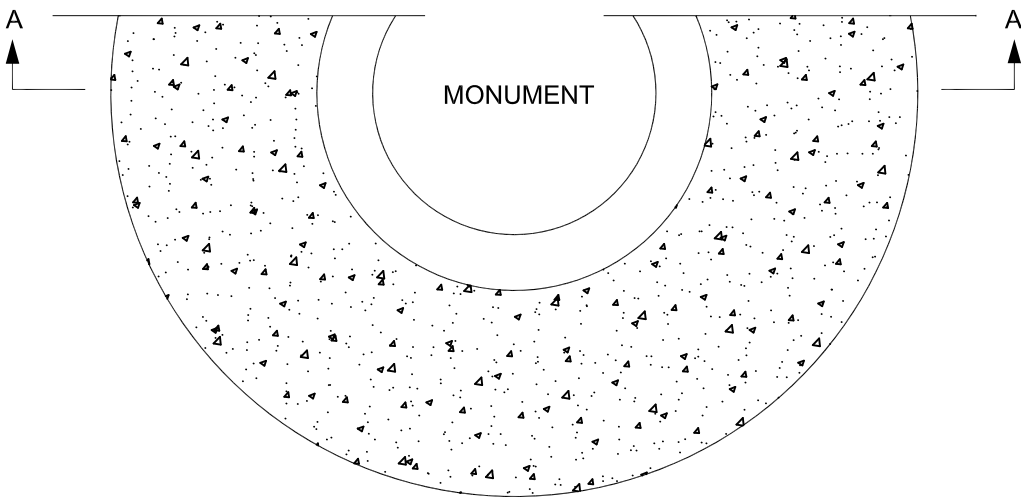
END VIEW

SPECIFICATION REFERENCE

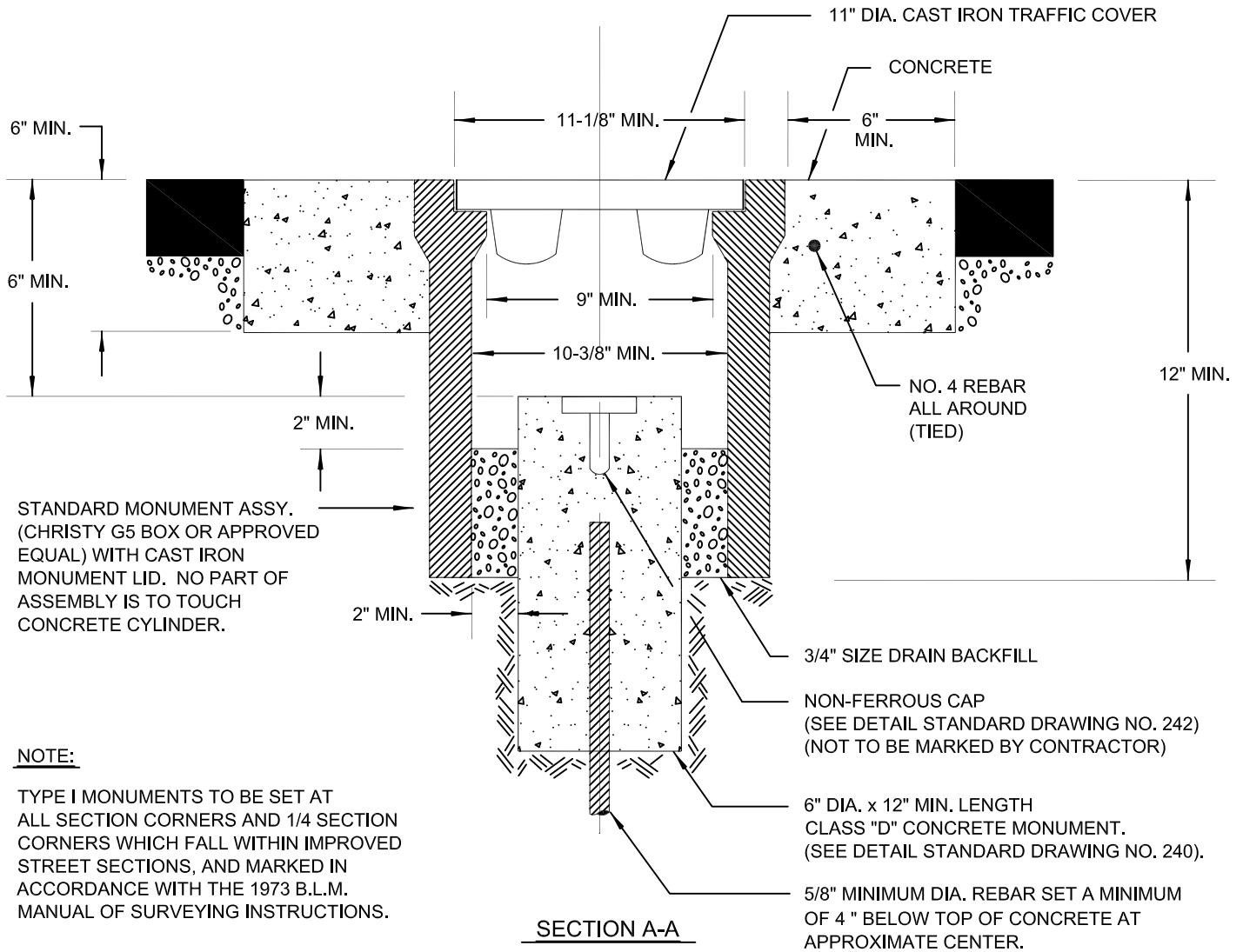
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501	CONCRETE
505	REINFORCING STEEL

PRECAST BUMPER BLOCK



PLAN



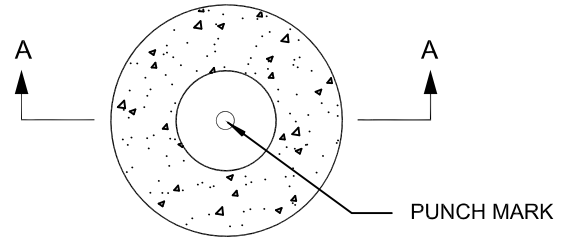
NOTE:

TYPE I MONUMENTS TO BE SET AT ALL SECTION CORNERS AND 1/4 SECTION CORNERS WHICH FALL WITHIN IMPROVED STREET SECTIONS, AND MARKED IN ACCORDANCE WITH THE 1973 B.L.M. MANUAL OF SURVEYING INSTRUCTIONS.

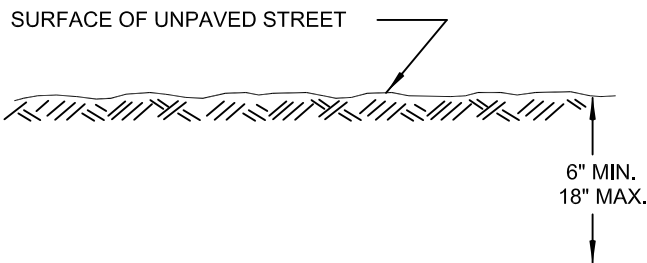
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	TYPE I MONUMENT		
621	MONUMENTS			
704	BASE AGGREGATE			
		DATE 12-14-00	DWG. NO. 239	PAGE NO. 45

NOTES:

1. TYPE II-A MONUMENTS TO BE SET AT ALL SECTION CORNERS, 1/4 SECTION CORNERS AND 1/16 SECTION CORNERS WHICH FALL WITHIN UNIMPROVED STREET SECTIONS.
2. TYPE II-B MONUMENTS TO BE SET AT ALL 1/16 SECTION CORNERS WHICH FALL WITHIN IMPROVED STREET SECTIONS.
3. ALL TYPE II MONUMENTS ARE TO BE MARKED IN ACCORDANCE WITH THE 1973 B.L.M. MANUAL OF SURVEYING INSTRUCTIONS.
4. 6" x 6" SQUARE MONUMENTS ARE ALSO ACCEPTABLE.
5. IF MONUMENTS ARE TO BE "PRECAST" THEY ARE TO BE EMBEDDED IN FRESH CONCRETE TO PREVENT MOVEMENT.
6. THE COUNTY/CITY SURVEYOR MAY REQUIRE TYPE II MONUMENTS IN ADDITIONAL LOCATIONS.



PLAN



12" MIN.

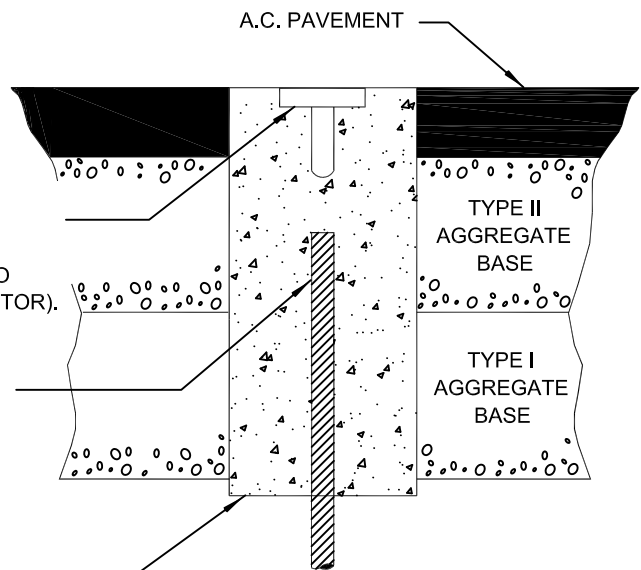
6" MIN.
18" MAX.

BRONZE OR BRASS CAP
(SEE DETAIL STANDARD
DRAWING NO. 242, NOT TO
BE MARKED BY CONTRACTOR).

5/8" MIN. DIA. REBAR
SET A MIN. OF 4" BELOW
TOP OF CONCRETE AT
APPROXIMATE CENTER.

CONCRETE

SECTION A-A
TYPE II-A
UNPAVED STREET



A.C. PAVEMENT

TYPE II
AGGREGATE
BASE

TYPE I
AGGREGATE
BASE

SECTION A-A
TYPE II-B
PAVED STREET

SPECIFICATION REFERENCE

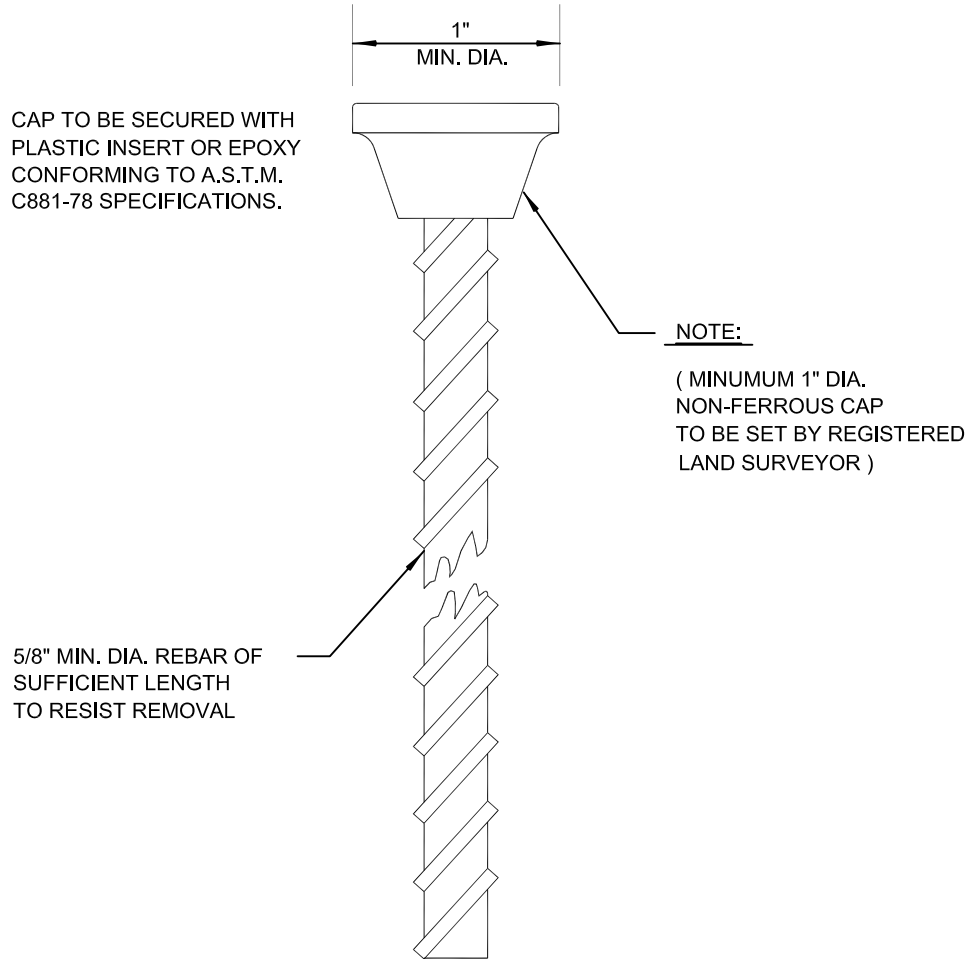
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501	CONCRETE
621	MONUMENTS

TYPE II MONUMENT

NOTES:

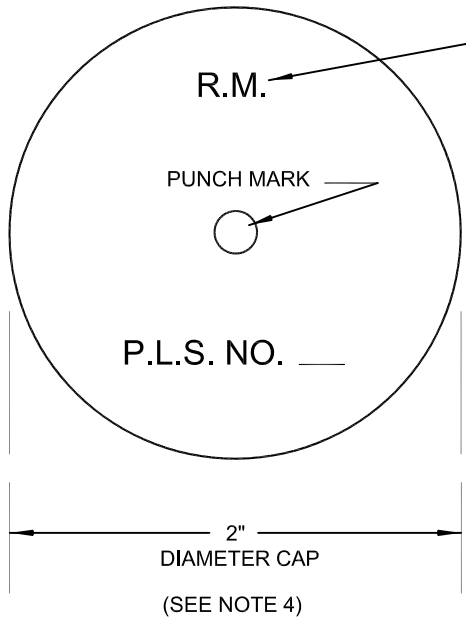
1. TYPE III MONUMENTS TO BE SET AT ALL CENTERLINE CONTROL POINTS NOT OTHERWISE IDENTIFIED BY A TYPE I OR TYPE II MONUMENT, INCLUDING STREET INTERSECTIONS, POINTS OF CURVATURE, POINTS OF TANGENCY, POINTS OF INTERSECTION AND CENTERS OF HAMMERHEAD TURNAROUNDS OR CIRCULAR CUL-DE-SACS.
2. THE REGISTERED LAND SURVEYOR'S NUMBER, AND A PUNCH MARK ARE TO APPEAR ON THE SURFACE OF THE CAP.



TYPE III MONUMENT

Effective 07/01/10 - 12/30/10

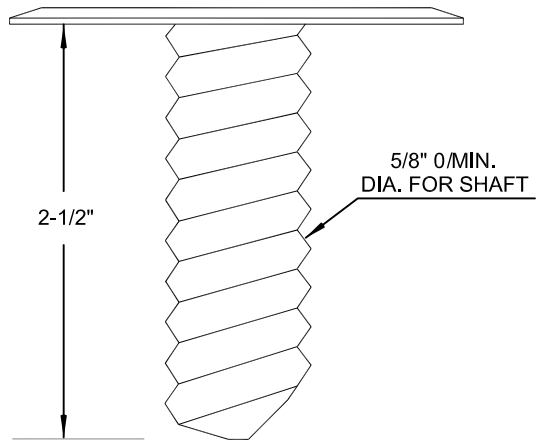
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
621	MONUMENTS	TYPE III MONUMENT				
		DATE	DWG. NO.	241	PAGE NO.	47



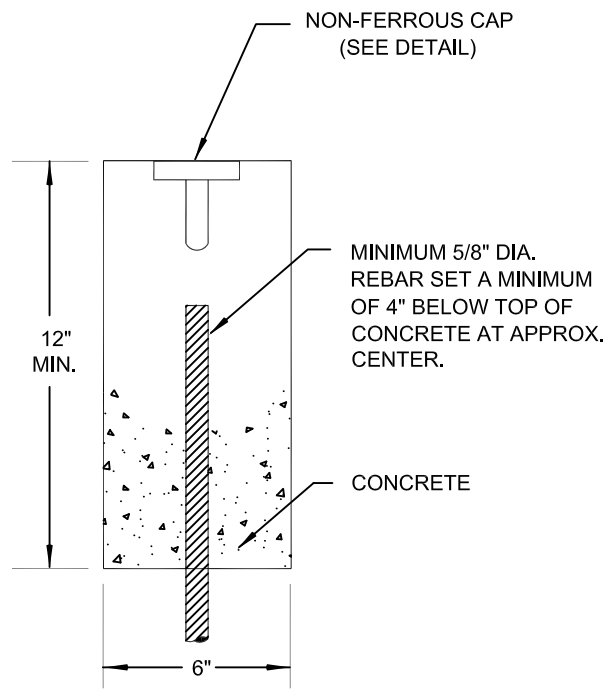
SEE NOTE 2

NOTES:

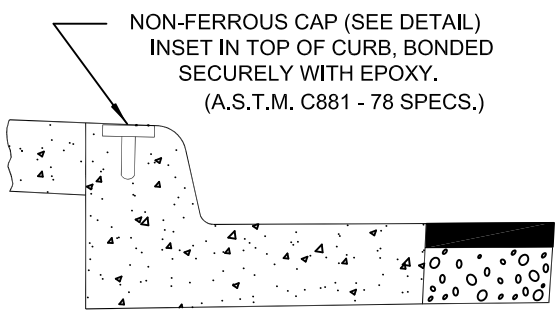
1. FOUR (4) TYPE IV REFERENCE MONUMENTS TO BE SET WITHIN A RADIUS OF TWENTY (20) TO ONE HUNDRED (100) FEET FROM ALL TYPE I, II, AND III MONUMENTS.
2. THE TIE DISTANCE AND THE INITIALS R.M. ARE TO BE STAMPED ON THE CAP, FOR TYPE IV MONUMENTS.
3. NON-FERROUS CAP TO BE MADE FROM CAST VIRGIN METAL IN ONE PIECE, FREE FROM CASTING IMPERFECTIONS, WITH CORRUGATED SHAFT.
4. TYPE III AND TYPE IV MONUMENT CAP DIAMETER MAY BE REDUCED TO 1".



DETAIL
STANDARD CAP



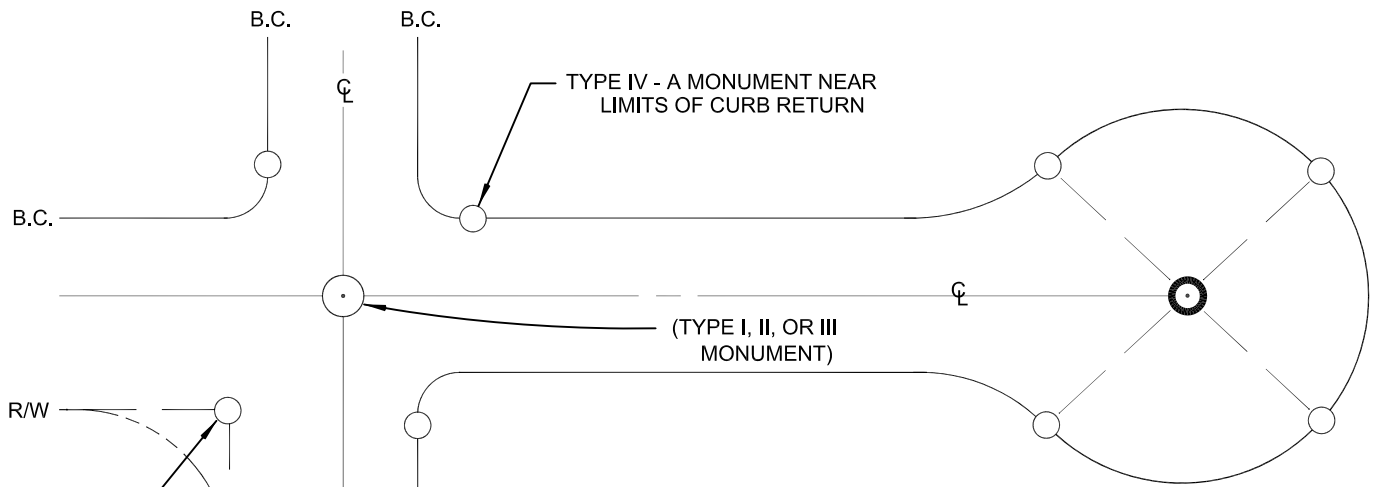
TYPE IV-B MONUMENT
NO CURB & GUTTER



TYPE IV-A MONUMENT
EXISTING CURB & GUTTER

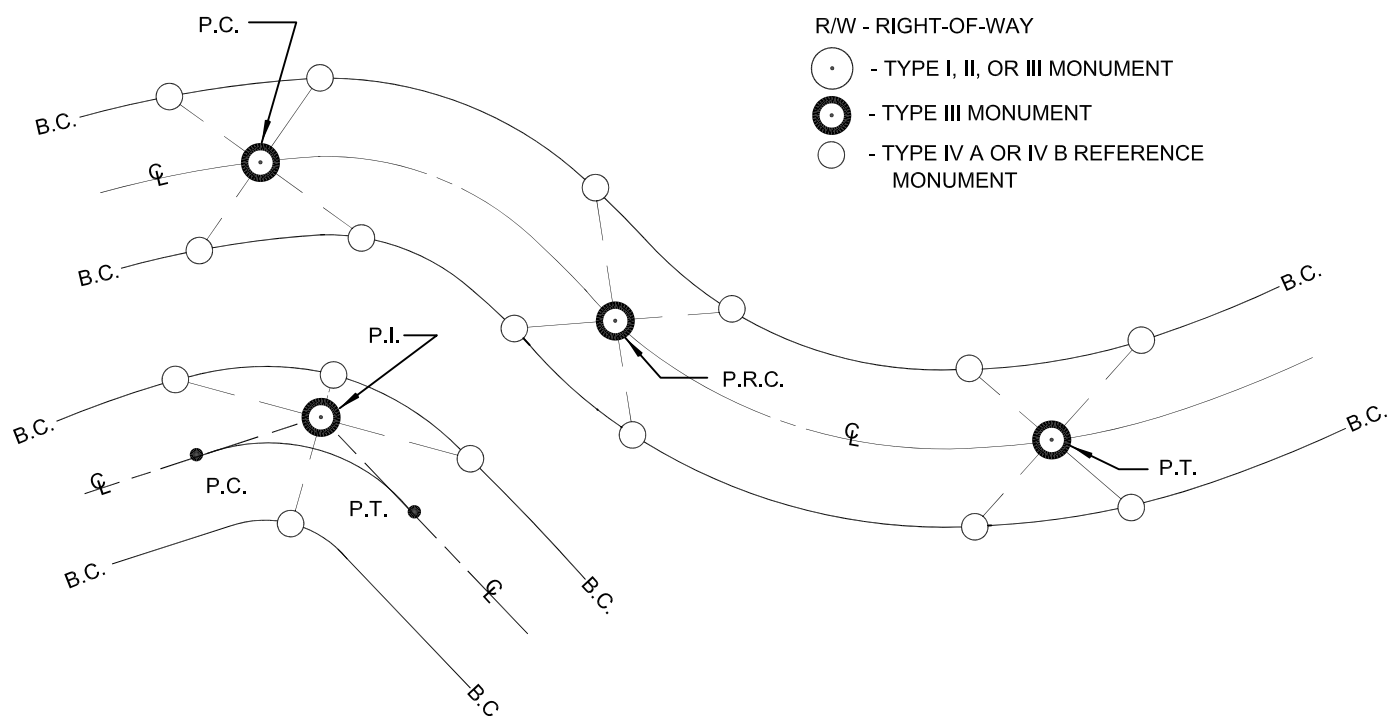
Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	TYPE IV MONUMENT		
621	MONUMENTS			
		DATE 12-14-00	DWG. NO. 242	PAGE NO. 48



LEGEND

- P.C. - POINT OF CURVE
- P.R.C. - POINT OF REVERSE CURVE
- P.T. - POINT OF TANGENCY
- CL - CENTERLINE
- B.C. - BACK OF CURB
- P.I. - POINT OF INTERSECTION
- R/W - RIGHT-OF-WAY
- - TYPE I, II, OR III MONUMENT
- - TYPE III MONUMENT
- - TYPE IV A OR IV B REFERENCE MONUMENT



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL MONUMENT LOCATION

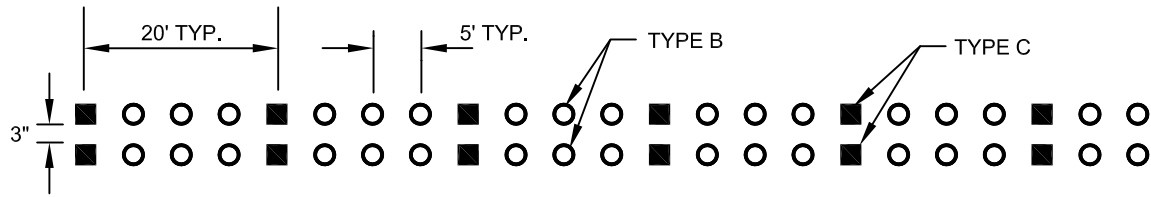
DATE

DWG. NO.

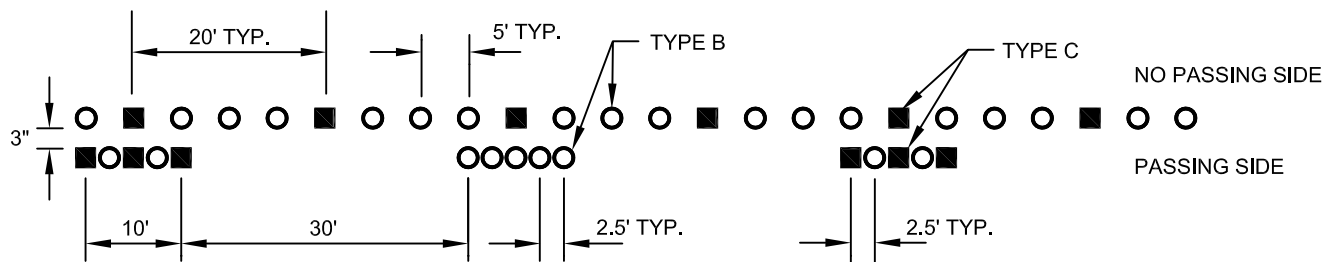
243

PAGE NO.

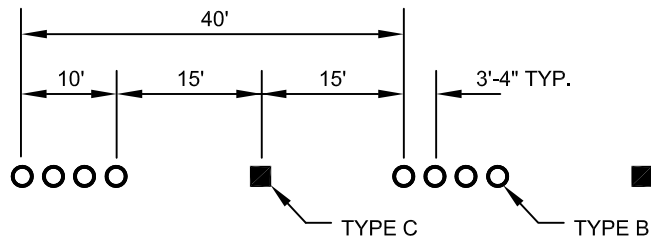
49



TYPE 1 CENTERLINE



TYPE 2 CENTERLINE



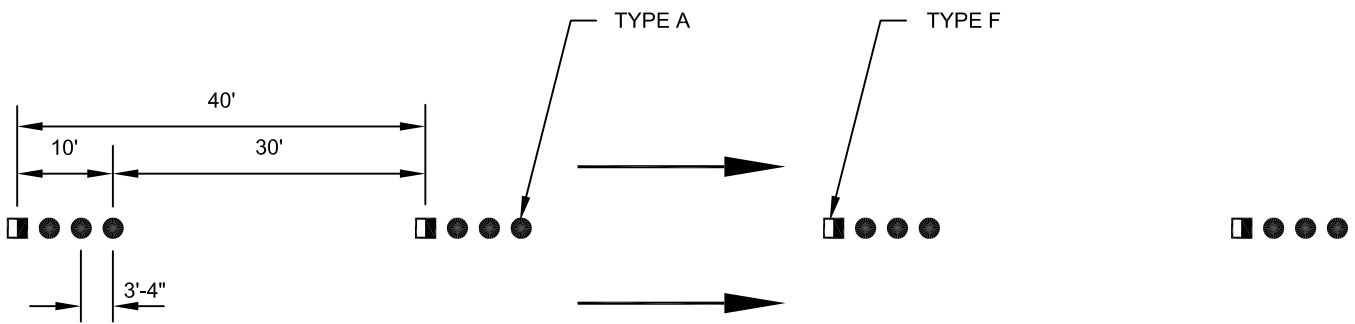
TYPE 3 CENTERLINE

SPECIFICATION REFERENCE

633 PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL CENTERLINE
DELINEATION



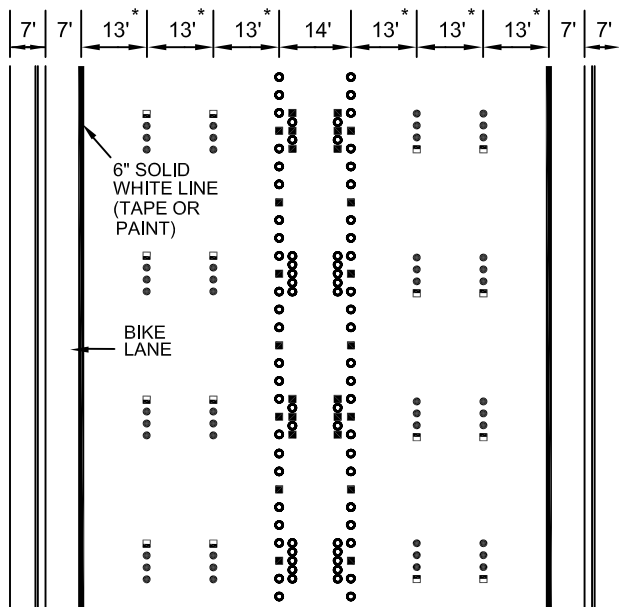
TYPE 4 LANE LINE
(DIVIDED, UNDIVIDED OR ONE-WAY ROADWAY)

SPECIFICATION REFERENCE

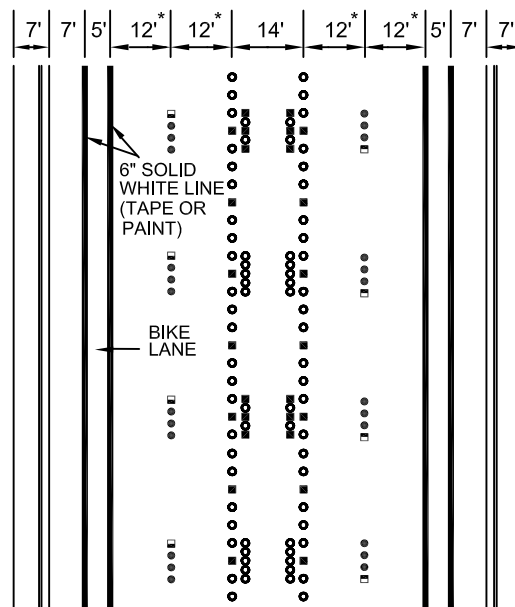
633 PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

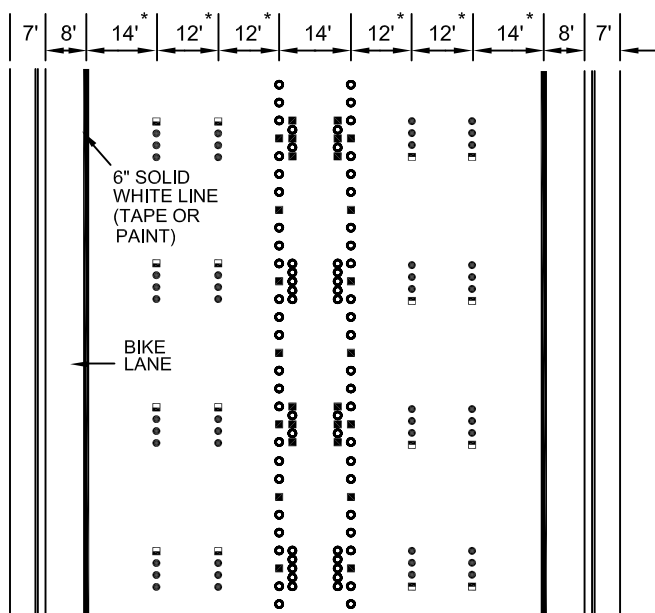
TYPICAL LANE LINE
DELINEATION



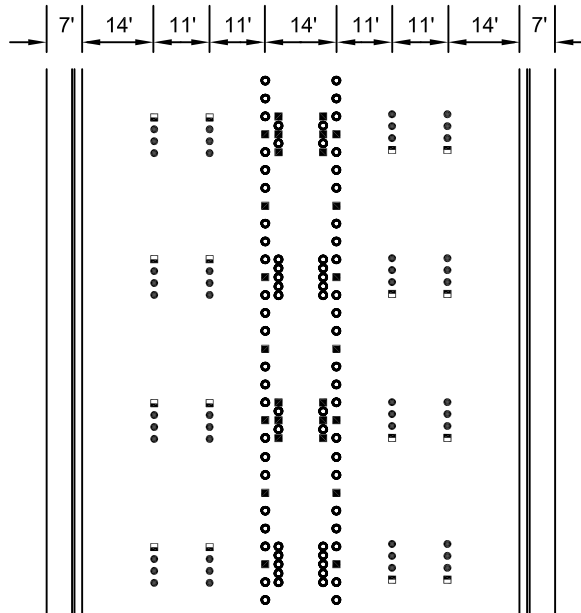
120 FT. STREET WITH BIKE LANE
(WITHOUT PARKING)



100 FT. STREET WITH BIKE LANE
(WITH PARKING/EMERGENCY LANE)



120 FT. STREET WITH BIKE LANE
(WITHOUT PARKING)



100 FT. STREET WITH SHARED
BIKE/VEHICLE LANE (WITHOUT PARKING)

* THE WIDTH OF TRAVEL LANES ADJACENT TO BIKE LANES MAY VARY FROM 12 FT. TO 16 FT.
WIDTHS OF INTERIOR TRAVEL LANES MAY VARY FROM 11 FT. TO 13 FT.

NOTES:

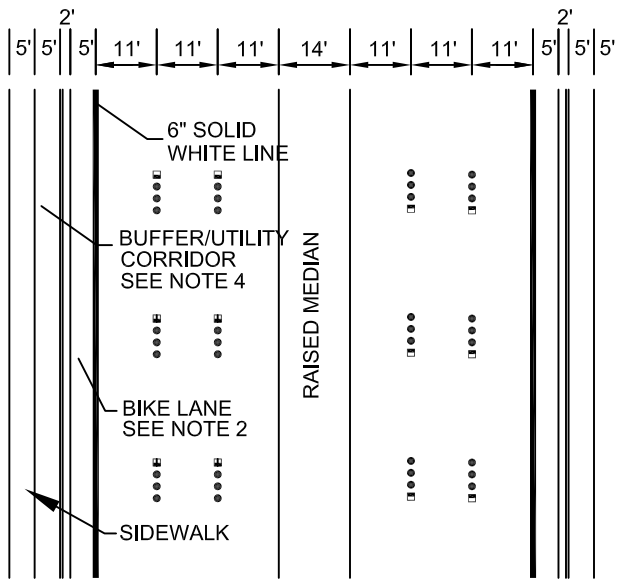
1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NOS. 244 & 244A.
2. IN SOME CASES, A MEDIAN WILL EXIST INSTEAD OF TWO-WAY LEFT TURN LANE.
3. BIKE LANES MUST BE A MINIMUM OF 4 FT. AND NO GREATER THAN 8 FT. WIDE; HOWEVER, A WIDTH OF 5 FT. IS PREFERRED.
4. WHERE 6 FT. SIDEWALK EXISTS, WIDTH OF MEDIAN MAY BE REDUCED BY 2 FT. OR TRAVEL LANES MAY BE REDUCED TO 11 FT.
5. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.

SPECIFICATION REFERENCE

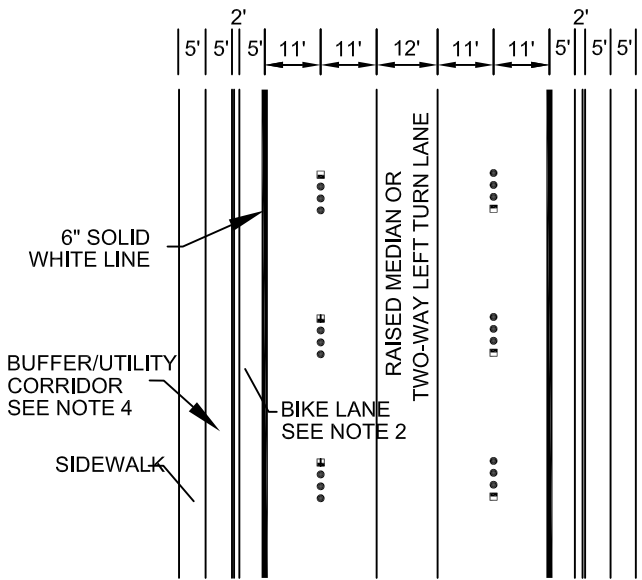
628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

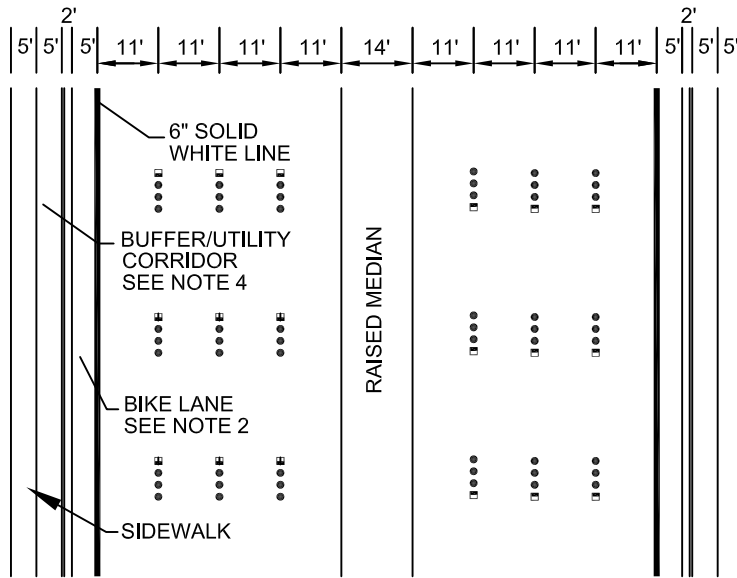
TYPICAL DELINEATION FOR ROADWAYS
100 FT. OR GREATER RIGHT-OF-WAY
WITH CURBSIDE SIDEWALK



114 FT. STREET



90 FT. STREET

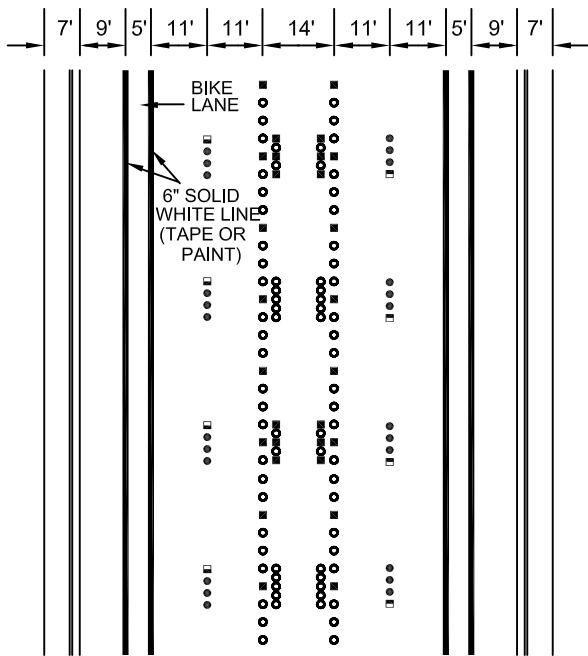


136 FT. STREET

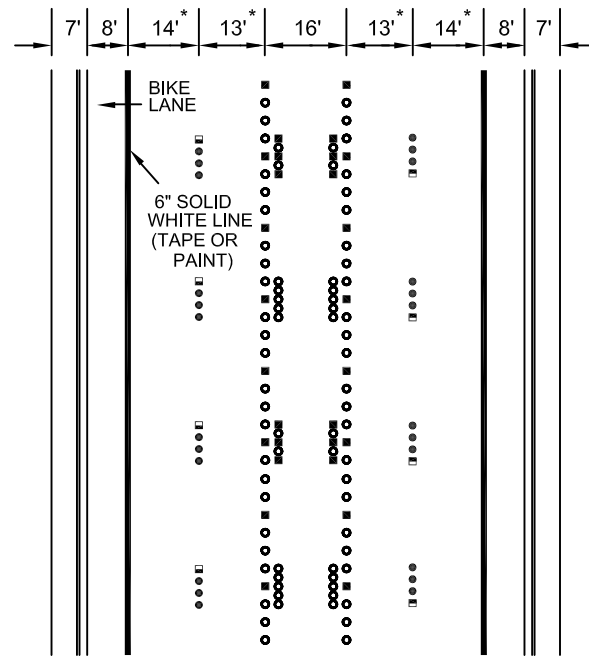
NOTES:

1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NO. 244 & 244A.
2. BIKE LANES TO BE PROVIDED IF SEGMENT CONNECTS TO OTHER BIKE LANES OR IF ROADWAY SEGMENT IS 1 MILE OR GREATER. IF BIKE LANE IS NOT PROVIDED, TRAVEL LANES SHOULD REMAIN AT DIMENSIONS SHOWN SO A BICYCLE LANE COULD BE PROVIDED IN THE FUTURE. SEE DRAWING NUMBER 246.1 FOR BIKE LANE SIGNING AND STRIPING DETAILS.
3. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.
4. CONTACT THE LOCAL JURISDICTIONAL FOR DEVELOPMENT REQUIREMENTS FOR THE AREA BETWEEN THE CURB AND SIDEWALK.

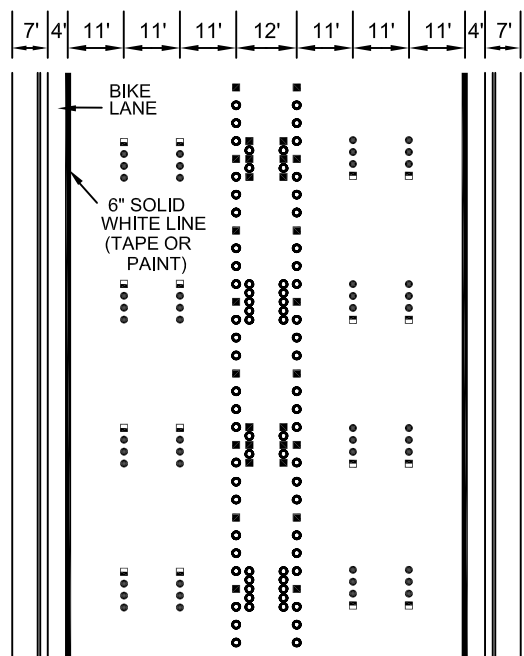
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING						
633	PAVEMENT MARKERS	TYPICAL DELINEATION FOR ALTERNATE ROADWAYS WITH OFFSET SIDEWALK					
		DATE	7-10-03	DWG. NO.	244.1 ALT	PAGE NO.	50.1ALT



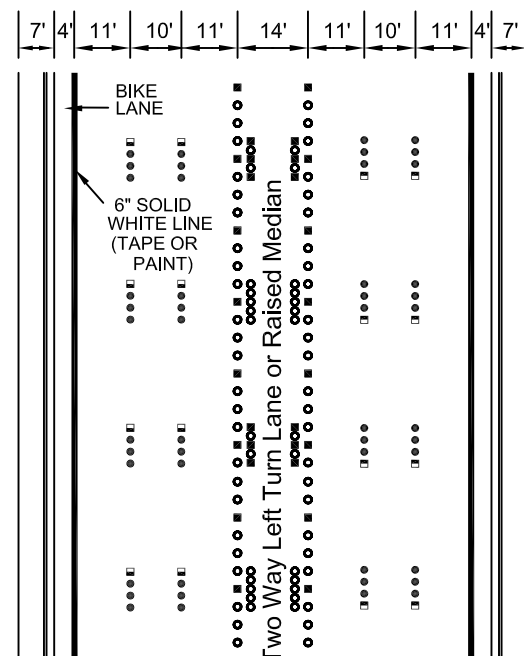
100 FT. STREET WITH BIKE LANE
(WITH PARKING/EMERGENCY LANE)



100 FT. STREET WITH BIKE LANE
(WITHOUT PARKING/EMERGENCY LANE)



100 FT. STREET WITH BIKE LANE
(WITHOUT PARKING/EMERGENCY LANE)



100 FT. STREET WITH BIKE LANE
(WITHOUT PARKING/EMERGENCY LANE)

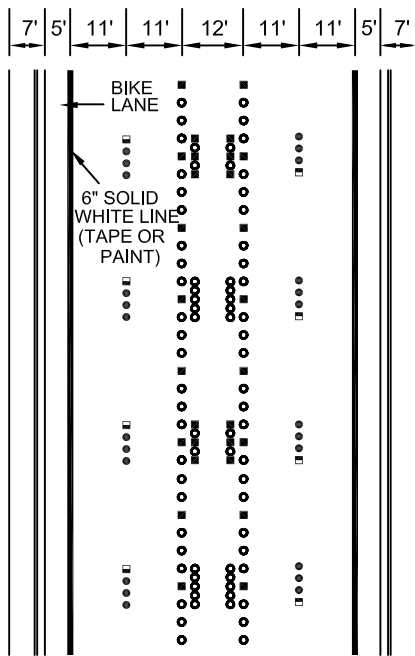
NOTES:

1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NOS. 244 & 244A.
2. IN SOME CASES, A MEDIAN WILL EXIST INSTEAD OF TWO-WAY LEFT TURN LANE.
3. BIKE LANES MUST BE A MINIMUM OF 4 FT. AND NO GREATER THAN 8 FT. WIDE; HOWEVER, A WIDTH OF 5 FT. IS PREFERRED.
4. WHERE 6 FT. SIDEWALK EXISTS, WIDTH OF MEDIAN MAY BE REDUCED BY 2 FT. OR TRAVEL LANES MAY BE REDUCED TO 11 FT.
5. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.

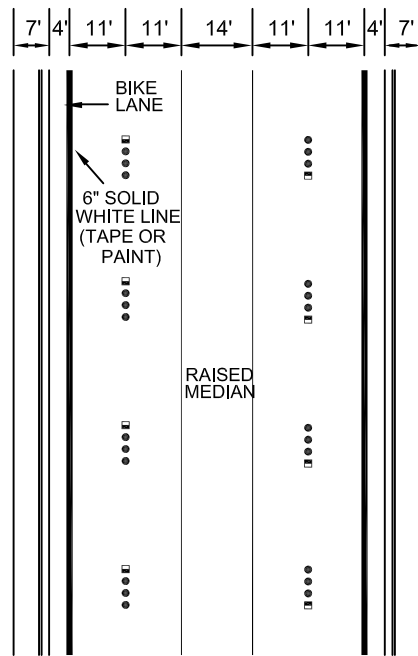
* THE WIDTH OF TRAVEL LANES ADJACENT TO BIKE LANES MAY VARY FROM 12 FT. TO 16 FT. WIDTHS OF INTERIOR TRAVEL LANES MAY VARY FROM 11 FT. TO 13 FT.

Effective 07/01/10 - 12/30/10

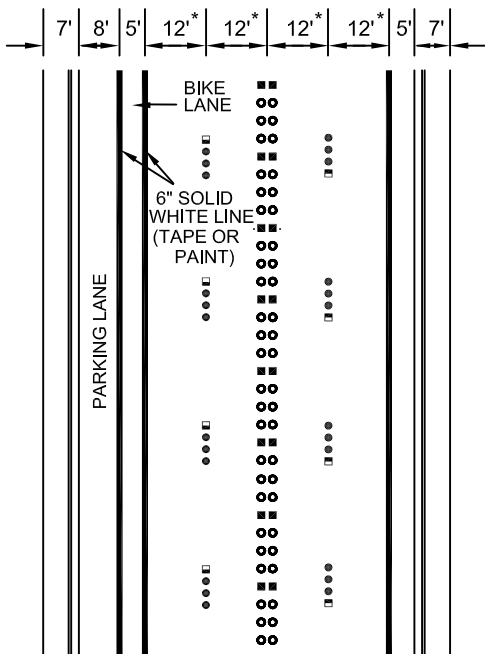
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
628	PAINTING TRAFFIC STRIPING			
401	PAVEMENT MARKERS	TYPICAL DELINEATION FOR ROADWAYS 100 FT. RIGHT-OF-WAY WITH CURBSIDE SIDEWALK		
		DATE 3-9-06	DWG. NO. 244.1A	PAGE NO. 50.1A



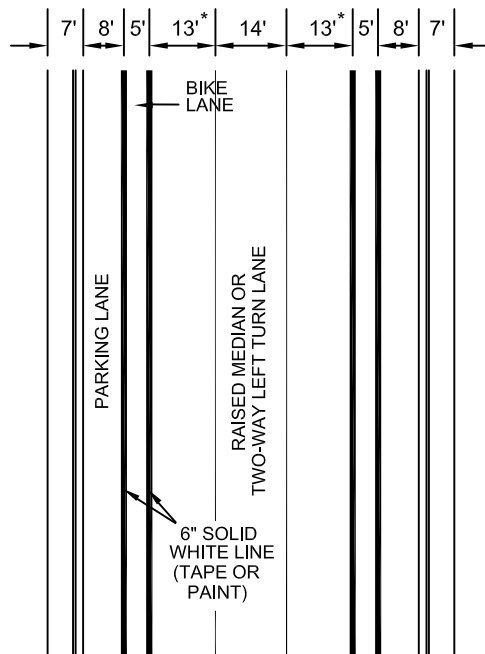
80 FT. STREET WITH BIKE LANE
(WITHOUT PARKING/EMERGENCY LANE)



80 FT. STREET WITH BIKE LANE
(WITHOUT PARKING/EMERGENCY LANE)



80 FT. STREET WITH BIKE LANE
(WITH PARKING ON ONE SIDE)



80 FT. STREET WITH BIKE LANE
(WITH PARKING ON BOTH SIDES)

NOTES:

* THE WIDTH OF TRAVEL LANE MAY VARY FROM 11 FT. TO 13 FT.

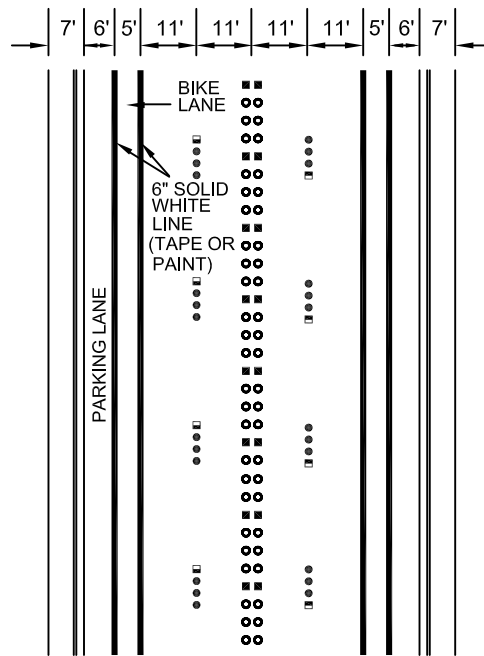
1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NOS. 244 & 244A.
2. BIKE LANES MUST BE A MINIMUM OF 4 FT. AND NO GREATER THAN 8 FT. WIDE; HOWEVER, A WIDTH OF 5 FT. IS PREFERRED.
3. WHERE 6 FT. SIDEWALK EXISTS, WIDTH OF MEDIAN MAY BE REDUCED BY 2 FT. OR TRAVEL LANES MAY BE REDUCED TO 11 FT.
4. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.

SPECIFICATION REFERENCE

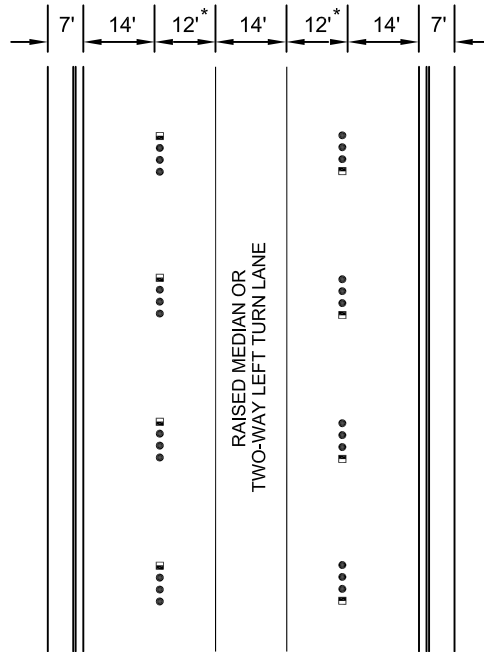
628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL DELINEATION FOR
ROADWAYS 80 FT. RIGHT-OF-WAY
WITH CURBSIDE SIDEWALK



80 FT. STREET WITH BIKE LANE
(WITH PARKING ON BOTH SIDES)



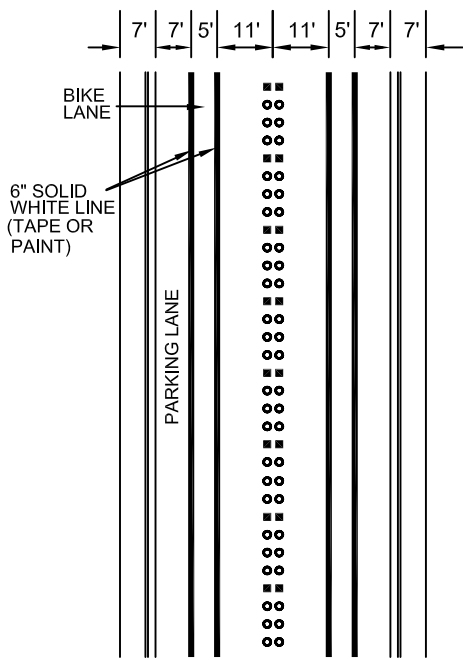
* WIDTH OF TRAVEL LANE MAY VARY FROM 11 FT. TO 12 FT.

80 FT. STREET WITH SHARED BIKE/VEHICLE LANE
(WITHOUT PARKING/EMERGENCY LANE)

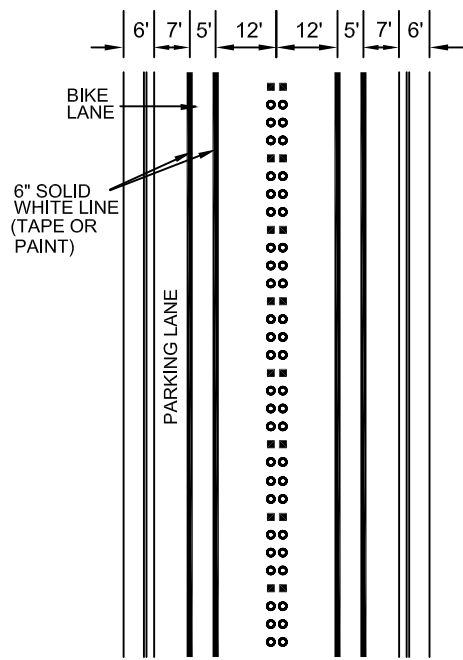
NOTES:

1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NO. 244 & 244A.
2. BIKE LANES MUST BE A MINIMUM OF 4 FT. AND NO GREATER THAN 8 FT. WIDE; HOWEVER, A WIDTH OF 5 FT. IS PREFERRED.
3. WHERE 6 FT. SIDEWALK EXISTS, WIDTH OF MEDIAN MAY BE REDUCED BY 2 FT. OR TRAVEL LANES MAY BE REDUCED TO 11 FT.
4. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.

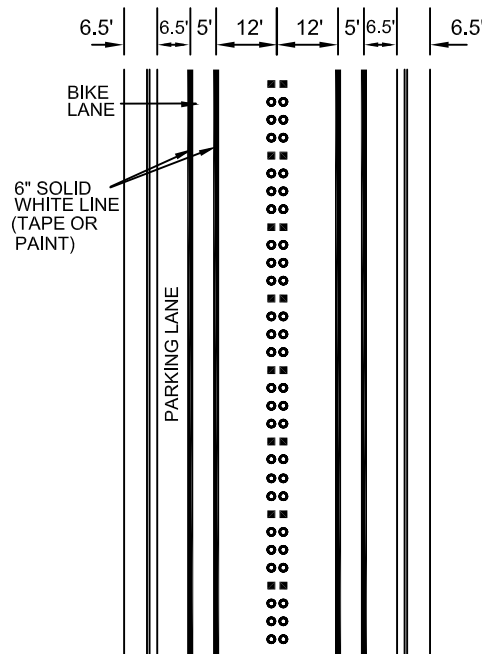
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	TYPICAL DELINEATION FOR ROADWAYS 80 FT. RIGHT-OF-WAY WITH CURBSIDE SIDEWALK					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	244.2A	PAGE NO.	50.2A



ALTERNATE A



ALTERNATE B



ALTERNATIVE C

NOTES:

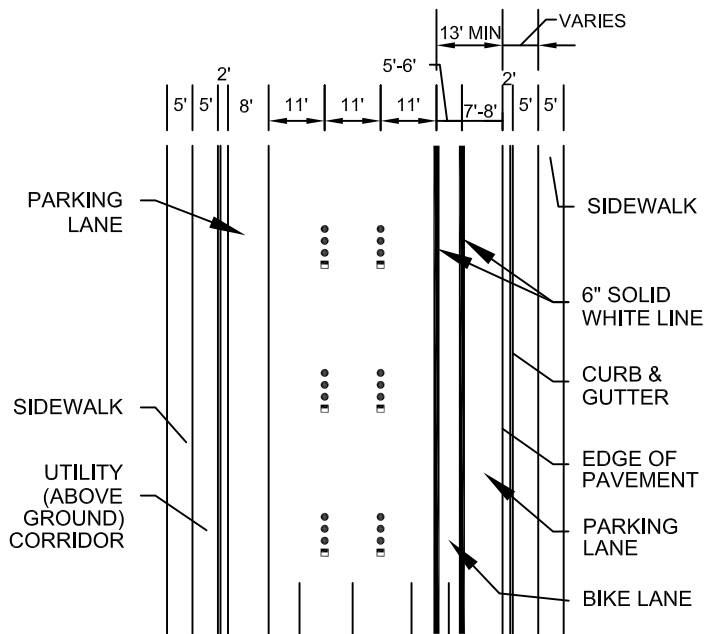
1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NO. 244 & 244A.
2. BIKE LANES MUST BE A MINIMUM OF 4 FT. AND NO GREATER THAN 8 FT. WIDE; HOWEVER, A WIDTH OF 5 FT. IS PREFERRED.
3. ALL CURB LANES ARE MEASURED TO LIP OF GUTTER OR EDGE OF PAVEMENT IF CURB AND GUTTER DO NOT EXIST.

SPECIFICATION REFERENCE

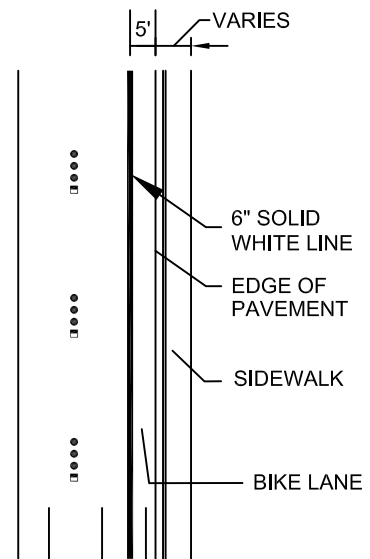
628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

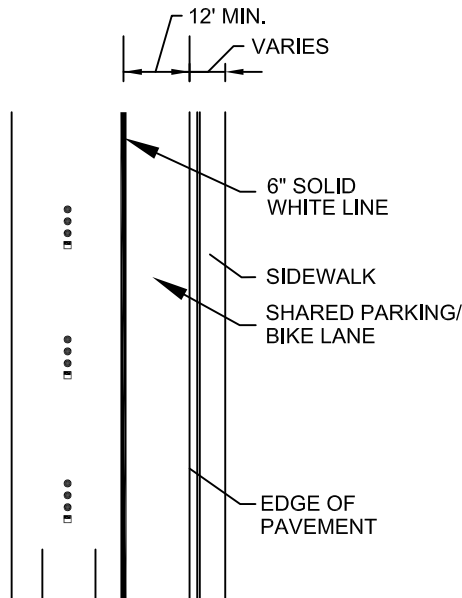
TYPICAL DELINEATION FOR BIKE FACILITIES
60 FT. RIGHT-OF-WAY
(PARKING ON BOTH SIDES)



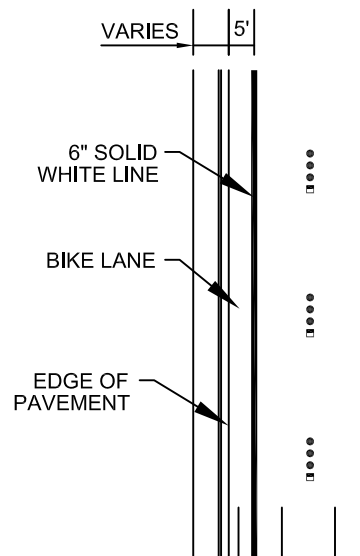
ONE-WAY STREET WITH BIKE LANE
(WITH PARKING ON RIGHT SIDE)



ONE-WAY STREET WITH BIKE LANE
(WITHOUT PARKING ON RIGHT SIDE)



ONE-WAY STREET WITH SHARED
BIKE/PARKING LANE



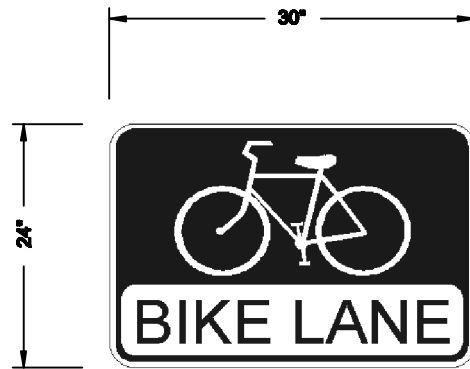
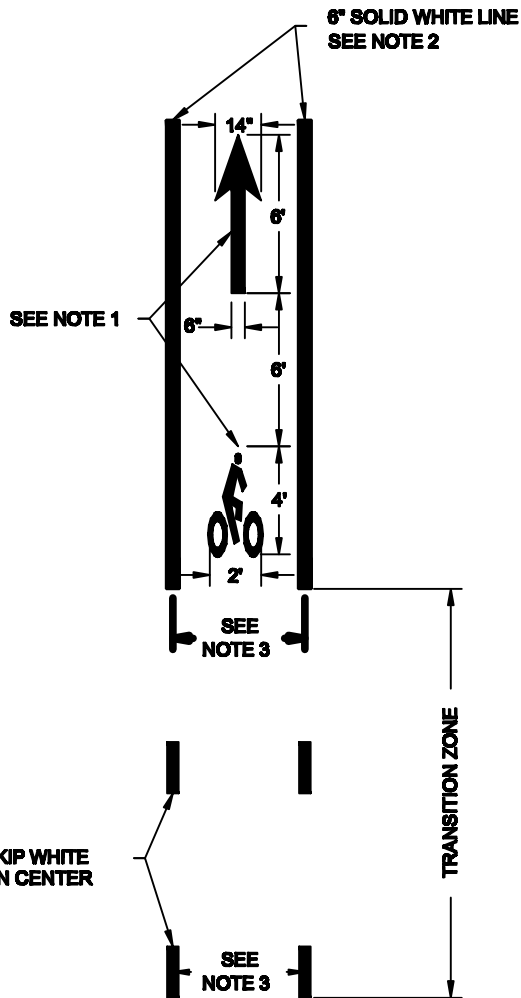
ONE-WAY STREET WITH LEFT-SIDE
BIKE LANE (SEE NOTE 4)

NOTES:

1. LANE LINE DELINEATION SHALL COMPLY WITH STANDARD DRAWING NO. 244 & 244A.
2. BIKE LANES MUST BE A MINIMUM OF 5 FEET WHERE ADJACENT TO A PARKING LANE, 4 FEET MINIMUM IN OTHER CASES AND NO GREATER THAN 8 FEET WIDE.
3. ALL CURB LANES ARE MEASURED TO THE EDGE OF PAVEMENT. THE TOP OF PAVEMENT SHALL BE FLUSH WITH GUTTER.
4. BICYCLE LANE SHALL BE ON RIGHT SIDE OF ONE-WAY ROADWAYS, EXCEPT IN LIMITED SITUATIONS, SUCH AS WHEN THERE ARE SIGNIFICANTLY LESS POTENTIAL CONFLICTS ALONG THE LEFT SIDE OF THE ROADWAY OR WHEN SIGNIFICANT BICYCLE TRIP GENERATION ARE ALONG THE LEFT SIDE OF THE ROADWAY.
5. SEE DRAWING NO. 244.5 FOR BIKE LANE SIGNAGE DETAILS.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	BICYCLE LANE DELINEATION ON ONE-WAY STREET					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	244.4	PAGE NO.	50.4



R3-17

BIKE LANE SIGNAGE

BIKE LANE DELINEATION AND LEGEND

NOTES:

1. BIKE LANE LEGENDS SHALL BE APPROVED TYPE II PAVEMENT MARKING FILM AND SHALL BE SLIP RESISTANT.
2. BIKE LANE LINES SHALL BE APPROVED TYPE II PAVEMENT MARKING FILM AND SHALL BE SLIP RESISTANT.
3. BIKE LANES MUST BE A MINIMUM OF 5 FEET WHEN ADJACENT TO A PARKING LANE, 4 FEET MINIMUM IN OTHER CASES AND NO GREATER THAN 8 FT WIDE; HOWEVER A WIDTH OF 5 FEET IS PREFERRED.
4. BICYCLE LANE DELINEATION, LEGEND, AND SIGNING SHALL CONFORM TO THE MUTCD LATEST EDITION.
5. PER THE MUTCD LATEST EDITION, BICYCLE LANE SIGNS SHALL BE USED IN ADVANCE OF THE BEGINNING OF A MARKED BICYCLE LANE.
6. THE BIKE LANE SIGNAGE SHALL BE CLASS 6 SHEETING.

SPECIFICATION REFERENCE	UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA
	BICYCLE LANE DELINEATION, LEGEND, AND SIGNAGE
DATE	12-08-09
DWG. NO.	244.5

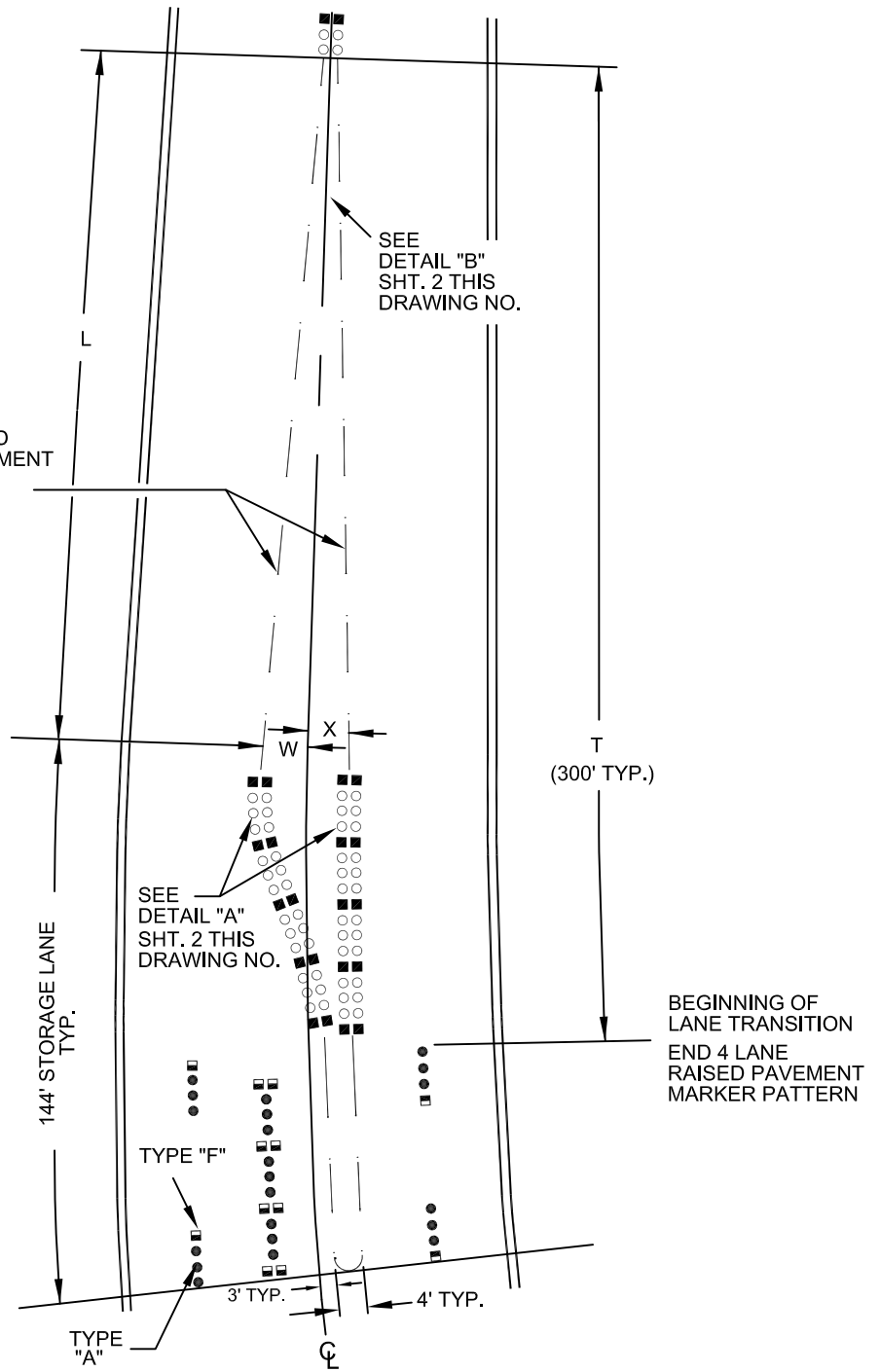
$$T \text{ OR } L = \frac{(W \text{ OR } X)S^2}{60}$$

(DESIGN SPEED
40 MPH
OR LESS)

$$= (W \text{ OR } X)S$$

(DESIGN SPEED
45 MPH
OR GREATER)

FORM ENTIRE ISLAND
USING RAISED PAVEMENT
MARKER PATTERN
FOR TRANSITION
AREA



NOTE:

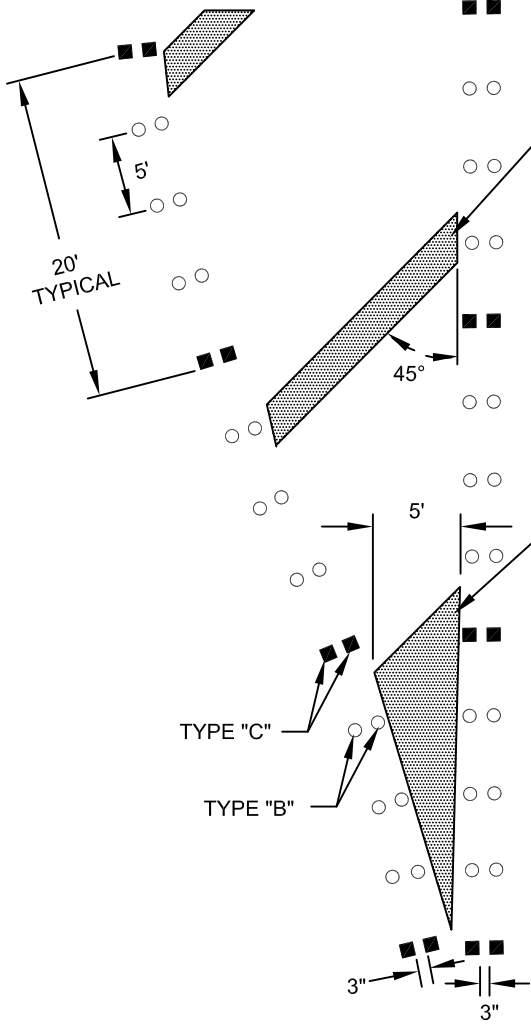
SEE SHEET 3 THIS DRAWING NUMBER IF PATTERN IS TO BE USED AT A GORE POINT TO DIVIDE TRAFFIC MOVING IN SAME DIRECTION.

SPECIFICATION REFERENCE

628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

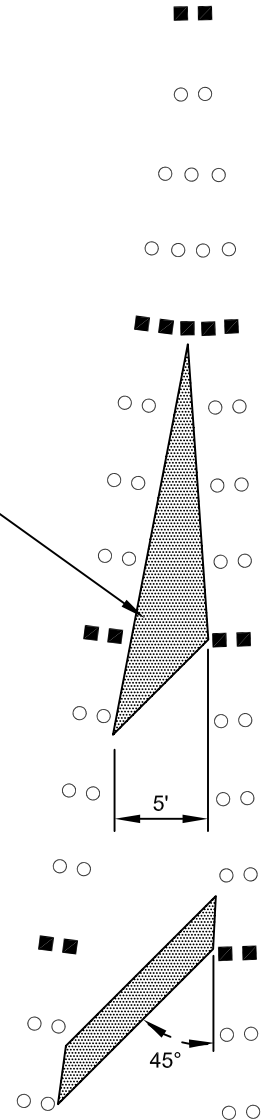
TYPICAL LANE DELINEATION IN
TRANSITION SECTIONS



DETAIL "A"

TAPE 24" WIDE
YELLOW STRIPES
AT 25' O.C. INSIDE
MEDIAN PATTERN

SOLID YELLOW
TAPE



DETAIL "B"

NOTE:

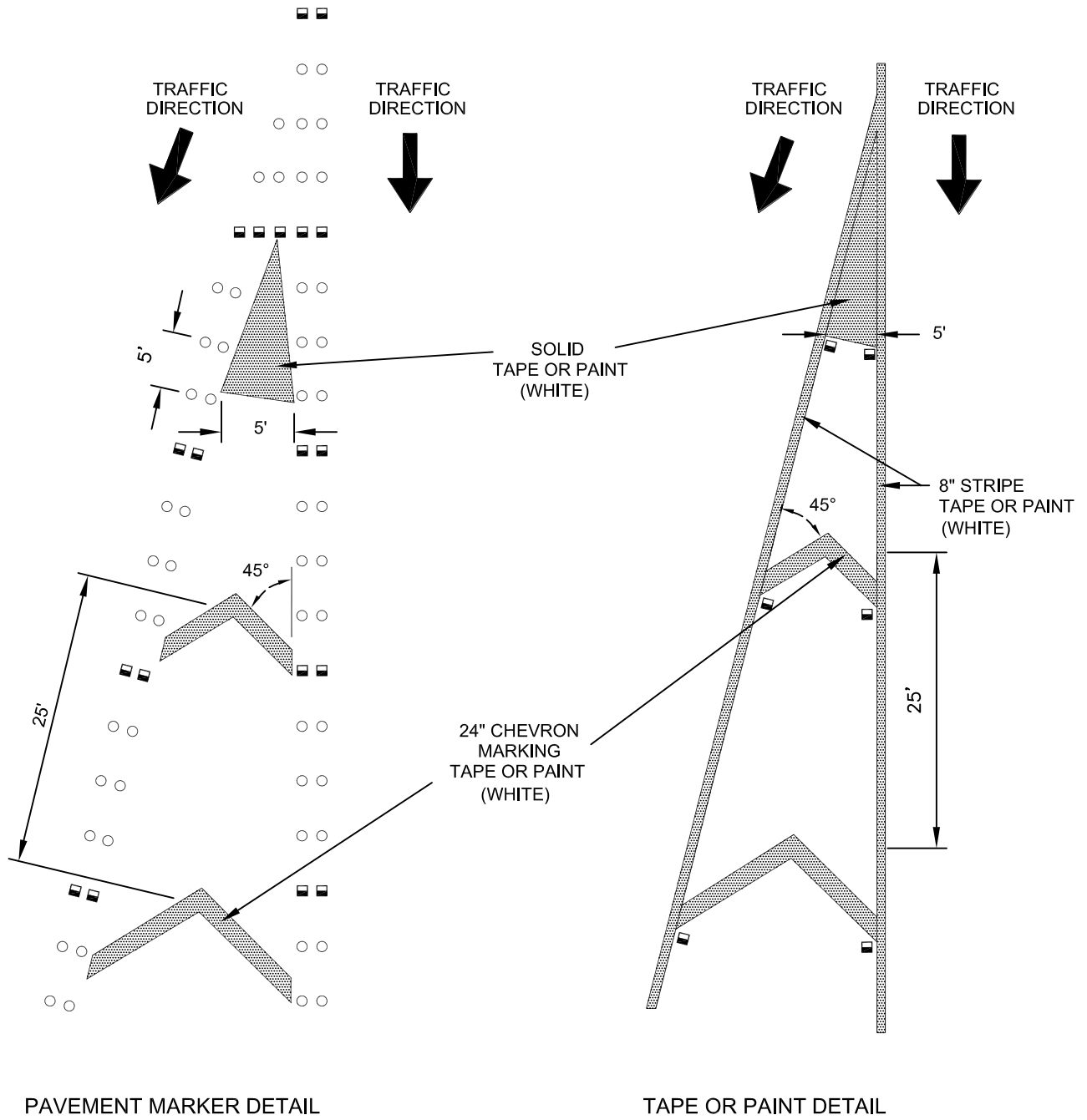
PAINT MAY BE USED IN LIEU OF TAPE AND/OR RAISED PAVEMENT MARKERS AT THE DISCRETION OF THE ENGINEER.

SPECIFICATION REFERENCE

628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

DETAILS FOR
TYPICAL LANE DELINEATION IN
TRANSITION SECTIONS

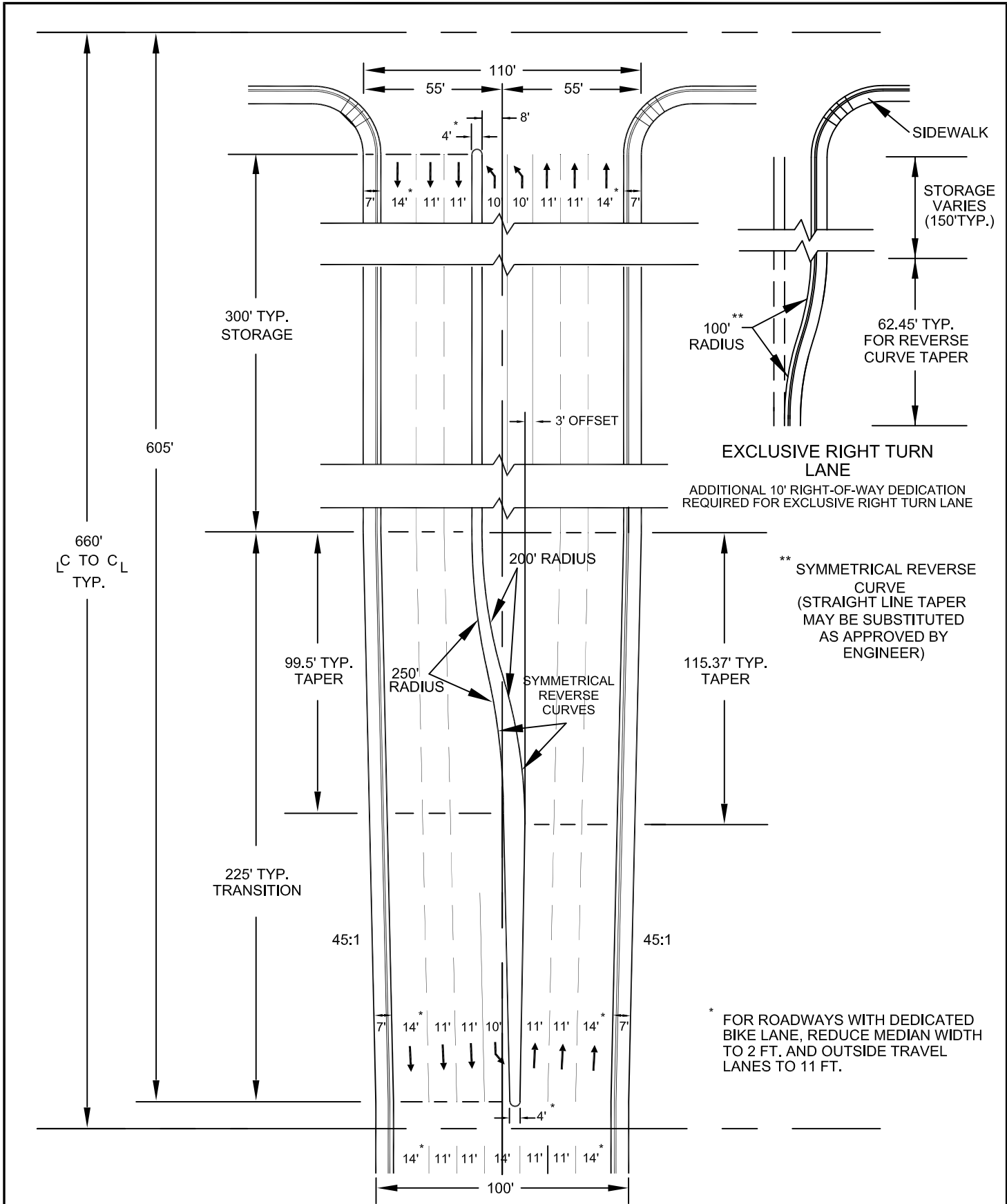


SPECIFICATION REFERENCE

628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL LANE DELINEATION IN
TRANSITION SECTIONS WHERE
TRAFFIC FLOW IN SAME DIRECTION



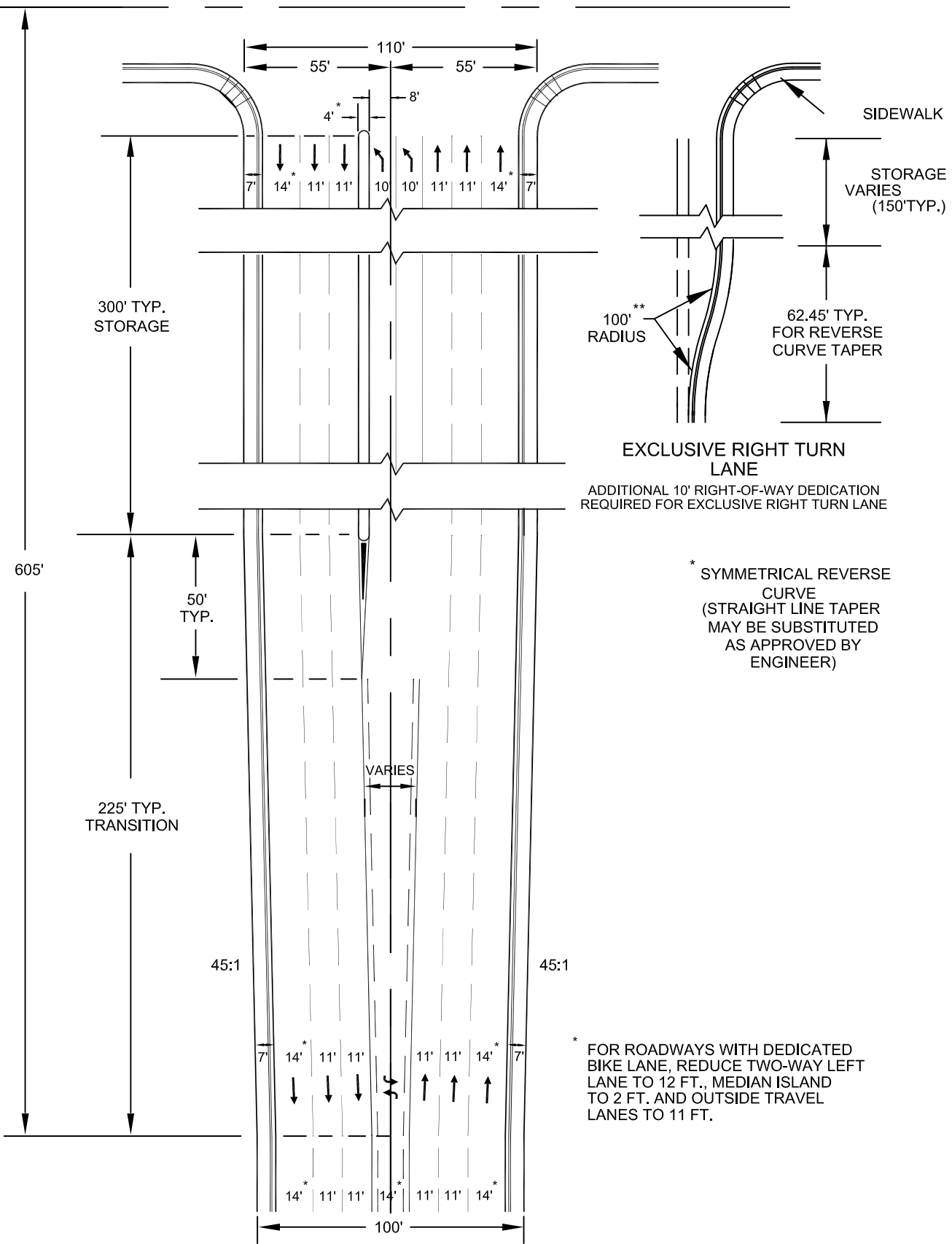
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL LANE CONFIGURATION FOR
MAJOR STREET INTERSECTIONS
AND MEDIAN DETAIL
CASE I - WITH CURBSIDE SIDEWALK

DATE 7-10-03 | DWG. NO. 245.1 (1 OF 2) | PAGE NO. 51.1

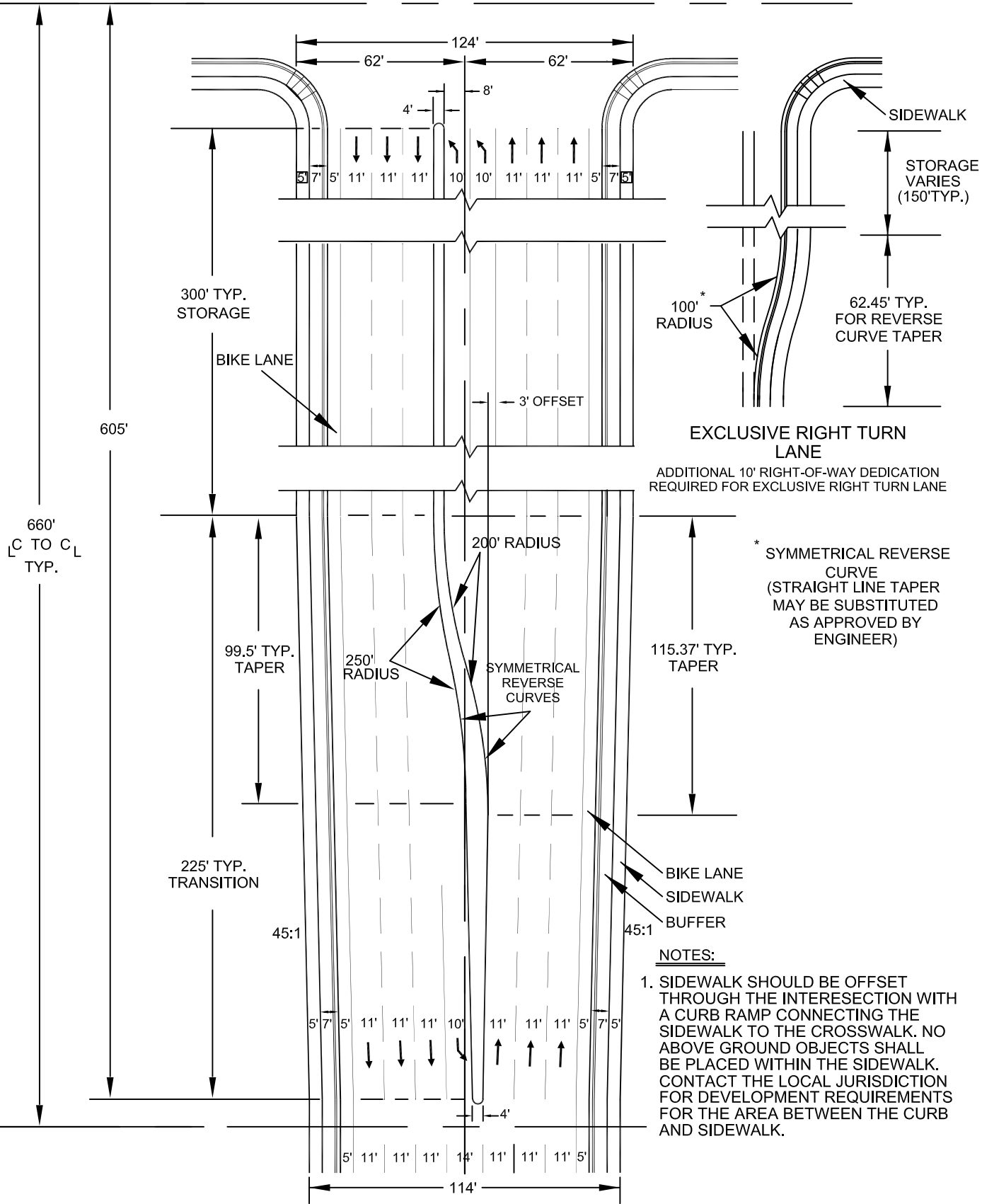
Effective 07/01/10 - 12/30/10



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL LANE CONFIGURATION FOR
MAJOR STREET INTERSECTIONS
AND MEDIAN DETAIL
CASE II - WITH CURBSIDE SIDEWALK

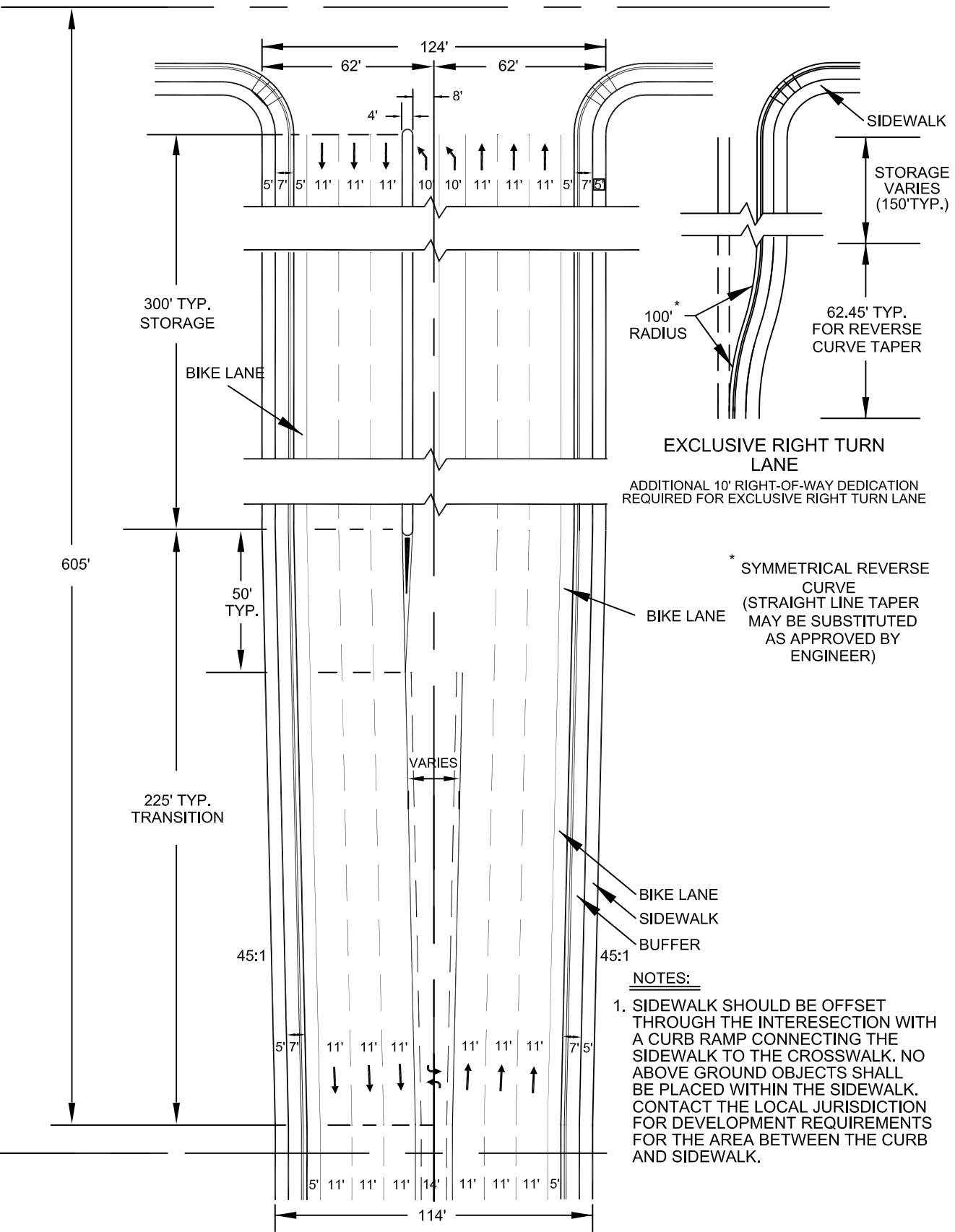


SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL LANE CONFIGURATION FOR
MAJOR STREET INTERSECTIONS
AND MEDIAN DETAIL
CASE I - WITH OFFSET SIDEWALK

Effective 07/01/10 - 12/30/10



ADDITIONAL 10' RIGHT-OF-WAY DEDICATION
REQUIRED FOR EXCLUSIVE RIGHT TURN LANE

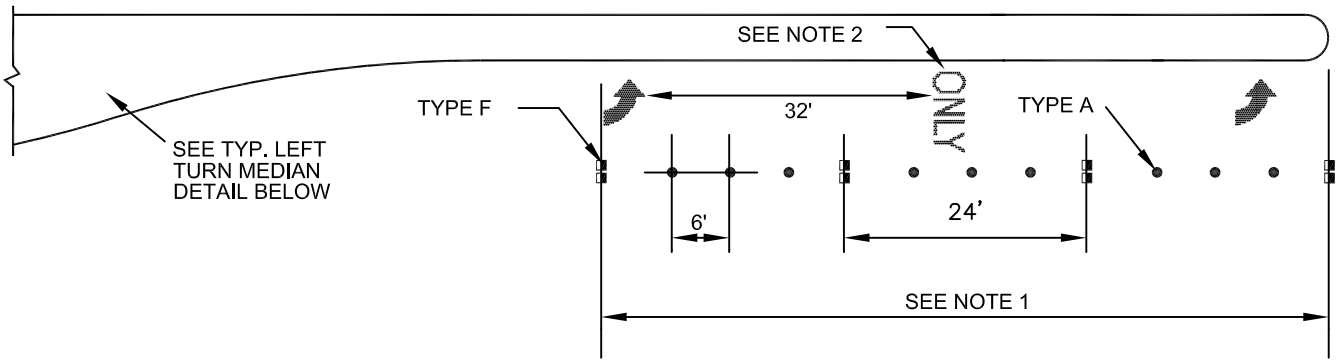
* SYMMETRICAL REVERSE
CURVE
(STRAIGHT LINE TAPER
MAY BE SUBSTITUTED
AS APPROVED BY
ENGINEER)

NOTES:

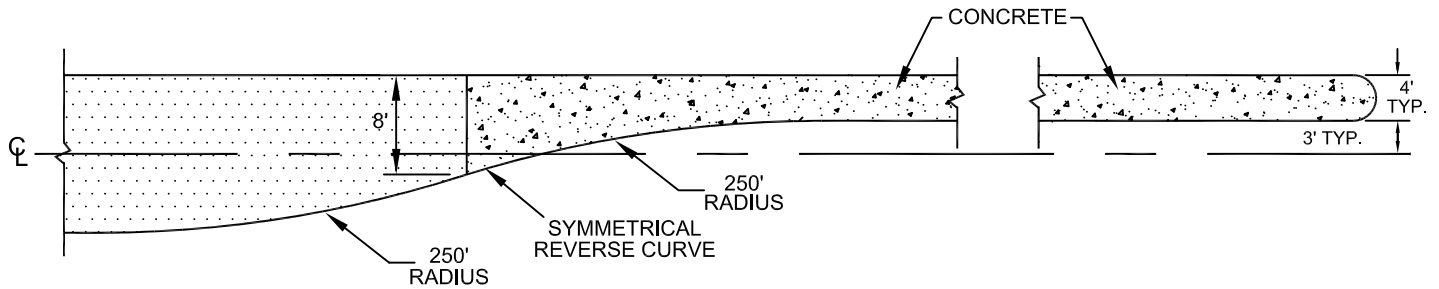
1. SIDEWALK SHOULD BE OFFSET THROUGH THE INTERSECTION WITH A CURB RAMP CONNECTING THE SIDEWALK TO THE CROSSWALK. NO ABOVE GROUND OBJECTS SHALL BE PLACED WITHIN THE SIDEWALK. CONTACT THE LOCAL JURISDICTION FOR DEVELOPMENT REQUIREMENTS FOR THE AREA BETWEEN THE CURB AND SIDEWALK.

Effective 07/01/10 - 12/30/10

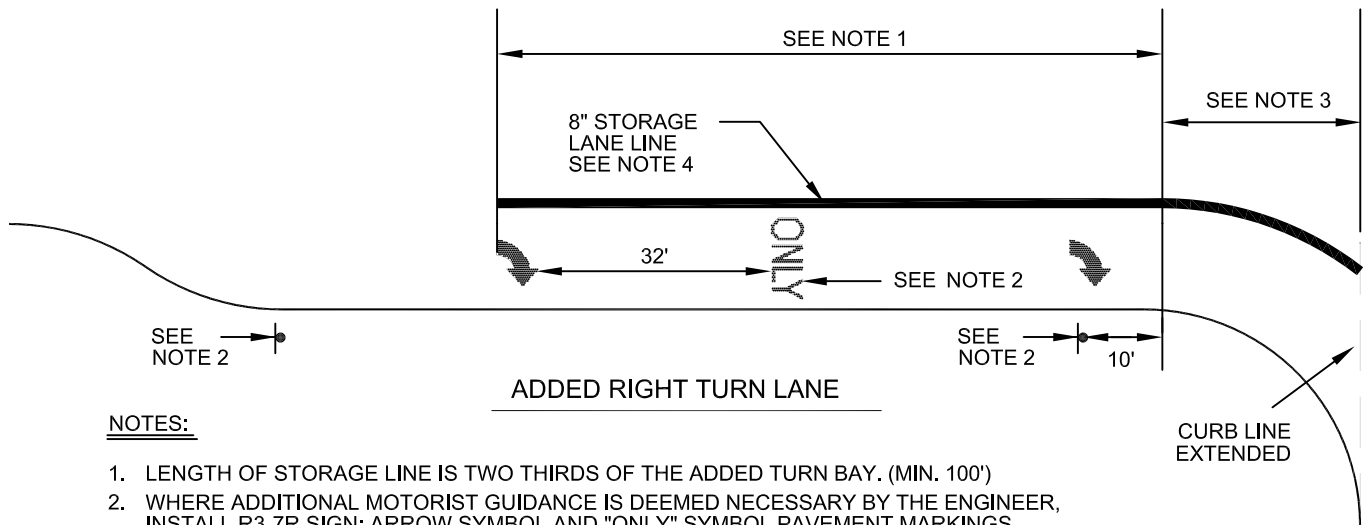
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
		TYPICAL LANE CONFIGURATION FOR MAJOR STREET INTERSECTIONS AND MEDIAN DETAIL CASE II - WITH OFFSET SIDEWALK	
DATE	7-10-03	DWG. NO.	245.1 ALT (2 OF 2)
		PAGE NO.	51.1AALT



ADDED LEFT TURN LANE



TYPICAL LEFT TURN MEDIAN DETAIL



ADDED RIGHT TURN LANE

NOTES:

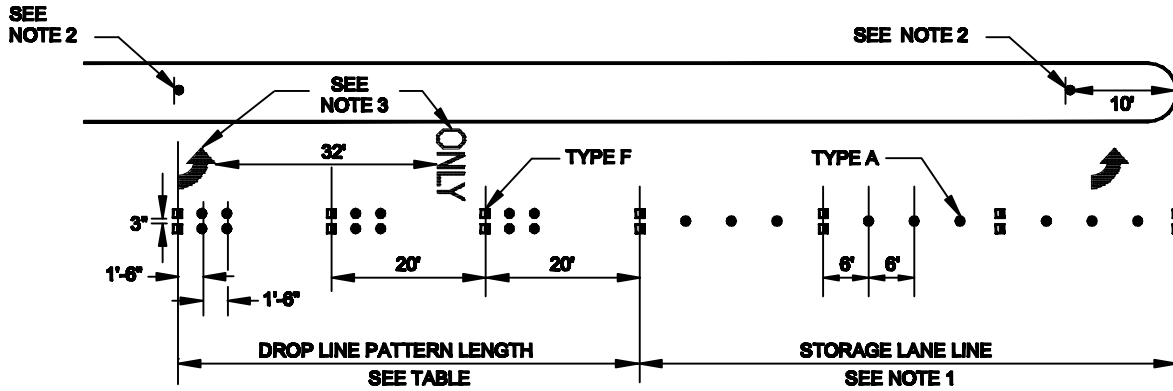
1. LENGTH OF STORAGE LINE IS TWO THIRDS OF THE ADDED TURN BAY. (MIN. 100')
2. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, INSTALL R3-7R SIGN; ARROW SYMBOL AND "ONLY" SYMBOL PAVEMENT MARKINGS FOR THE LENGTH OF THE STORAGE LINE. SYMBOLS SHALL BE APPROVED TYPE I PAVEMENT MARKING FILM.
3. APPROVED TYPE II PAVEMENT MARKING FILM OR RAISED PAVEMENT MARKERS MAY BE USED FOR ADDITIONAL GUIDANCE AT THE DISCRETION OF THE ENGINEER.
4. STORAGE LANE LINE SHALL BE APPROVED TYPE II PAVEMENT MARKING FILM OR IF APPROVED BY THE ENGINEER, RAISED PAVEMENT MARKERS MAY BE USED.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

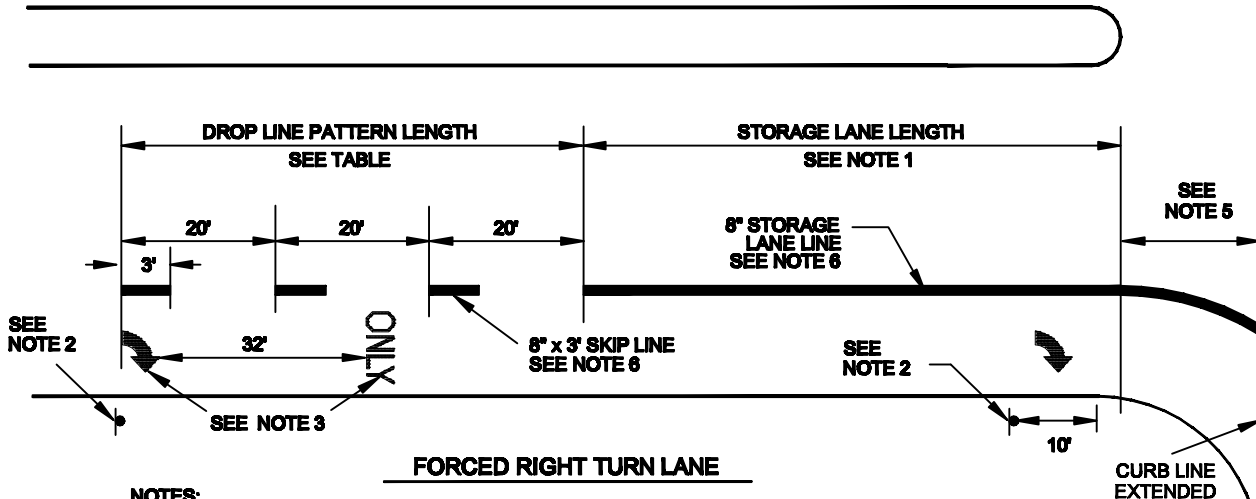
633	PAVEMENT MARKERS
628	PAINTING TRAFFIC STRIPING, PAVEMENT MARKINGS...

STANDARD PAVEMENT MARKERS
ADDED TURN LANE



TYP. DROP LINE LENGTHS	
POSTED SPEED (MPH)	LENGTH (FT)
25	240
30	320
35	400
40	480
45	560
50	640
55	720

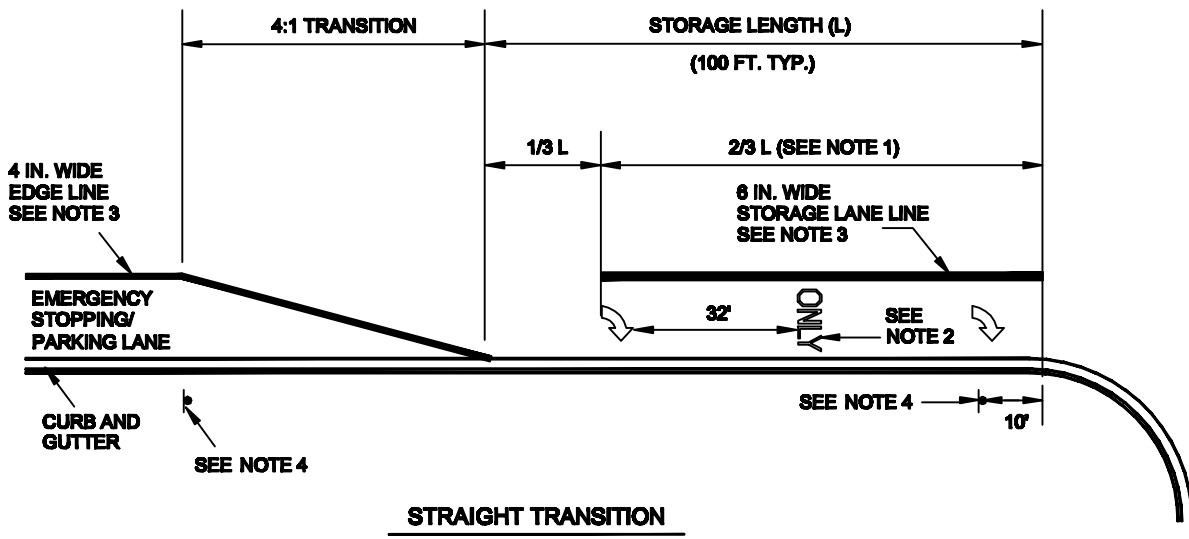
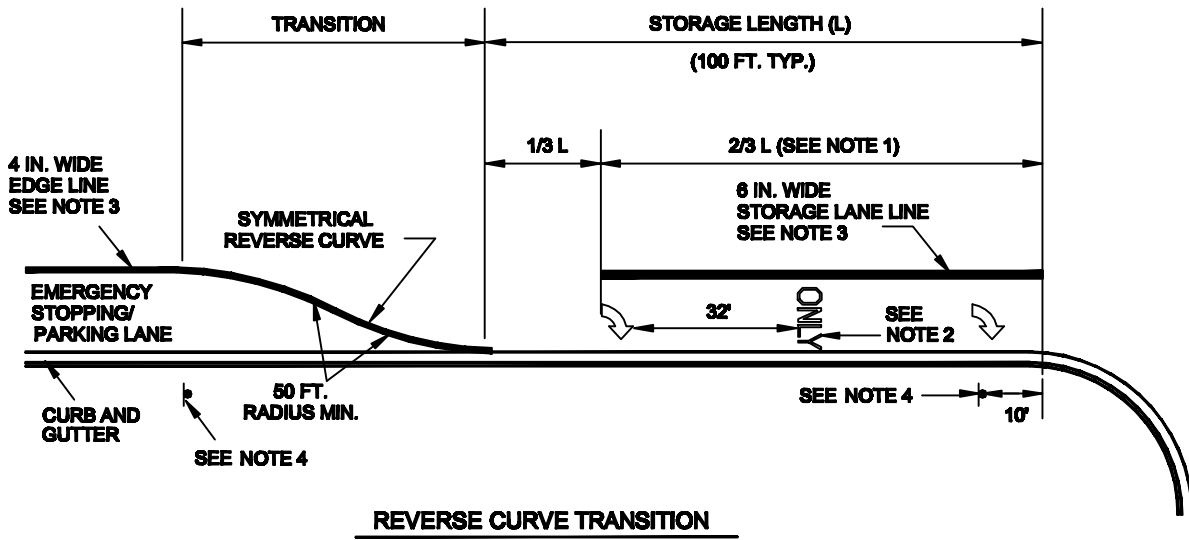
FORCED LEFT TURN LANE



NOTES:

1. THE MINIMUM LENGTH OF STORAGE LINE IS 250 FT. ON ARTERIALS AND 150 FT. ON ALL OTHERS.
2. A MINIMUM OF 20 R3-7R OR R3-7L SIGNS SHALL BE INSTALLED IN ADVANCE OF THE INTERSECTION AT DISTANCES APPROVED BY THE ENGINEER. RECOMMENDED LOCATIONS ARE SHOWN ABOVE.
3. ONE SET OF PAVEMENT MARKINGS CONTAINING ONE ARROW SYMBOL AND ONE "ONLY" SYMBOL SHALL BE PLACED AT THE BEGINNING OF THE DROP LANE.
4. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, ADDITIONAL ARROW AND "ONLY" SYMBOL PAVEMENT MARKINGS AND OVERHEAD MOUNTED R3-5 SIGNS MAY BE INSTALLED. SYMBOLS SHALL BE APPROVED TYPE II PAVEMENT MARKING FILM.
5. APPROVED TYPE I PAVEMENT MARKING FILM OR RAISED PAVEMENT MARKERS MAY BE USED FOR ADDITIONAL GUIDANCE AT THE DISCRETION OF THE ENGINEER.
6. STORAGE LANE LINE AND SKIP LINES SHALL BE APPROVED TYPE I PAVEMENT MARKING FILM OR IF APPROVED BY THE ENGINEER, RAISED PAVEMENT MARKERS MAY BE USED.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
633	PAVEMENT MARKERS		
628	PAINTING TRAFFIC STRIPING, PAVEMENT MARKINGS...	STANDARD PAVEMENT MARKERS FORCED TURN LANE	
		DATE 12-08-09	DWG. NO. 246A



NOTES:

1. LENGTH OF STORAGE LANE LINE IS TWO THIRDS OF THE TURN LANE STORAGE LENGTH.
2. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, INSTALL ARROW AND "ONLY" SYMBOL PAVEMENT MARKINGS FOR THE LENGTH OF THE STORAGE LINE.
3. PAVEMENT MARKINGS SHALL BE TYPE I TAPE OR PAINT AS DIRECTED BY THE ENGINEER.
4. INSTALL "NO PARKING" SIGNS FOR ENTIRE LENGTH OF TURN LANE. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, INSTALL R3-7R SIGNS.

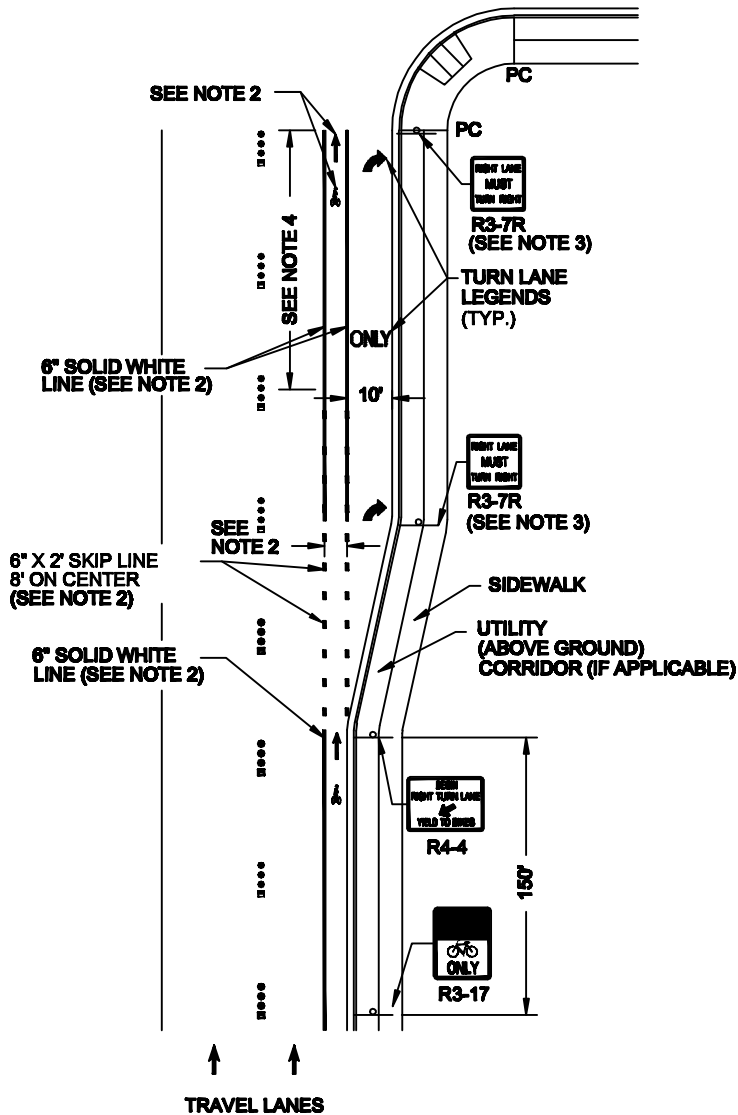
SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

633 PAVEMENT MARKERS

**PAVEMENT MARKING AND SIGNAGE
RIGHT TURN LANE AT MINOR INTERSECTIONS
(ARTERIALS WITH EMERGENCY/PARKING LANE)**

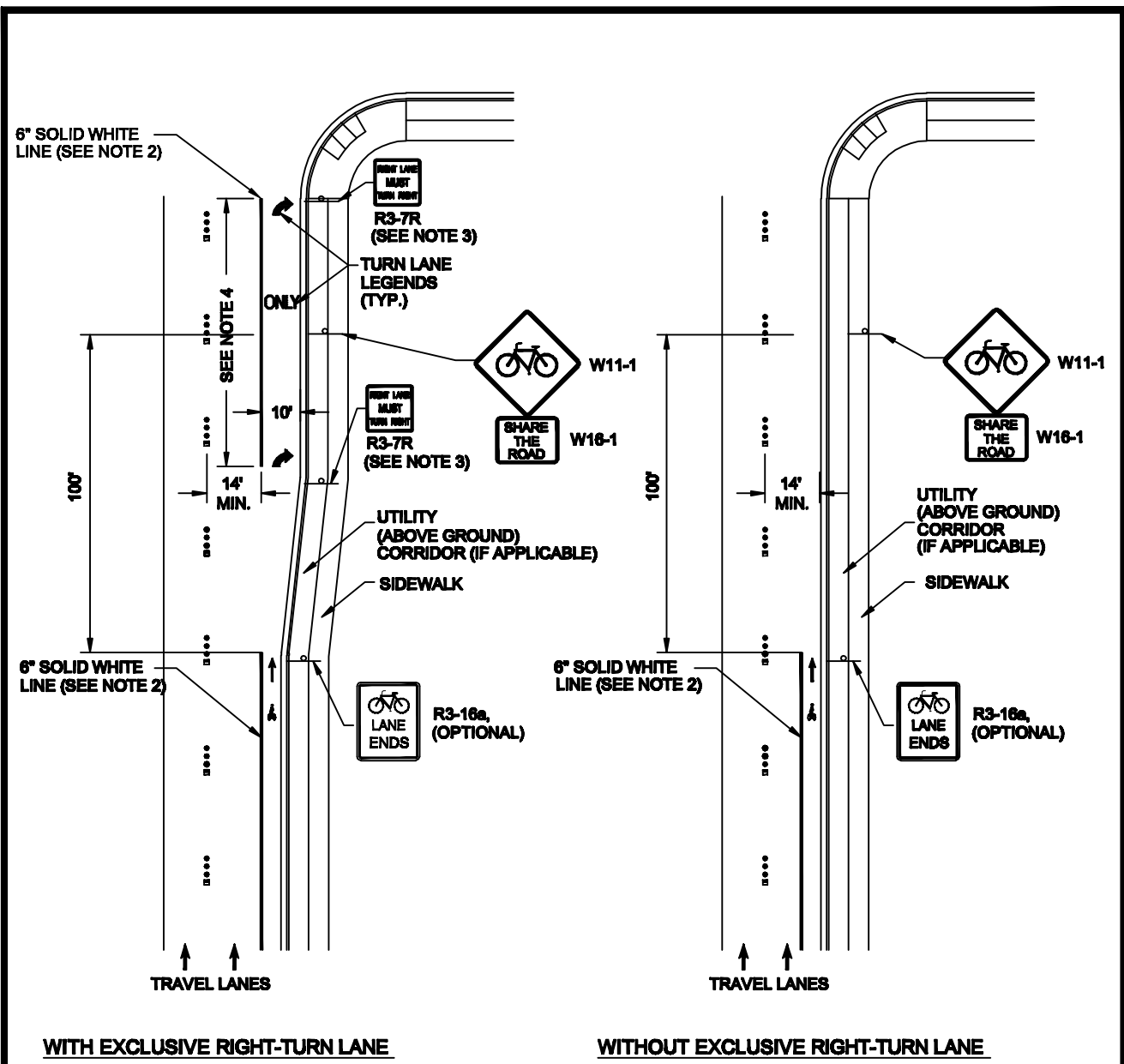
DATE 12-08-09 | DWG. NO. 246B



NOTES:

1. STORAGE LENGTH TO BE DETERMINED BY TRAFFIC ENGINEER.
2. SEE DRAWING NO. 244.5 FOR BIKE LANE LEGEND AND SIGNAGE.
3. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE TRAFFIC ENGINEER, INSTALL R3-7R SIGN, ARROW SYMBOL AND "ONLY" SYMBOL PAVEMENT MARKINGS FOR THE LENGTH OF THE STORAGE LINE. APPROVED TYPE II PAVEMENT MARKING FILM SHALL BE USED FOR SYMBOL MARKINGS.
4. SEE DRAWING NO. 246 NOTE 1 FOR STANDARD PAVEMENT MARKERS ADDED TURN LANE.

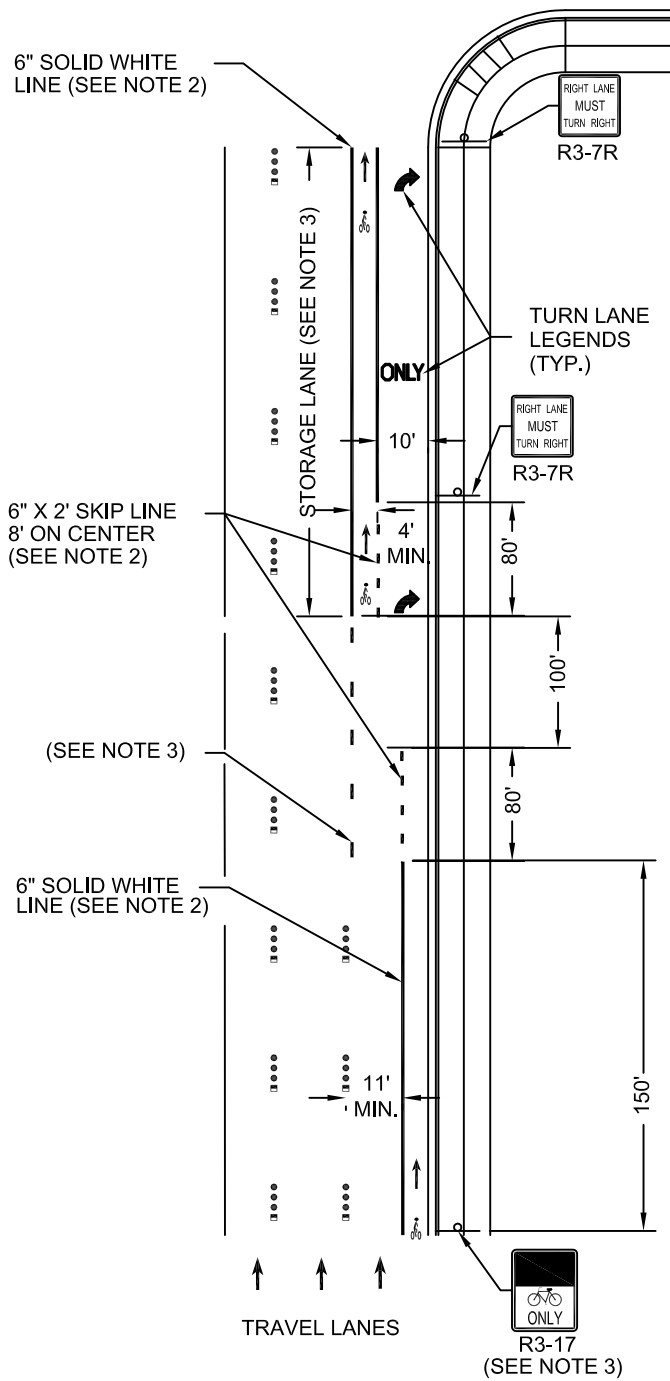
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
628	PAINTING TRAFFIC STRIPING	BICYCLE LANE APPROACH TO INTERSECTION WITH EXCLUSIVE RIGHT TURN LANE	
633	PAVEMENT MARKERS		
		DATE 12-08-09	DWG. NO. 246.1



NOTES:

1. STORAGE LENGTH TO BE DETERMINED BY TRAFFIC ENGINEER.
2. SEE DRAWING NUMBER 244.5 FOR BIKE LANE LEGEND AND SIGNAGE.
3. WHERE ADDITIONAL MOTORIST GUIDANCE IS DEEMED NECESSARY BY THE ENGINEER, INSTALL R3-7R SIGN, ARROW SYMBOL AND "ONLY" SYMBOL PAVEMENT MARKINGS FOR THE LENGTH OF THE STORAGE LINE. APPROVED TYPE II PAVEMENT MARKING FILM SHALL BE USED FOR SYMBOL MARKINGS.
4. SEE DWG. 246 NOTE 1 FOR STANDARD PAVEMENT MARKERS ADDED TURN LANE.
5. THE ABOVE DETAIL SHOULD BE FOLLOWED IN SITUATIONS WHERE THERE IS NOT ADEQUATE SPACE TO PROVIDE A SEPARATE BICYCLE LANE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
628	PAINTING TRAFFIC STRIPING		
633	PAVEMENT MARKERS	BICYCLE LANE TRANSITION TO SHARED LANE AT INTERSECTION	
		DATE 12-08-09	DWG. NO. 246.2



NOTES:

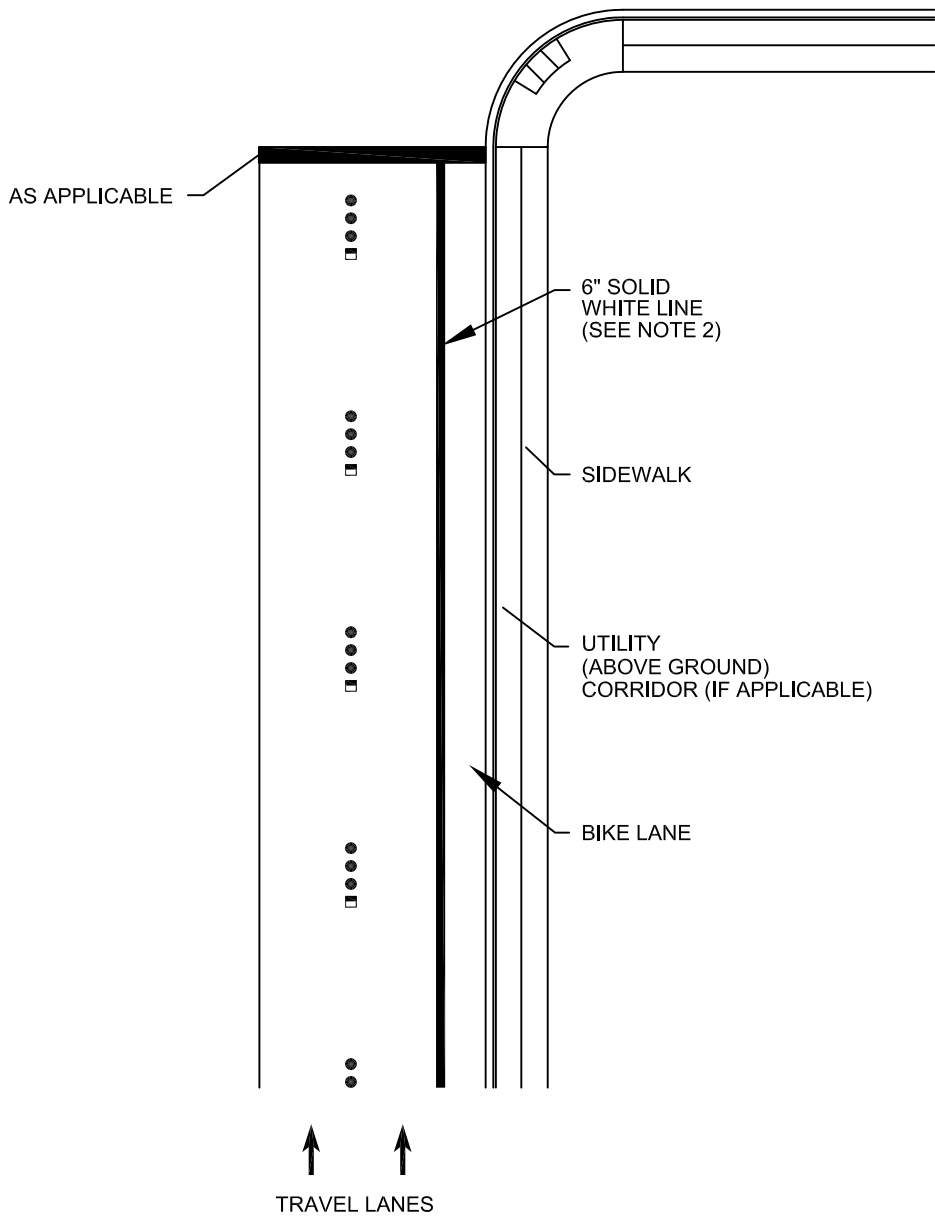
1. FORCED RIGHT-TURN LANES AND LONG RIGHT TURN POCKETS ARE NOT DESIRABLE FOR BICYCLISTS AND SHOULD BE AVOIDED WHEN POSSIBLE.
2. SEE DRAWING NO. 244.5 FOR BIKE LANE DELINEATION, LEGEND, AND SIGNAGE DETAILS.
3. SEE DRAWING NO. 246A FOR DETAILS ON THE FORCED TURN LANE.

SPECIFICATION REFERENCE

628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**BICYCLE LANE AT A
RIGHT TURN DROP LANE**

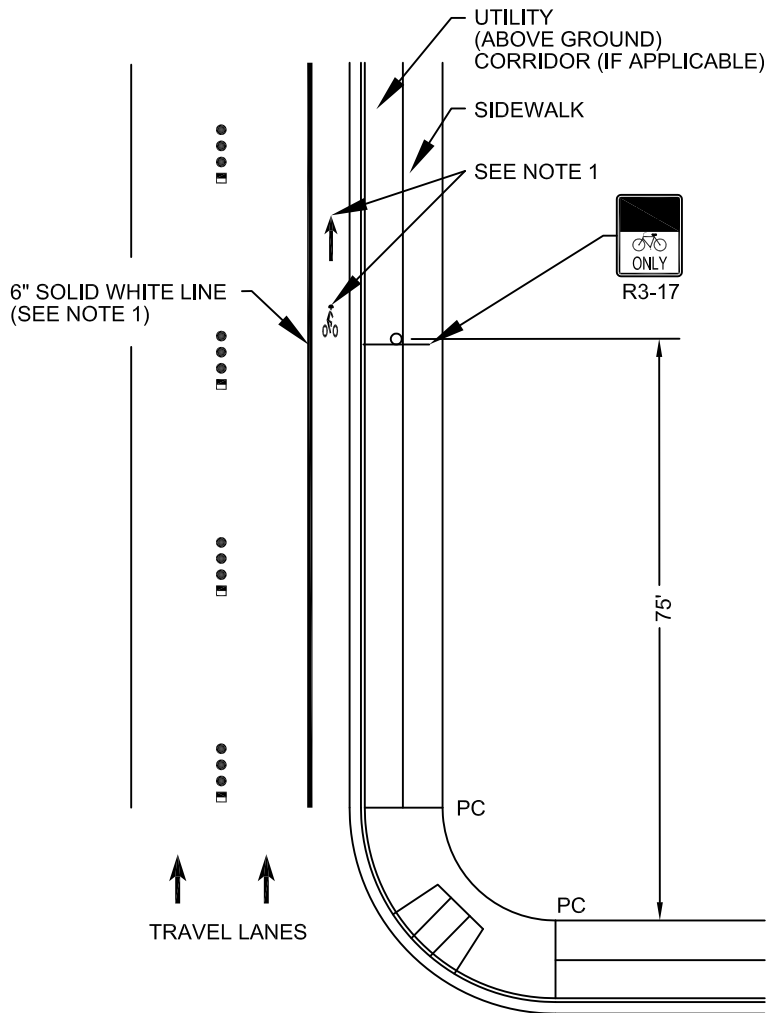


NOTES:

1. A SOLID BICYCLE LANE STRIPE SHOULD CONTINUE ACROSS DRIVEWAY ACCESS POINTS.
2. SEE DRAWING NO. 244.5 FOR BIKE LANE LEGEND AND SIGNAGE DETAILS.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING						
633	PAVEMENT MARKERS	BICYCLE LANE DELINEATION AT APPROACH TO INTERSECTION WITHOUT EXCLUSIVE RIGHT-TURN LANE					
		DATE	7-10-03	DWG. NO.	246.4	PAGE NO.	52.4



NOTES:

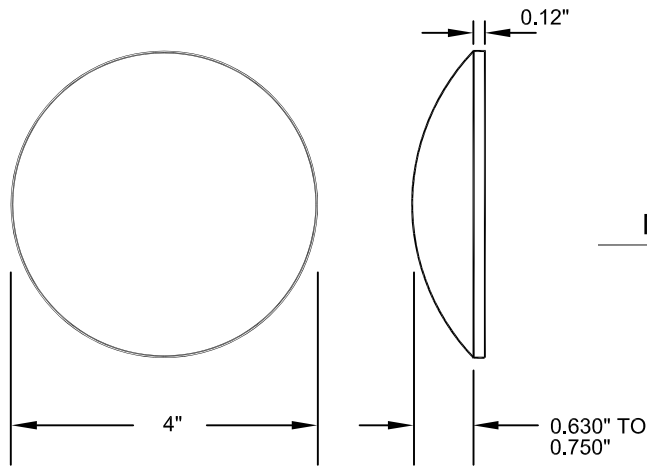
1. SEE DRAWING NUMBER 244.5 FOR BIKE LANE LEGEND AND SIGNAGE DETAILS.
2. USE 2 FOOT LONG SKIP LINE, 8 FEET ON CENTER, FOR LOCATIONS WITH BUS STOPS. FOR TYPICAL BUS STOP, TRANSITION FROM SOLID LINE TO SKIP LINE FOR 150 FEET CENTERED ON BUS STOP.

SPECIFICATION REFERENCE

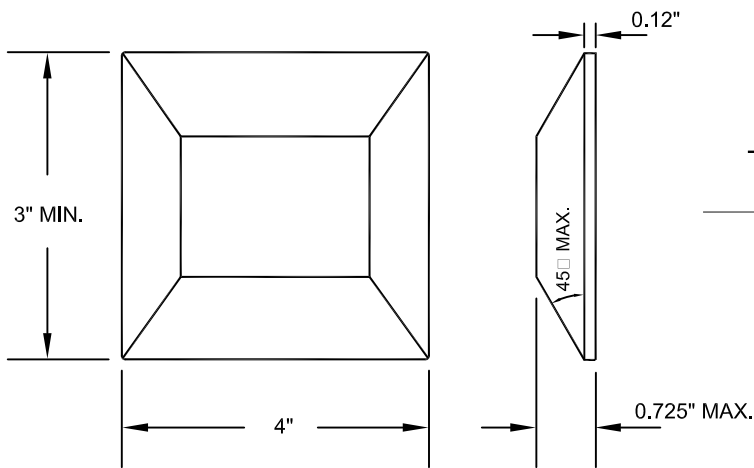
628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

BICYCLE LANE DEPARTURE
FROM INTERSECTIONS



**TYPE A & B
MARKER DETAIL**
(NON-REFLECTIVE)



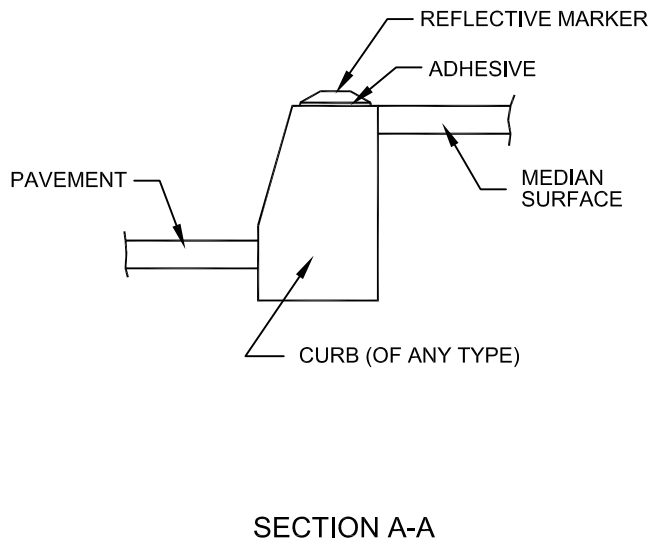
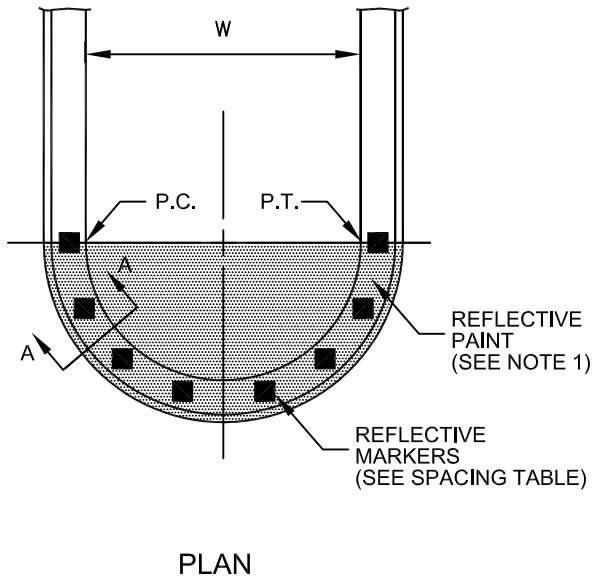
**TYPE C, D, E & F
MARKER DETAIL**
(REFLECTIVE)

LANE MARKER SCHEDULE

- TYPE A ● CIRCULAR WHITE CERAMIC MARKER
- TYPE B ○ CIRCULAR YELLOW CERAMIC MARKER
- TYPE C ■ TWO WAY YELLOW REFLECTOR
- TYPE D ▣ ONE WAY YELLOW REFLECTOR,
YELLOW TOWARD ONCOMING TRAFFIC
- TYPE E □ ONE WAY WHITE REFLECTOR,
WHITE TOWARD ONCOMING TRAFFIC
- TYPE F ■ TWO WAY WHITE AND RED REFLECTOR,
WHITE TOWARD ONCOMING TRAFFIC

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
633	PAVEMENT MARKERS	MARKER DETAILS AND LANE MARKER SCHEDULE		
		DATE 5-13-99	DWG. NO. 247	PAGE NO. 53

Effective 07/01/10 - 12/30/10



SPACING TABLE

"W"	NUMBER OF REFLECTORS PER MEDIAN NOSE *
1.0' TO 2.0'	3
2.0' TO 3.0'	4
3.0' TO 4.0'	5
4.0' & GREATER	1 EACH FOR EVERY 1.0' OF CURB LENGTH

* 1 MARKER EACH SHALL BE PLACED ON THE P.C. AND THE P.T. OF THE MEDIAN NOSE; ALL OTHERS SPACED EQUALLY BETWEEN P.T. & P.C.

NOTES:

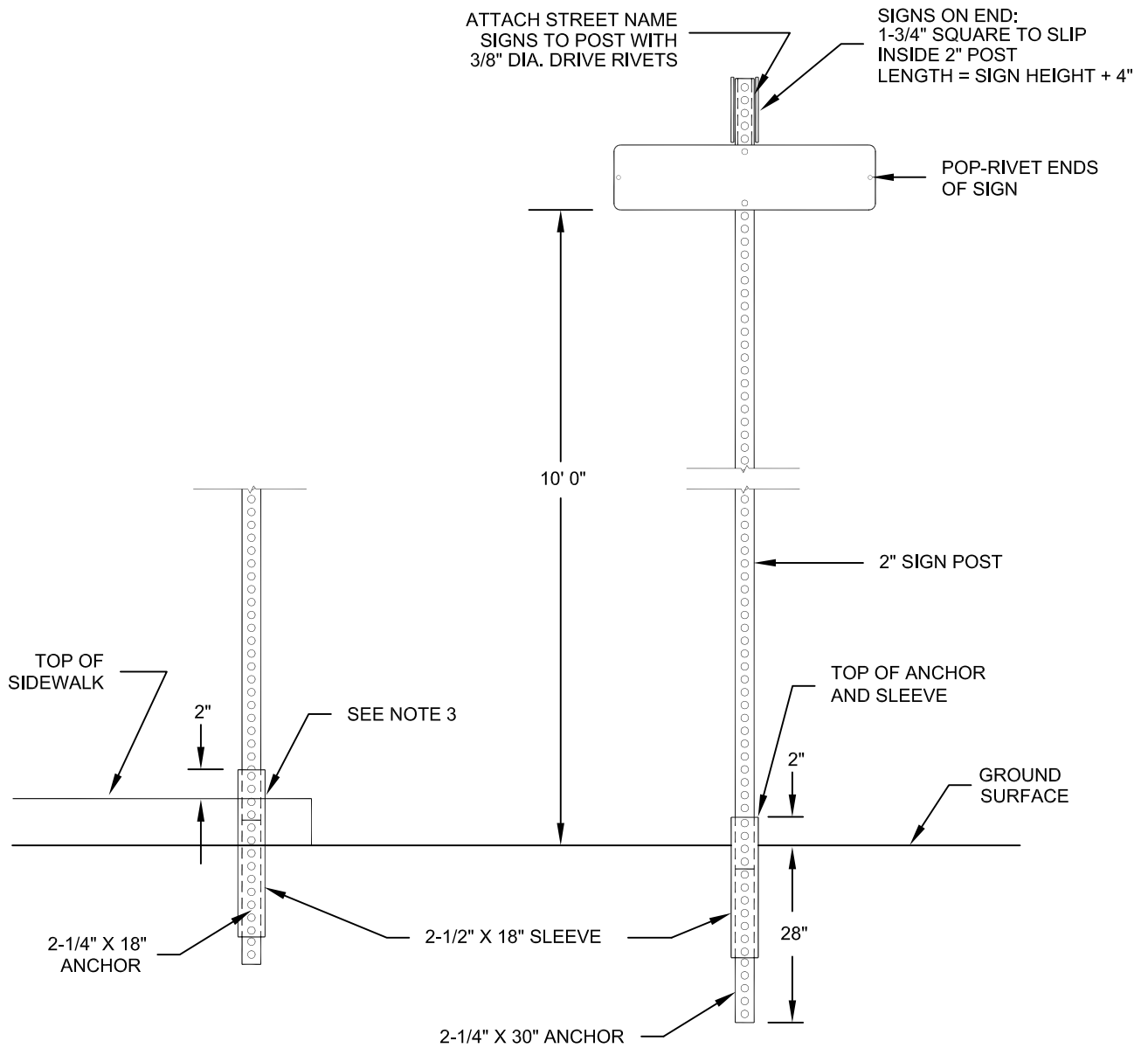
1. ENTIRE MEDIAN SHALL BE PAINTED WITH REFLECTIVE PAINT, OF SAME COLOR AS REFLECTIVE MARKERS, FROM THE MEDIAN NOSE BACK 5 FEET OR TO THE P.C., WHICHEVER IS GREATER.
2. REFLECTIVE PAVEMENT MARKERS USED ON MEDIAN SHALL CONFORM TO STANDARD DRAWING NO. 247.
3. ORIENTATION OF THE REFLECTIVE MARKERS FACES SHALL BE MADE IN THE FIELD TO ENSURE THAT MARKERS ARE AIMED AT APPROACHING VEHICLES TO BEST ADVANTAGE, ESPECIALLY IN HORIZONTALLY CURVED ROAD SECTIONS.

SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

628	PAINTING TRAFFIC STRIPING
633	REFLECTIVE PAVEMENT MARKERS

MEDIAN NOSE MARKINGS



NOTES:

1. ALL COMPONENTS SHALL BE SQUARE POST, PERFORATED ON ALL FOUR SIDES.
2. ATTACH ANCHOR AND SLEEVE TOGETHER PRIOR TO DRIVING INTO GROUND. LEAVE AT LEAST ONE HOLE, BUT NO MORE THAN TWO, ABOVE GROUND OR ABOVE SIDEWALK.
3. FOR SIDEWALK INSTALLATION, DRILL SIDEWALK WITH A 3" HOLE, THE CENTER TO BE 6" FROM BACK OF SIDEWALK.
4. ATTACH POST TO ANCHORING SYSTEM BY USING AT LEAST TWO 3/8" DIA. DRIVE RIVETS.
5. PROVIDE 4" MINIMUM LAP BETWEEN POST AND THE ANCHOR/SLEEVE ASSEMBLY.
6. ALL STREET NAME SIGNS SHALL BE 9 INCH STANDARD IN THE CITY OF MESQUITE ONLY.

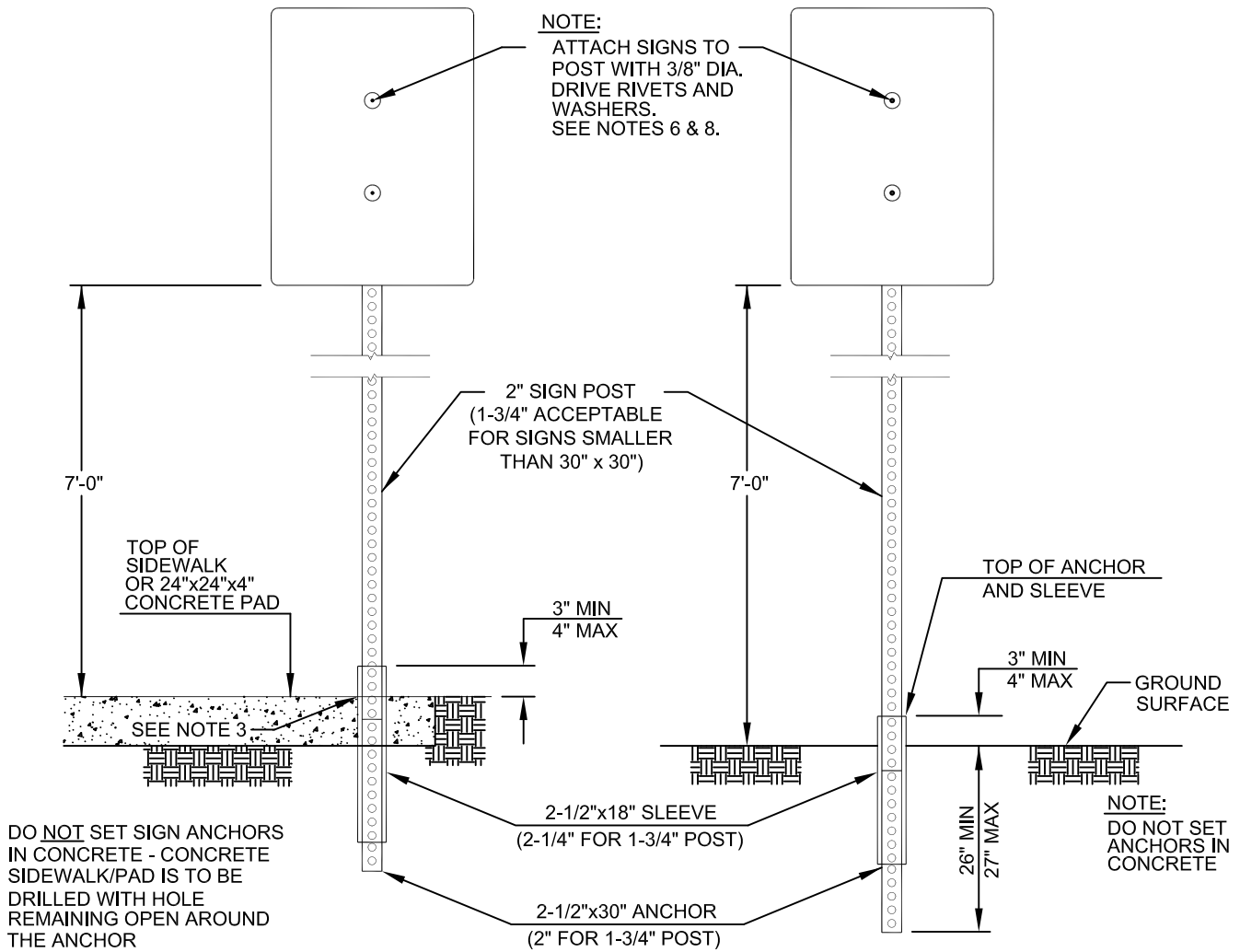
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA						
631	STREET NAME SIGNS							
		STANDARD STREET NAME SIGN POST INSTALLATION						
					DATE	5-20-04	DWG. NO.	249
					PAGE NO.	55		

URBAN INSTALLATIONS

SEE NOTE 9

RURAL INSTALLATIONS

(NO LANDSCAPING)



NOTES:

1. ALL COMPONENTS SHALL BE MINIMUM 12 GA. SQUARE POST WITH 7/16" PUNCHED THRU HOLES @ 1' ON CENTER, ON ALL FOUR SIDES. ANCHORS SHALL BE TWO PIECE BREAKAWAY ANCHORS.
2. ATTACH ANCHOR AND SLEEVE TOGETHER PRIOR TO DRIVING INTO GROUND. LEAVE AT LEAST TWO HOLES, BUT NO MORE THAN THREE HOLES ABOVE GROUND OR ABOVE SIDEWALK.
3. FOR SIDEWALK INSTALLATION, DRILL SIDEWALK AND CONCRETE PAD INSTALLATION, DRILL A 3" TO 4" DIA. HOLE (DEPENDENT UPON ANCHOR SIZE), THE CENTER TO BE 6" FROM THE BACK OF SIDEWALK.
4. ATTACH POST TO ANCHORING SYSTEM BY USING AT LEAST TWO 3/8" DIA. DRIVE RIVETS.
5. PROVIDE 4" MINIMUM LAP BETWEEN BOTTOM OF POST AND THE BOTTOM OF THE ANCHOR/SLEEVE ASSEMBLY.
6. SIGNS LARGER THAN 24"x30" REQUIRE 3/8" x 1-1/2" FENDER WASHERS UNDER DRIVE RIVETS.
7. "U-CHANNEL" POSTS ARE NOT ACCEPTABLE.
8. BOLTS IN LIEU OF DRIVE RIVETS ARE NOT ACCEPTABLE.
9. ALL URBAN SIGN INSTALLATIONS ARE TO BE INSTALLED IN A CONCRETE SIDEWALK, OR IN A CONCRETE PAD (24"x24"x4") WHEN NO SIDEWALK EXISTS.
10. INSTALLATION OF SIGNS SHALL MEET LATEST ADA REQUIREMENTS.
11. SIGNS SHALL HAVE A STICKER AT THE BACK WITH THE NAME OF THE CONTRACTOR AND THE DATE OF INSTALLATION.

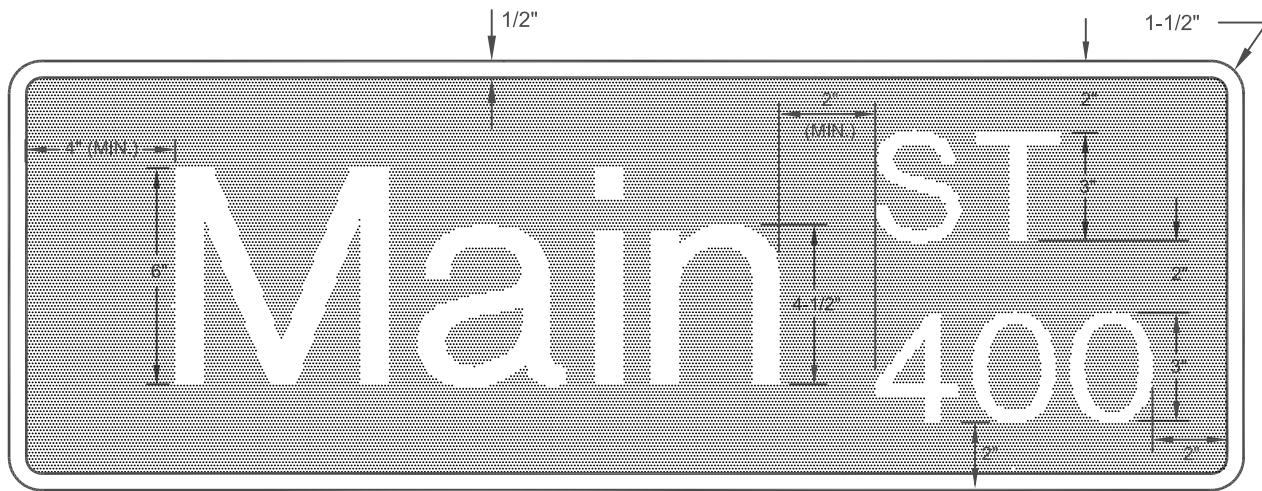
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

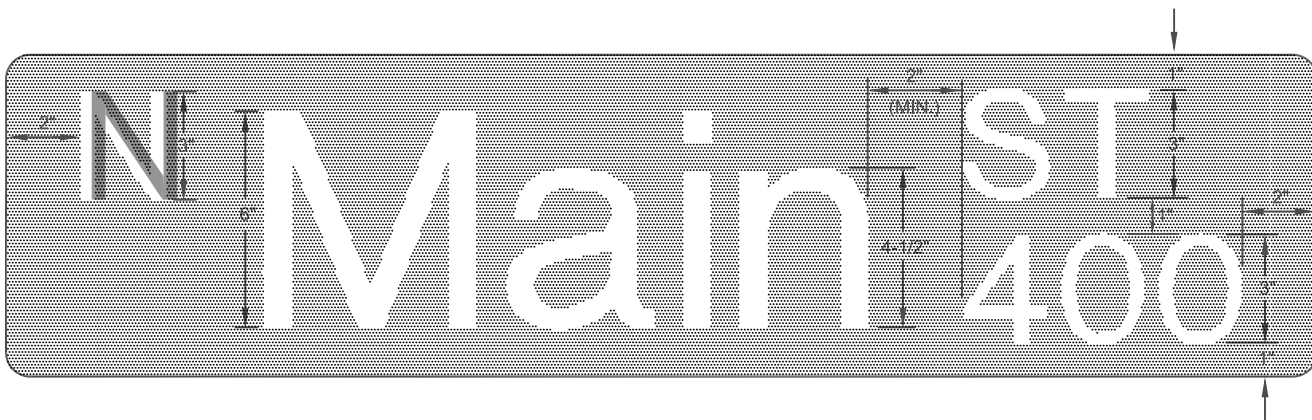
631 STREET NAME SIGNS

SIGN INSTALLATION DETAIL

DATE 11-10-04 DWG. NO. 249A PAGE NO. 55A



12" (MAJOR STREETS)



9" (MINOR STREETS)

NOTES:

1. SIGN SHALL BE WHITE LETTERS AND NUMBERS ON GREEN BACKGROUND. (THE CITY OF NORTH LAS VEGAS BACKGROUND IS BLUE.) CUT-OUT LETTERS AND NUMBERS ARE NOT ACCEPTABLE (EXCEPT FOR THE BLOCK NUMBER).
2. REFLECTIVE SHEETING MATERIAL SHALL BE CLASS 6.
3. PRIMARY COPY FOR 9" AND 12" SIGNS SHALL BE 6" SERIES 'C' UPPERCASE WITH 4 1/2" SERIES 'C' LOWERCASE; HOWEVER, WHEN DESCENDERS ARE REQUIRED ON 9" SIGNS, PRIMARY COPY SHALL BE 5 1/2". ORDINAL, SUFFIX AND BLOCK NUMBER SHALL BE 3" SERIES 'C' UPPERCASE. (ORDINAL MAY BE OMITTED FROM 12" SIGNS, EXCEPT IN CLARK COUNTY.) SPACING BETWEEN LETTERS SHALL BE AS ON SHEET 2 OF THIS DRAWING.
4. THE SIGN SHALL HAVE A MINIMUM LENGTH OF 30". WHERE EXTRA LENGTH IS REQUIRED, IT SHALL BE PROVIDED IN 6" INCREMENTS. GROUND MOUNTED SIGNS SHALL HAVE A MAXIMUM LENGTH OF 42".
5. BOTH SIGNS PLACED ON MAJOR STREETS WITH RIGHTS-OF-WAY 80' OR GREATER SHALL HAVE A HEIGHT OF 12"; SIGNS PLACED ON MINOR STREETS WITH RIGHTS-OF-WAY OF LESS THAN 80' SHALL HAVE A HEIGHT OF 9".
6. 12" SIGNS SHALL HAVE A 1/2" WHITE BORDER AT THE EDGE.
7. SIGN BLANKS SHALL HAVE ROUNDED CORNERS.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
631	STREET NAME SIGNS	STREET NAME SIGNS FACE COPY			
716	SIGN MATERIALS				
DATE	6-8-06	DWG. NO.	250 (1 OF 2)	PAGE NO.	56

SPACING OF STREET NAME SIGN LEGENDS

SPACING FOR STREET NAME SIGN LEGENDS SHALL BE OBTAINED BY MODIFICATION TO THE REQUIREMENTS OF THE FHWA STANDARD SPACING CHART FOR 6" UPPERCASE LETTERS. THE FOLLOWING STEPS SHALL BE USED TO DETERMINE REQUIRED SPACING:

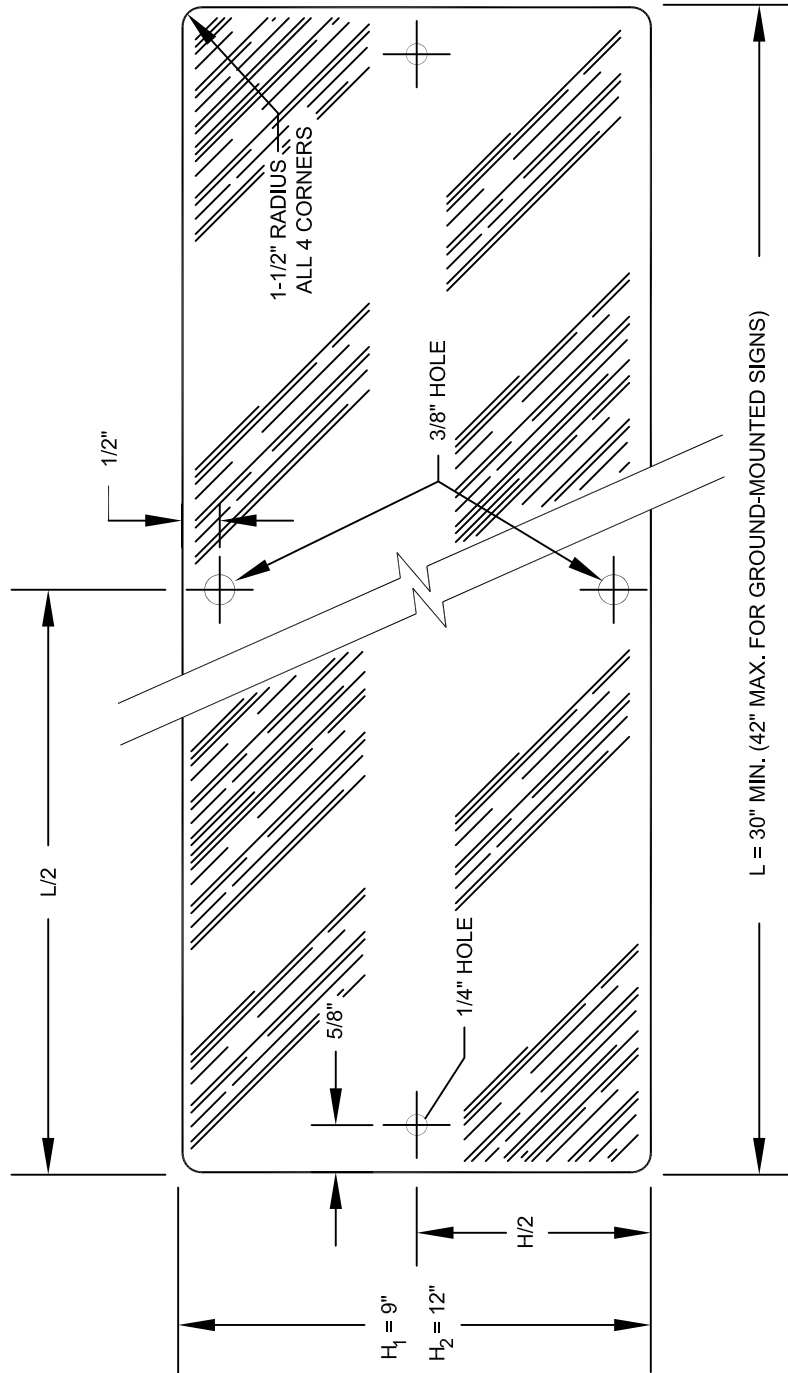
1. SIGN LAYOUT COMPUTER SOFTWARE SHALL BE EVALUATED TO DETERMINE THE "CORRECTION FACTOR" NECESSARY FOR LAYOUT SOFTWARE LETTER SPACING TO BE APPROXIMATELY EQUAL TO THE FHWA STANDARD SPACING FOR UPPERCASE LETTERS.
2. CORRECTION FACTOR SHALL BE USED TO ADJUST THE SPACING FOR THE LOWERCASE LETTERS.
3. SPACING FOR STREET NAME SIGN LEGENDS SHALL BE EQUAL TO 110% OF THE "CORRECTED" LAYOUT SOFTWARE LETTER SPACING.

(SAME STEPS ARE TO BE FOLLOWED WHEN FONT SIZE OF LEGEND IS REDUCED IN ORDER NOT TO EXCEED THE MAXIMUM LENGTH LIMITATIONS.)

IF LEGEND SPACED ACCORDING TO RECOMMENDED PROCEDURE ABOVE EXCEEDS THE MAXIMUM ALLOWABLE SIGN LENGTH (42" FOR GROUND-MOUNTED), THE FOLLOWING ACTIONS, LISTED IN PRIORITY ORDER, SHALL BE TAKEN TO REDUCE LENGTH OF THE SIGNBLANK.

- A. REDUCE THE FONT TO 5 1/2" SERIES 'C'.
- B. REDUCE THE SPACING TO 100% OF THE "FEDERAL STANDARD".
- C. REDUCE THE FONT TO 5 1/2" SERIES "B".
- D. CONSIDER ABBREVIATING ANY LEGEND WORDS WHICH ARE EXTREMELY COMMON (I.E., "MTN" FOR "MOUNTAIN") SUCH ABBREVIATIONS MUST BE APPROVED BY THE TRAFFIC ENGINEER AND THE FIRE DEPARTMENT.
- E. REDUCE THE LEADING AND TRAILING BLANK GREEN SPACE BY 50%.
- F. CONSTRUCT THE SIGN ACCORDING TO THE STANDARD SPACING WHICH WILL BE GREATER THAN 42" IN LENGTH, AND MOUNT ON A STREETLIGHT POLE OR OTHER ELEVATED MOUNT AS APPROVED BY THE TRAFFIC ENGINEER WITH APPROPRIATE SIGN BRACING AND MOUNTING HARDWARE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
631	STREET NAME SIGNS	STREET NAME SIGNS LETTER SPACING					
716	SIGN MATERIALS						
		DATE	6-12-97	DWG. NO.	250 (2 OF 2)	PAGE	56A



L = 30" MIN. (42" MAX. FOR GROUND-MOUNTED SIGNS)

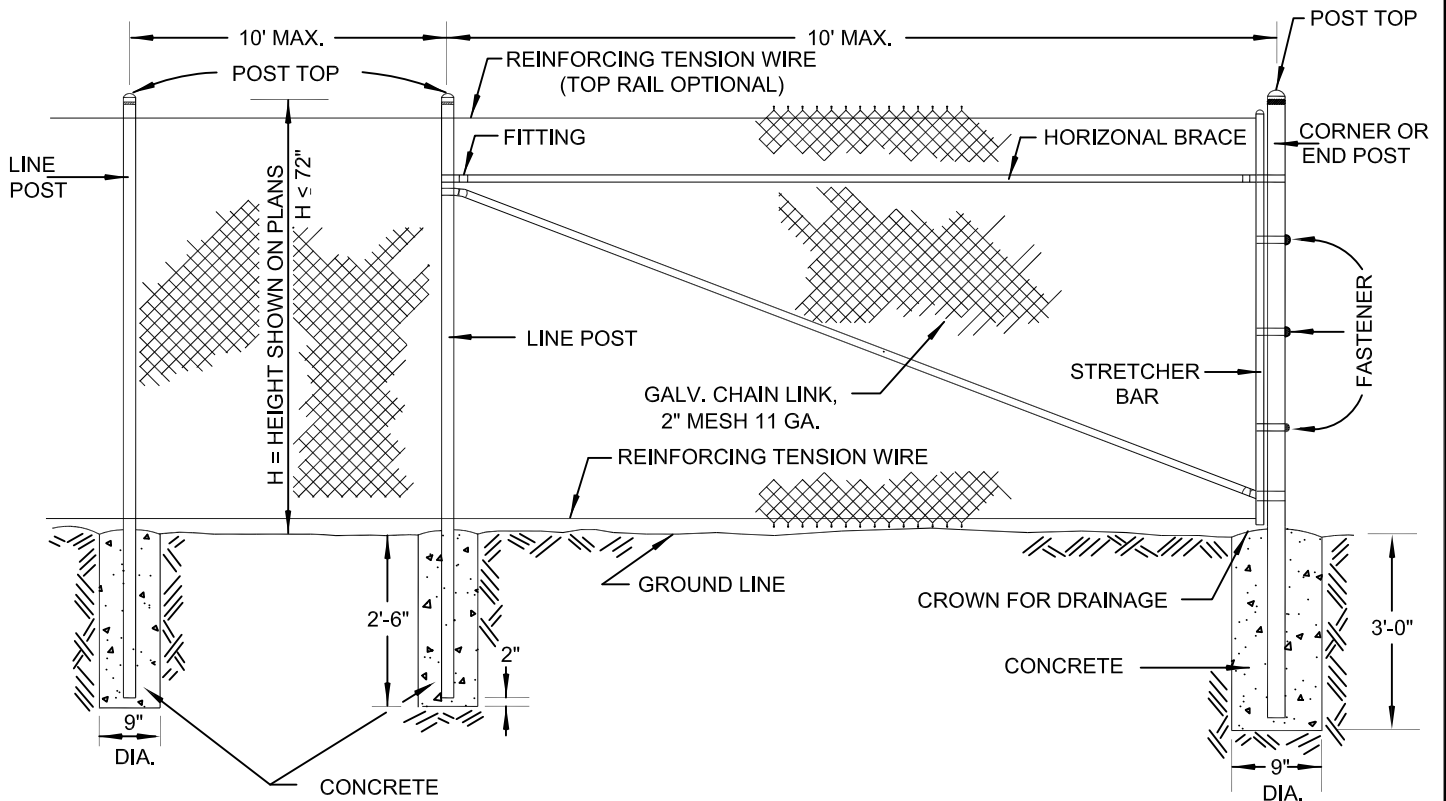
ALUMINUM BLANK

5052-H38 OR 6061-T6, HEAT-TREATED, HIGH TENSILE, DEGREASED AND ALODINE 1200 FINISH.
THICKNESS TO BE 0.080" FOR SIGNS LESS THAN 36" AND 0.100" FOR SIGNS 36" AND LONGER.

NOTE:

1. FOR SIGN FACE SPECIFICATIONS SEE STANDARD DRAWING NO. 250.

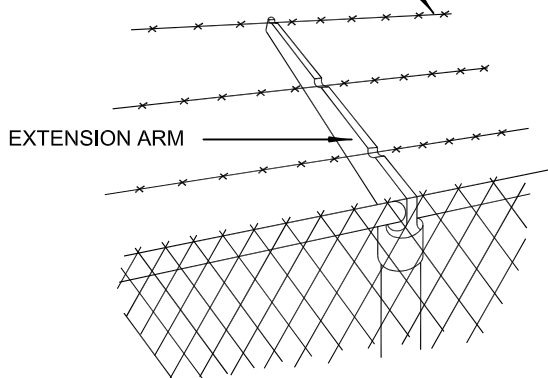
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
631	STREET NAME SIGNS	STREET NAME SIGN BLANKS			
716	SIGN MATERIALS				
DATE	6-8-06	DWG. NO.	251	PAGE	57



NOTES:

1. FENCING SHALL BE CHAIN LINK AND SHALL CONSIST OF GALVANIZED CHAIN LINK FABRIC ON STEEL POSTS.
 - (A) ALL POSTS TOPS SHALL BE FITTED WITH SUITABLE FINIALS.
 - (B) BRACES SHALL BE SPACED APPROXIMATELY 12" BELOW TOP OF TERMINAL POSTS AND SHALL EXTEND FROM END, GATE, OR CORNER POSTS TO FIRST ADJACENT LINE POST.
 - (C) ALL FITTINGS SHALL BE HOT-DIPPED GALVANIZED MALLEABLE, CAST IRON, OR PRESSED STEEL.
 - (D) TOP AND BOTTOM SELVAGES OF THE FENCE SHALL HAVE A TWISTED AND BARBED FINISH.
2. BARBED WIRE, EXTENSION ARMS, AND TOP HORIZONTAL RAILS SHALL BE INSTALLED ONLY WHEN SHOWN ON THE PLANS AND/OR CALLED FOR IN THE SPECIAL PROVISIONS.

BARB WIRE (IF SPECIFIED)
2 STRAND, 12 GA., 4 PT.



EXTENSION ARM & BARBED WIRE

TABLE I

FOR CHAIN LINK FENCE 72" AND LESS

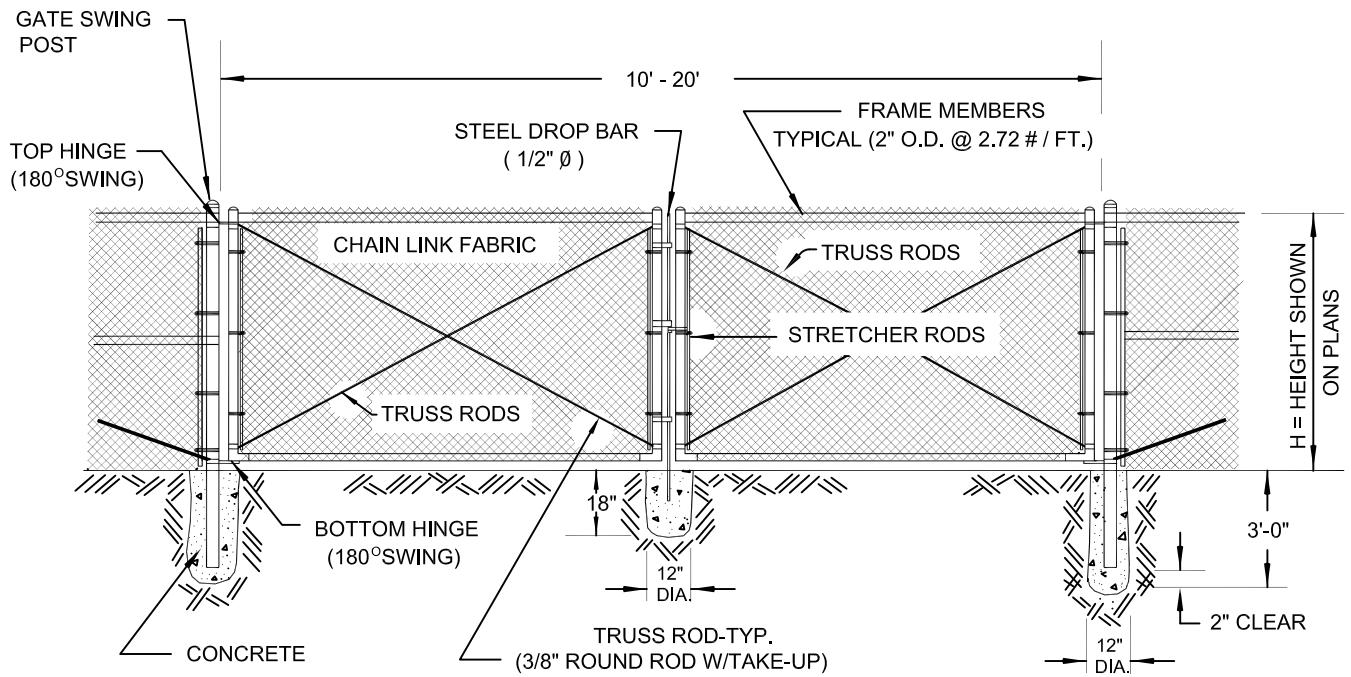
LOCATION	MIN. SIZE	MIN. WEIGHT
END, CORNER & PULL	2.351 O.D.	3.10
LINE	2.00 O.D.	2.72
BRACES	1.630 O.D.	2.27
TOP RAIL	1.630 O.D.	2.27

SPECIFICATION REFERENCE

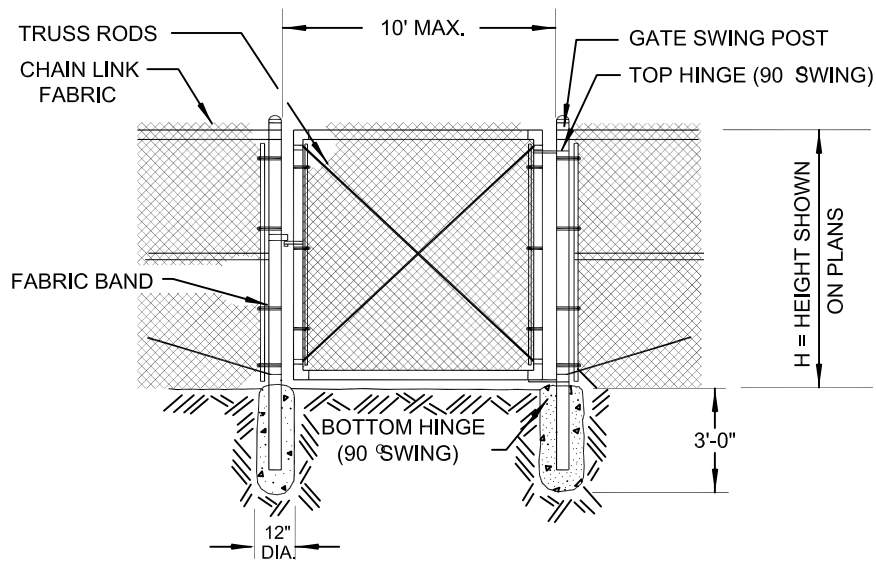
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

501	CONCRETE
616	FENCING

**CHAIN LINK FENCE
(72" HIGH OR LESS)**



DOUBLE SWING GATE



SINGLE GATE

GATE SWING POST DIA.	
GATE SIZE	PIPE DIA.
6' AND LESS	3" O.D. - 5.79 LBS./FT.
6' - 10'	4" O.D. - 9.10 LBS./FT.

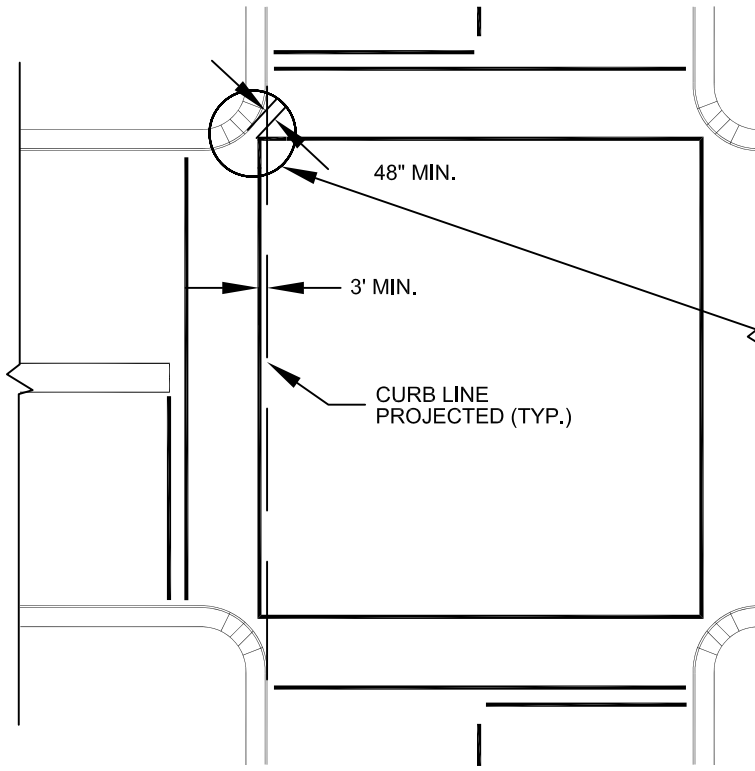
SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

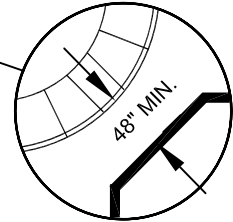
501 CONCRETE

616 FENCING

CHAIN LINK GATES



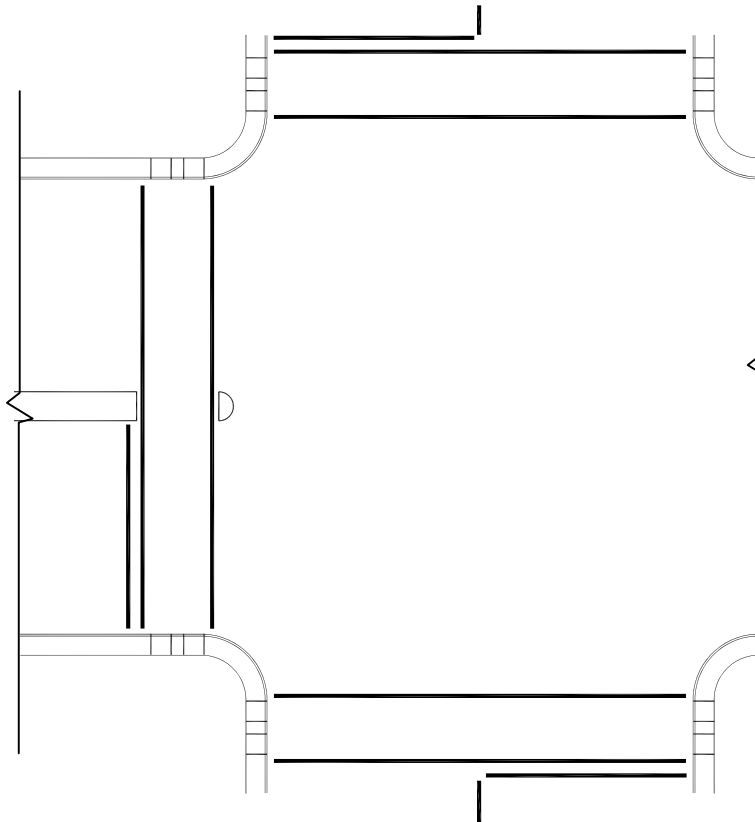
TYPICAL MARKING
CURB RAMP IN
MIDDLE OF
CURB RETURN



OPTIONAL DETAIL

NOTE:

USE MARKING PER
OPTIONAL DETAIL IF
NECESSARY TO OBTAIN
3' MINIMUM CLEARANCE
BETWEEN CROSSWALK
AND CURB LINE
PROJECTED.

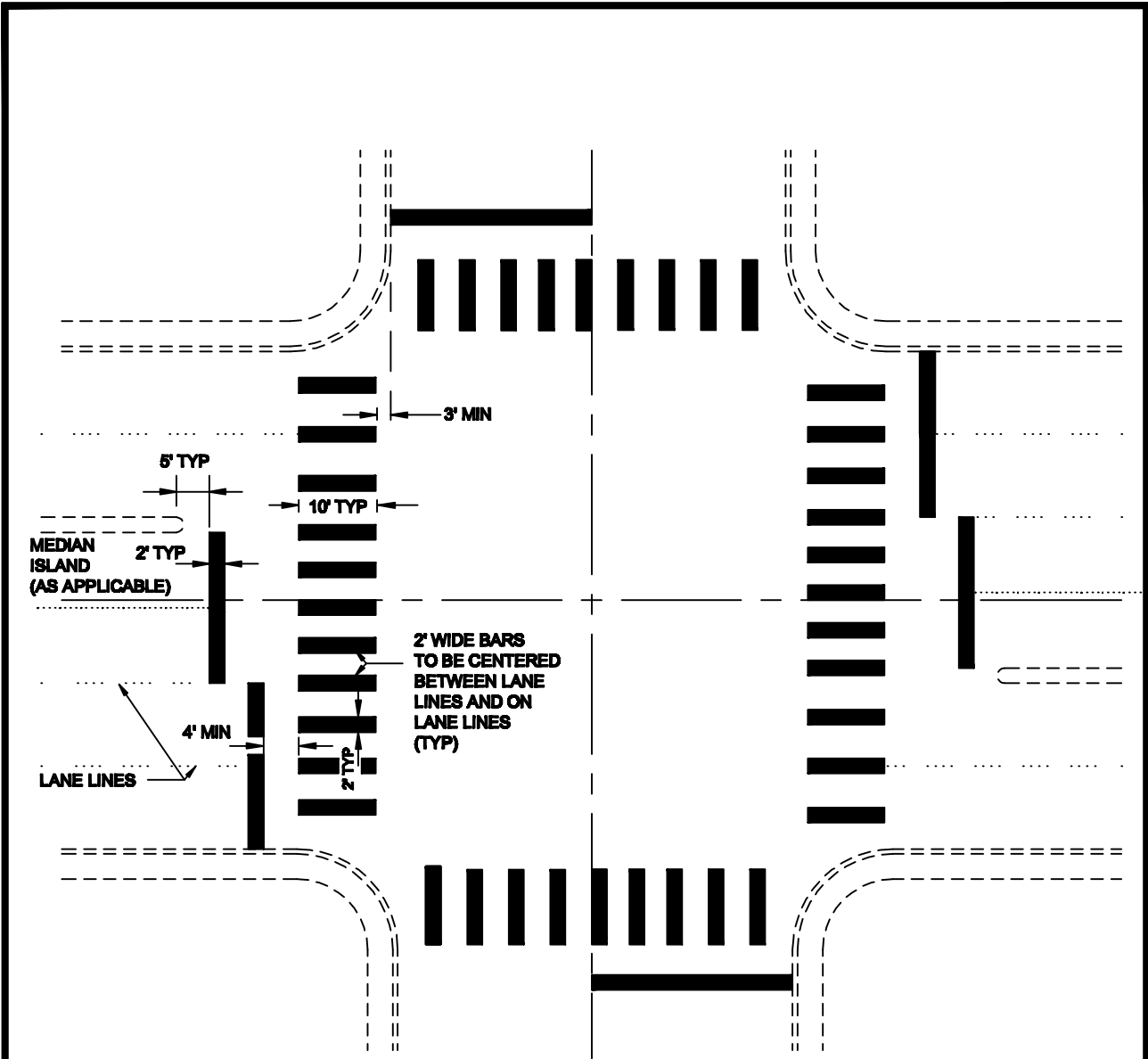


TYPICAL MARKING
CURB RAMP
ADJOINING
CURB RETURN

SPECIFICATION REFERENCE

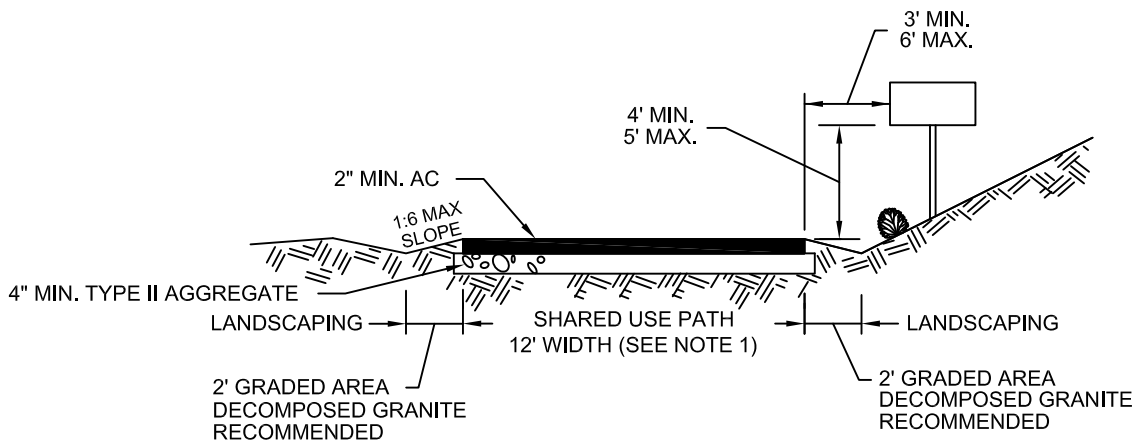
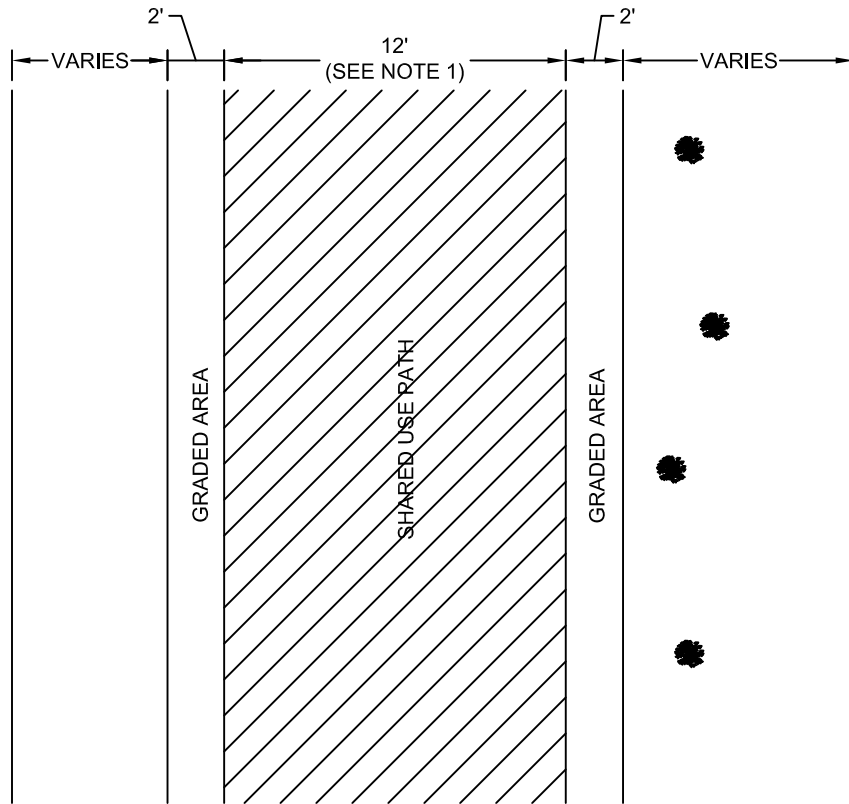
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

CROSSWALK
MARKINGS
(EXCEPT CLARK COUNTY)



**TYPICAL CROSSWALK
STRIPING DETAIL**

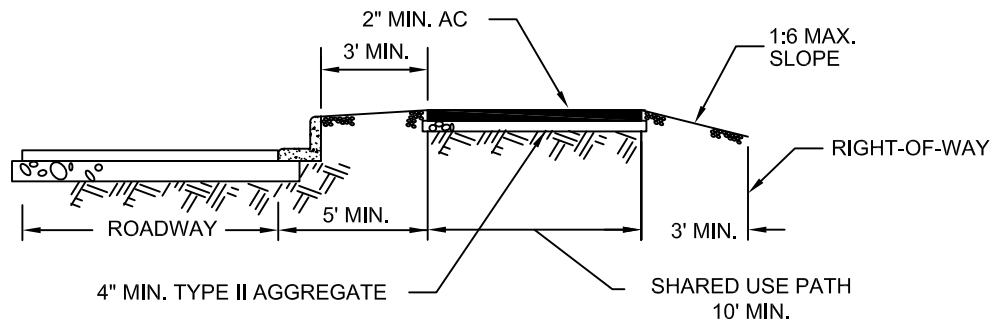
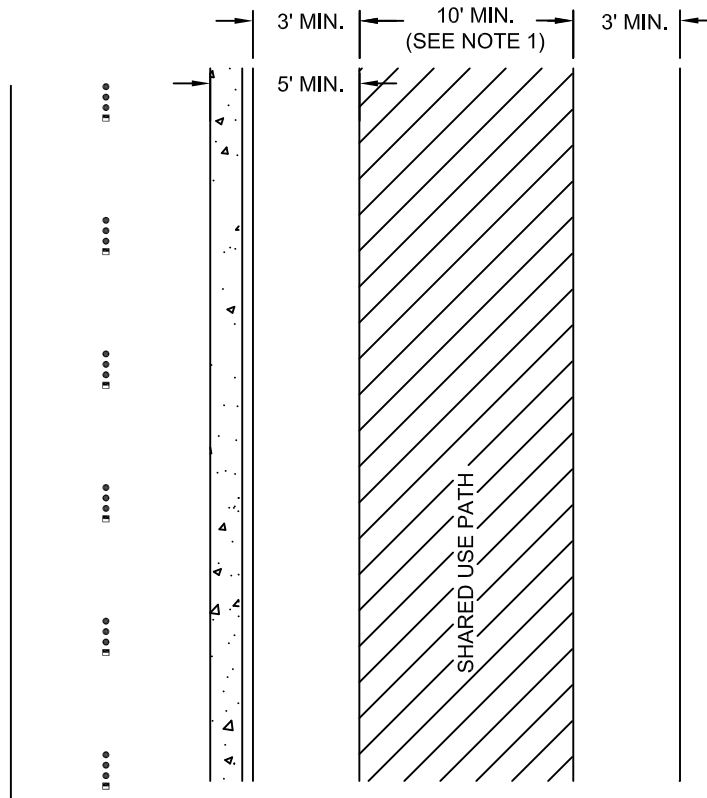
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
		CROSSWALK MARKINGS	
		DATE 11-12-09	DWG. NO. 254A



NOTES:

1. 12 FOOT WIDTH IS RECOMMENDED. 10 FOOT WIDTH IS ALLOWABLE ALONG A PATH PARALLEL TO A ROADWAY OR WHERE SPACE IS LIMITED. PAVEMENT AND BASE DEPTH WILL VARY BASED ON SOIL CONDITIONS. PORTLAND CEMENT CONCRETE MAY BE USED INSTEAD OF ASPHALT.
2. SEE DRAWING NUMBER 255.1 FOR SHARED USE PATH ALONG A ROADWAY.
3. SEE THE GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES, AASHTO 1999, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR ADDITIONAL GUIDELINES AND STANDARDS.
4. SEE LOCAL JURISDICTIONS FOR LANDSCAPING REQUIREMENTS.

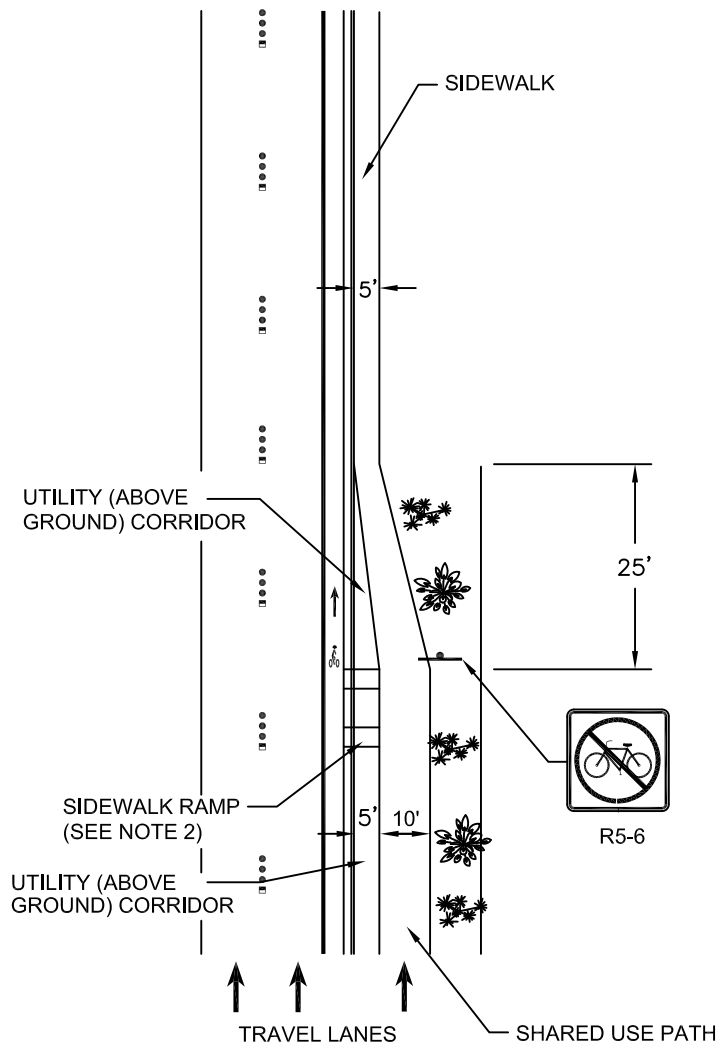
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	255	PAGE NO.	59.2



NOTES:

1. 12 FOOT WIDTH IS RECOMMENDED. 10 FOOT WIDTH IS ALLOWABLE ALONG A PATH PARALLEL TO A ROADWAY OR WHERE SPACE IS LIMITED. PAVEMENT AND BASE DEPTH WILL VARY BASED ON SOIL CONDITIONS. PORTLAND CEMENT CONCRETE (PCC) MAY BE USED INSTEAD OF ASPHALT AND PCC MAY BE REQUIRED BY THE LOCAL JURISDICTION.
2. SEE DRAWING NUMBER 255 FOR SHARED USE PATH NOT ALONG A ROADWAY.
3. SEE THE GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES, ASSHTO 1999, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR ADDITIONAL GUIDELINES AND STANDARDS.
4. SEE LOCAL JURISDICTIONS FOR LANDSCAPING REQUIREMENTS.
5. 3 FOOT LATERAL CLEARANCE RECOMMENDED BETWEEN EDGE OF PATH AND A FIXED OBJECT, 2 FOOT MINIMUM.
6. IF 16 FEET IS NOT AVAILABLE FROM THE BACK OF CURB TO THE RIGHT-OF-WAY LINE, A BICYCLE LANE/ROUTE AND THE SIDEWALK WILL SUBSTITUTE FOR THE PATH. PLACE A PATH ENDS SIGN (W9) 25 FEET IN ADVANCE OF THE PATH ENDING.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH ALONG A ROADWAY					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	255.1	PAGE NO.	59.3



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMP DETAILS.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	TYPICAL TRANSITION SECTION FROM SHARED USE PATH ALONG ROADWAY TO SIDEWALK					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	255.2	PAGE NO.	59.4

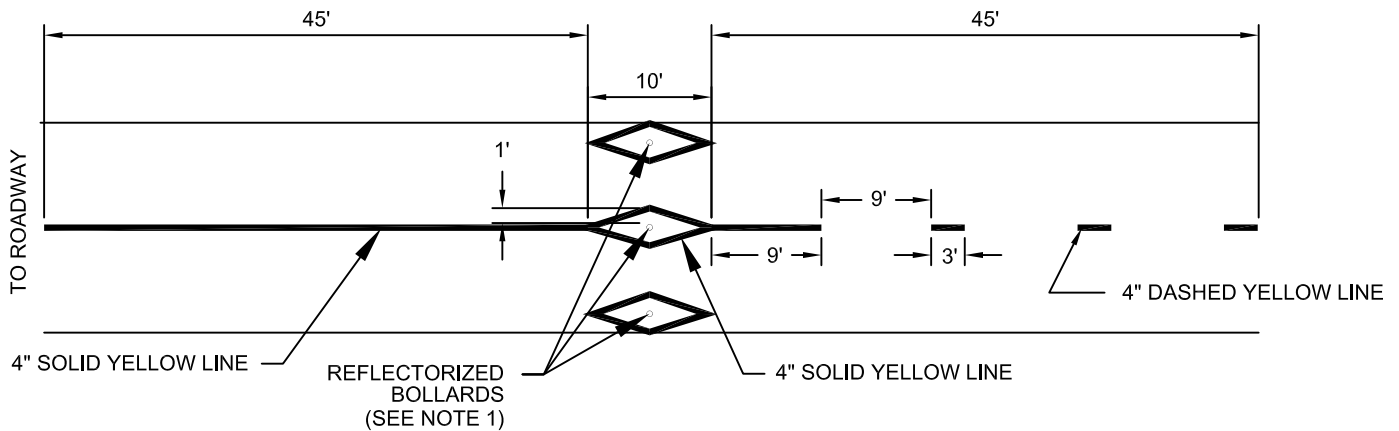
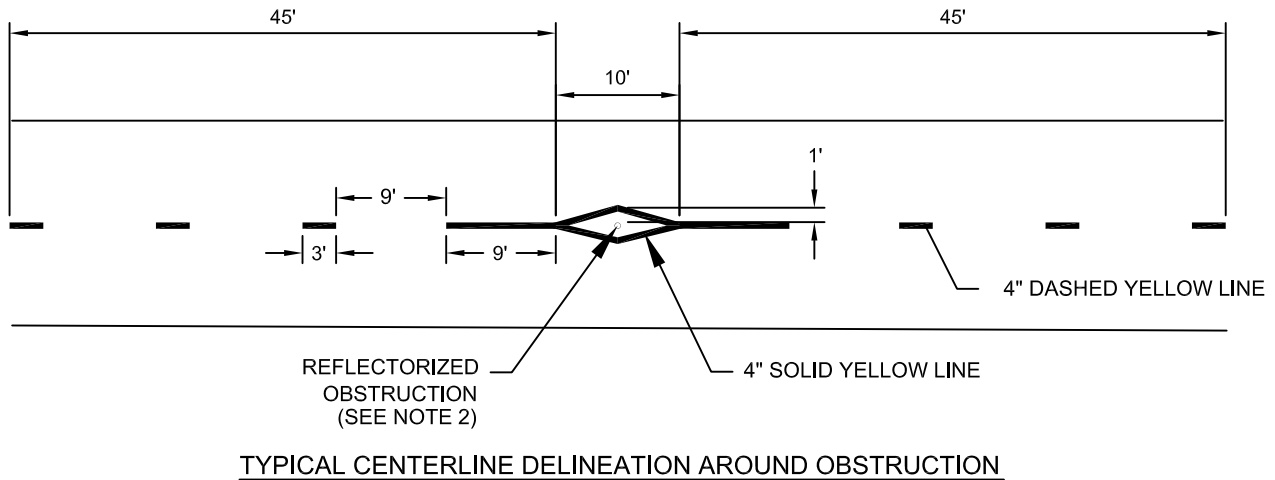
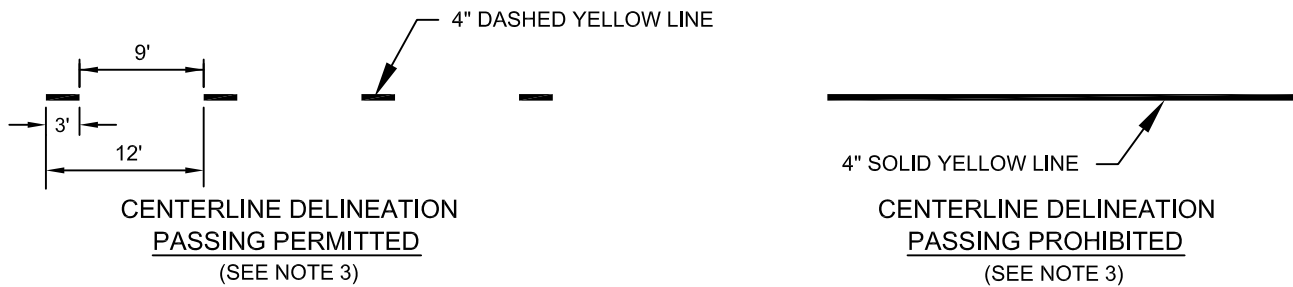
SIGN SIZES FOR SHARED-USE PATHS

<u>MUTCD CODE</u>	<u>SIGN</u>	<u>MINIMUM SIGN SIZE (IN)</u>
R1-1	STOP	18 X 18
R1-2	YIELD	24 X 24 X 24
R3-16, 16A, 17, 17A	BICYCLE LANE	24 X 30
R4-1, 2, 3, 7	MOVEMENT RESTRICTION	12 X 18
R4-4	BEGIN RIGHT TURN LANE YIELD TO BIKES	36 X 30
R5-3	NO MOTOR VEHICLES	24 X 24
R5-6	BICYCLE PROHIBITION	24 X 24
R7-9, 9A	NO PARKING BIKE LANE	12 X 18
R9-3A	PEDESTRIANS PROHIBITED	18 X 18
R9-5, 6	BICYCLE REGULATORY	12 X 18
R9-7	SHARED-USE PATH RESTRICTION	12 X 18
R15-1	RAILROAD CROSSBUCK	24 X 4.5
W1-1, 2, 3, 4, 5	TURN AND CURVE WARNING	18 X 18
W1-6, 7	ARROW WARNING	24 X 12
W2-1, 2, 3, 4, 5	INTERSECTION WARNING	18 X 18
W3-1A, 2A, 3	STOP, YIELD, SIGNAL AHEAD	18 X 18
W5-2A	ROAD NARROWS	18 X 18
W5-4	BIKEWAY NARROWS	18 X 18
W7-5	HILL SIGN	18 X 18
W8-1, 2	BUMP OR DIP	18 X 18
W8-10	BICYCLE SURFACE CONDITION	18 X 18
W10-1	ADVANCE GRADE CROSSING	18 DIA.
W11-1	BICYCLE CROSSING	18 X 18
W12-2	LOW CLEARANCE	18 X 18
W16-1	SHARE THE ROAD PLAQUE	24 X 30
D1-1	SUPPLEMENTAL BIKE ROUTE PLAQUE	24 X 6
D4-3	BICYCLE PARKING	12 X 18
D11-1	BIKE ROUTE	24 X 18
M1-8	BIKE ROUTE MARKER	12 X 18
M1-9	BIKE ROUTE MARKER	18 X 24
M4-11, 12, 13	SUPPLEMENTAL BICYCLE ROUTE GUIDE	12 X 4
M7-1, 2, 3, 4, 5, 6, 7	ROUTE MARKER SUPPLEMENTAL PLAQUES	12 X 9

NOTES:

1. SIGN TABLE INSERTED FROM MUTCD FOR REFERENCE. SEE CURRENT MUTCD FOR UPDATED INFORMATION.
2. SIGNS R3-16(A), R3-17(A), R4-4, W5-2A, AND W16-1 NOT USED FOR SHARED USE PATHS.

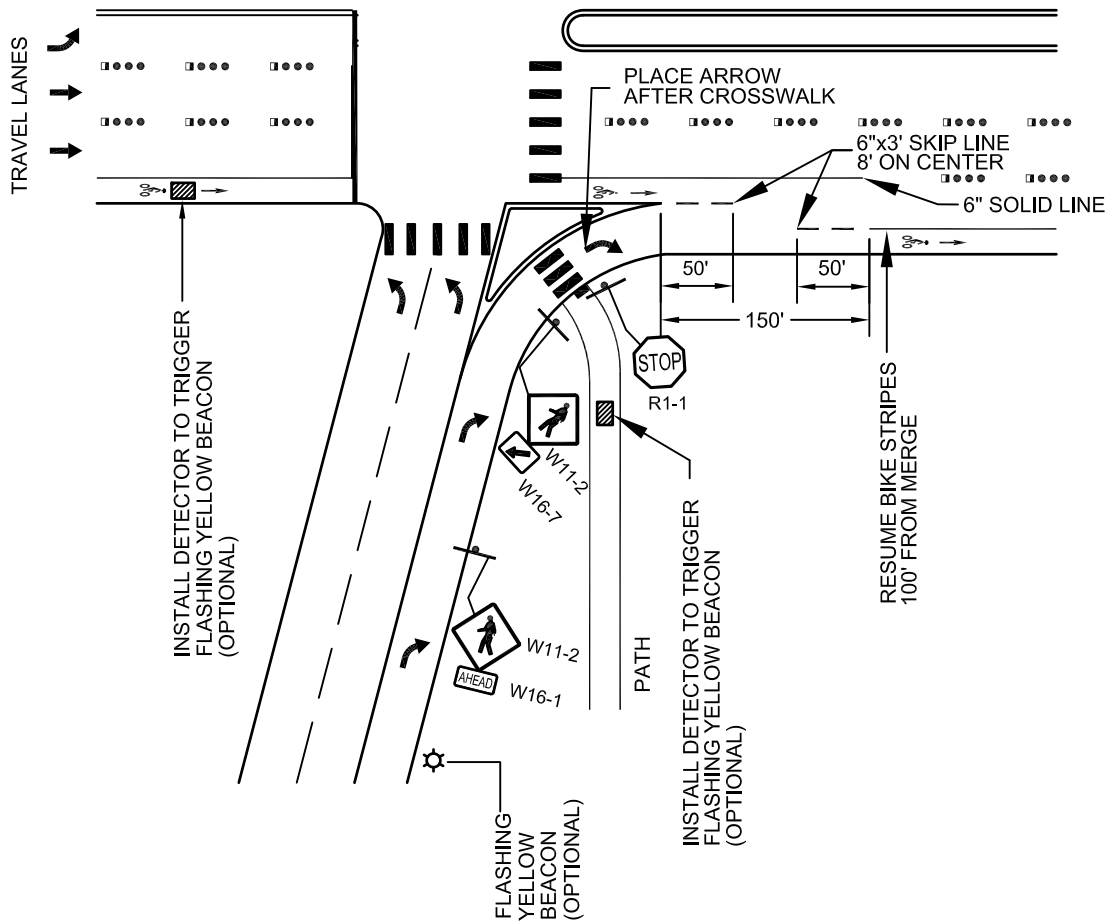
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SIGN SIZES FOR SHARED USE PATH					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	255.3	PAGE NO.	59.5



NOTES:

1. USE BOLLARDS ONLY AT LOCATIONS WHERE UNAUTHORIZED ACCESS IS ANTICIPATED. INSTALL EITHER 1 OR 3 (5 FOOT SPACING DESIRABLE) SIX-INCH DIAMETER BY 3 FT. TALL REFLECTORIZED BOLLARDS WHEN NECESSARY. CENTERLINE DELINEATION SHOULD BE PROVIDED AT APPROACH TO INTERSECTION EVEN WHEN BOLLARD IS NOT PROVIDED.
2. ANY OBSTRUCTION IN PATH SHOULD BE REMOVED. IF OBSTRUCTION CANNOT BE REMOVED, OBSTRUCTION MUST BE REFLECTORIZED.
3. USE CENTERLINE DELINEATION AT APPROACHES TO INTERSECTIONS AND AROUND OSTRUCTIONS IN ALL CASES. ONLY USE CENTERLINE DELINEATION IN OTHER CASES WHERE CONFLICTS BETWEEN USERS TRAVELING IN OPPOSITE DIRECTIONS ARE ANTICIPATED.

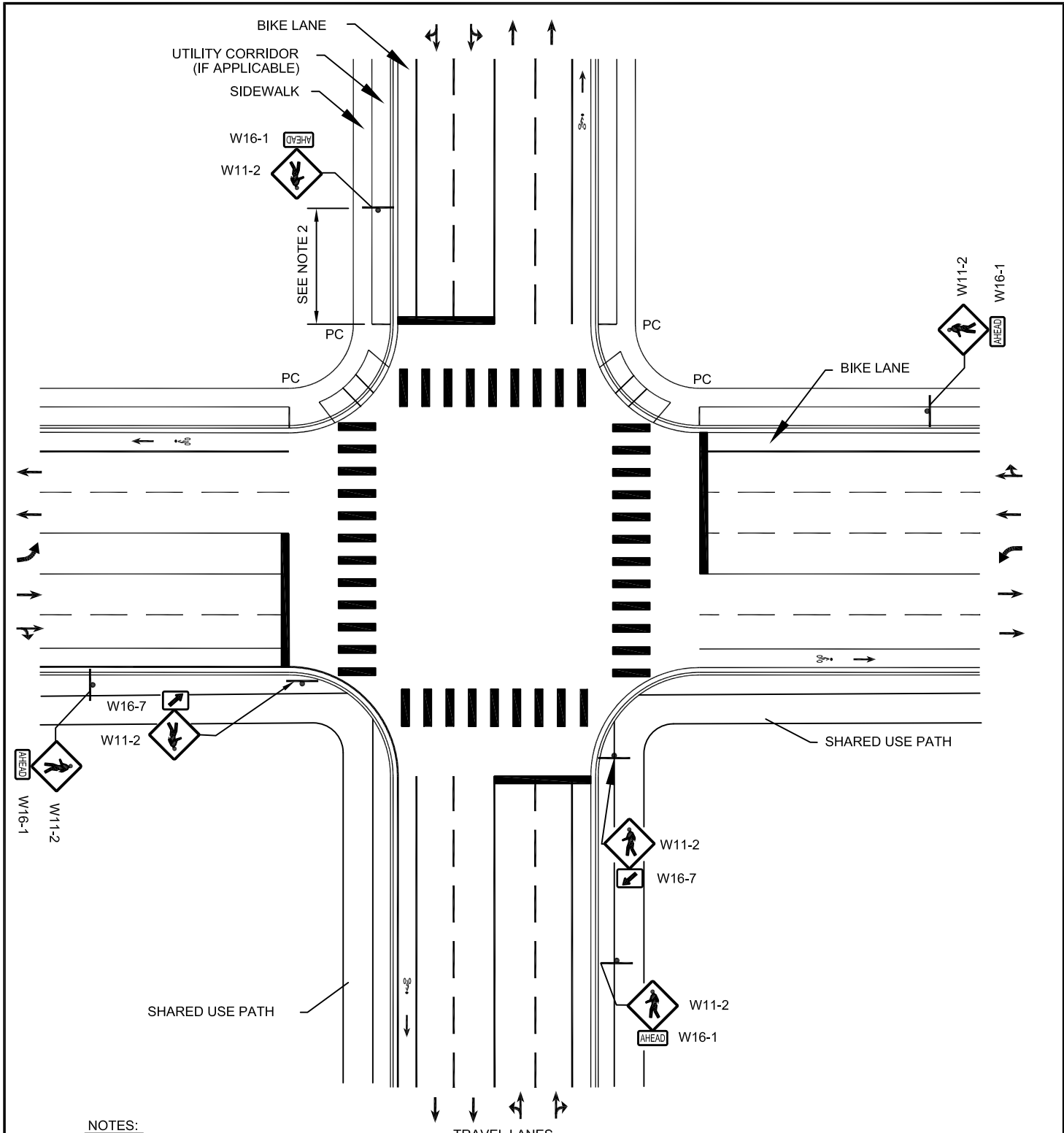
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	DELINEATION AND BOLLARED USAGE ON SHARED USE PATH					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	255.4	PAGE NO.	59.6



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERENCES TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-2 SIGN.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH CROSSING OF FREE RIGHT TURN LANE					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	256	PAGE NO.	59.7

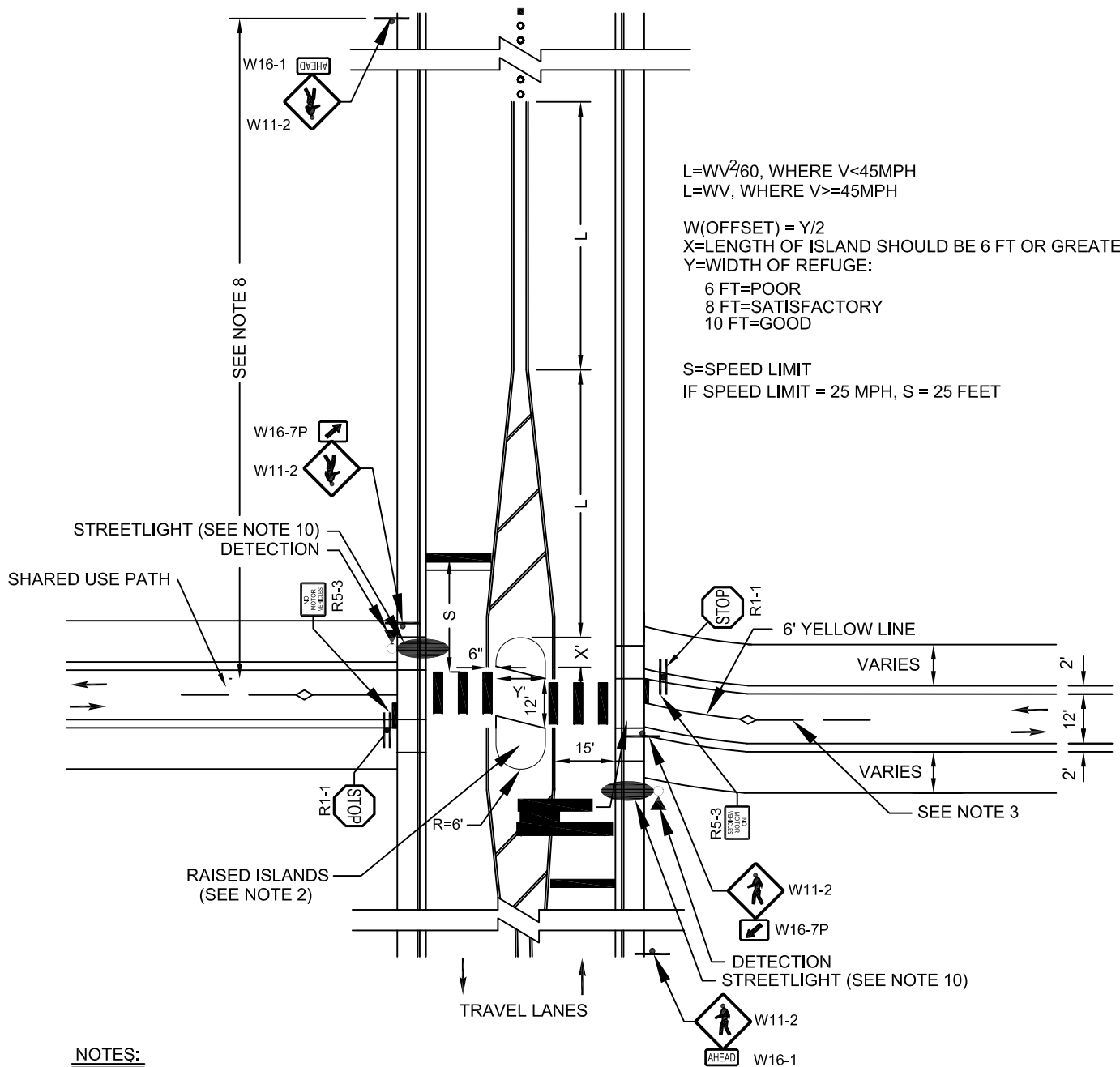


NOTES:

1. USE ENGINEERING JUDGMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE MUTCD TABLE 2C-4 FOR ADVANCED WARNING PLACEMENT.
3. INSTALL BOLLARDS ONLY AT LOCATIONS WHERE UNAUTHORIZED ACCESS IS ANTICIPATED. INSTALL EITHER 1 OR 3 SIX INCH DIAMETER BY 3 FEET TALL BOLLARDS WHEN REQUIRED.
4. SEE DRAWING NO. 201.2 FOR SIGHT VISIBILITY ZONES AT INTERSECTIONS.
5. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERS TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-2 SIGN.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
628	PAINTING TRAFFIC STRIPING	TYPICAL SIGNAGE FOR SHARED USE PATH AT INTERSECTION			
633	PAVEMENT MARKERS				
DATE	7-10-03	DWG. NO.	256.1	PAGE NO.	59.8

Effective 07/01/10 - 12/30/10



$L = WV^2/60$, WHERE $V < 45$ MPH
 $L = WV$, WHERE $V \geq 45$ MPH

$W(\text{OFFSET}) = Y/2$
 $X = \text{LENGTH OF ISLAND SHOULD BE 6 FT OR GREATER}$
 $Y = \text{WIDTH OF REFUGE:}$
 6 FT = POOR
 8 FT = SATISFACTORY
 10 FT = GOOD

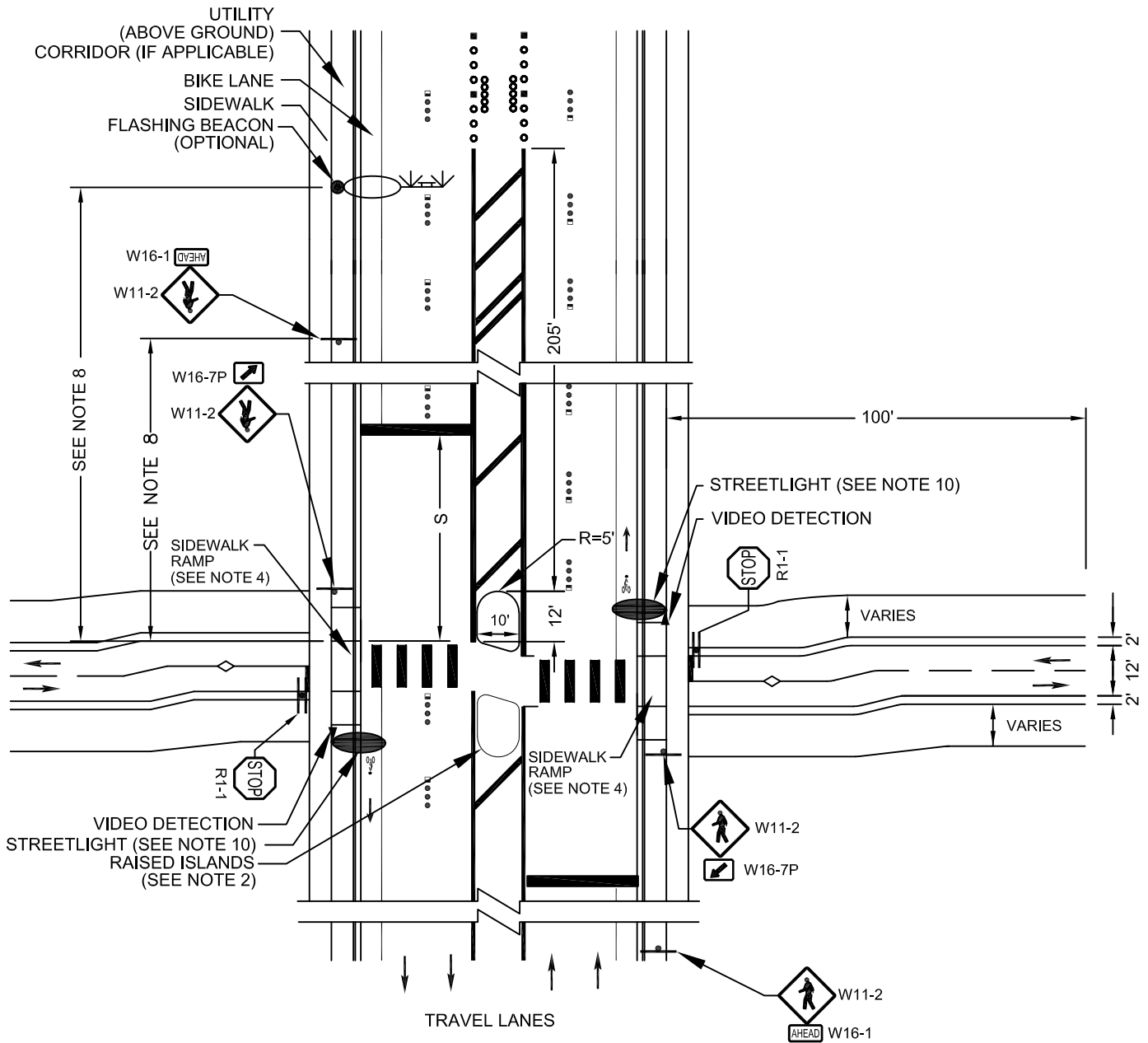
$S = \text{SPEED LIMIT}$
 IF SPEED LIMIT = 25 MPH, $S = 25$ FEET

NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE DRAWING NO. 218, 248 FOR MEDIAN ISLAND. A 15 DEGREE SKEW ANGLING IN DIRECTION OF ONCOMING TRAFFIC IS DESIRABLE.
3. SEE DRAWING NO. 255.4 FOR BOLLARDS AND CENTERLINE DELINEATION.
4. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMPS (USE PATH WIDTH FEET INSTEAD 5 FEET).
5. SEE DRAWING NO. 254 AND 254A FOR CROSSWALKS.
6. SEE DRAWING NO. 255.3 FOR SIGN SIZES FOR SHARED USE PATHS.
7. SEE DRAWING NO. 345 (2 OF 3) FOR DELINEATION IN TRANSITION SECTIONS.
8. SEE TABLE 2C-4 IN MUTCD 2000 FOR ADVANCE PLACEMENT OF WARNING SIGNS.
9. SEE PAGE 654 TO 680 IN AASHTO HIGHWAYS AND STREETS 2001 FOR SIGHT VISIBILITY ZONES (SIGHT TRIANGLES).
10. SEE STREET LIGHTING SECTION.
11. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERS TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-2 SIGN.

Effective 07/01/10 - 12/30/10

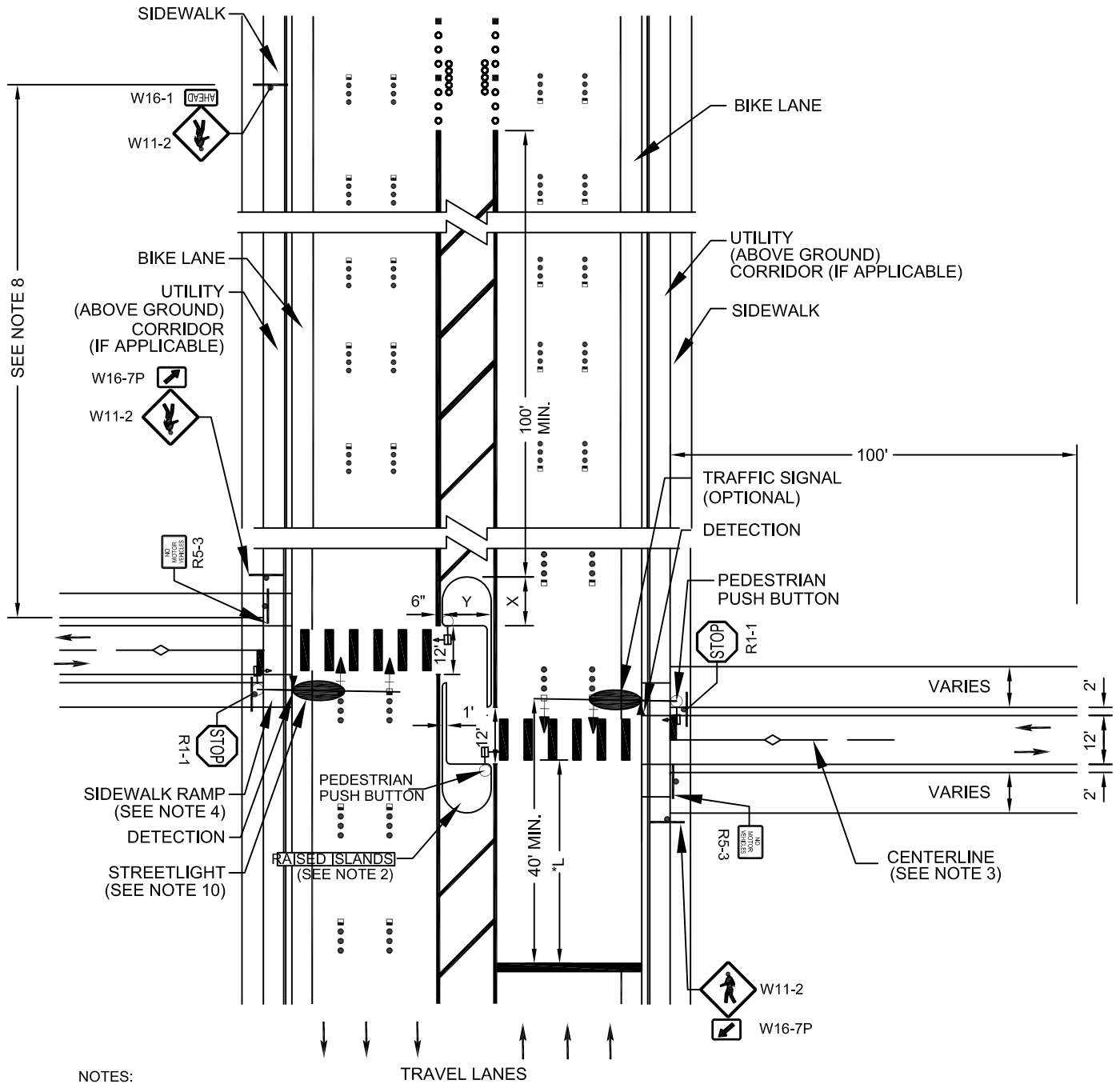
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH CROSSING TWO LANE ROADWAY					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	256.2	PAGE NO.	59.9



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE DRAWING NO. 218, 248, AND 256.2 FOR MEDIAN ISLAND.
3. SEE DRAWING NO. 255.4 FOR BOLLARDS AND CENTERLINE STRIPING.
4. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMPS (USE 12 FEET INSTEAD 5 FEET).
5. SEE DRAWING NO. 254 AND 254A FOR CROSSWALKS.
6. SEE DRAWING NO. 255.3 FOR SIGN SIZES FOR SHARED USE PATHS.
7. SEE DRAWING NO. 345 (2 OF 3) FOR DELINEATION IN TRANSITION SECTIONS.
8. SEE TABLE 2C-4 IN MUTCD 2000 FOR ADVANCE PLACEMENT OF WARNING SIGNS.
9. SEE PAGE 654 TO 680 IN AASHTO HIGHWAYS AND STREETS 2001 FOR SIGHT VISIBILITY ZONES (SIGHT TRIANGLES).
10. SEE STREET LIGHTING SECTION.
11. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERS TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-1 SIGN.

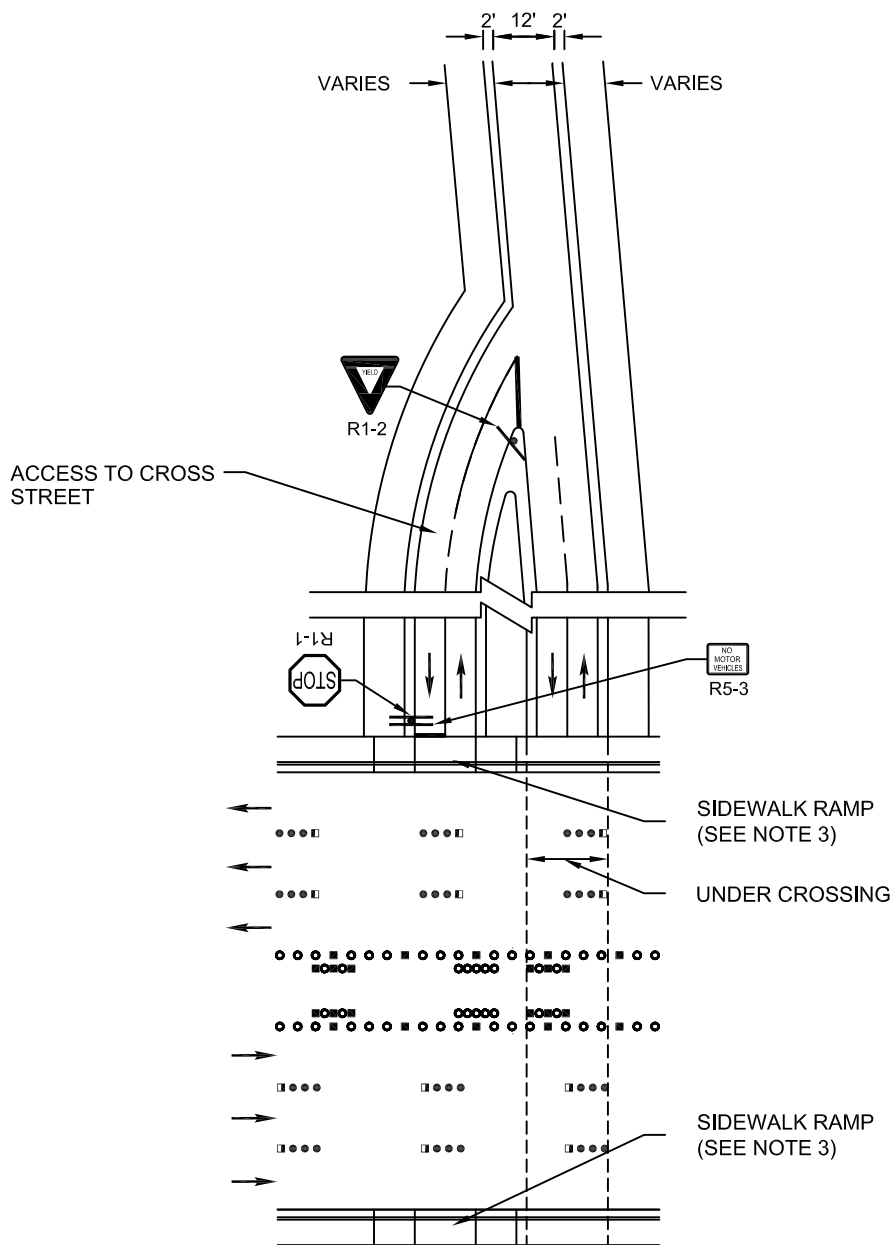
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH CROSSING 4 LANE ROADWAY					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	256.3	PAGE NO.	59.15



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
 2. SEE DRAWING NO. 218, 248, AND 256.2 FOR MEDIAN ISLAND.
 3. SEE DRAWING NO. 255.4 FOR BOLLARDS AND CENTERLINE STRIPING.
 4. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMPS (USE 12 FEET INSTEAD OF 5 FEET).
 5. SEE DRAWING NO. 254 AND 254A FOR CROSSWALKS.
 6. SEE DRAWING NO. 255.3 FOR SIGN SIZES FOR SHARED USE PATHS.
 7. SEE DRAWING NO. 345 (2 OF 3) FOR DELINEATION IN TRANSITION SECTIONS.
 8. SEE TABLE 2C-4 IN MUTCD 2000 FOR ADVANCE PLACEMENT OF WARNING SIGNS.
 9. SEE PAGE 654 TO 680 IN AASHTO HIGHWAYS AND STREETS 2001 FOR SIGHT VISIBILITY ZONES (SIGHT TRIANGLES).
 10. SEE STREET LIGHTING SECTION.
 11. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERS TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-1 SIGN.
- * L=SPEED LIMIT

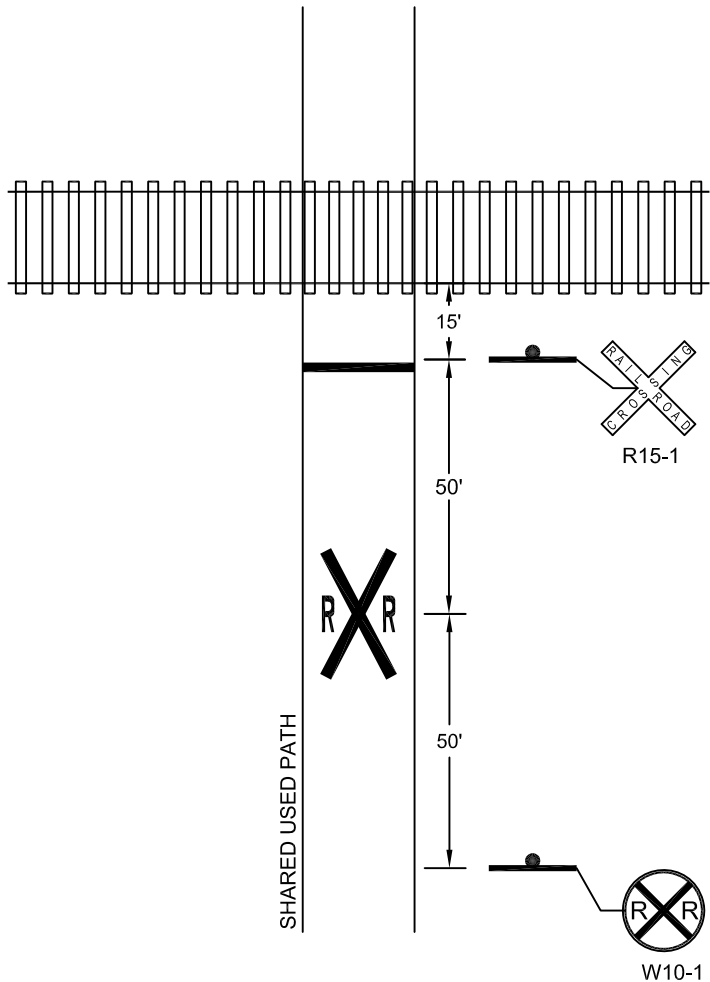
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH CROSSING 6 LANE ROADWAY			
633	PAVEMENT MARKERS				
DATE	7-12-07	DWG. NO.	256.4	PAGE NO.	59.25



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE DRAWING NO. 255.4 FOR BOLLARDS AND CENTERLINE STRIPING.
3. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMPS (USE 12 FEET INSTEAD 5 FEET OF CENTER SECTION OF SIDEWALK).
4. SEE DRAWING NO. 255.3 FOR SIGN SIZES FOR SHARED USE PATHS.
5. SEE TABLE 2C-4 IN MUTCD FOR ADVANCE PLACEMENT OF WARNING SIGNS.
6. SEE DRAWING NO. 256.4 FOR THE AN MID-BLOCK AT-GRADE CROSSING DESIGN.
7. A MINIMUM 8 FOOT CLEARANCE IS REQUIRED FOR THE UNDER CROSSING. GRADES GREATER THAN 5 PERCENT ARE UNDESIRABLE. SEE THE 1999, OR CURRENT EDITION, AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES FOR GRADE RESTRICTIONS IF A 5 PERCENT GRADE IS EXCEEDED.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	SHARED USE PATH UNDER CROSSING					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	256.5	PAGE NO.	59.35



NOTES:

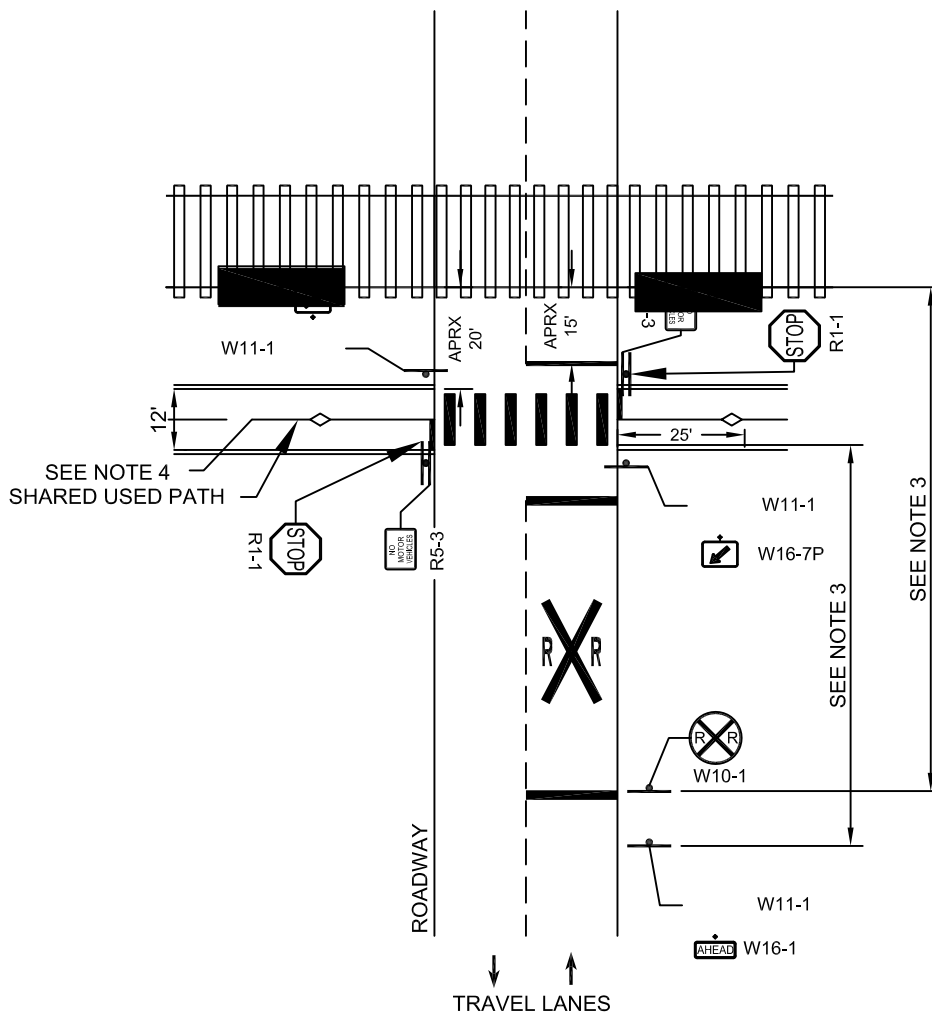
1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE MUTCD 2000 FOR GUIDELINES REFERENCED IN FIGURE.

SPECIFICATION REFERENCE

628	PAINTING TRAFFIC STRIPING
633	PAVEMENT MARKERS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SHARED USE PATH
CROSSING RAILROAD



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
2. SEE DRAWING NO. 1-5 FOR CROSSWALK STRIPING GUIDELINES.
3. SEE MUTCD 2000 FOR ADVANCED PLACEMENT OF WARNING SIGNS IN TABLE 2C-4.
4. SEE DRAWING NO. 255.5 FOR BOLLARDS AND CENTERLINE STRIPING.
5. SEE DRAWING NO. 256.2 - 256.4 FOR ADDITIONAL CROSSING DETAILS.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
628	PAINTING TRAFFIC STRIPING	TYPICAL DELINEATION FOR SHARED USE PATH PARALLEL TO RAILROAD CROSSING A ROADWAY					
633	PAVEMENT MARKERS						
		DATE	7-10-03	DWG. NO.	256.7	PAGE NO.	59.55

1. INSTALL STREETLIGHT STANDARDS AT INTERSECTIONS INCLUDING "L" AND "T" TYPES, PER STANDARD DRAWINGS 301 THROUGH 310 IN ACCORDANCE WITH THE APPROPRIATE RIGHT-OF-WAY.

2. STREET CLASSIFICATION AND STREETLIGHT STANDARD APPLICATION SHALL BE AS LISTED IN TABLE 1 BELOW. ACTUAL LUMINAIRE WATTAGE AND/OR STREETLIGHT STANDARD SPACING MAY BE VARIED BY THE ENGINEER, WHEN SUPPORTED BY AN APPROVED LIGHTING STUDY IN ACCORDANCE WITH THE IES RECOMMENDED PRACTICE FOR ROADWAY LIGHTING IN ORDER TO MEET CURRENT AND FUTURE TRAFFIC CONTROL NEEDS AND APPROVED BY THE RESPECTIVE AGENCY. AVERAGE LEVELS ARE MAINTAINED LEVELS AT A 0.8 MAINTENANCE FACTOR (0.82 FOR CLARK COUNTY) IN FOOTCANDLES MEASURED HORIZONTALLY AT THE SURFACE.

TABLE 1

<u>CLASSIFICATION</u>	<u>R/W</u>	<u>DWG</u>	<u>LUMINAIRE</u>	<u>LEVEL</u>	AVERAGE IES LIGHTING <u>AVG./MIN</u>	IES UNIFORMITY
MAJOR ARTERIAL		100' OR MORE	---	250W HPS	1.58 FC	3:1
INTERMEDIATE COLLECTOR		80'	---	150W HPS 250W HPS (CLARK COUNTY & COH ONLY)	0.84 FC	4:1
LOCAL		60'	---	150W HPS 100W HPS (CLARK COUNTY & COH ONLY)	0.38 FC	6:1
RESIDENTIAL		51' OR LESS	---	100W HPS	0.38 FC	6:1

3. NEW STREETLIGHT STANDARDS INSTALLED ADJACENT TO OR OPPOSITE FROM EXISTING STREETLIGHTS SHALL MATCH THE EXISTING LOCATION, SPACING, POLE AND LUMINAIRE TYPES UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

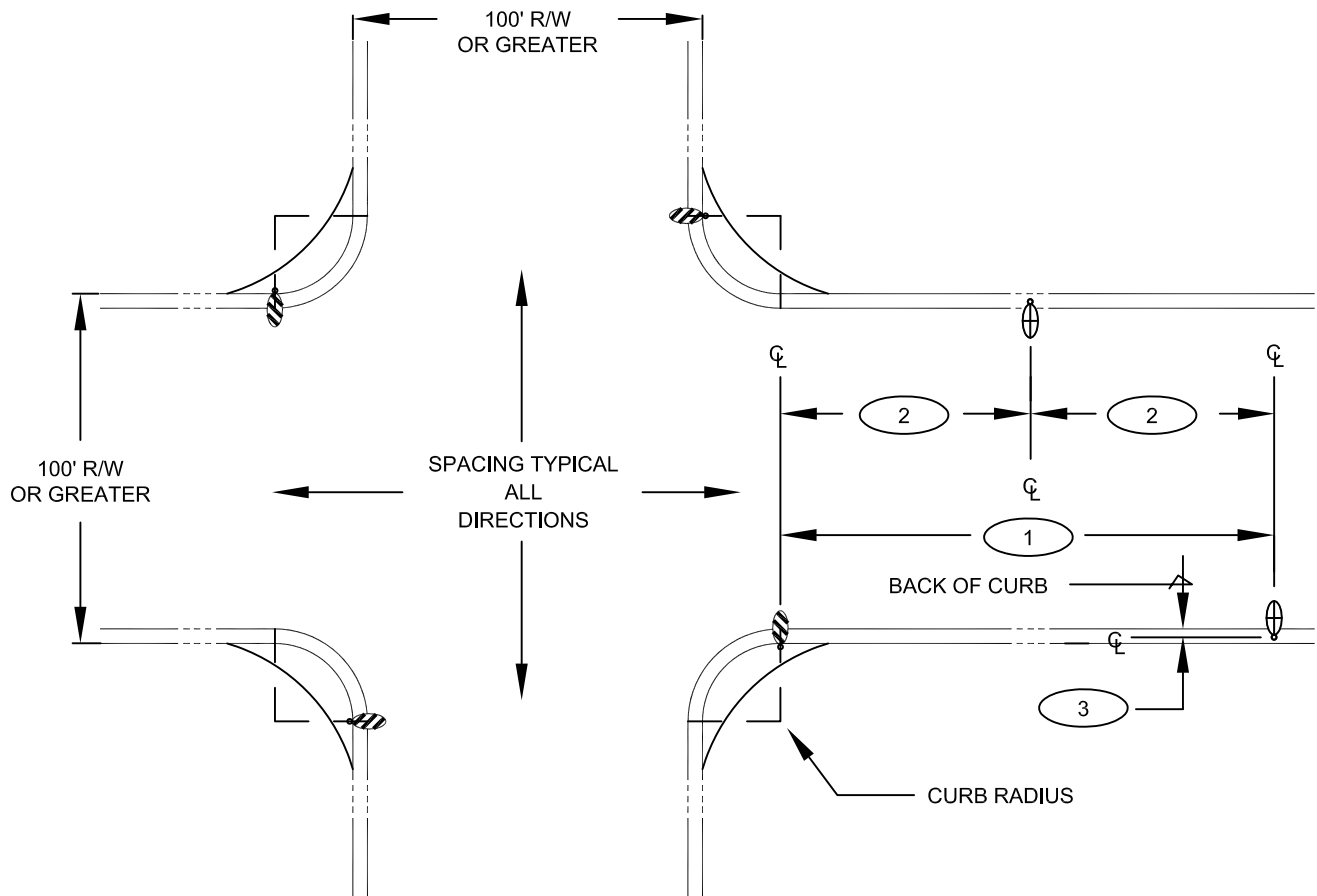
4. STREETLIGHT STANDARDS INSTALLED ON 60' OR LESS RIGHT-OF-WAYS MAY BE INSTALLED ON EITHER SIDE OF ROADWAY AS DIRECTED BY THE ENGINEER.

5. TRAFFIC SIGNAL FOUNDATIONS AND ADAPTOR PLATES MAY BE REQUIRED AT INTERSECTIONS AS DIRECTED BY THE ENGINEER.

6. AT LEAST ONE STREETLIGHT SHALL BE REQUIRED IN THE BULB SECTION OF A CUL-DE-SAC OR HAMMERHEAD. LOCATION SHALL BE AS REQUIRED BY THE ENGINEER.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATION GENERAL NOTES			
		DATE	2-08-07	DWG. NO.	300



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

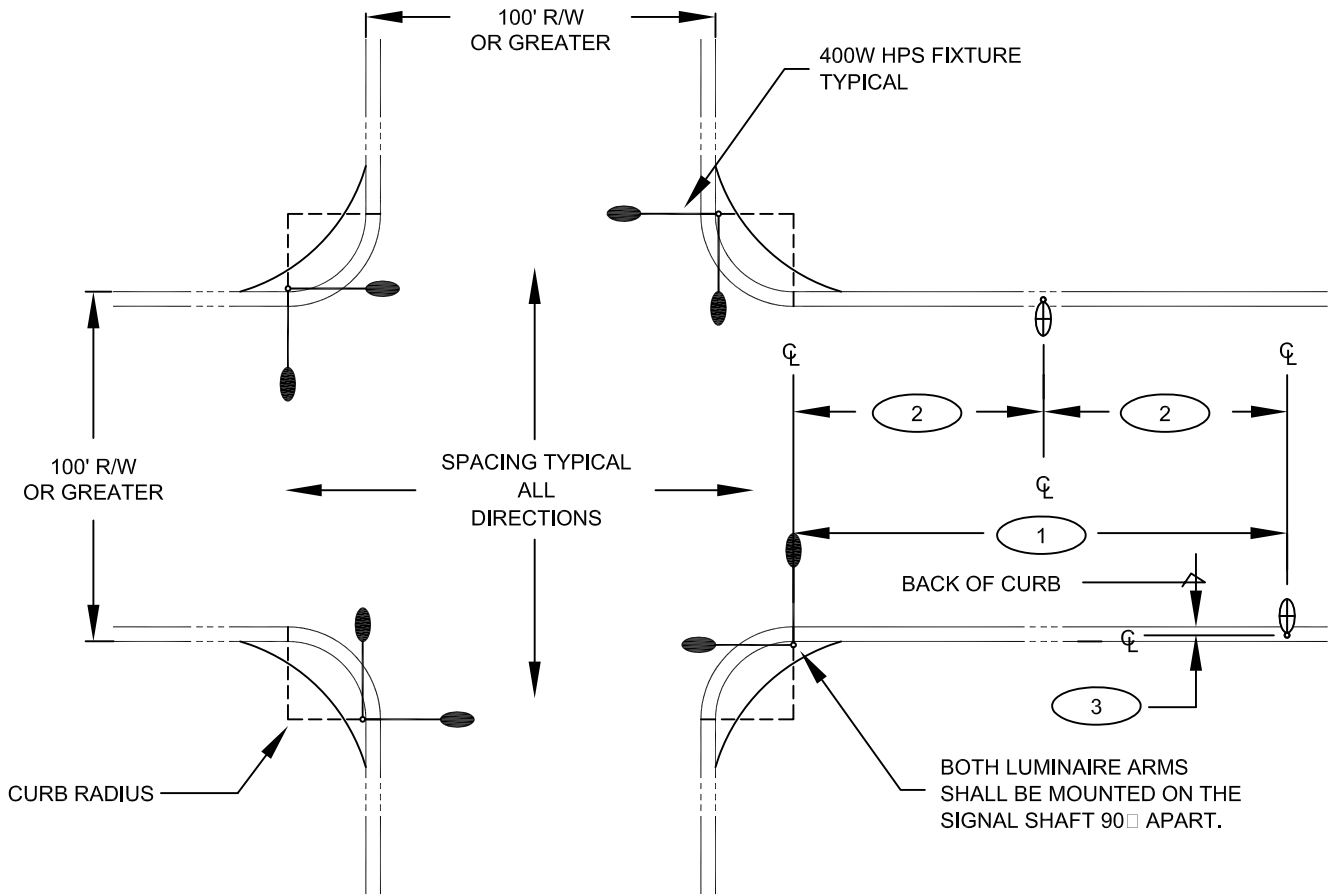
POLE LOCATION TABLE						
KEYED NOTE	ENTITY					
		CLV	NLV	MES	HND	BC
1				160'	(SEE NOTE 3)	
2				80'	(SEE NOTE 3)	
3		(SEE DRAWING NO. 320)			N/A	

SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STREETLIGHT LOCATIONS AT INTERSECTIONS
100' OR GREATER/100' OR GREATER
RIGHT-OF-WAY
(EXCEPT CLARK COUNTY)



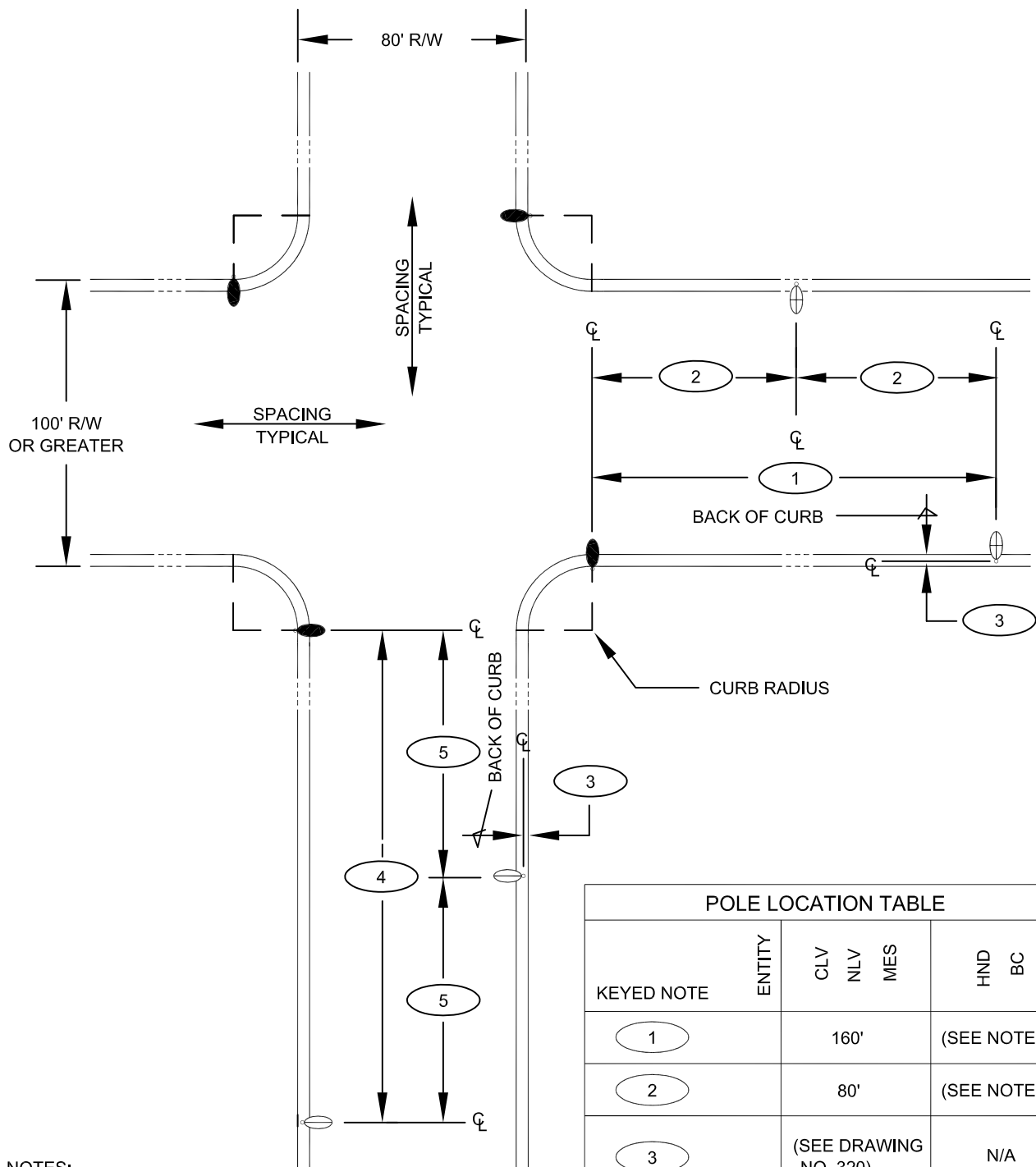
NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. WITH THE ENGINEER'S APPROVAL, A SECOND LUMINAIRE MOUNTING PLATE MAY BE FIELD WELDED BY A CERTIFIED WELDER.
4. ALL LUMINAIRE MAST ARMS FOR 400W FIXTURES SHALL BE 15 FT. LONG AND INSTALLED PER STANDARD DRAWING NO. 404.406 UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.

POLE LOCATION TABLE		
KEYED NOTE	ENTITY	Ø
1		120'
2		60'
3		(SEE DRAWING NO. 320)

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY ONLY			
		STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/100' OR GREATER RIGHT-OF-WAY			
		DATE	9-12-02	DWG. NO.	301.1
		PAGE NO.			61.1



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

POLE LOCATION TABLE						
KEYED NOTE	ENTITY	CLV	NLV	MES	HND	BC
1		160'			(SEE NOTE 3)	
2		80'			(SEE NOTE 3)	
3		(SEE DRAWING NO. 320)			N/A	
4		170'			170'	
5		85'			85'	

SPECIFICATION REFERENCE

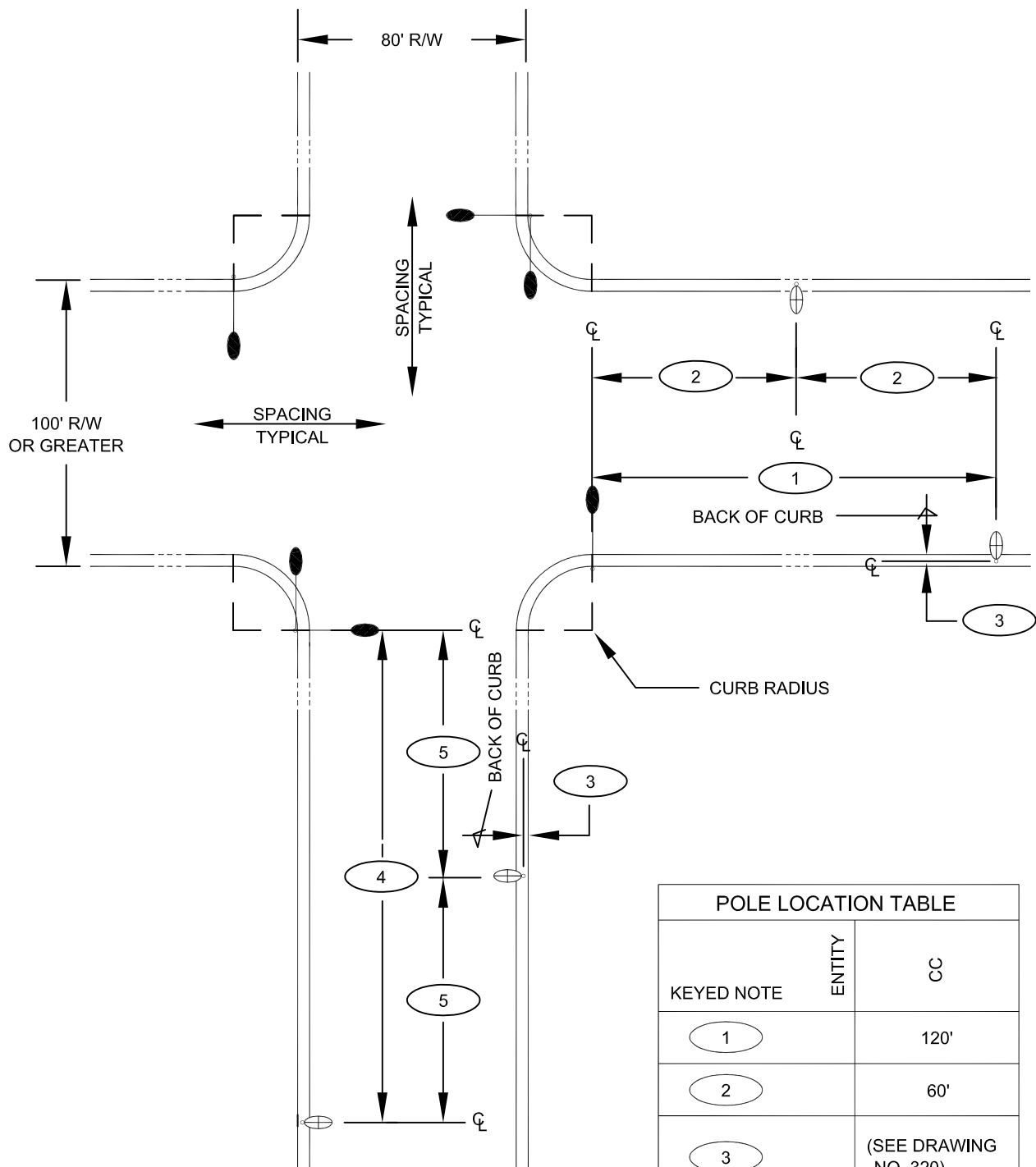
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

623 TRAFFIC SIGNALS & STREETLIGHTING

**STREETLIGHT LOCATIONS AT INTERSECTIONS
100' OR GREATER/80' RIGHT-OF-WAY**

(EXCEPT CLARK COUNTY)

DATE 8-12-99 | DWG. NO. 302 | PAGE NO. 62



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. ALL LUMINAIRE MAST ARMS FOR 400W FIXTURES SHALL BE 15 FT. LONG AND INSTALLED PER STANDARD DRAWING NO. 404.406 UNLESS OTHERWISE APPROVED BY THE ENGINEER.

SPECIFICATION REFERENCE

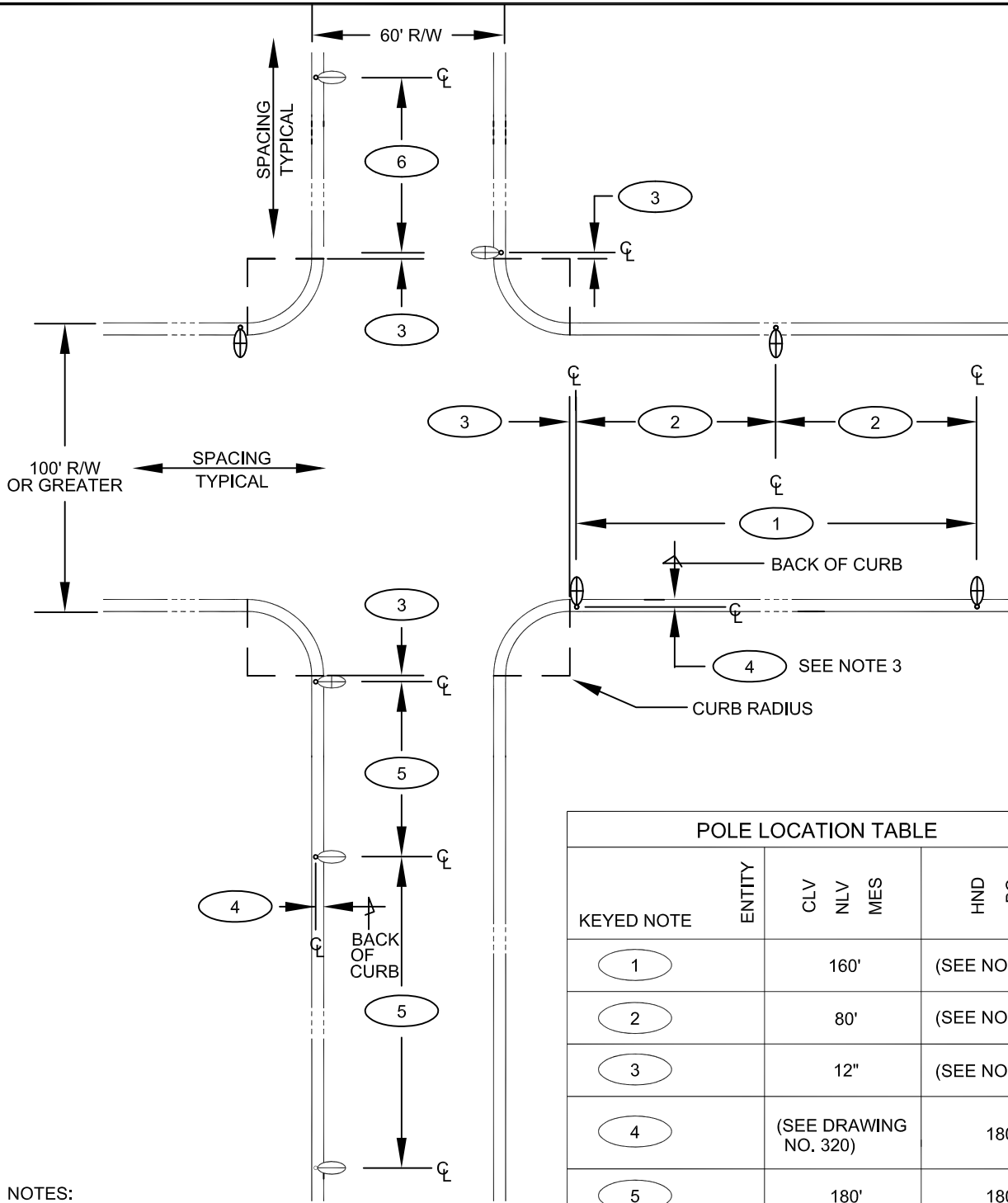
623 TRAFFIC SIGNALS & STREETLIGHTING

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

CLARK COUNTY ONLY

**STREETLIGHT LOCATIONS AT INTERSECTIONS
100' OR GREATER/80' RIGHT-OF-WAY**

DATE 9-12-02 DWG. NO. 302.1 PAGE NO. 62.1



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

POLE LOCATION TABLE						
KEYED NOTE	ENTITY	CLV	NLV	MES	HND	BC
1				160'	(SEE NOTE 3)	
2				80'	(SEE NOTE 3)	
3				12"	(SEE NOTE 3)	
4		(SEE DRAWING NO. 320)			180'	
5				180'	180'	
6				90'	90'	

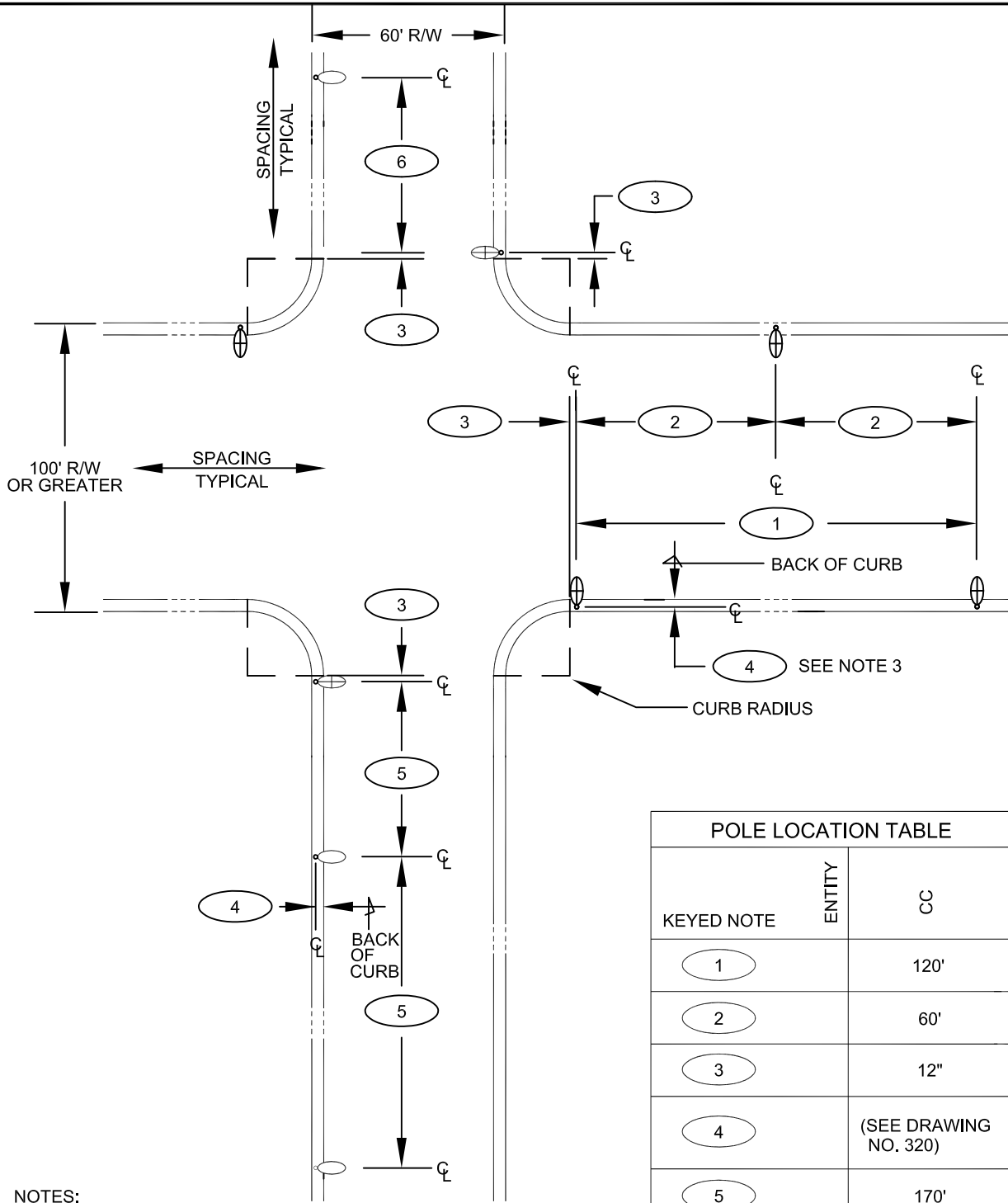
SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**STREETLIGHT LOCATIONS AT INTERSECTIONS
100' OR GREATER/60' RIGHT-OF-WAY
(EXCEPT CLARK COUNTY & HENDERSON)**

DATE 2-08-07 DWG. NO. 303



POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CL
1		120'
2		60'
3		12"
4		(SEE DRAWING NO. 320)
5		170'
6		170'

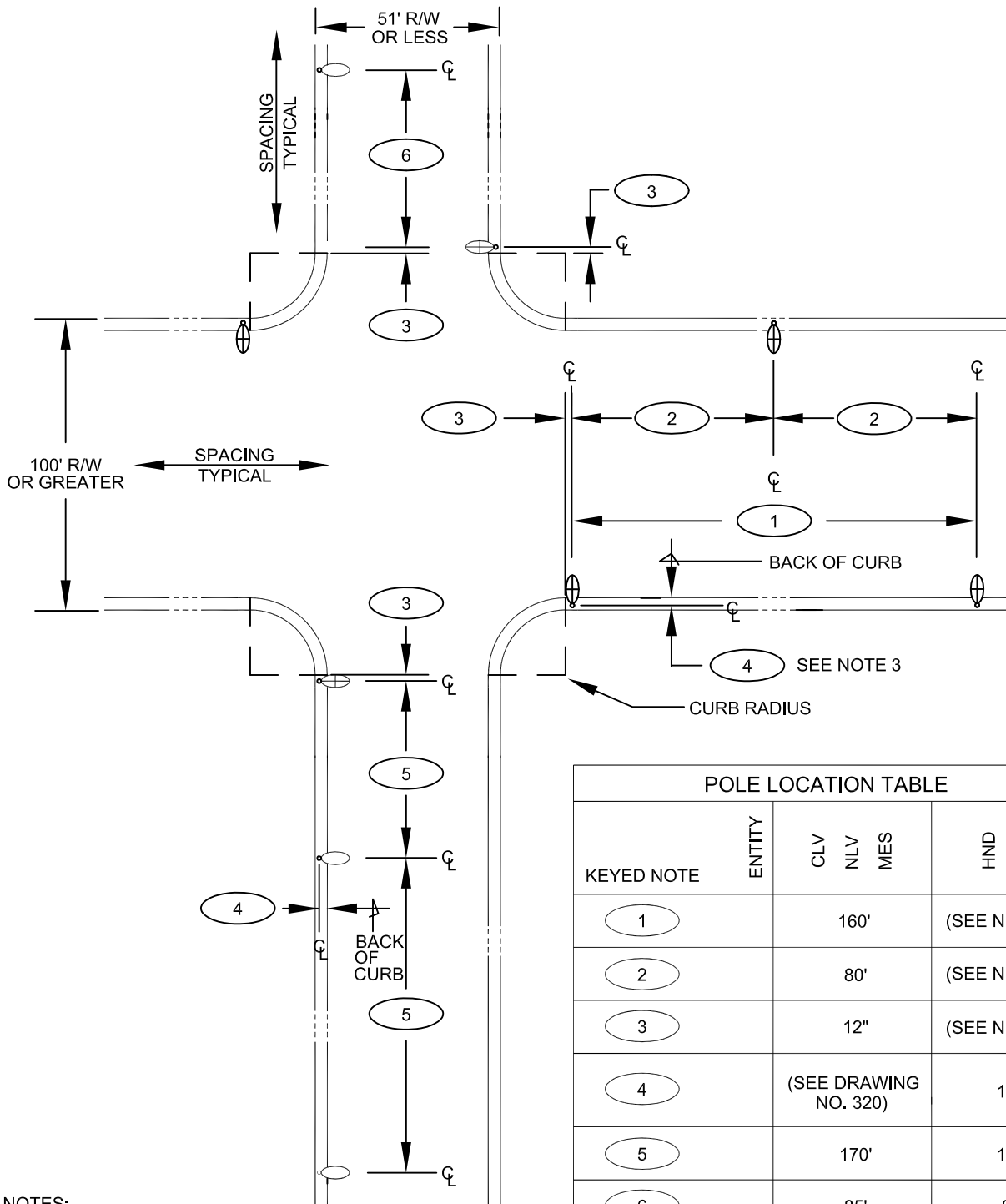
NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. IF INTERSECTION IS SIGNALIZED, 400 WATT LUMINAIRES SHALL BE INSTALLED ON ALL CORNERS AND DUAL ARM CONFIGURATION SHALL BE USED FOR 100 FT. RIGHT-OF-WAY SIMILAR TO STANDARD DRAWING NO. 302.1 IN CLARK COUNTY. USE SIGLE ARM CONFIGURATION PER DRAWING 302 IN HENDERSON.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/60' RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO.	303.1

Effective 07/01/10 - 12/30/10



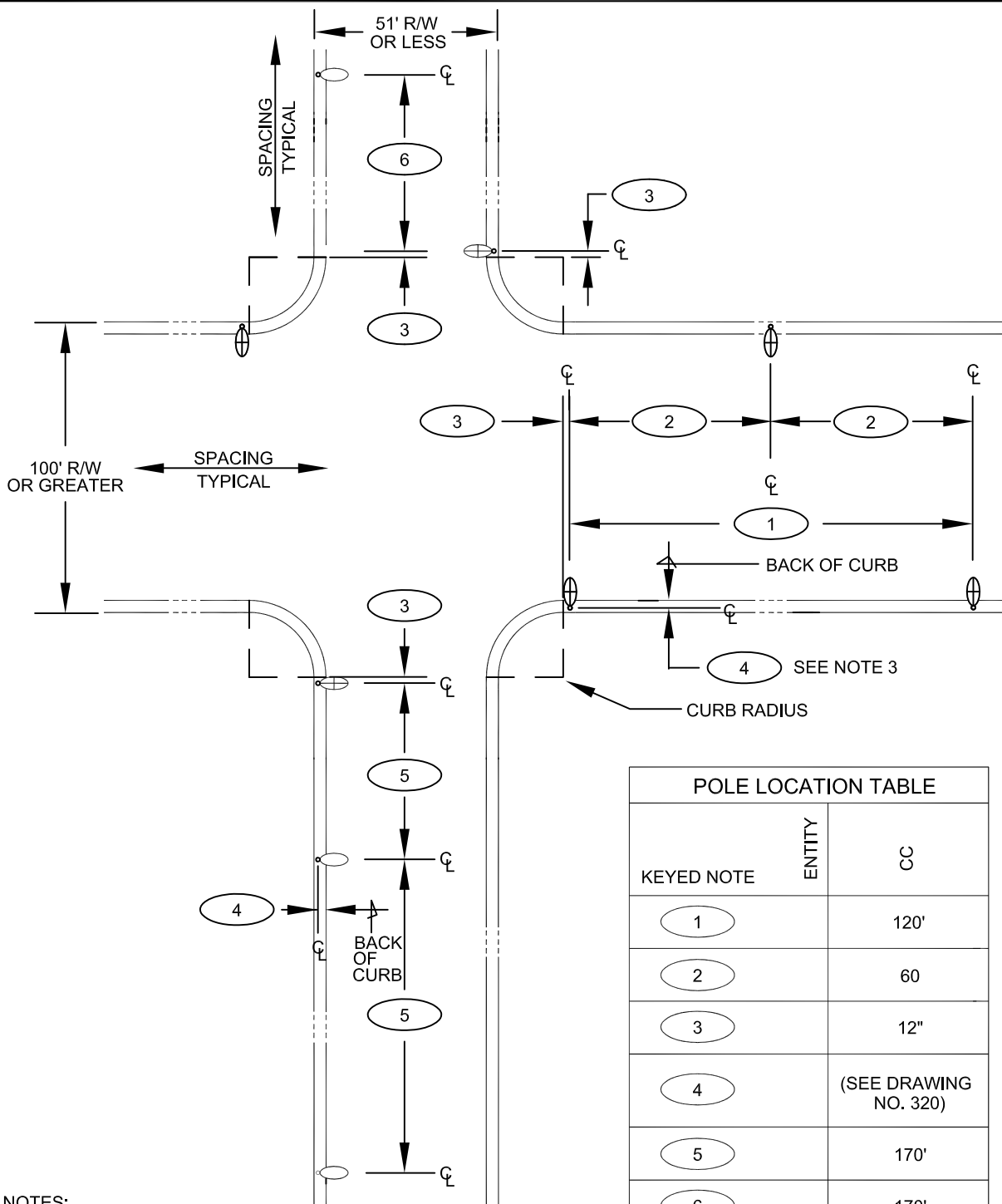
POLE LOCATION TABLE						
KEYED NOTE	ENTITY	CLV	NLV	MES	HND	BC
1			160'		(SEE NOTE 3)	
2			80'		(SEE NOTE 3)	
3			12"		(SEE NOTE 3)	
4		(SEE DRAWING NO. 320)			180'	
5			170'			170'
6			85'			85'

NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET. ADEQUATE INTERSECTION LIGHTING SHALL ALSO BE ADDRESSED IN THE LIGHTING STUDY.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/51' OR LESS RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON)		
		DATE 2-08-07	DWG. NO.	304



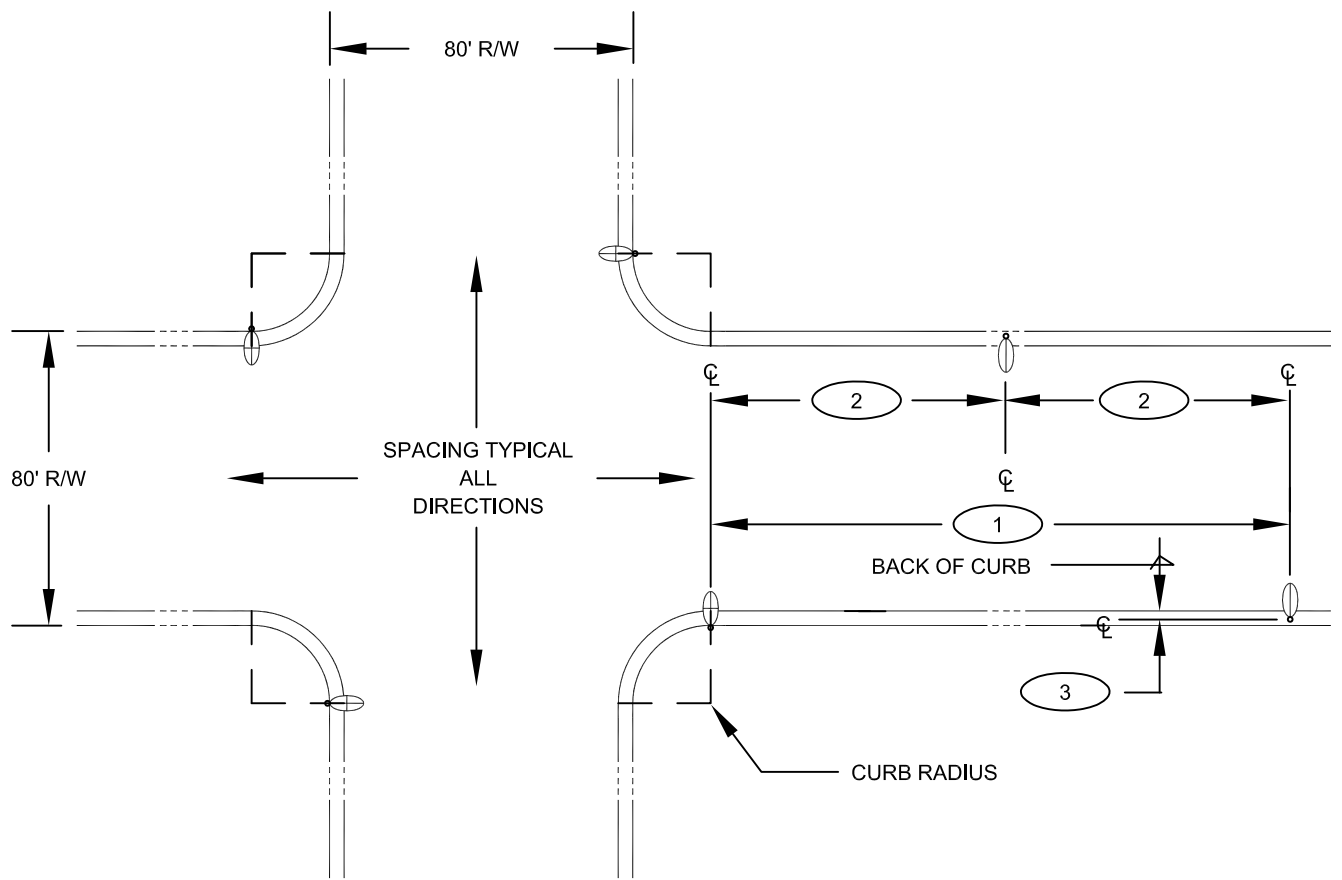
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CC
1		120'
2		60
3		12"
4		(SEE DRAWING NO. 320)
5		170'
6		170'

NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET. ADEQUATE INTERSECTION LIGHTING SHALL ALSO BE ADDRESSED IN THE LIGHTING STUDY.
3. IF INTERSECTION IS SIGNALIZED, 400 WATT LUMINAIRES SHALL BE INSTALLED ON ALL CORNERS AND DUAL ARM CONFIGURATION SHALL BE USED FOR 100 FT. RIGHT-OF-WAY SIMILAR TO STANDARD DRAWING NO. 302.1 IN CLARK COUNTY. USE SINGLE ARM CONFIGURATION PER DRAWING 302 IN HENDERSON.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/51' OR LESS RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO. 304.1	



NOTE:

SEE GENERAL NOTES STANDARD DRAWING NO. 300.

POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC)
1		170'
2		85'
3		(SEE DRAWING NO. 320)

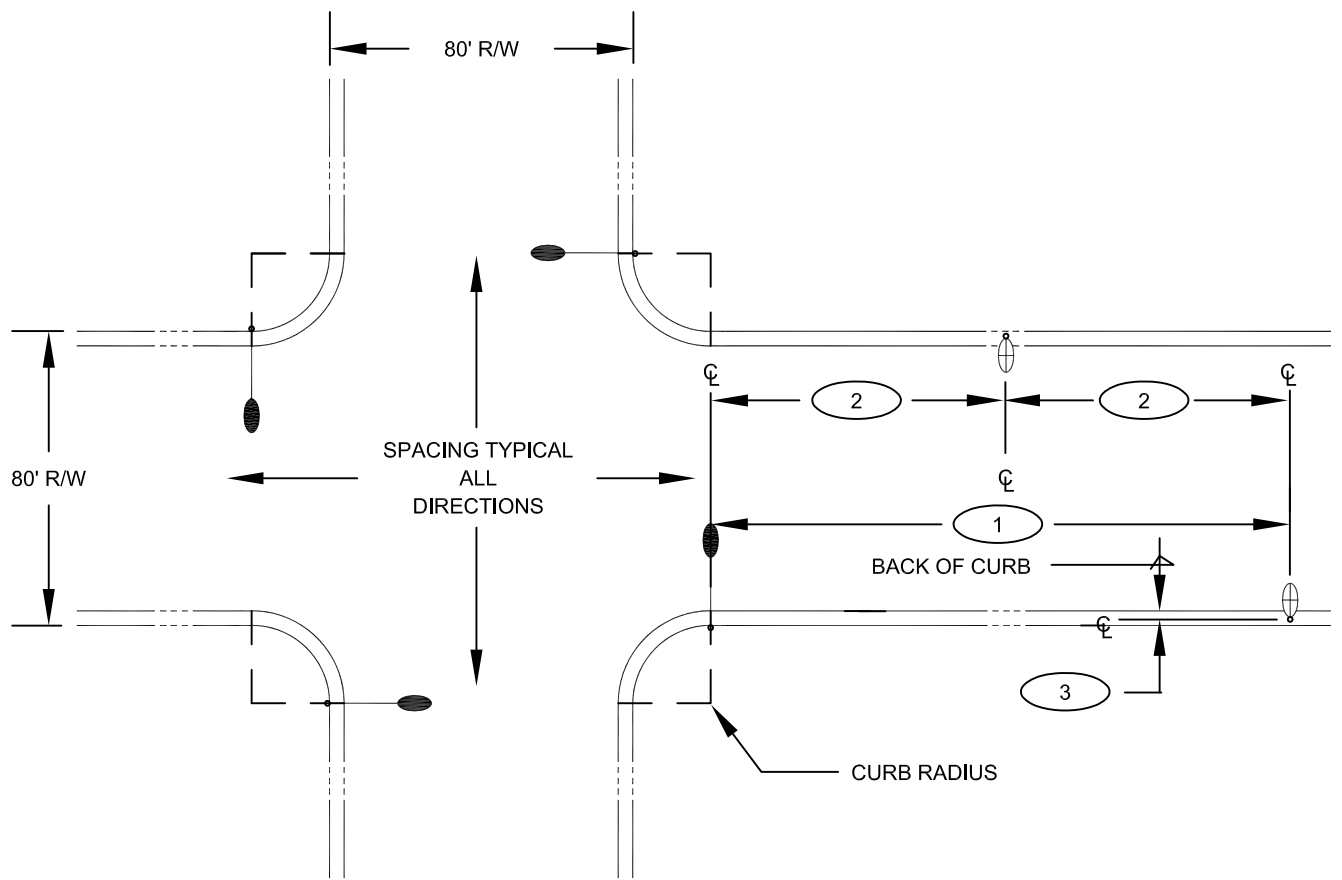
SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STREETLIGHT LOCATIONS AT INTERSECTIONS
80'/80' RIGHT-OF-WAY
(EXCEPT CLARK COUNTY)

DATE 8-12-99 | DWG. NO. 305 | PAGE NO. 65

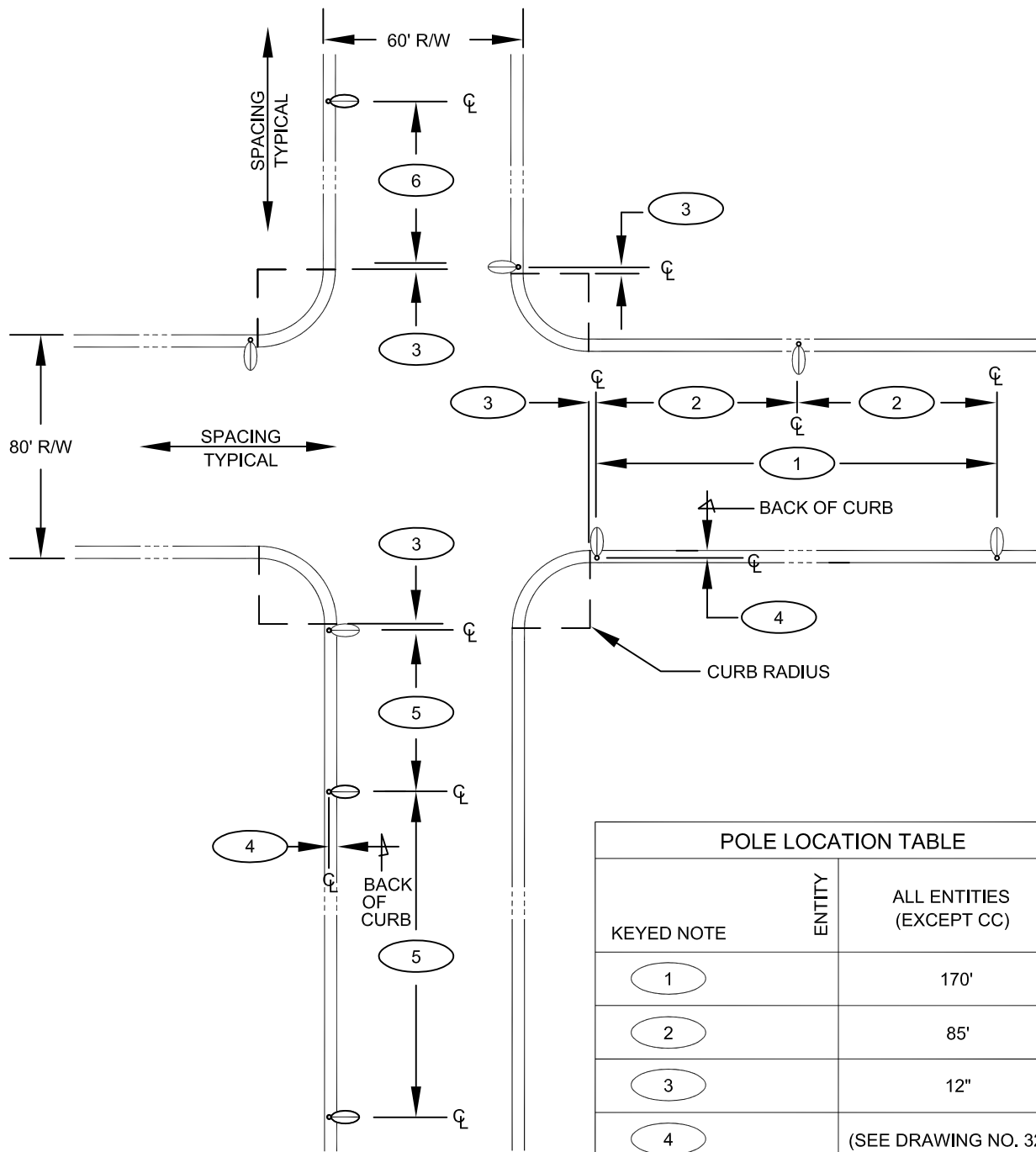


NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CLARK COUNTY
1		170'
2		85'
3		(SEE DRAWING NO. 320)

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 80'/80' RIGHT-OF-WAY		
		DATE 8-12-99	DWG. NO. 305.1	PAGE NO. 65.1

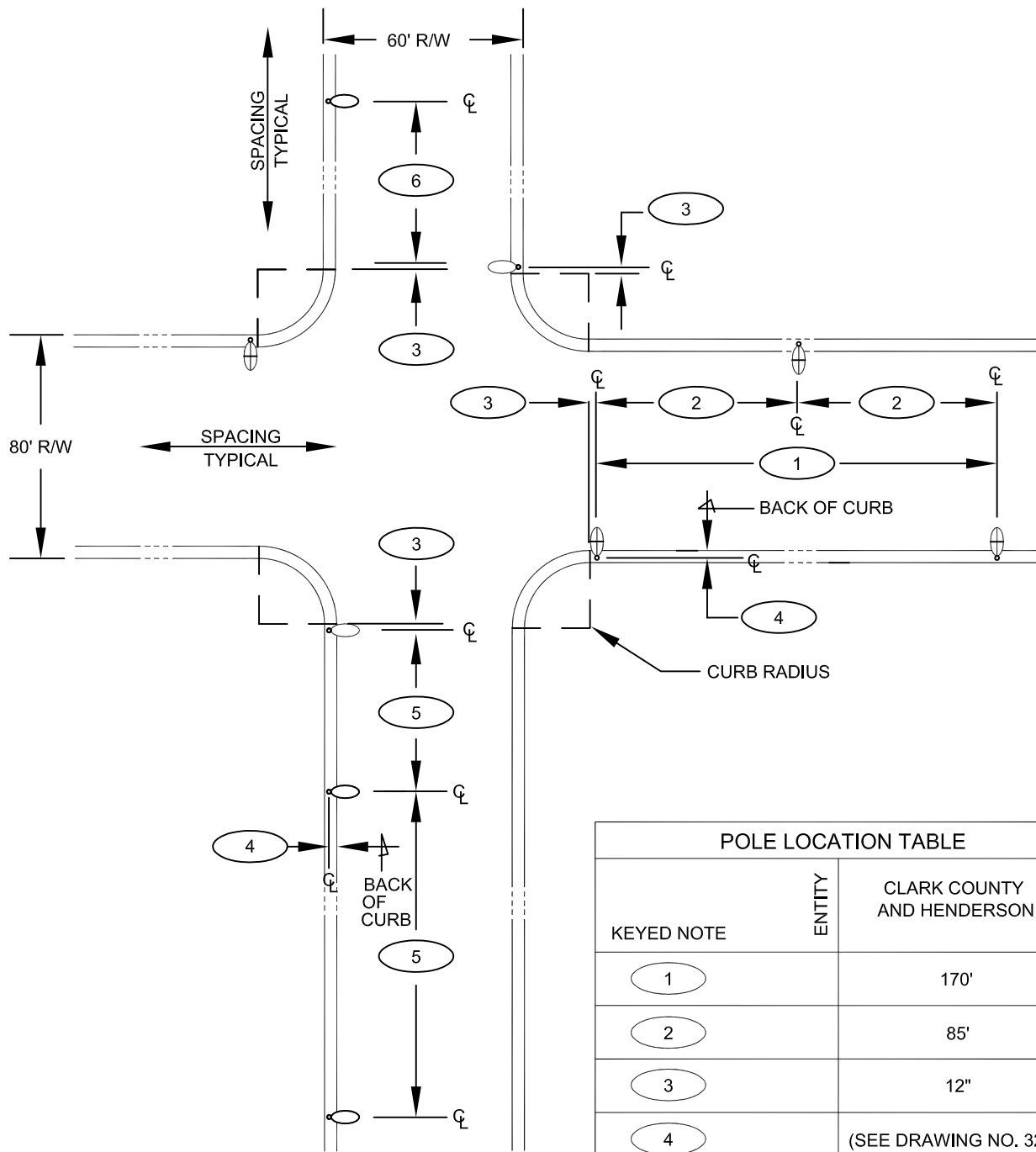


POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC)
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		180'
6		90'

NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 80'/60' RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON)		
		DATE 2-08-07	DWG. NO.	306



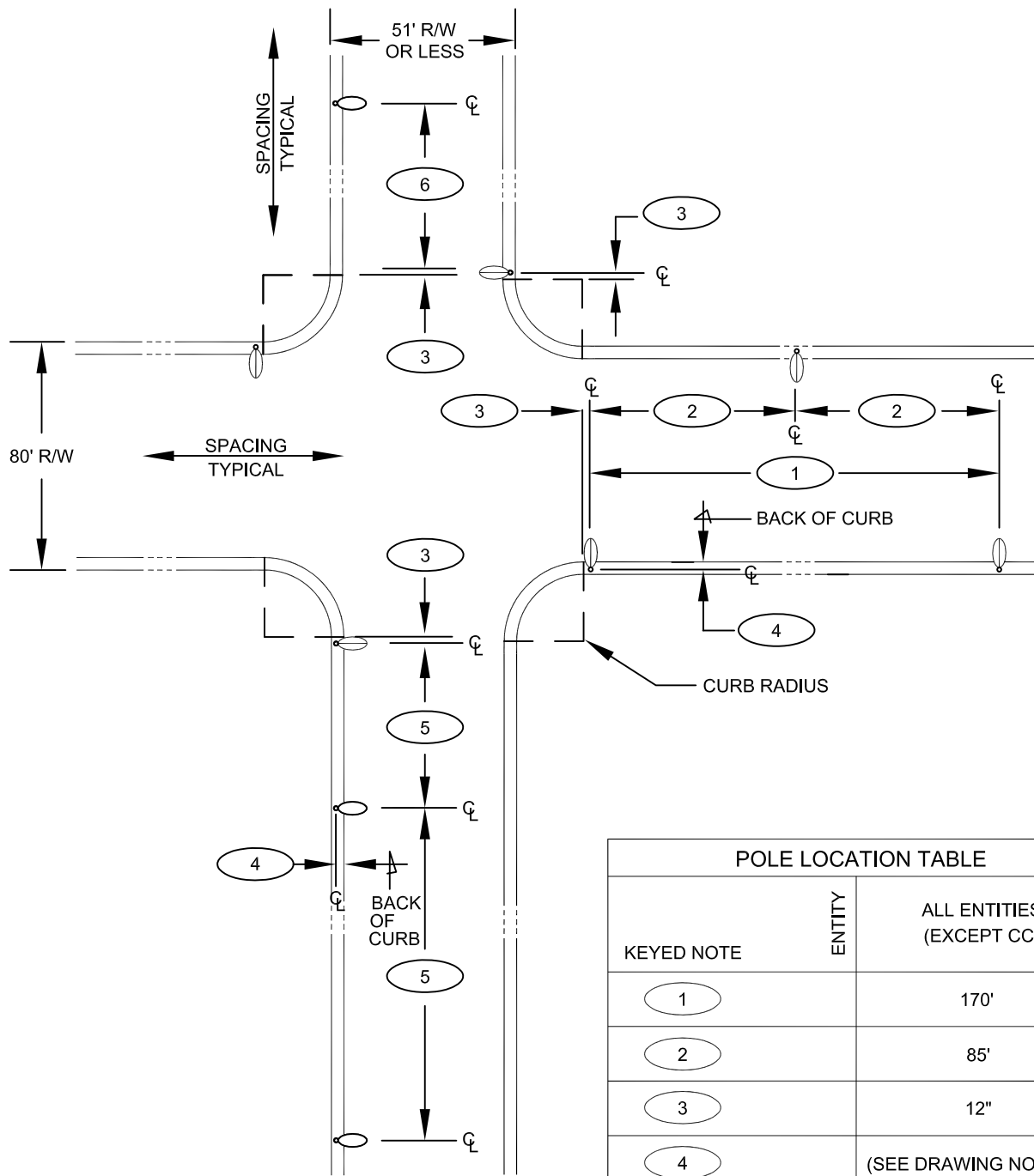
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CLARK COUNTY AND HENDERSON
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		170'
6		85'

NOTE:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. IF THE INTERSECTION IS SIGNALIZED, 400 WATT LUMINAIRES SHALL BE INSTALLED ON ALL CORNERS.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 80'/60' RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO.	306.1



POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC)
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		170'
6		85'

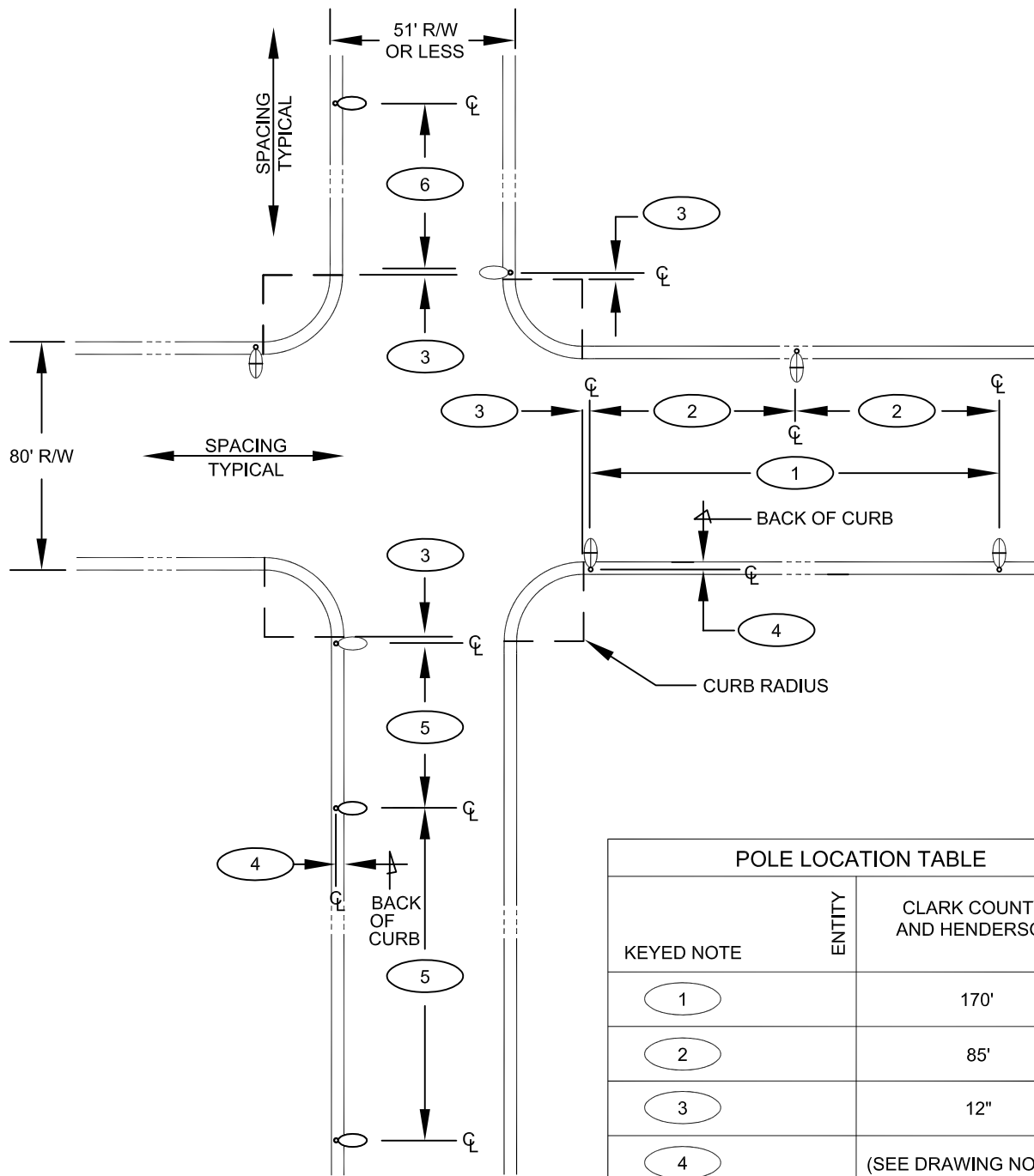
NOTE:

SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 80'/51' OR LESS RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON)		
		DATE 2-08-07	DWG. NO.	307



POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CLARK COUNTY AND HENDERSON
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		170'
6		85'

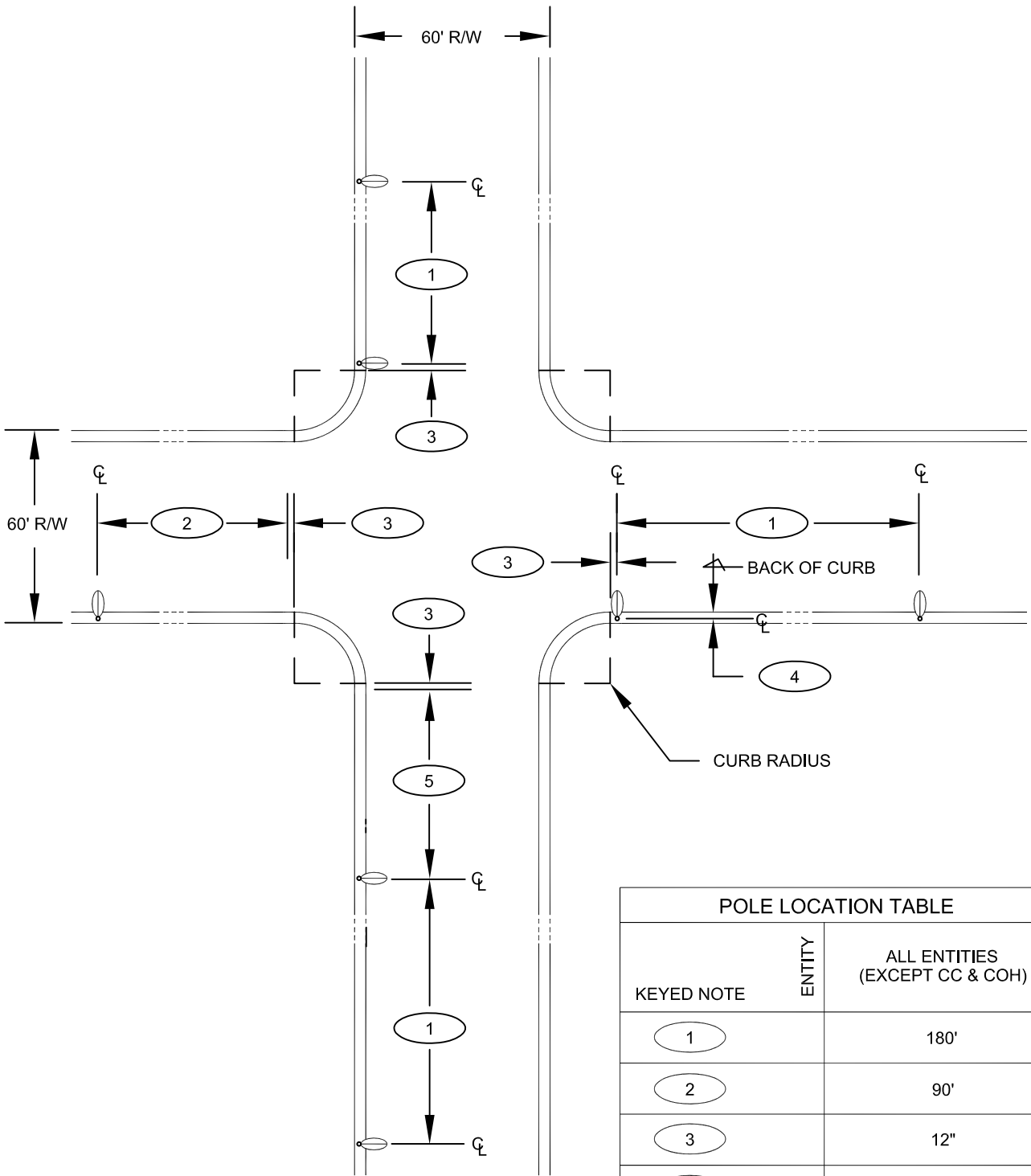
NOTE:

- SEE GENERAL NOTES STANDARD DRAWING NO. 300.
- IF INTERSECTION IS SIGNALIZED, 400 WATT LUMINAIRES SHALL BE INSTALLED ON ALL CORNERS.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 80'/51' OR LESS RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO.	307.1



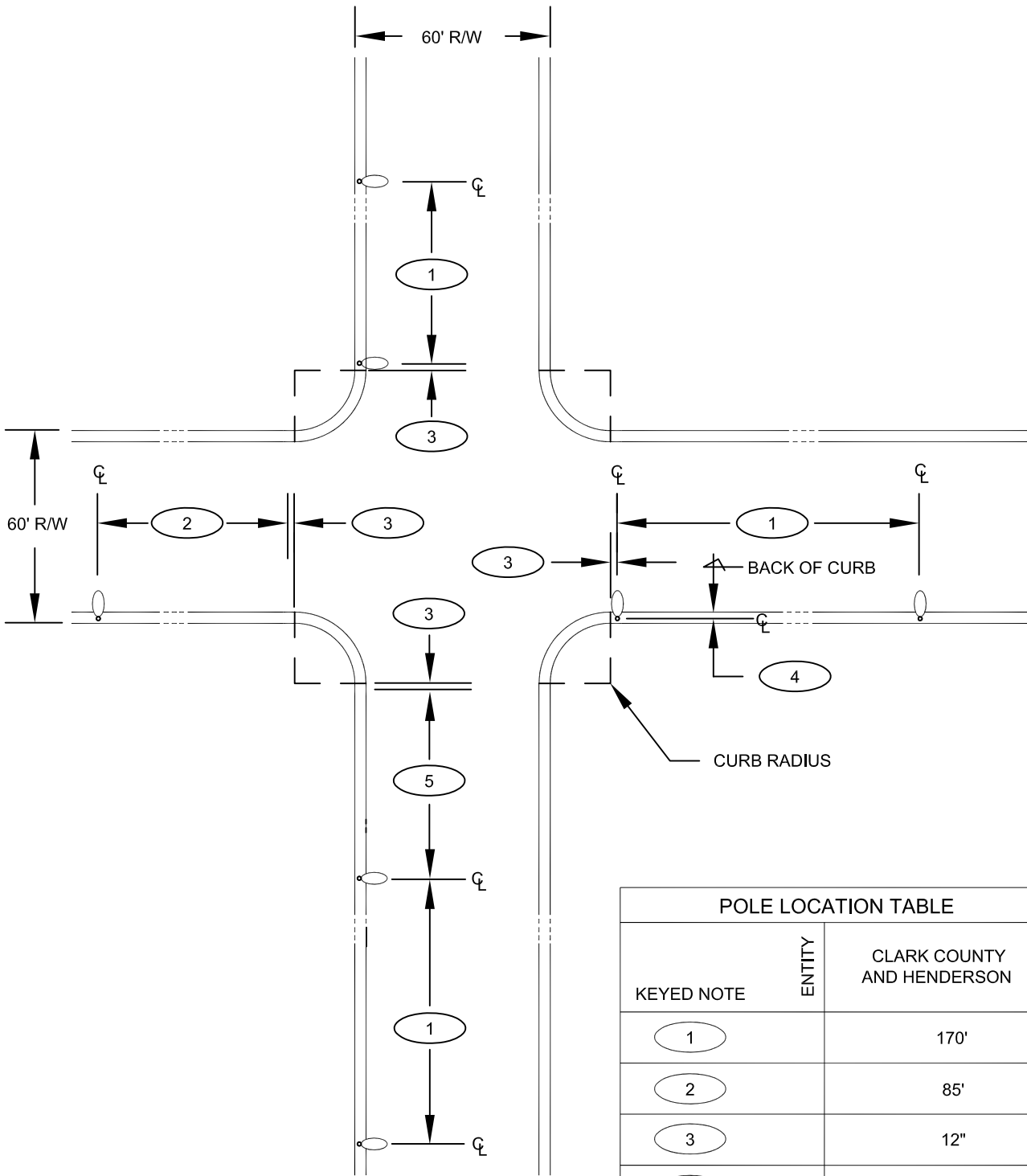
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC & COH)
1		180'
2		90'
3		12"
4		(SEE DRAWING NO. 320)
5		90'

NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA			
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 60'/60' RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON)			
		DATE	2-08-07	DWG. NO.	308

Effective 07/01/10 - 12/30/10



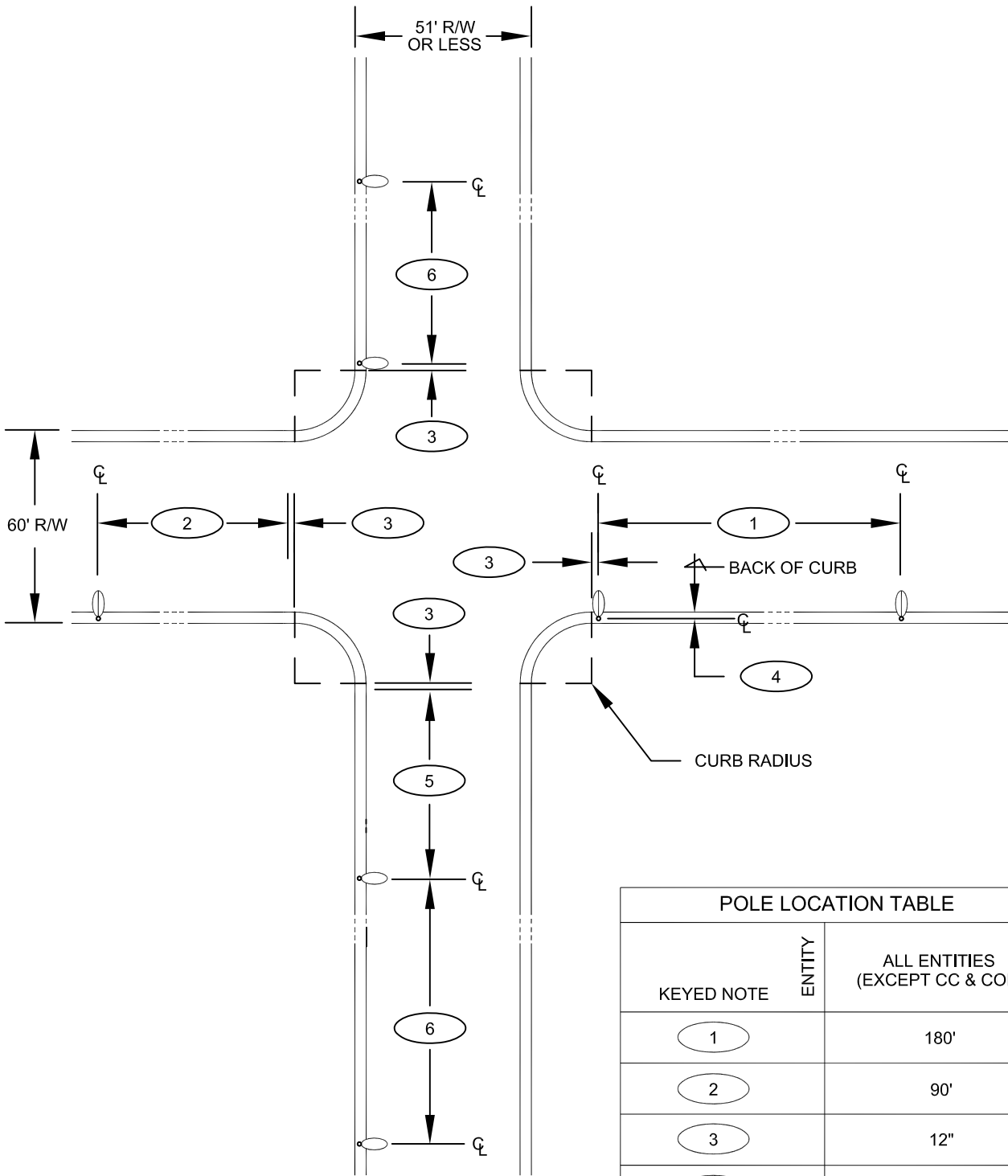
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CLARK COUNTY AND HENDERSON
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		85'

NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 60'/60' RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO. 308.1	

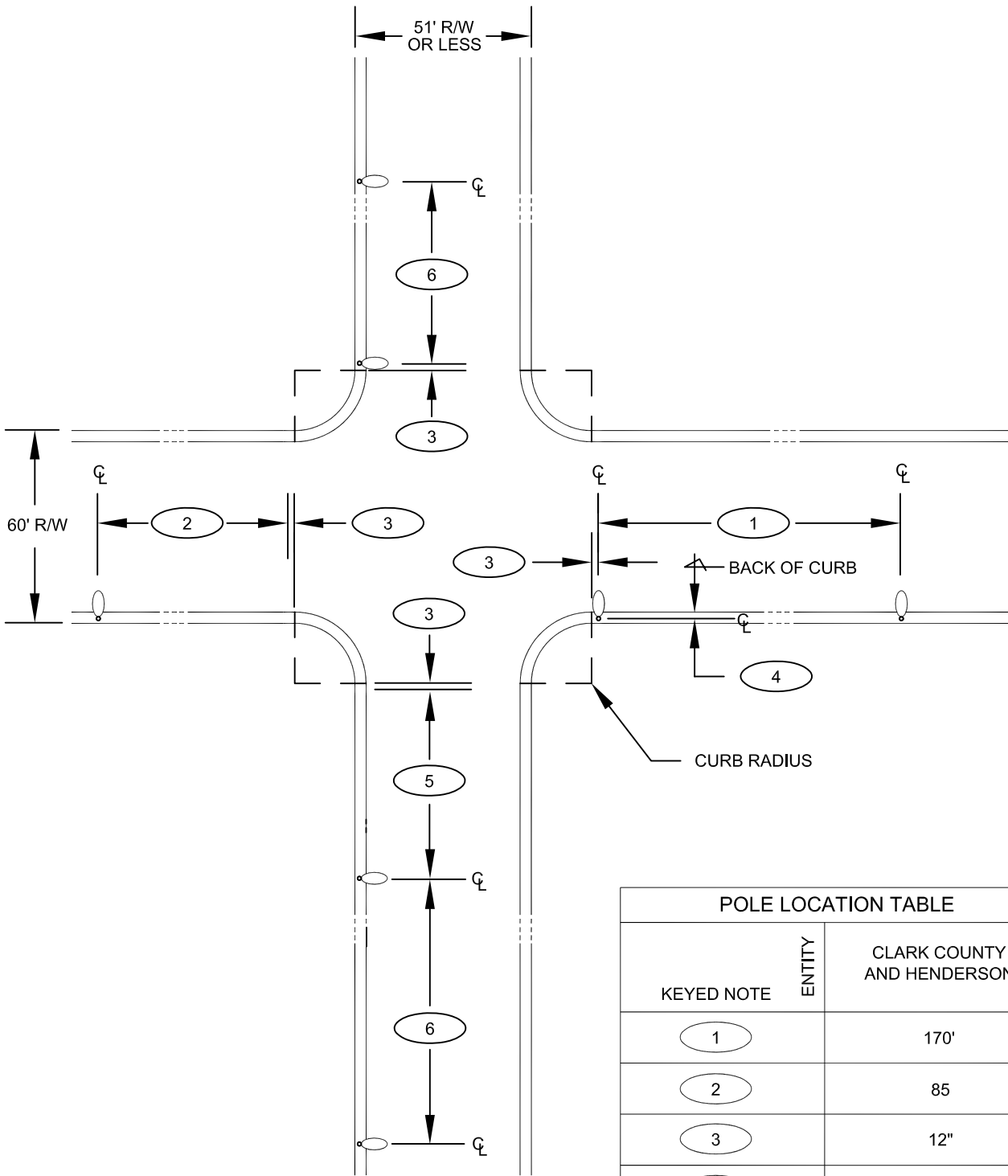


NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

POLE LOCATION TABLE	
KEYED NOTE	ENTITY ALL ENTITIES (EXCEPT CC & COH)
1	180'
2	90'
3	12"
4	(SEE DRAWING NO. 320)
5	85'
6	170'

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 60'/51' OR LESS RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON)	
		DATE 2-08-07	DWG. NO. 309



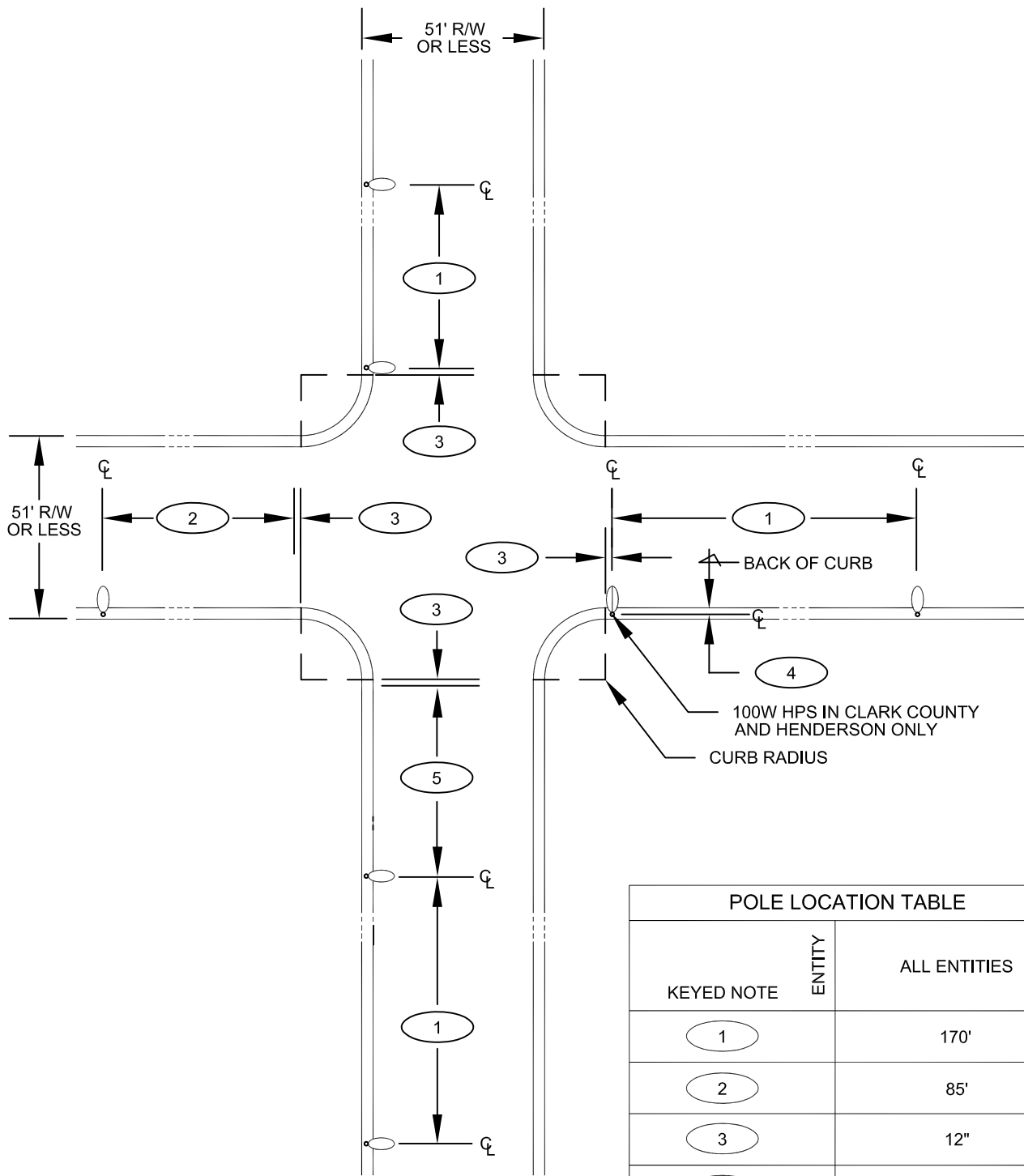
NOTE:
SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

POLE LOCATION TABLE	
KEYED NOTE	ENTITY CLARK COUNTY AND HENDERSON
1	170'
2	85
3	12"
4	(SEE DRAWING NO. 320)
5	85'
6	170'

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT LOCATIONS AT INTERSECTIONS 60'/51' OR LESS RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO. 309.1	



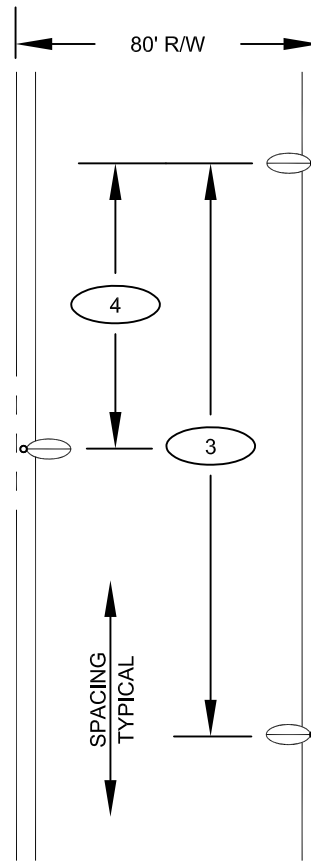
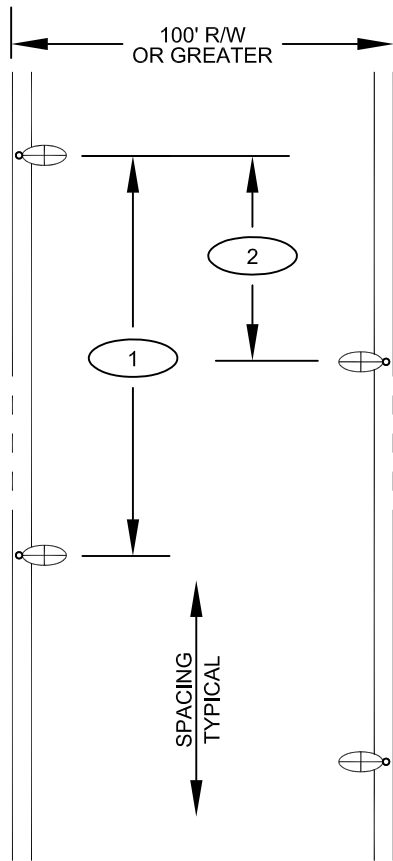
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES
1		170'
2		85'
3		12"
4		(SEE DRAWING NO. 320)
5		85'

NOTE:

SEE GENERAL NOTES STANDARD DRAWING NO. 300.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS AT INTERSECTIONS 51' OR LESS/51' OR LESS RIGHT-OF-WAY		
		DATE 2-08-07	DWG. NO. 310	



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.

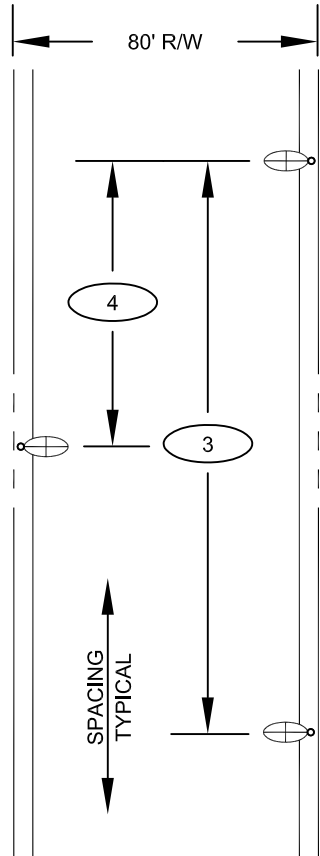
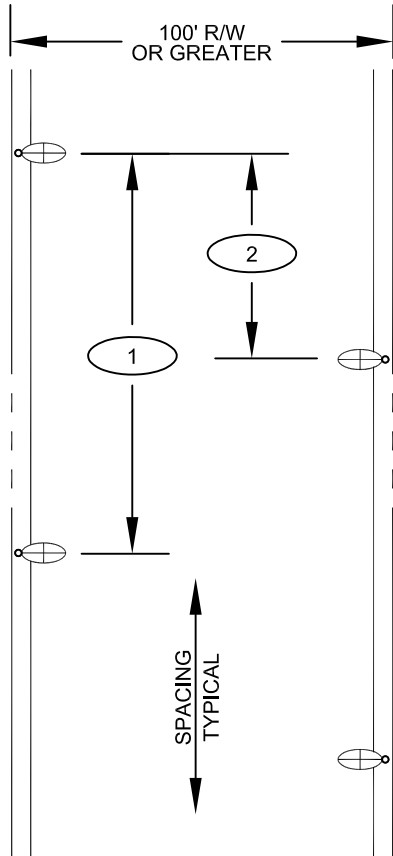
POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC)
1		160'
2		80'
3		170'
4		85'

SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STREETLIGHT STANDARDS
MAXIMUM SPACING
(80 FT. OR GREATER RIGHT-OF-WAY)
(EXCEPT CLARK COUNTY)



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.

POLE LOCATION TABLE		
KEYED NOTE	ENTITY	CLARK COUNTY
1		120'
2		60'
3		170'
4		85'

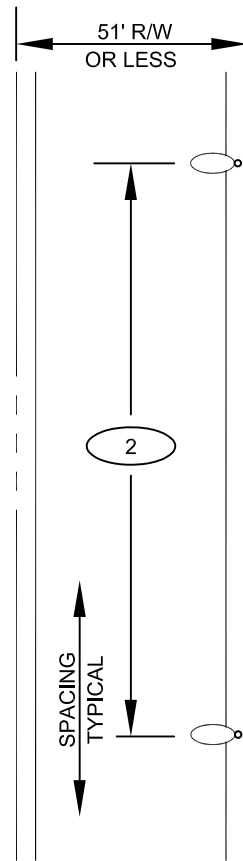
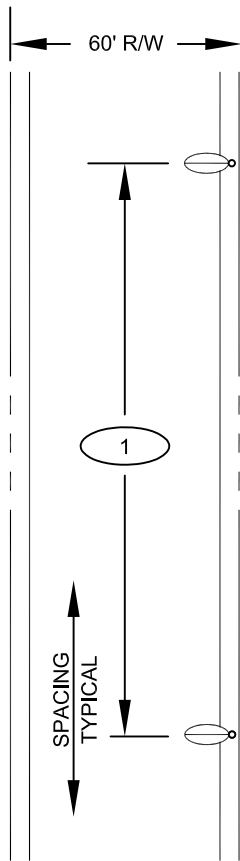
SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

CLARK COUNTY ONLY

STREETLIGHT STANDARDS
MAXIMUM SPACING
(80 FT. OR GREATER RIGHT-OF-WAY)



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.

POLE LOCATION TABLE		
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC & COH)
1		180'
2		170'

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

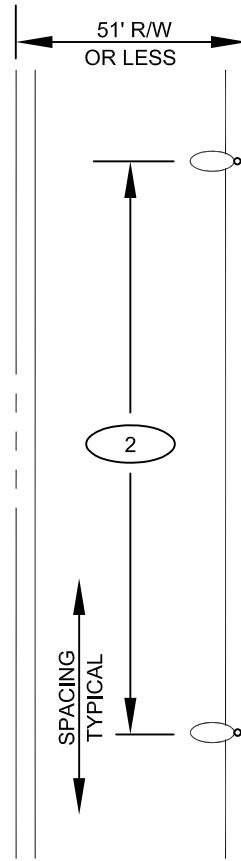
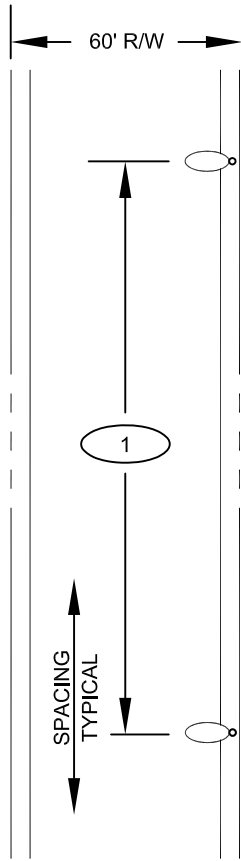
SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STREETLIGHT STANDARDS
MAXIMUM SPACING
(60 FT. OR LESS RIGHT-OF-WAY)
(EXCEPT CLARK COUNTY & HENDERSON)

DATE 2-08-07 DWG. NO. 311A



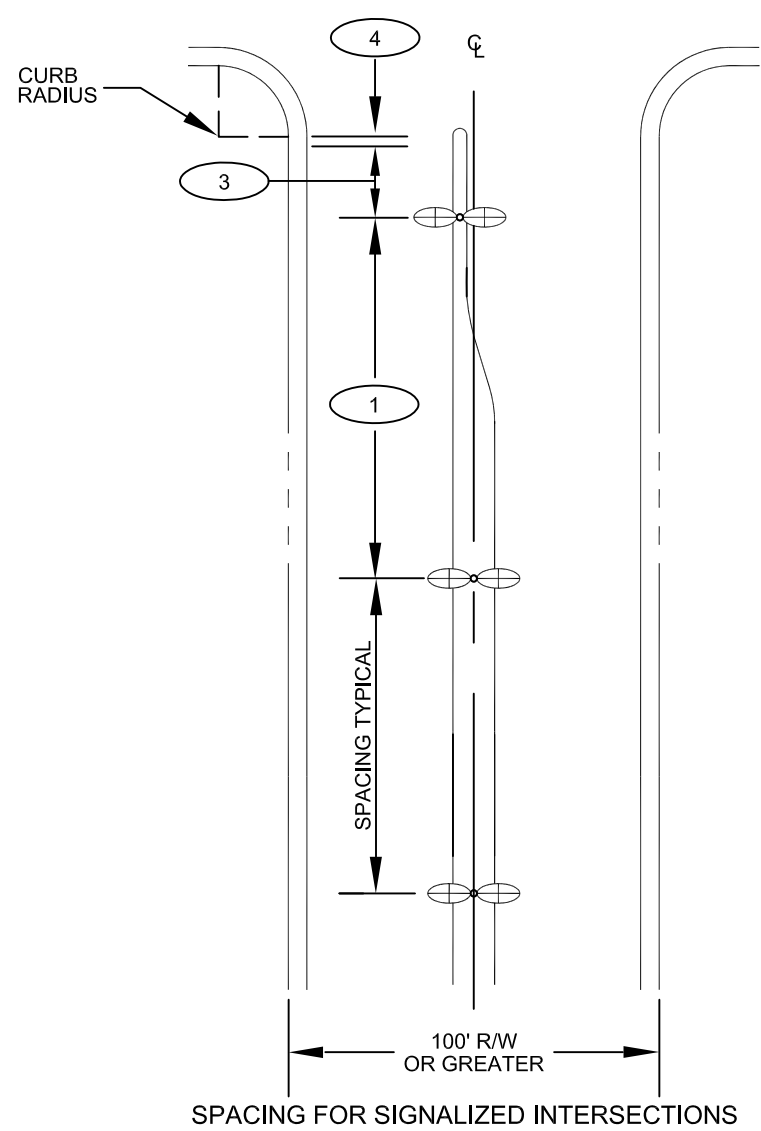
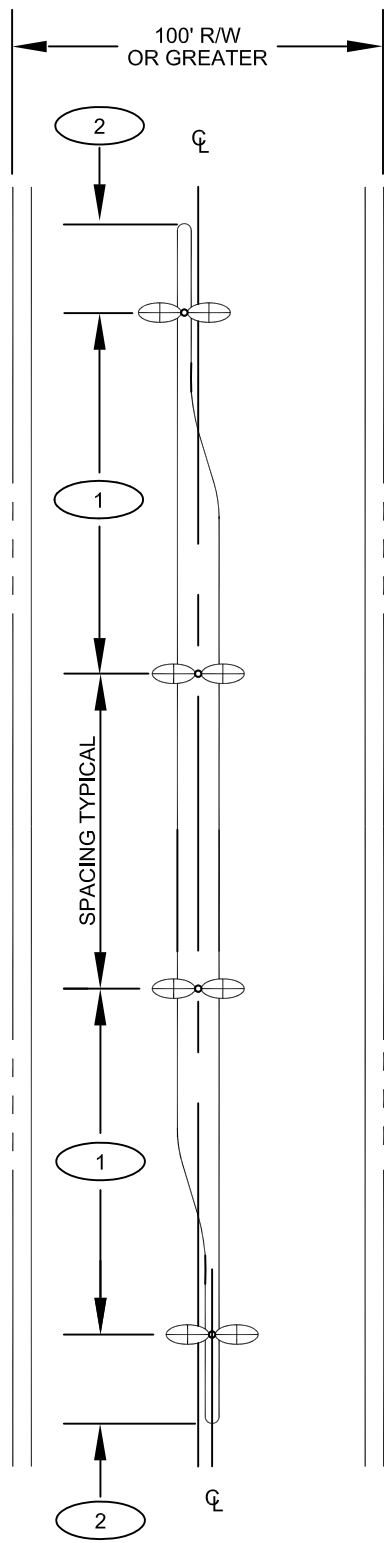
NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.

POLE LOCATION TABLE	
KEYED NOTE	ENTITY CLARK COUNTY & HENDERSON
1	170'
2	170'

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	CLARK COUNTY & HENDERSON ONLY		
		STREETLIGHT STANDARDS MAXIMUM SPACING (60 FT. OR LESS RIGHT-OF-WAY)		
		DATE 2-08-07	DWG. NO.	311.1A



SPACING FOR SIGNALIZED INTERSECTIONS

POLE LOCATION TABLE			
KEYED NOTE	ENTITY	ALL ENTITIES (EXCEPT CC)	CLARK COUNTY
* 1		160'	120'
2		10'	10' (MIN.)
3		80'	120'
4		12"	12"

NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300, IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.

* DISTANCE LISTED INDICATES MAXIMUM SPACING. LIGHTING STANDARDS SHALL BE EQUIDISTANT AFTER LOCATING THE END OF ISLAND POLES.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT LOCATIONS ON TRAFFIC ISLANDS 100' OR GREATER RIGHT-OF-WAY		
		DATE 8-12-99	DWG. NO. 312	PAGE NO. 72

1. ALL STREETLIGHT STANDARDS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PROVISIONS OF THE STANDARD SPECIFICATIONS AND AS INDICATED ON THESE DRAWINGS.

2. ALL COMPONENTS OF THE STREETLIGHT STANDARD INCLUDING THE POLE, ARM, HANDHOLE COVER, BASE COVER AND THE POLE CAP SHALL BE FERROUS METAL AND HOT-DIP GALVANIZED AFTER CONSTRUCTION IN ACCORDANCE WITH ASTM A123; ALUMINUM OR ALUMINUM ALLOY IS NOT ACCEPTABLE. FLAWS IN THE APPEARANCE OF THESE GALVANIZED COMPONENTS (i.e. "TIGER-STRIPED, "ZEBRA-STRIPED"), SHALL BE CAUSE FOR REJECTION. NON-METALLIC TYPE BASE COVERS MAY BE ACCEPTABLE AND SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. CONCRETE POLES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

3. ALL FASTENING HARDWARE SHALL BE NON-CORROSIVE, CADMIUM-PLATED, OR EQUAL, APPROVED BY THE ENGINEER. FASTENERS SHALL BE OF THE SIZE AND CONFIGURATION NOTED ON THE DRAWINGS.

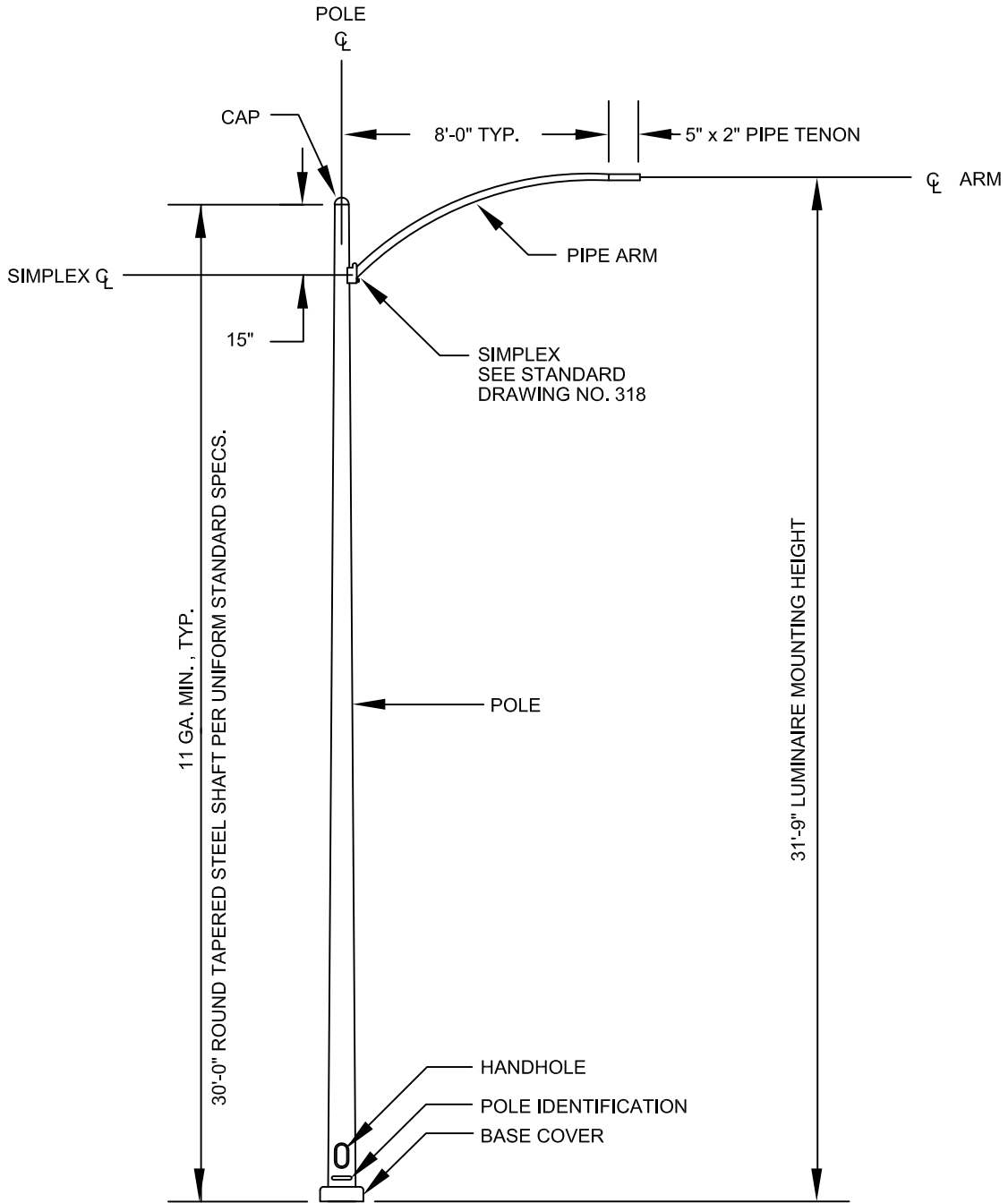
4. CONCRETE POLE FOUNDATIONS SHOULD BE Poured AGAINST UNDISTURBED, NATURAL SOIL OR IF FORMING MATERIAL IS USED IT SHALL BE STRIPPED AWAY FROM THE FOUNDATION AT LEAST ONE (1) FOOT BELOW FINISHED GRADE.

5. POLES SHALL BE INSTALLED ON CONCRETE FOUNDATIONS WITH ANCHOR BOLTS. EACH BOLT SHALL BE INSTALLED WITH TWO (2) HEX NUTS AND TWO (2) FLAT WASHERS. EXCEPT FOR "H" AND "L" FOUNDATIONS, THE ANCHOR BOLTS SHALL BE 1" X 36" X 4" FOR ELEVEN (11) GAGE POLES AND 1 1/8" X 40" X 4" FOR SEVEN (7) GAGE POLES. THE ANCHOR BOLTS, NUTS AND WASHERS SHALL BE HOT-DIP GALVANIZED. THE POLE SHALL BE PLUMBED PRIOR TO PLACING THE GROUT OR CONCRETE CAP. USE OF GROUT OR CONCRETE FOR CAP SHALL BE DESIGNATED BY ENTITY ENGINEER. SHIMS OR WEDGES OF ANY KIND ARE NOT ACCEPTABLE TO PLUMB THE POLE AFTER THE CAP HAS BEEN PLACED.

6. ALL UNDERGROUND CONDUIT INSTALLED SHALL HAVE RED, CONTINUOUS MARKING TAPE INSTALLED IN THE TRENCH AT 12" BELOW FINISHED GRADE.

7. WHERE SIGNALS AND STANDARDS ARE INSTALLED UNDER OVERHEAD POWER LINES, CLEARANCES SHALL BE PER NATIONAL ELECTRICAL SAFETY CODE SECTION 234 REQUIREMENTS. INSTALL STRAIGHT ARM STREETLIGHT ASSEMBLIES WHERE ADDITIONAL CLEARANCE IS REQUIRED.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT STANDARD GENERAL NOTES		
		DATE 7-8-04	DWG. NO. 313	PAGE NO. 73



NOTES:

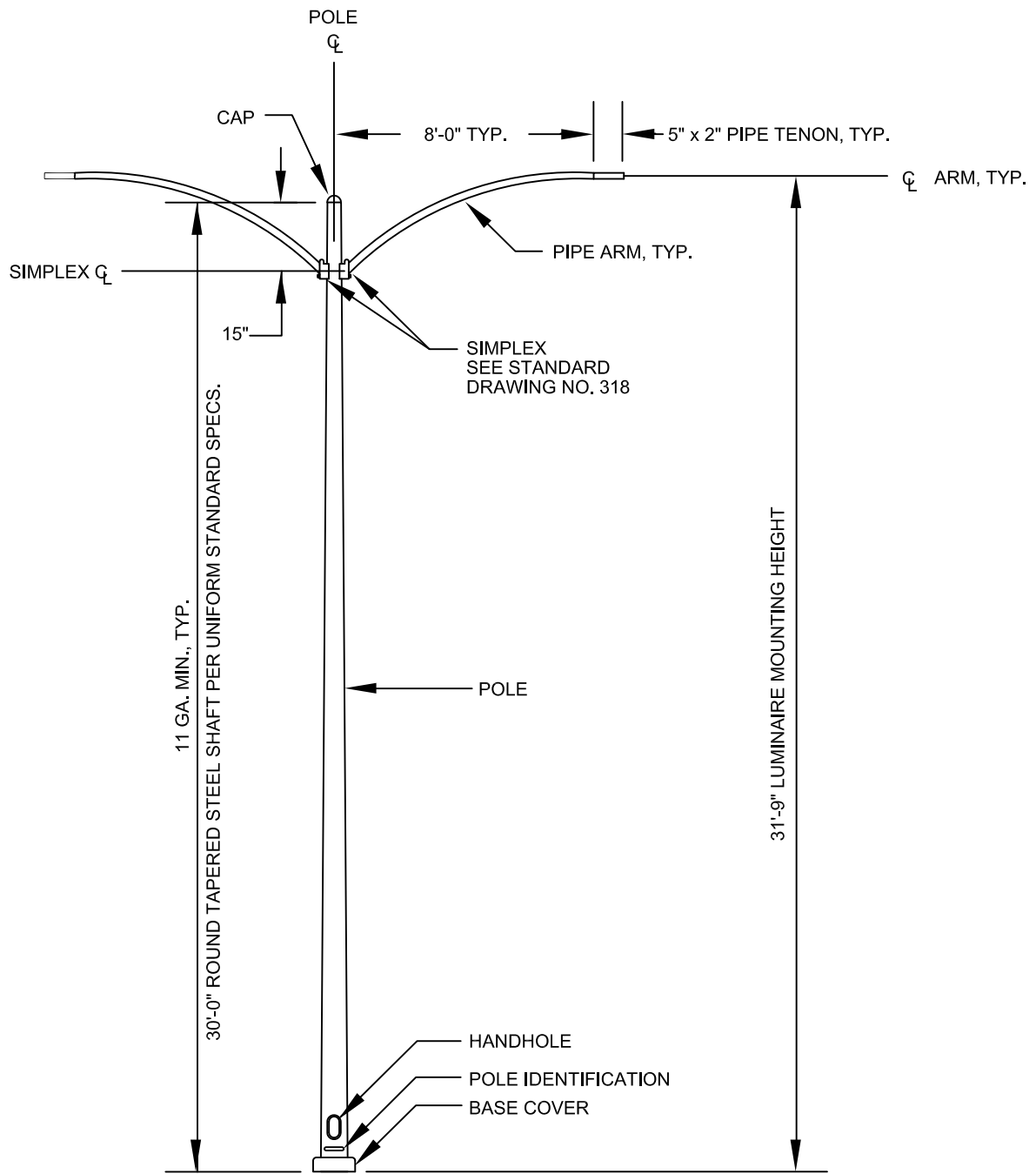
1. SEE GENERAL NOTES STANDARD DRAWING NO. 313.
2. SEE STANDARD DRAWING NO. 319 FOR DETAIL OF POLE BASE.
3. SEE STANDARD DRAWING NO. 318 FOR DETAIL OF POLE CAP.

SPECIFICATION REFERENCE

**UNIFORM STANDARD DRAWINGS
 CLARK COUNTY AREA**

506	STEEL STRUCTURES
623	TRAFFIC SIGNALS & STREETLIGHTING
715	GALVANIZING

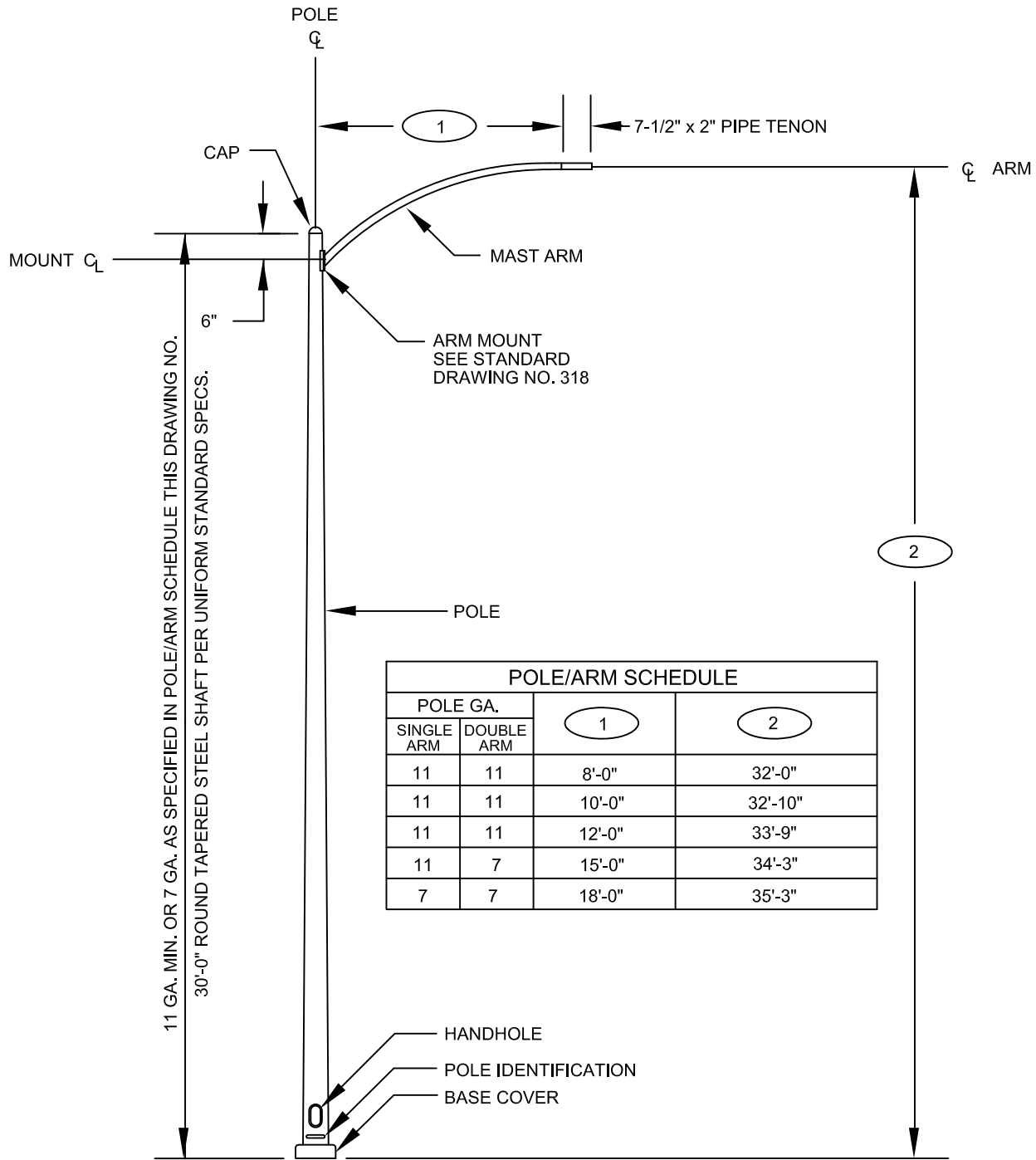
**STREETLIGHT STANDARD
 WITH 2" PIPE ARM**



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 313.
2. SEE STANDARD DRAWING NO. 319 FOR DETAIL OF POLE BASE.
3. SEE STANDARD DRAWING NO. 318 FOR DETAIL OF POLE CAP.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
506	STEEL STRUCTURES	STREETLIGHT STANDARD WITH DOUBLE 2" PIPE ARM		
623	TRAFFIC SIGNALS & STREETLIGHTING			
715	GALVANIZING			
		DATE 12-12-96	DWG. NO. 315	PAGE NO. 75



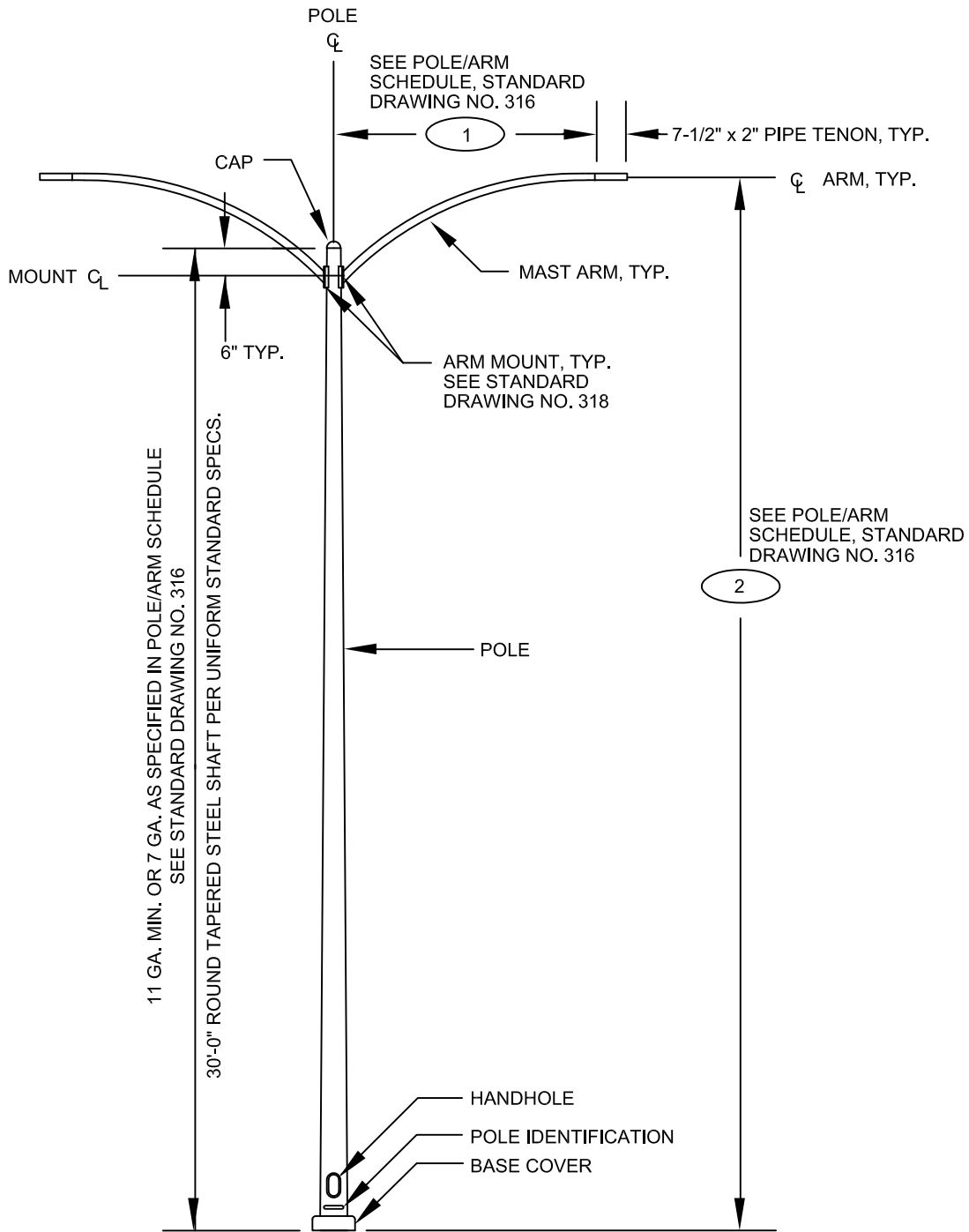
POLE/ARM SCHEDULE			
POLE GA.		1	2
SINGLE ARM	DOUBLE ARM		
11	11	8'-0"	32'-0"
11	11	10'-0"	32'-10"
11	11	12'-0"	33'-9"
11	7	15'-0"	34'-3"
7	7	18'-0"	35'-3"

NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 313.
2. SEE STANDARD DRAWING NO. 319 FOR DETAIL OF POLE BASE.
3. SEE STANDARD DRAWING NO. 318 FOR DETAIL OF POLE CAP.

Effective 07/01/10 - 12/30/10

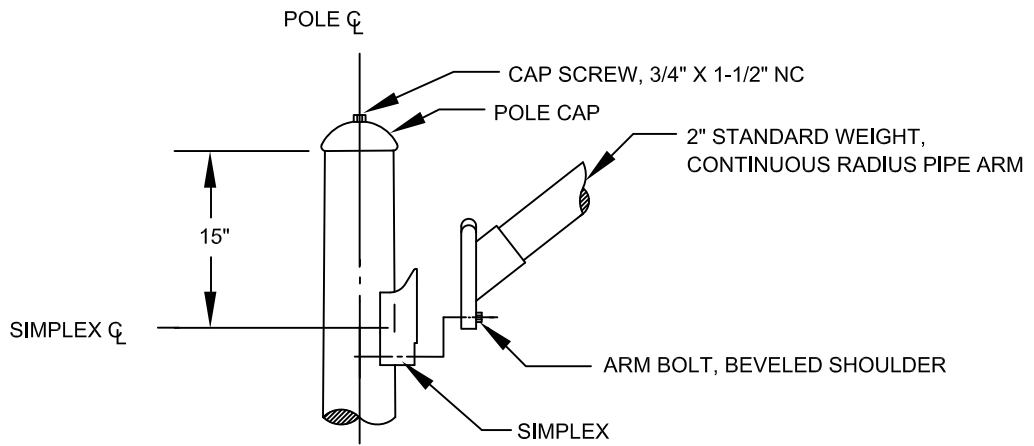
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
506	STEEL STRUCTURES	STREETLIGHT STANDARD WITH TAPERED MAST ARM		
623	TRAFFIC SIGNALS & STREETLIGHTING			
715	GALVANIZING			
		DATE 12-12-96	DWG. NO. 316	PAGE NO. 76



NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 313.
2. SEE STANDARD DRAWING NO. 319 FOR DETAIL OF POLE BASE.
3. SEE STANDARD DRAWING NO. 318 FOR DETAIL OF POLE CAP.

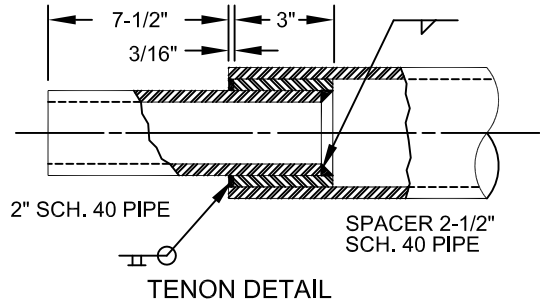
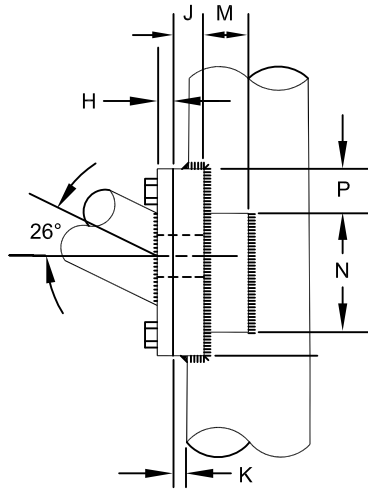
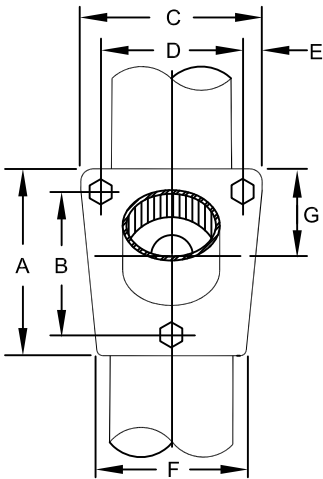
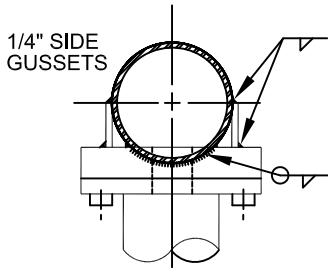
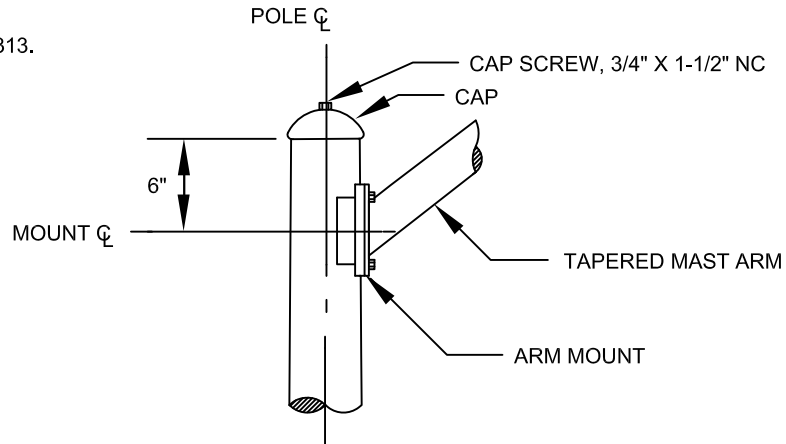
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
506	STEEL STRUCTURES	STREETLIGHT STANDARD WITH DOUBLE TAPERED MAST ARM		
623	TRAFFIC SIGNALS & STREETLIGHTING			
715	GALVANIZING			
		DATE 12-12-96	DWG. NO. 317	PAGE NO. 77



PIPE ARM

NOTE:

SEE GENERAL NOTES
STANDARD DRAWING NO. 313.

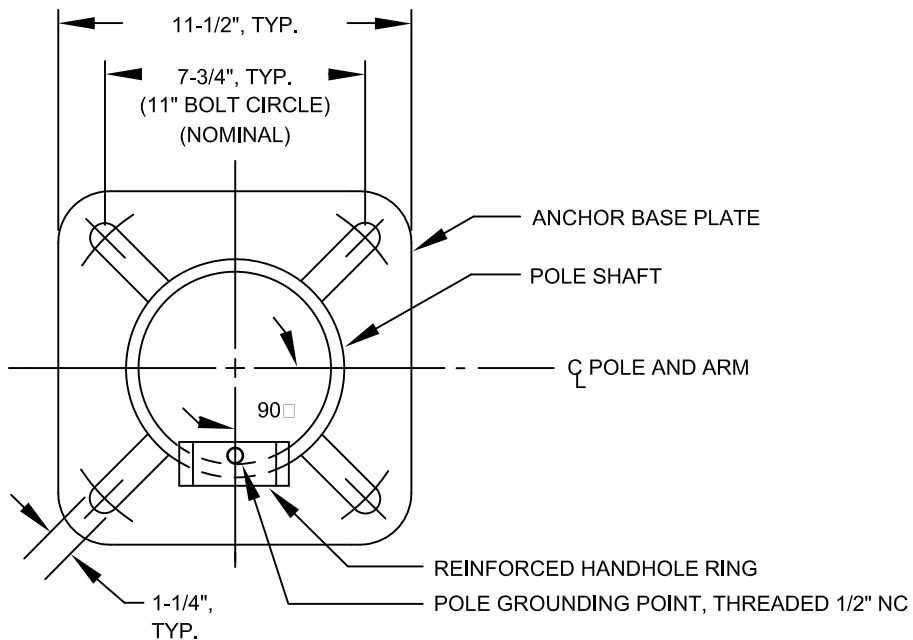
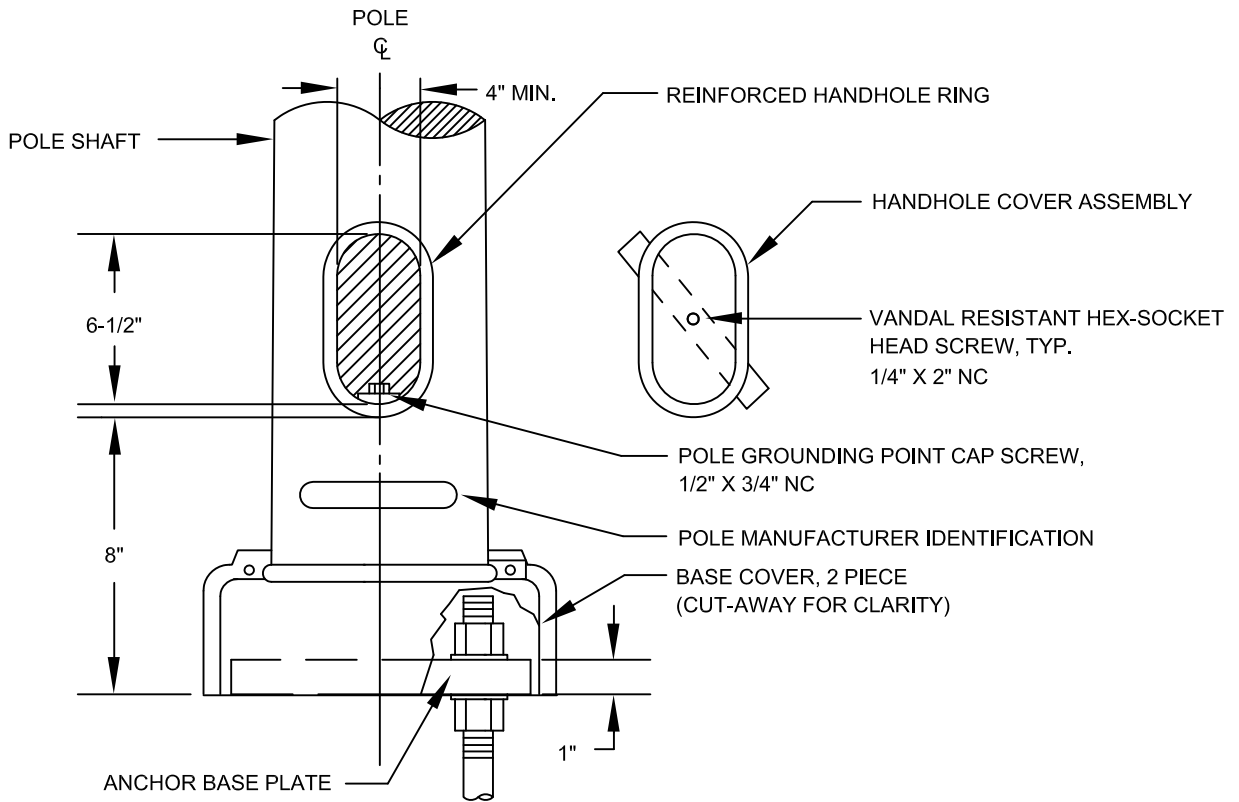


MAST ARM	A	B	C	D	E	F	G
8'-15' INCL.	7-1/2"	5-3/4"	6-3/8"	4-5/8"	7/8"	5"	3-3/8"
15'-18' INCL.	9"	7"	8"	6"	1"	6"	4"

MAST ARM	H	J	K	L	M	N	N
8'-15' INCL.	1/2"	1"	5/16"	5/8"-11x1/2"	1-1/2"	5"	1-5/8"
15'-18' INCL.	3/4"	1"	5/16"	3/4"-10x1-3/4"	2"	5"	1-3/4"

MAST ARM AND DETAILS

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
506	STEEL STRUCTURES	POLE TOP AND ARM MOUNTING DETAILS		
623	TRAFFIC SIGNALS & STREETLIGHTING			
715	GALVANIZING			
		DATE 12-12-96	DWG. NO. 318	PAGE NO. 78



NOTES:

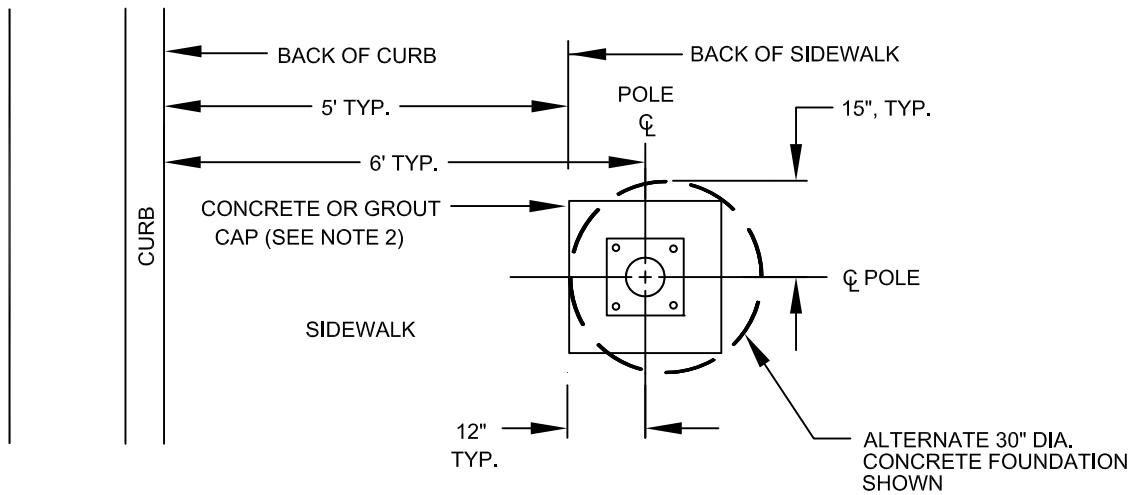
1. SEE GENERAL NOTES STANDARD DRAWING NO. 313
2. HANDHOLE SHALL FACE AWAY FROM ONCOMING TRAFFIC.

SPECIFICATION REFERENCE

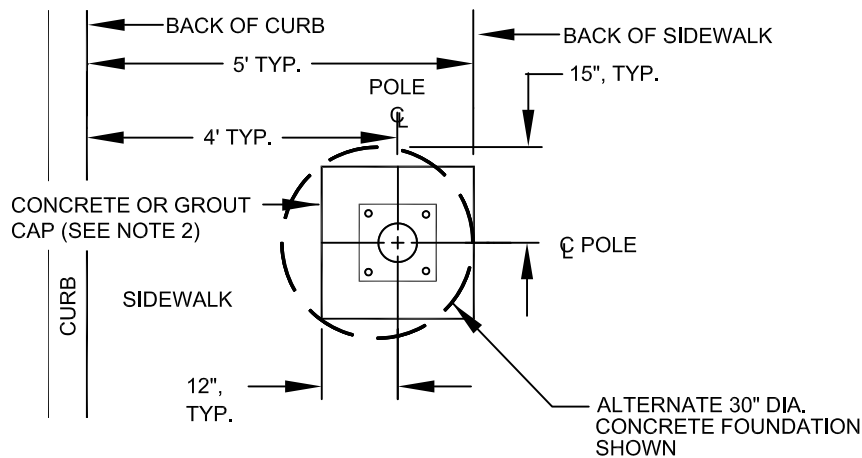
506	STEEL STRUCTURES
623	TRAFFIC SIGNALS & STREETLIGHTING
715	GALVANIZING

**UNIFORM STANDARD DRAWINGS
 CLARK COUNTY AREA**

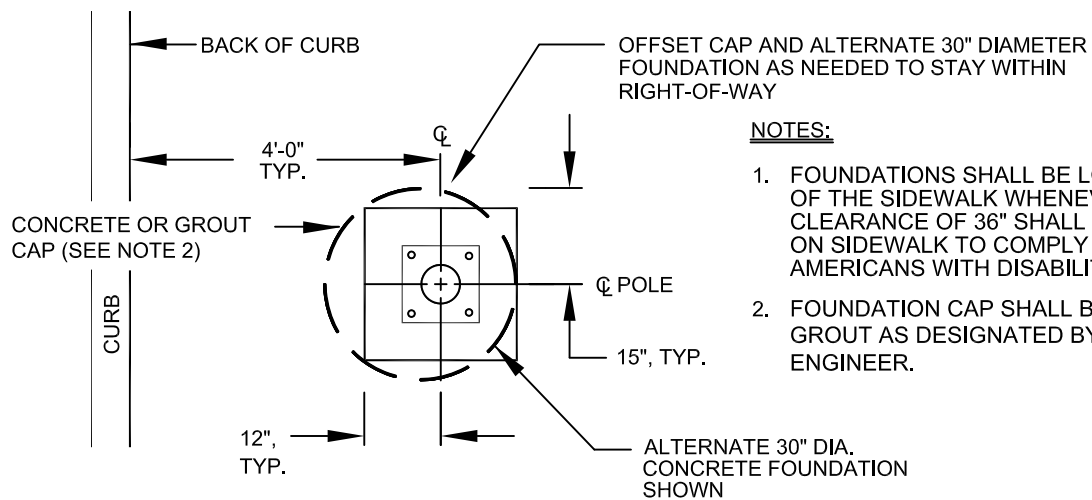
**LOWER POLE DETAILS
 FOR PIPE AND MAST ARM
 POLES**



BEHIND CURBSIDE SIDEWALK



BACK PORTION OF CURBSIDE SIDEWALK (NOT FOR NEW CONSTRUCTION)

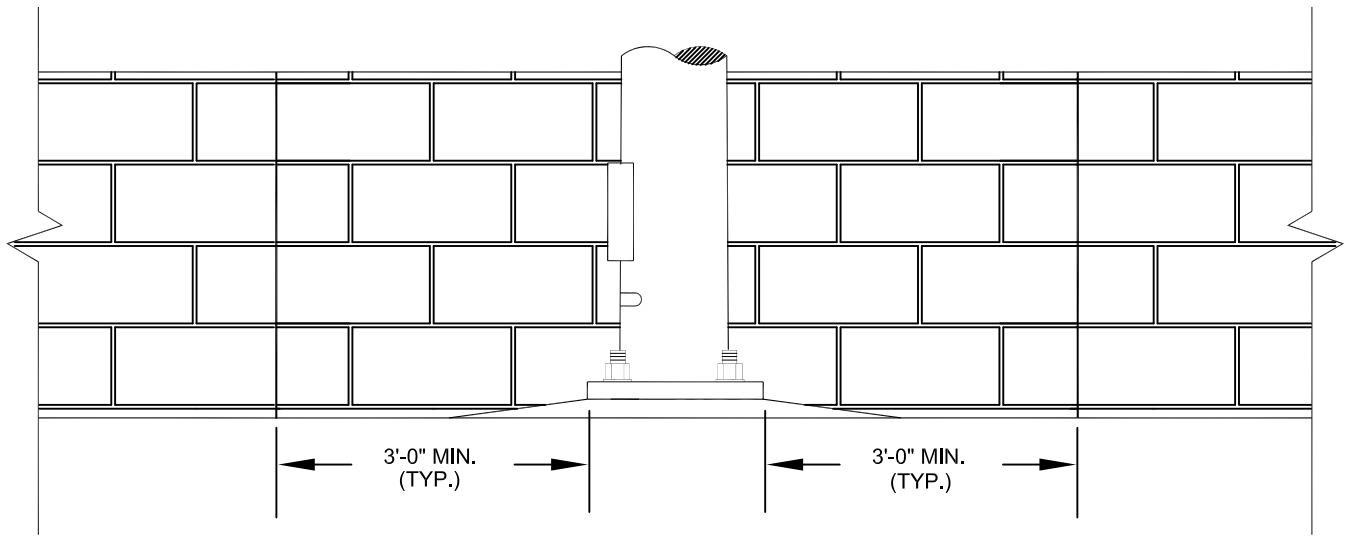


NOTES:

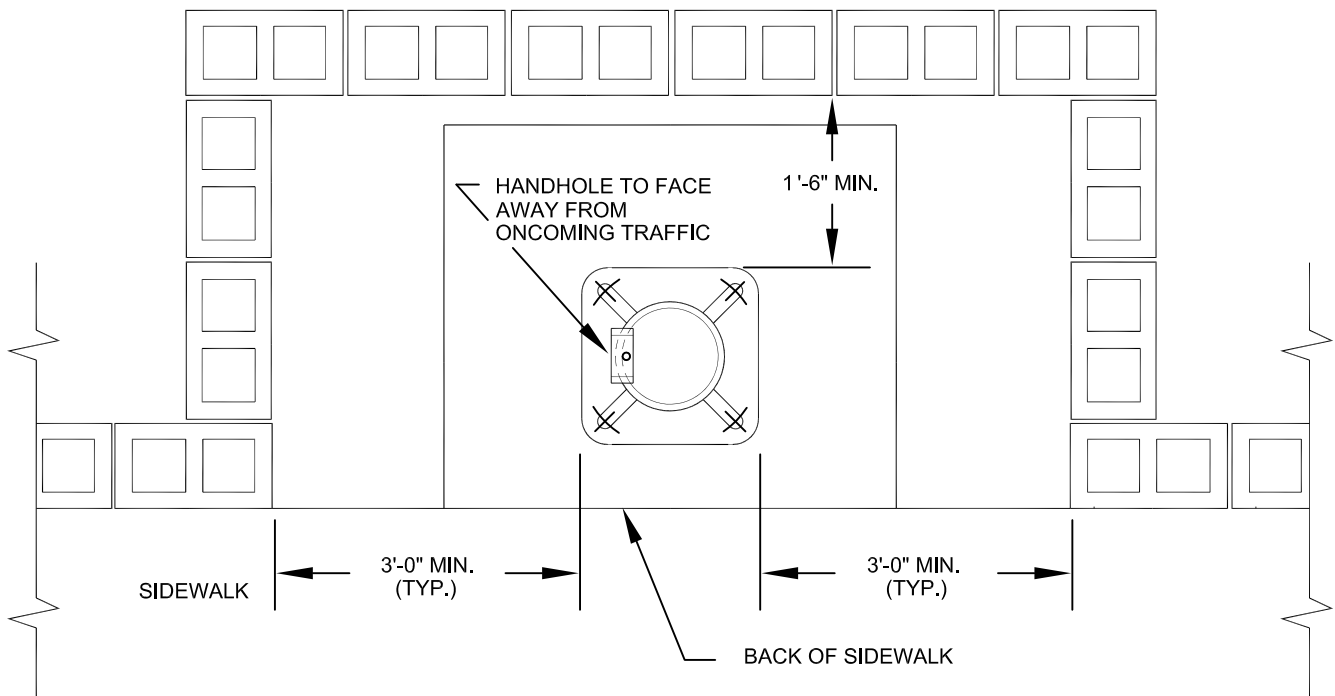
1. FOUNDATIONS SHALL BE LOCATED OUTSIDE OF THE SIDEWALK WHENEVER FEASIBLE. A CLEARANCE OF 36" SHALL BE MAINTAINED ON SIDEWALK TO COMPLY WITH THE AMERICANS WITH DISABILITIES ACT.
2. FOUNDATION CAP SHALL BE CONCRETE OR GROUT AS DESIGNATED BY THE ENTITY ENGINEER.

OPEN AREA OR BETWEEN CURB AND SIDEWALK

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
501	PORTLAND CEMENT CONCRETE	LIGHTING STANDARD SETBACK					
623	TRAFFIC SIGNALS & STREETLIGHTING						
		DATE	7-8-04	DWG. NO.	320	PAGE NO.	80



ELEVATION



PLAN

NOTE:

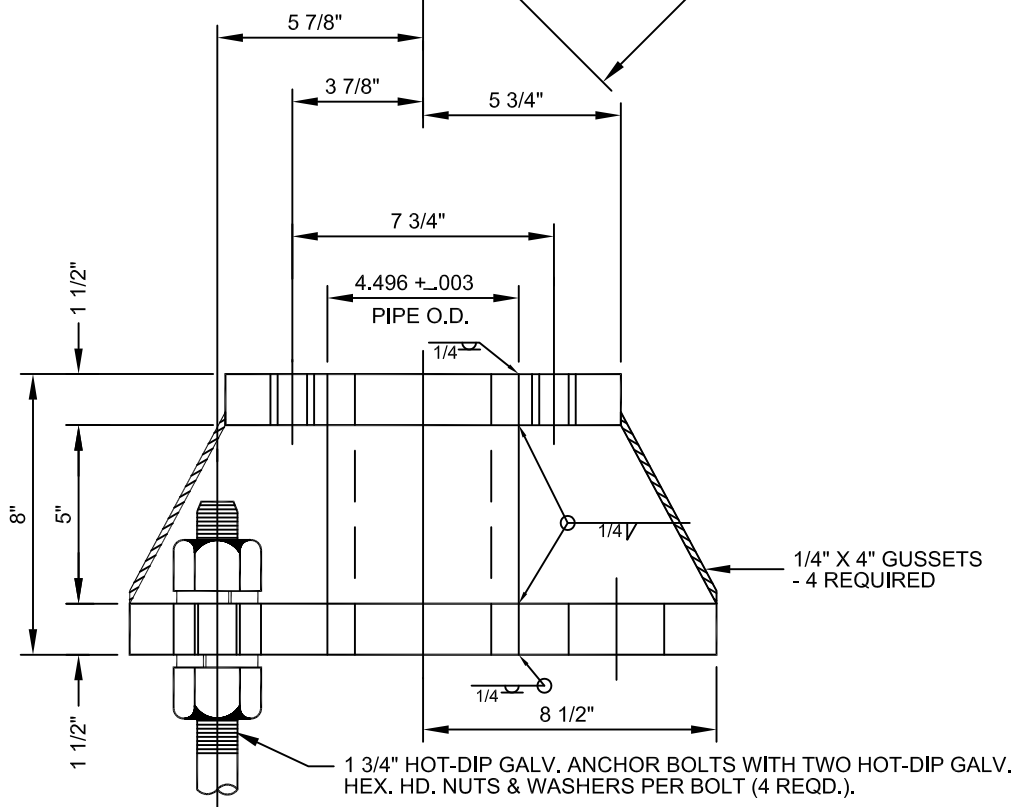
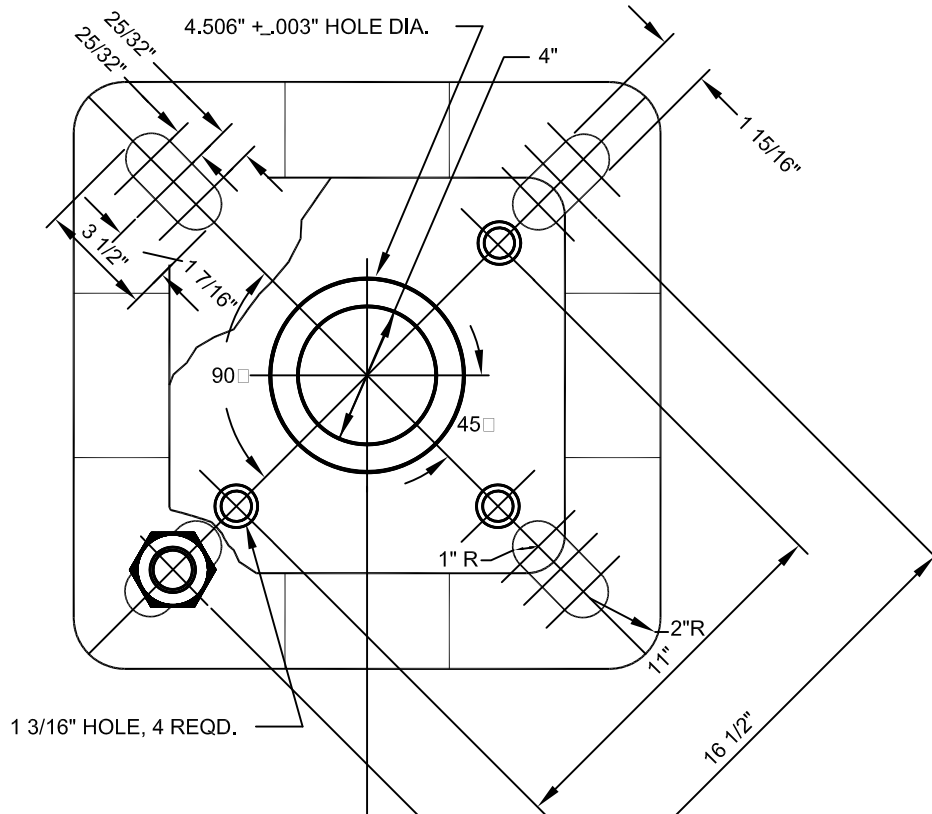
POLE BASE COVERS SHALL BE FURNISHED AND INSTALLED FOR ALL POLES PER THE STANDARD SPECIFICATIONS AND DRAWINGS.

SPECIFICATION REFERENCE

623 TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

LIGHTING STANDARD SETBACK
FROM BLOCK WALL



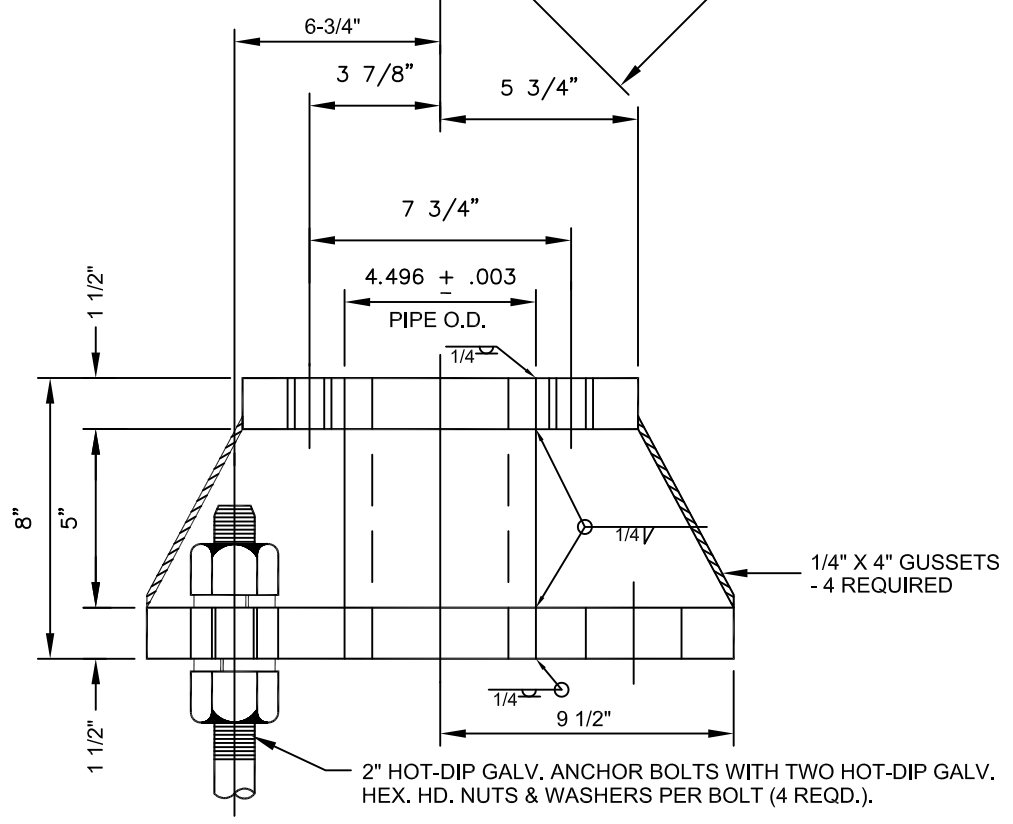
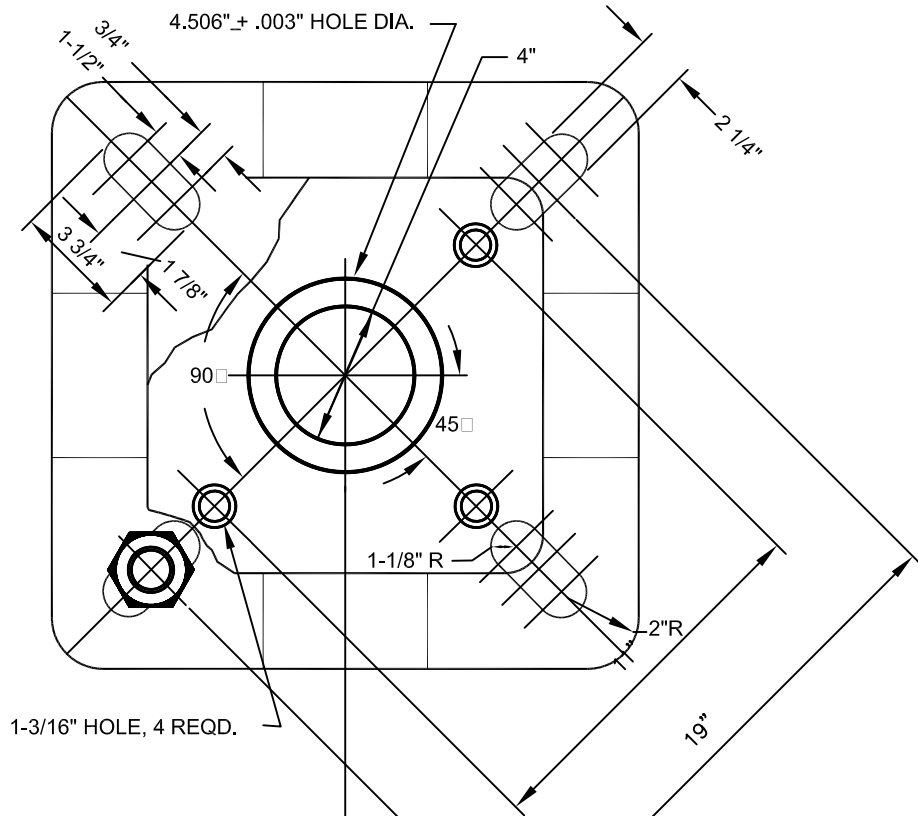
SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

BASE ADAPTOR PLATE
FOR 16-1/2" BOLT CIRCLE FOUNDATION

DATE 12-12-96 DWG. NO. 322

PAGE NO. 82

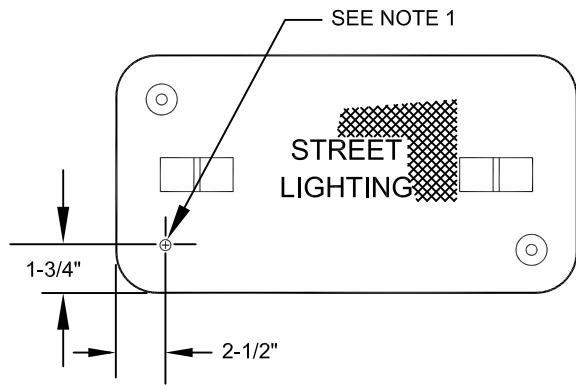


SPECIFICATION REFERENCE

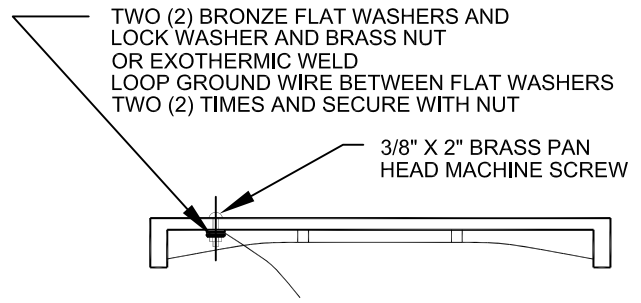
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

BASE ADAPTOR PLATE
FOR 19" BOLT CIRCLE FOUNDATION

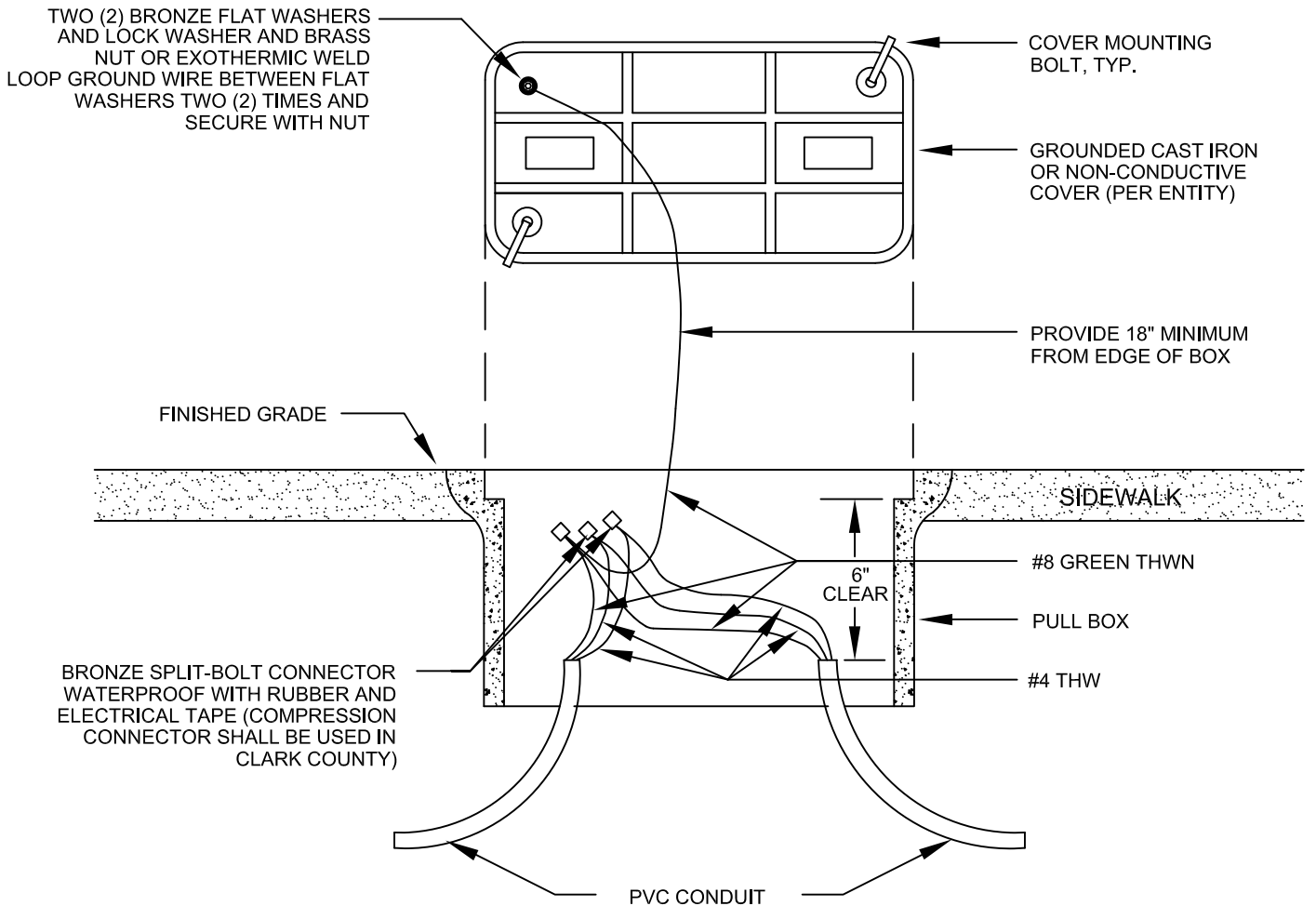
Effective 07/01/10 - 12/30/10



PULL BOX COVER - TOP VIEW



PULL BOX COVER - SIDE VIEW

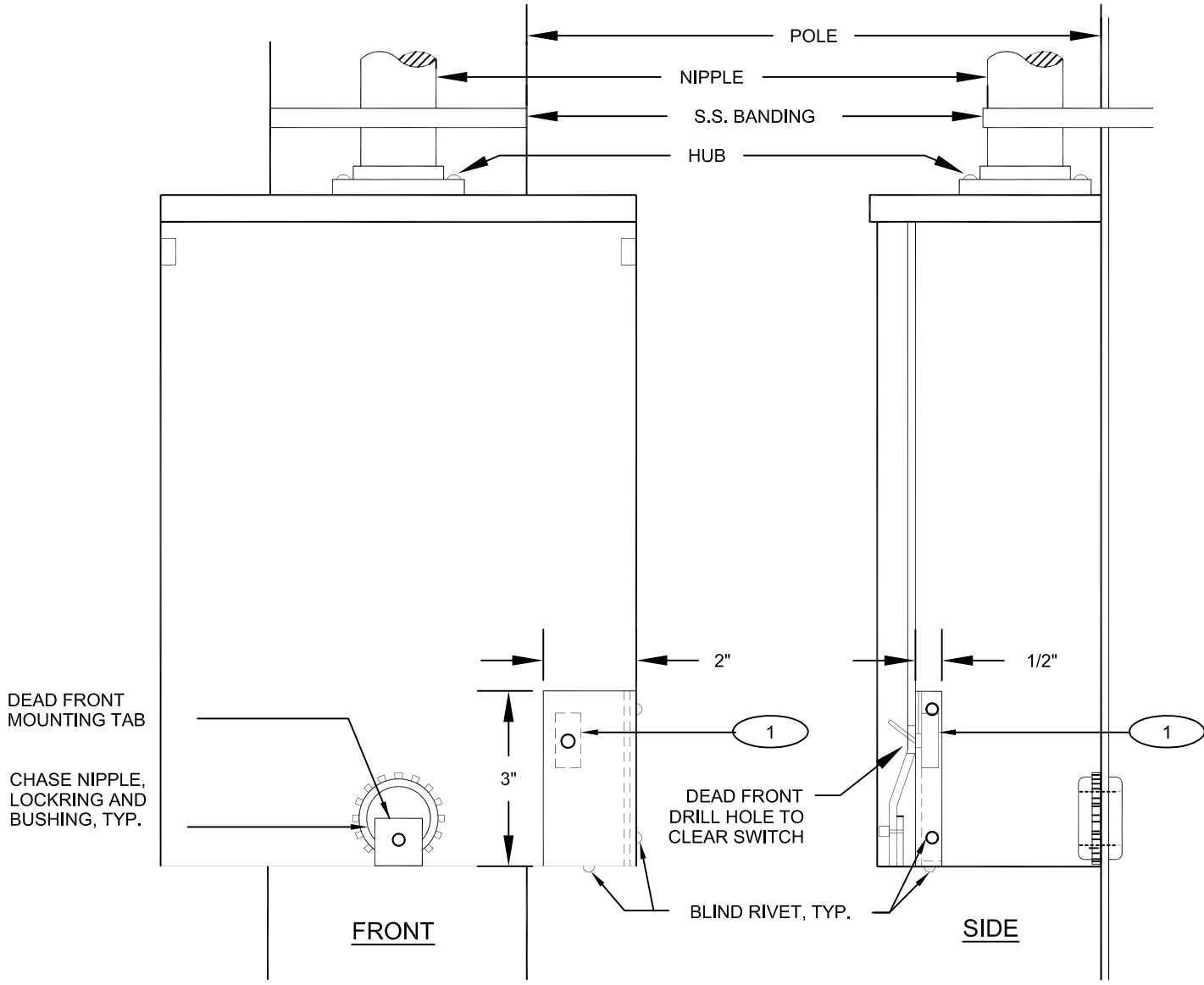


(TYPICAL CAST IRON LID SHOWN)

NOTES:

1. PULL BOX LID SHOULD BE TAPPED WITH A 3/8" X 16 COURSE THREAD TAP.
2. FOR TYPICAL NO. 7 PULL BOX COVER GROUNDING, SEE STANDARD DRAWING NO. 327.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING			
		PULL BOX COVER BONDING DETAIL		
		DATE 12-12-96	DWG. NO. 323	PAGE NO. 83

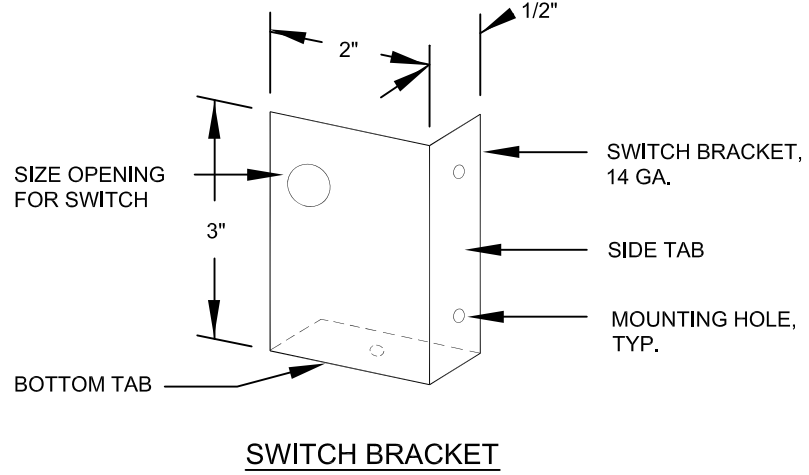


DEAD FRONT MOUNTING TAB
CHASE NIPPLE, LOCKRING AND BUSHING, TYP.

POLE
NIPPLE
S.S. BANDING
HUB

FRONT

SIDE



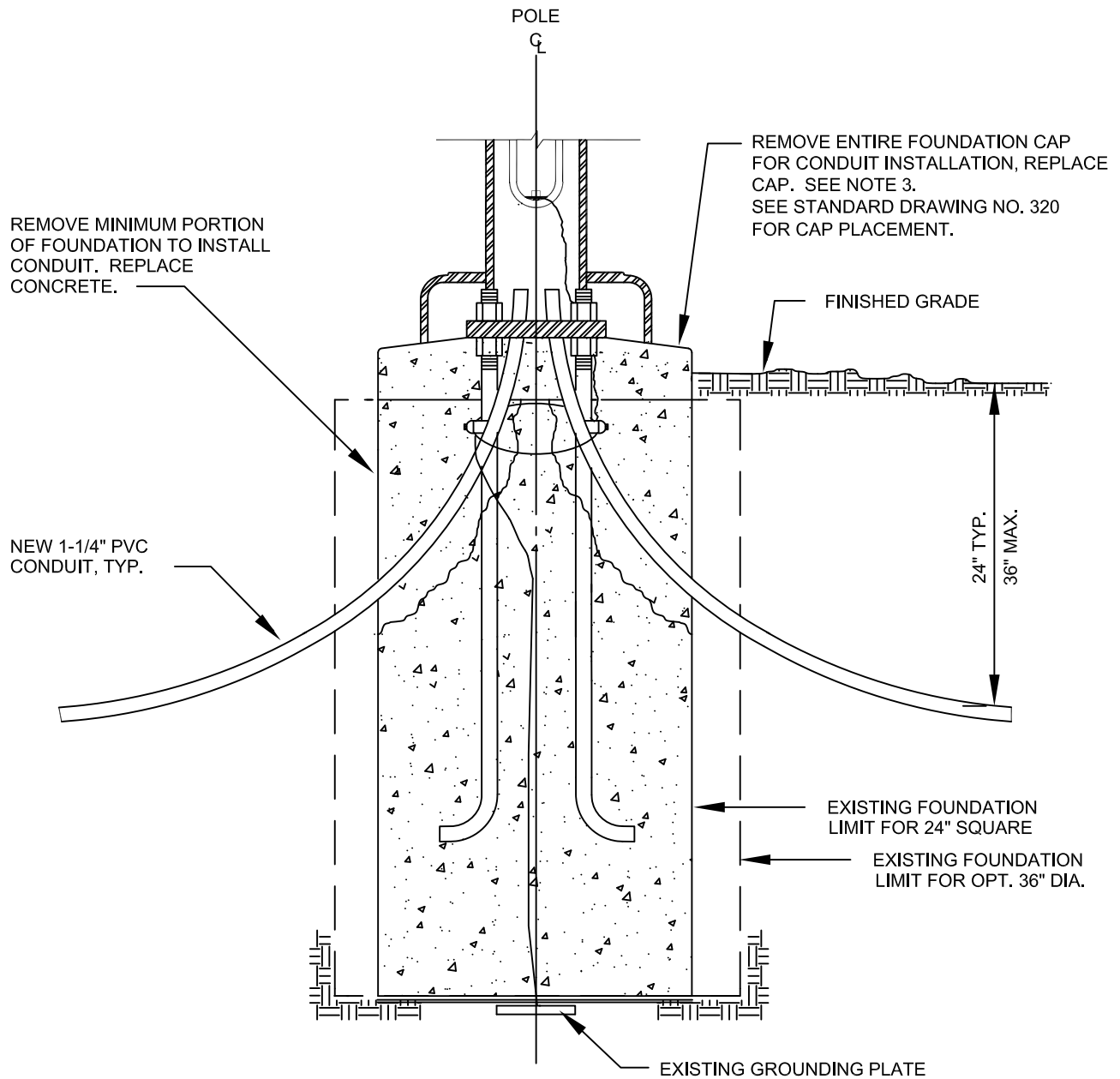
KEYED NOTE:

1 SINGLE POLE, SINGLE THROW ON-OFF, 10 AMP, 125 VAC SWITCH, SEALED, WITH 5 IN. WIRE LEADS

SWITCH BRACKET

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	BYPASS SWITCH BRACKET FOR POLE MOUNTED STREETLIGHTING SERVICE (CITY OF MESQUITE ONLY)		
		DATE 4-13-00	DWG. NO. 324	PAGE NO. 84

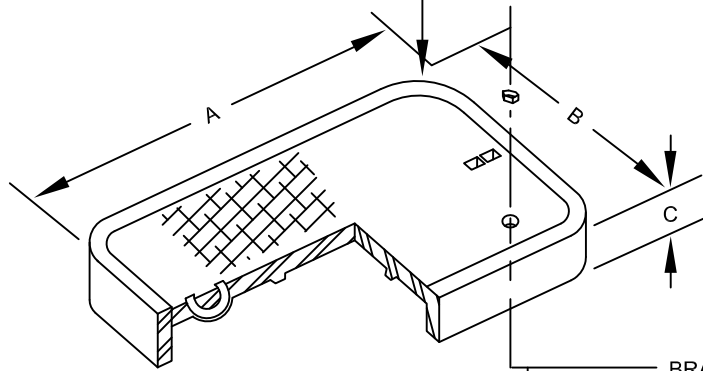


NOTES:

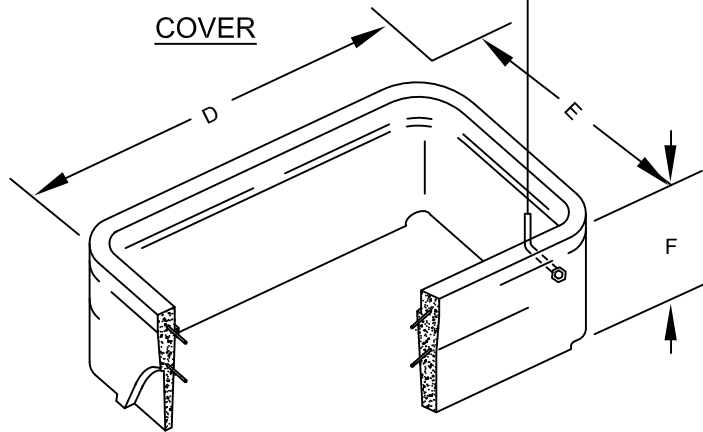
1. WHEN NO GROUNDING ELECTRODE EXISTS, 5/8 IN. DIA. SOLID COPPER GROUNDING ROD, 8 FT. IN LENGTH, SHALL BE INSTALLED.
2. ANCHOR BOLTS SHALL BE CONTINUOUS AND HAVE A MINIMUM 1 IN. FREE THREAD.
3. FOUNDATION CAP SHALL BE CONCRETE OR GROUT AS DESIGNATED BY ENTITY ENGINEER.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	PORTLAND CEMENT CONCRETE	MODIFIED EXISTING STREETLIGHT FOUNDATION FOR INSTALLATION OF UNDERGROUND CONDUIT (CITY OF LAS VEGAS AND CLARK COUNTY ONLY)		
623	TRAFFIC SIGNALS & STREETLIGHTING			
		DATE 7-8-04	DWG. NO. 325	PAGE NO. 85

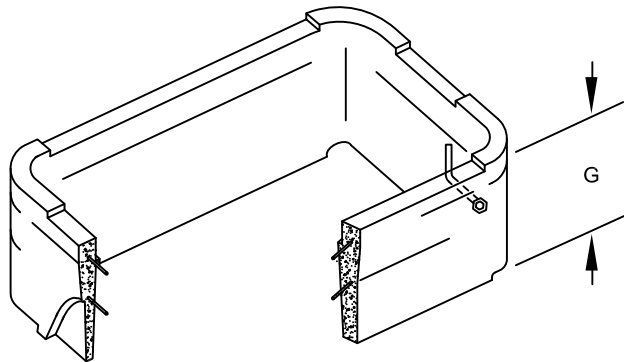
CAST IRON OR NON-CONDUCTIVE
COVER FOR PEDESTRIAN AREAS



COVER



BODY



EXTENSION

AS SPECIFIED BY THE ENGINEER

BRASS "L" BOLT AND NUT

PULL BOX			
	SIZE (COMMERCIAL DESIGNATION)		
	3-1/2	5	7
A	15	21-3/4	30-5/8
B	10	11-3/4	17-5/8
C	3/4	2	2
D	19-3/8	25	34-3/4
E	14-3/8	15	21-3/4
F	12	12	12
G	N/A	10-1/4	11-1/2

NOMINAL DIMENSION IN INCHES

NOTES:

- COVERS INSTALLED IN TRAFFIC AND OPEN AREAS ACCESSIBLE TO TRAFFIC SHALL BE PER STANDARD DRAWING NO. 327.
- SEE STANDARD DRAWING NO. 323 FOR COVER GROUNDING.

SPECIFICATION REFERENCE

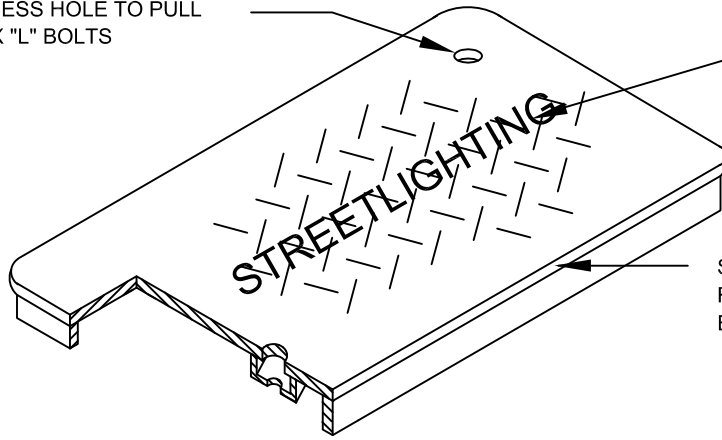
503	PRECAST PRESTRESSED CONCRETE MEMBERS
623	TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

PRECAST REINFORCED
CONCRETE PULL BOX

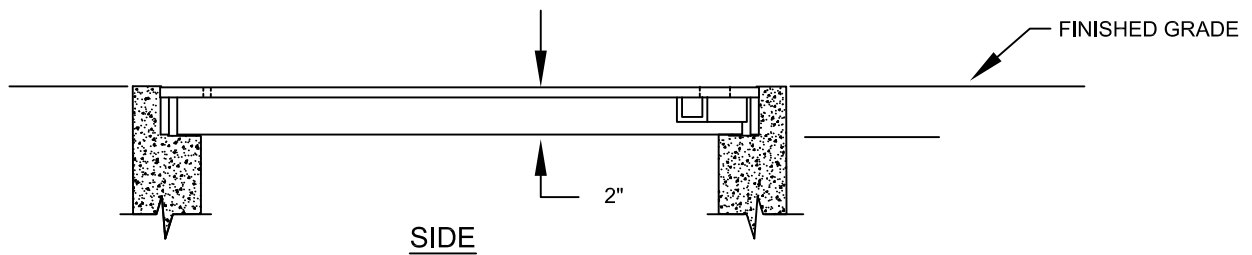
ACCESS HOLE TO PULL BOX "L" BOLTS

BEAD WELD INSCRIPTION



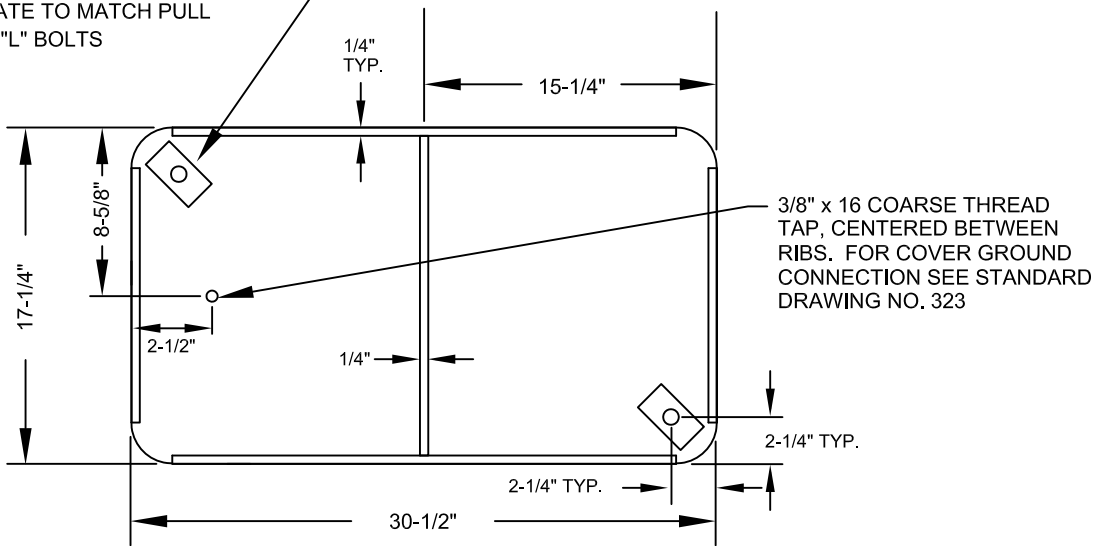
STEEL FLOOR PLATE, 3/8" THICK, ROUND CORNERS TO MATCH EDGES OF PULL BOX

COVER



SIDE

MOUNTING BRACKET, WELDED TO COVER, TYP. LOCATE TO MATCH PULL BOX "L" BOLTS



BOTTOM

NOTES:

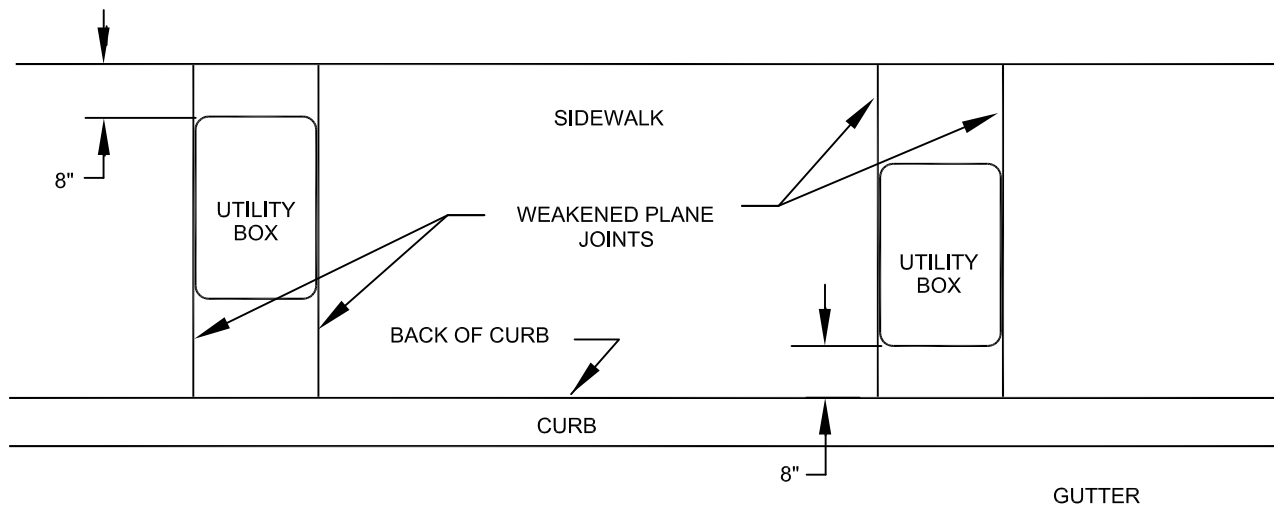
1. COVER USED IN TRAFFIC AND OPEN AREAS ACCESSIBLE TO TRAFFIC ONLY.
2. TYPICAL NO. 7 PULL BOX COVER SHOWN. SUBMIT OTHERS TO THE ENGINEER FOR APPROVAL.
3. ALL TRAFFIC AND OPEN AREA COVERS SHALL BE H 20 RATED.

SPECIFICATION REFERENCE

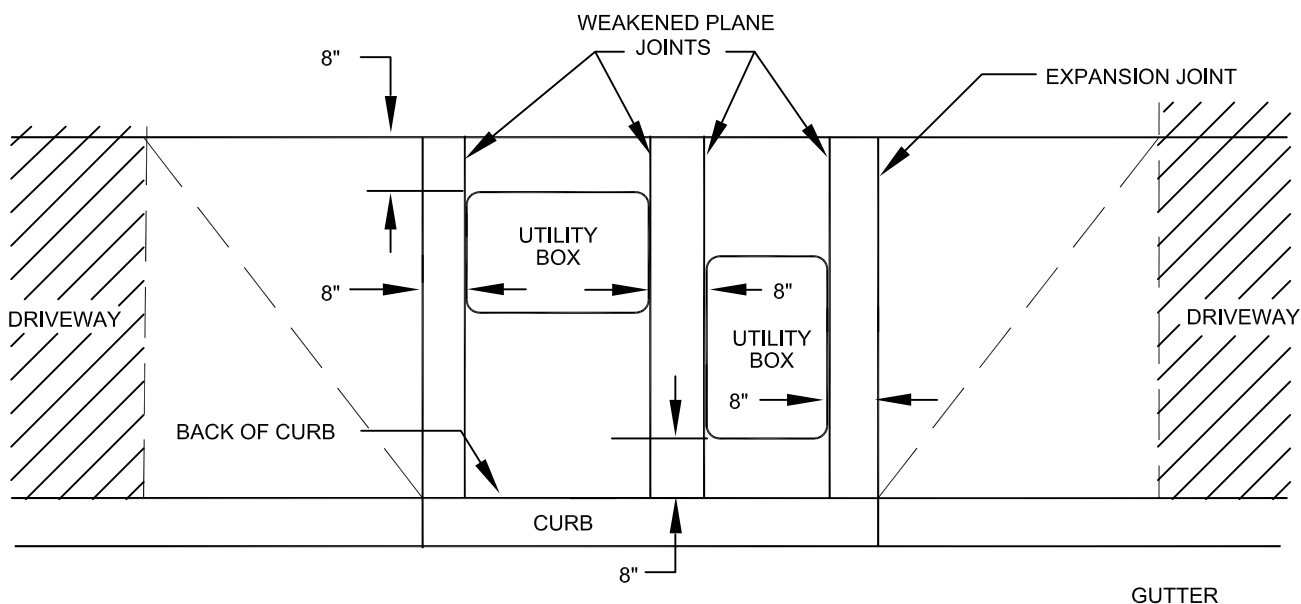
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

506	STEEL STRUCTURES
623	TRAFFIC SIGNALS & STREETLIGHTING

PULL BOX STREET COVER



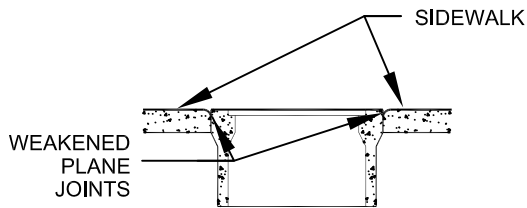
UTILITY BOXES IN SIDEWALK



UTILITY BOXES BETWEEN DRIVEWAYS

NOTE:

1. PROVIDE A MINIMUM OF 8" AROUND ALL BOXES. ANY BOX SHALL NOT BE PLACED WITHIN 3'-3" OF FIRE HYDRANTS IN DRIVEWAYS OR DRIVEWAY APRONS. THIS DRAWINGS IS NOT INTENDED TO LIMIT THE NUMBER OF BOXES BETWEEN DRIVEWAYS TO TWO.
2. FOR WATER SERVICE BOXES, REFER TO UDACS PLATE 1A-1D.



TYPICAL SECTION

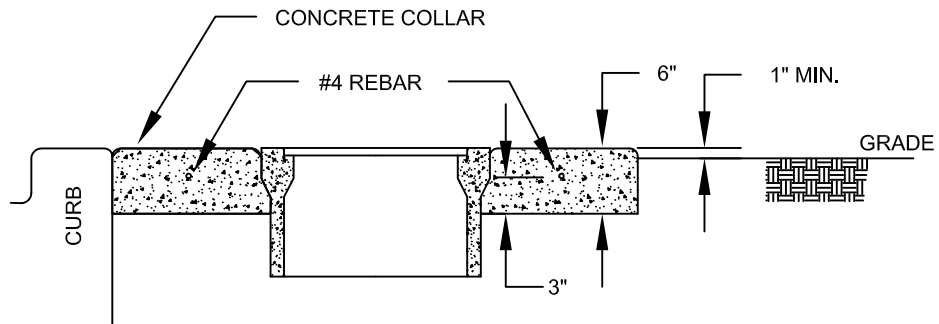
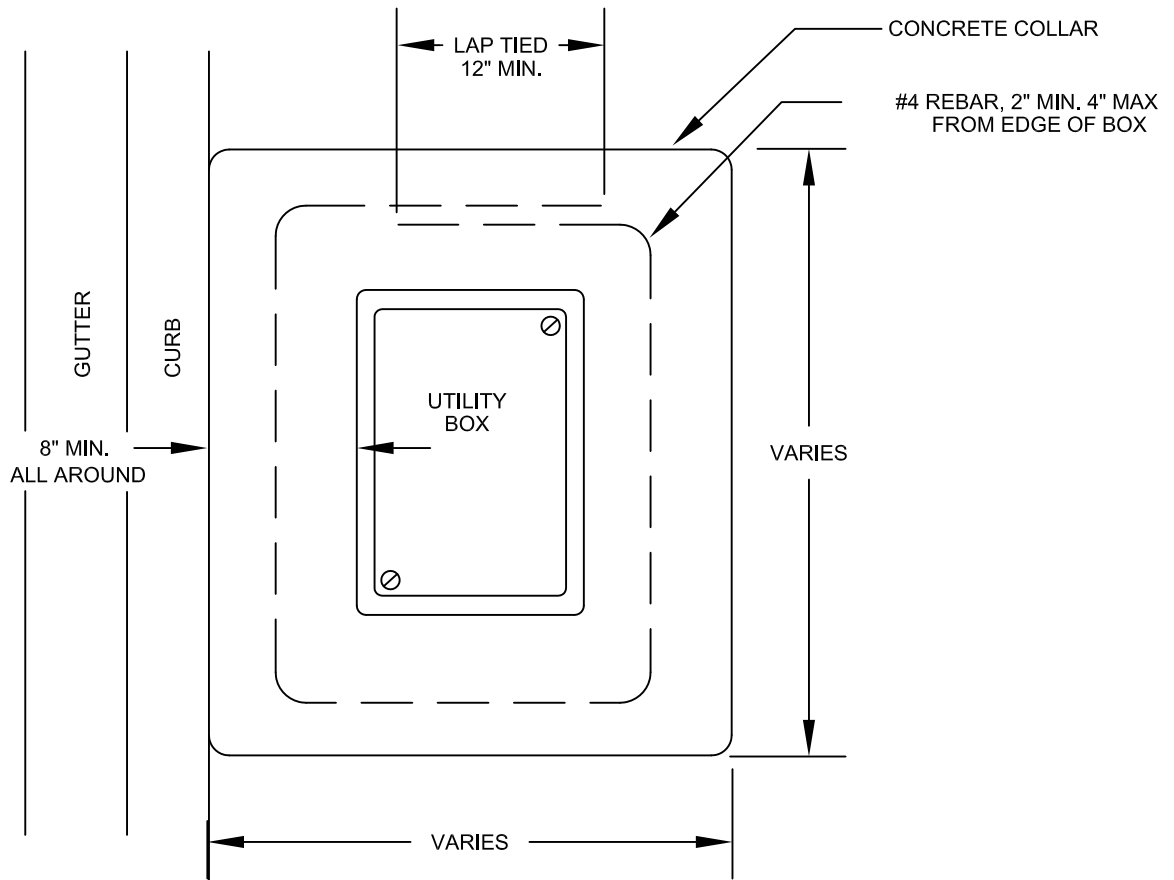
SPECIFICATION REFERENCE

505	REINFORCING STEEL
623	TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

UTILITY PULL BOX LOCATIONS
SIDEWALK AND BETWEEN DRIVEWAYS

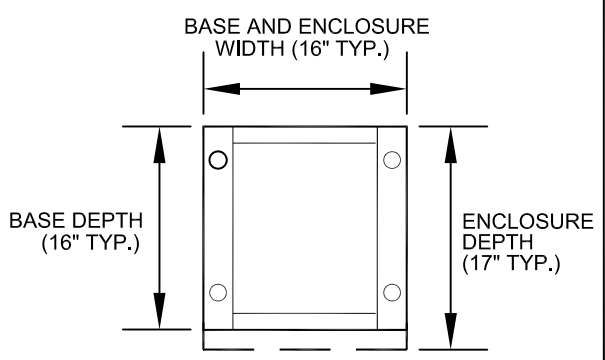
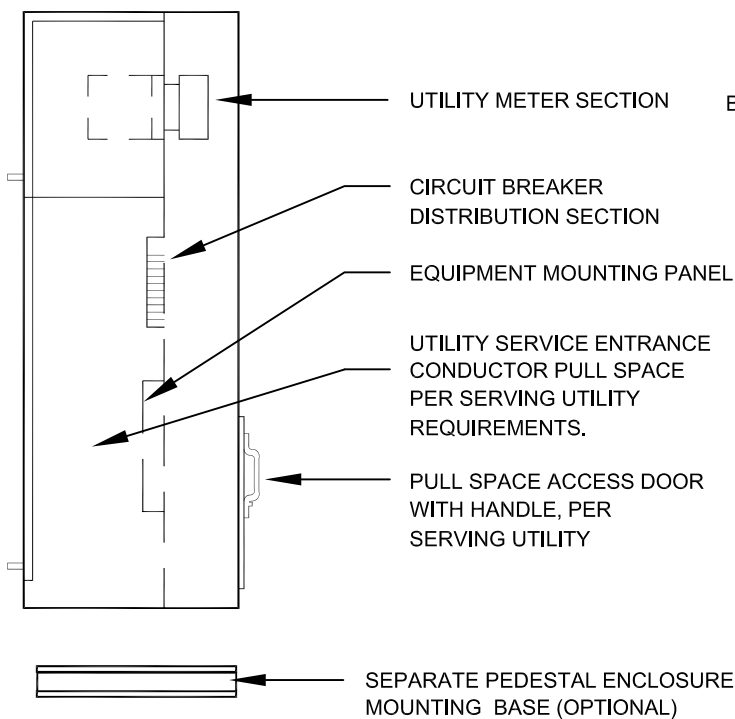
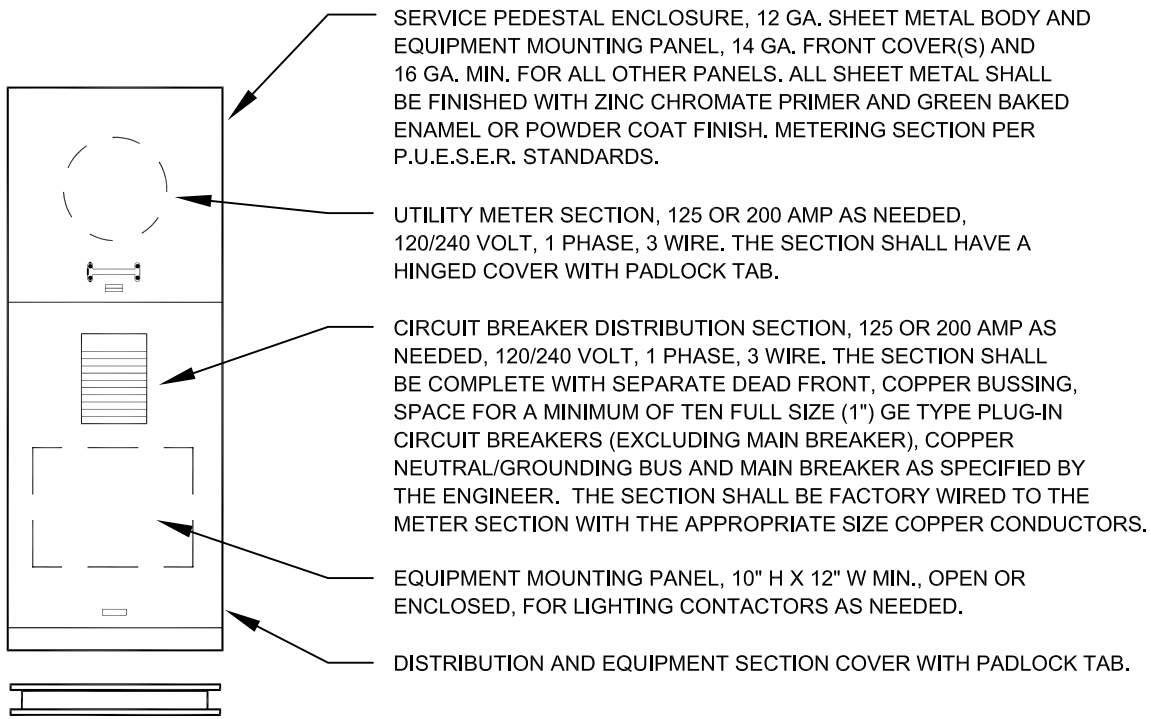
DATE 04-12-07 DWG. NO. 328



TYPICAL SECTION

Effective 07/01/10 - 12/30/10

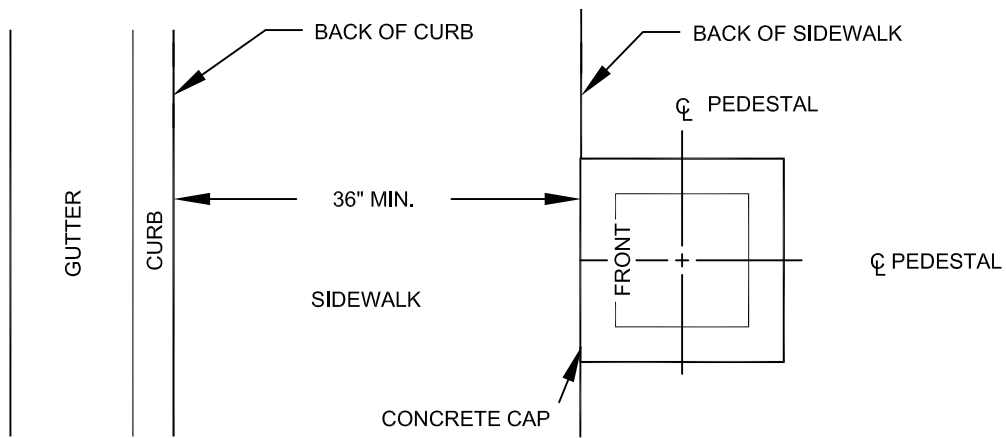
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
501	PORTLAND CEMENT CONCRETE	CONCRETE AROUND PULL BOXES IN UNDEVELOPED AREAS	
505	REINFORCING STEEL		
623	TRAFFIC SIGNALS & STREETLIGHTING		
		DATE 12-12-96	DWG. NO. 329
		PAGE NO. 89	



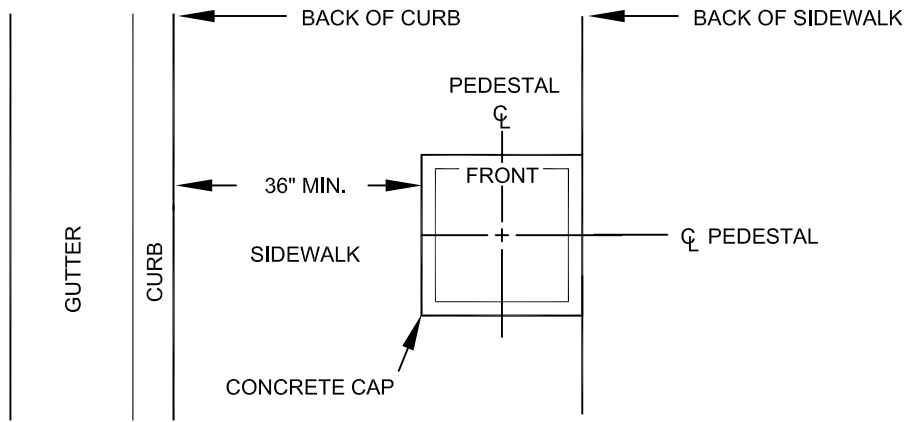
TYPICAL MOUNTING BASE DETAIL
(DIMENSIONS MAY VARY DEPENDING ON MANUFACTURER)

Effective 07/01/10 - 12/30/10

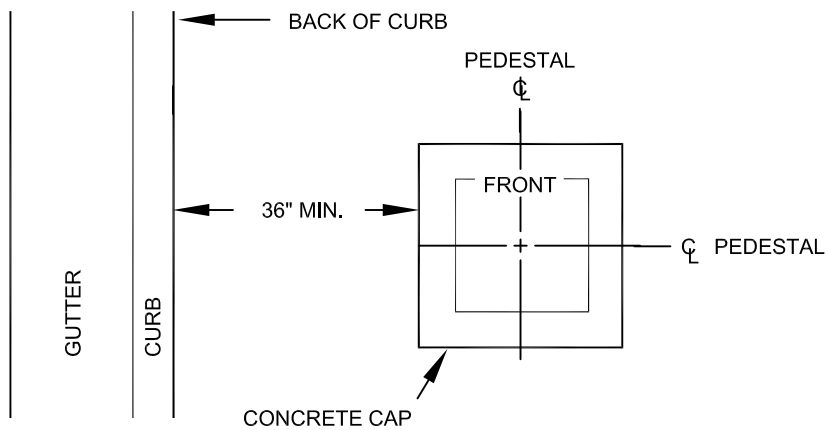
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
506	STEEL STRUCTURES	SINGLE METER SERVICE PEDESTAL		
623	TRAFFIC SIGNALS & STREETLIGHTING			
		DATE 8-12-99	DWG. NO. 330	PAGE NO. 90



BEHIND SIDEWALK (FOR WIDTHS LESS THAN 5 FT.)



BACK PORTION OF SIDEWALK (FOR WIDTHS OF 5 FT. OR GREATER)



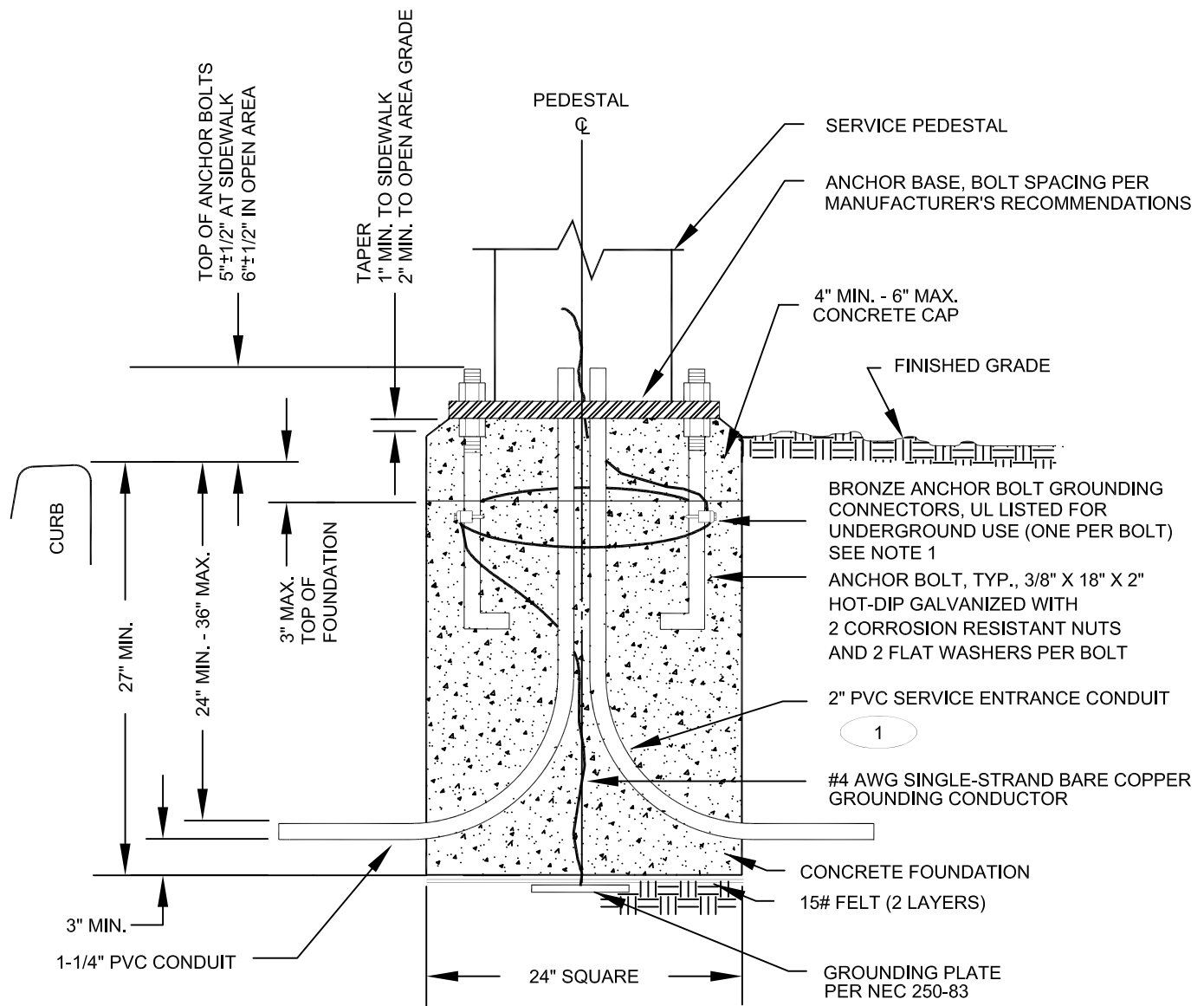
OPEN AREA

SPECIFICATION REFERENCE

501	PORTLAND CEMENT CONCRETE
623	TRAFFIC SIGNALS & STREETLIGHTING

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SERVICE PEDESTAL
SETBACK



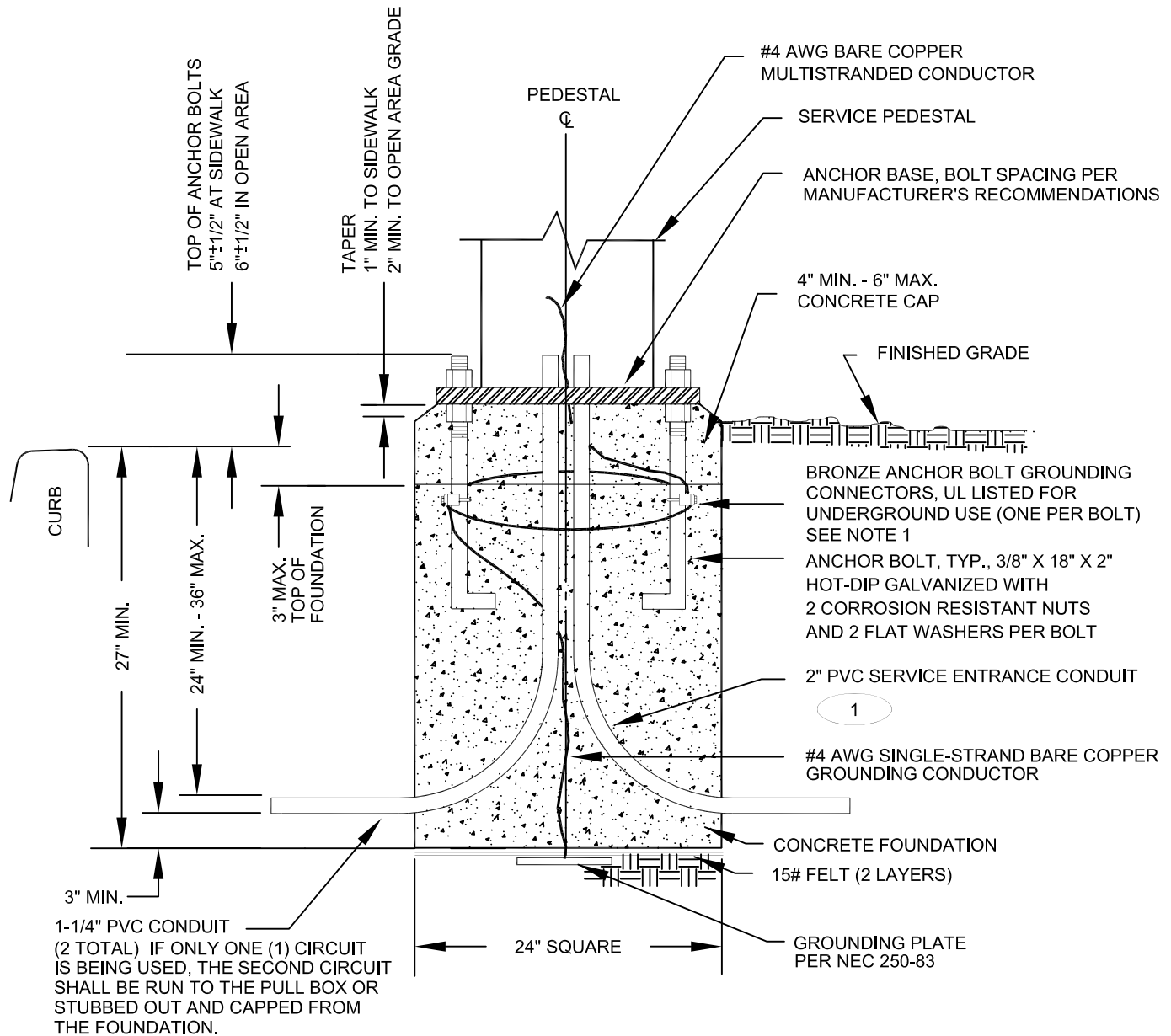
1 125 AMP SERVICE: 2" CONDUIT, 2 #1 THW AND 1 #6 WHITE THW
 200 AMP SERVICE: 2" CONDUIT, 2 #3/0 THW AND 1 #2 WHITE THW

NOTES:

1. BARE COPPER GROUNDING CONDUCTOR SHALL BE LOOPED AROUND ANCHOR BOLTS ONE TIME AND CONNECTED TO EACH ANCHOR BOLT BEFORE CONTINUING DOWN TO THE GROUNDING PLATE.
2. CABINET COVERS SHALL BE PARALLEL WITH CURB.
3. IN AREAS WHERE R/W PERMITS, THE CONCRETE BASE SHALL BE PLACED AT THE BACK EDGE OF THE SIDEWALK.
4. CABINET COVERS SHALL OPEN TOWARDS THE STREET WHEN CABINETS ARE LOCATED AT BACK OF WALK. CABINET COVERS SHALL OPEN PARALLEL TO THE SIDEWALK FACING THE DIRECTION OF TRAFFIC WHEN LOCATED WITHIN THE SIDEWALK.
5. WIRE SIZES ARE BASED ON UNDERGROUND FEED.
6. WIRE SIZES SHALL BE INCREASED FOR VOLTAGE DROP LIMITATION WHEN RUN IS LONG.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	PORTLAND CEMENT CONCRETE	SERVICE PEDESTAL FOUNDATION (EXCEPT CLARK COUNTY AND CITY OF LAS VEGAS)		
623	TRAFFIC SIGNALS & STREETLIGHTING			
		DATE 2-10-00	DWG. NO. 332	PAGE NO. 92



3" MIN.
 1-1/4" PVC CONDUIT
 (2 TOTAL) IF ONLY ONE (1) CIRCUIT IS BEING USED, THE SECOND CIRCUIT SHALL BE RUN TO THE PULL BOX OR STUBBED OUT AND CAPPED FROM THE FOUNDATION.

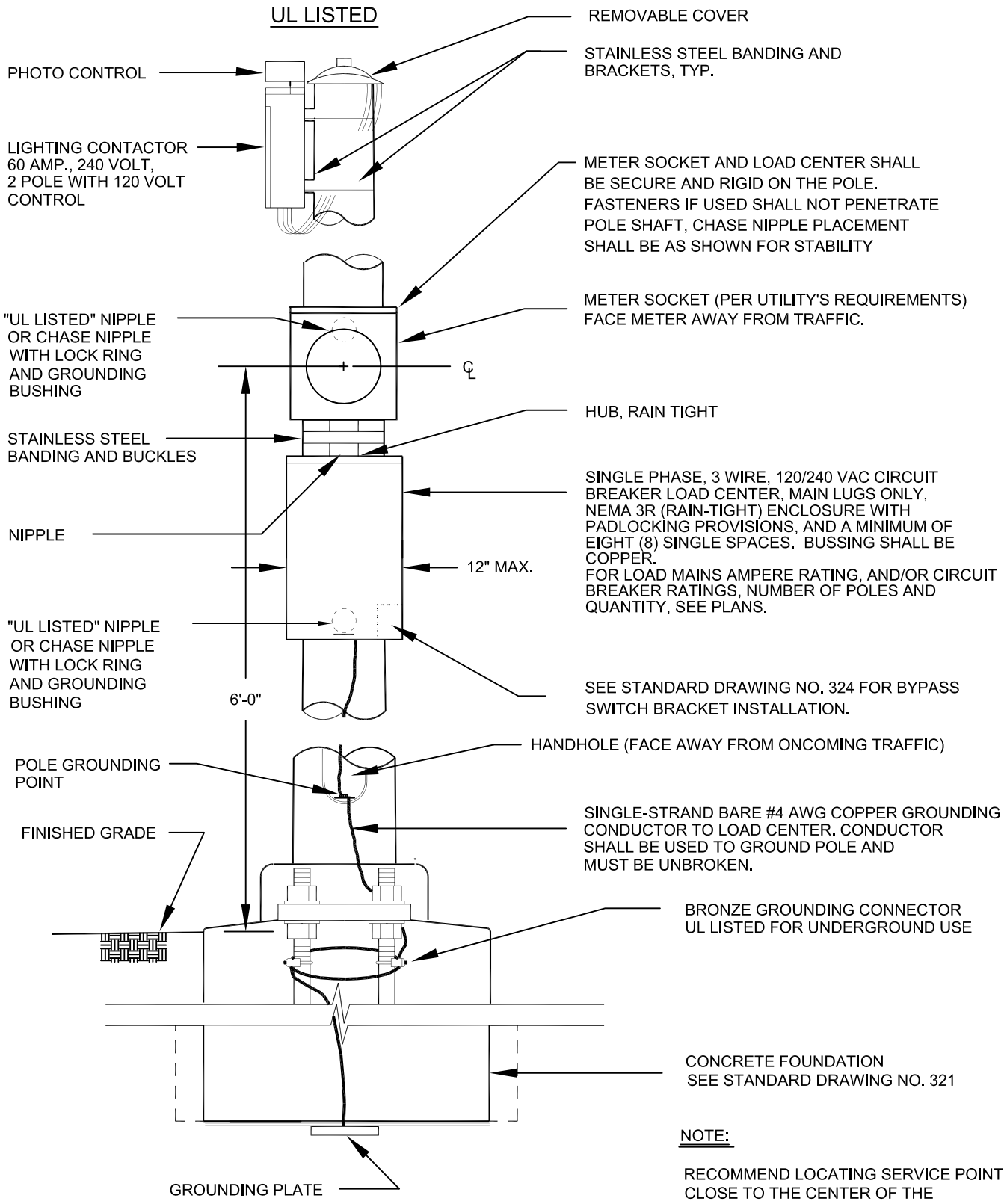
- 1
- 125 AMP SERVICE: 2" CONDUIT, 2 #1/0 THW AND 1 #1/0 WHITE THW
 - 200 AMP SERVICE (CLARK COUNTY): 2" CONDUIT, 2 #4/0 THW AND 1 #4/0 WHITE THW
 - 200 AMP SERVICE (LAS VEGAS): 2" CONDUIT, 2 #3/0 THW AND 1 #3/0 WHITE THW

NOTES:

1. BARE COPPER GROUNDING CONDUCTOR SHALL BE LOOPED AROUND ANCHOR BOLTS ONE TIME AND CONNECTED TO EACH ANCHOR BOLT BEFORE CONTINUING DOWN TO THE GROUNDING PLATE.
2. CABINET COVERS SHALL BE PARALLEL WITH CURB.
3. IN AREAS WHERE R/W PERMITS, THE CONCRETE BASE SHALL BE PLACED AT THE BACK EDGE OF THE SIDEWALK.
4. CABINET COVERS SHALL OPEN TOWARDS THE STREET WHEN CABINETS ARE LOCATED AT BACK OF WALK. CABINET COVERS SHALL OPEN PARALLEL TO THE SIDEWALK FACING THE DIRECTION OF TRAFFIC WHEN LOCATED WITHIN THE SIDEWALK.
5. WIRE SIZES ARE BASED ON UNDERGROUND FEED.
6. WIRE SIZES SHALL BE INCREASED FOR VOLTAGE DROP LIMITATION WHEN RUN IS LONG.

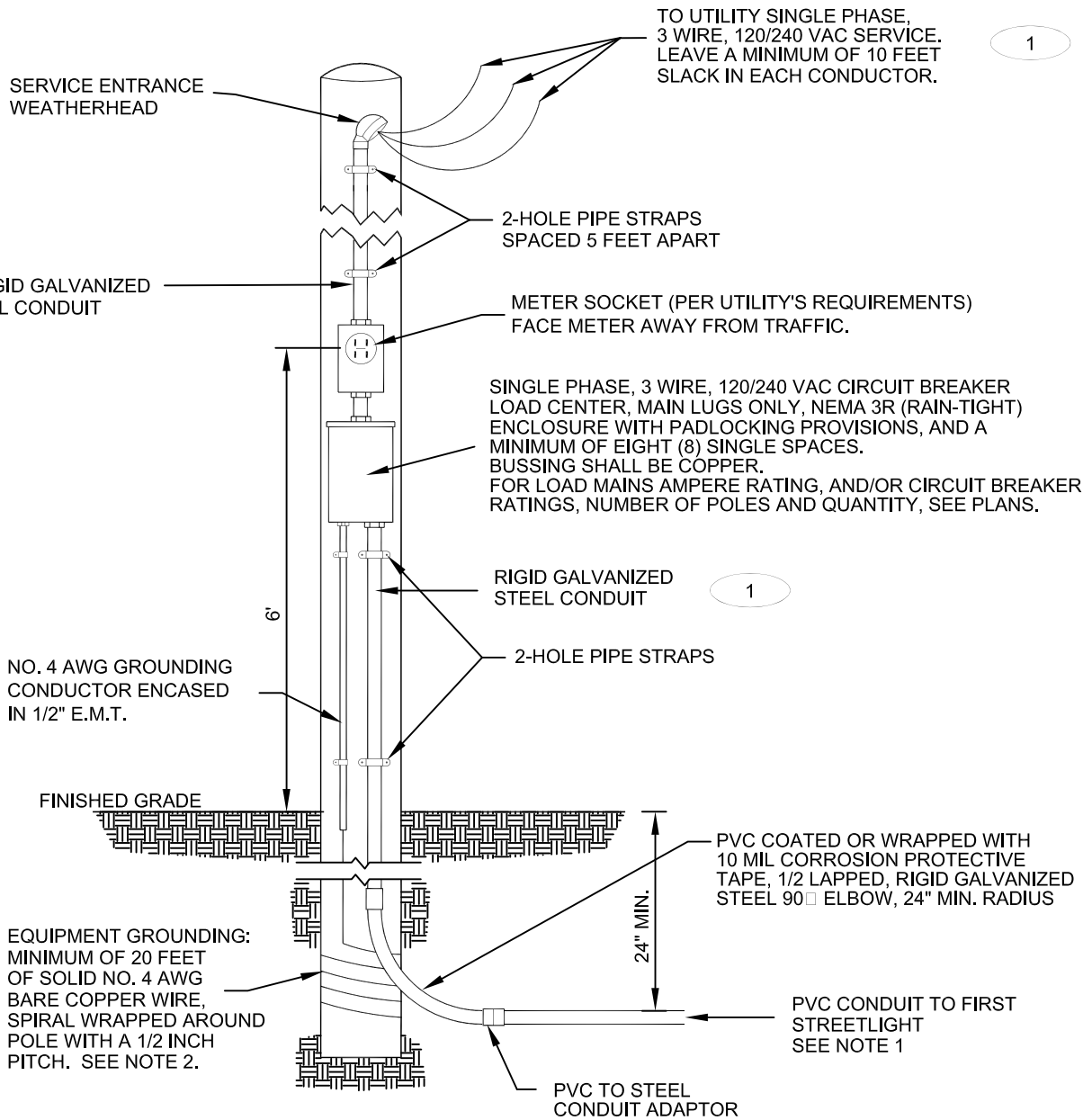
Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	PORTLAND CEMENT CONCRETE	CLARK COUNTY AND CITY OF LAS VEGAS		
623	TRAFFIC SIGNALS & STREETLIGHTING			
		SERVICE PEDESTAL FOUNDATION		
		DATE 2-10-00	DWG. NO. 332.1	PAGE NO. 92.1



Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHTING SERVICE POINT LOCATED ON STREETLIGHT STANDARD (CITY OF MESQUITE ONLY)		
		DATE 4-13-00	DWG. NO. 333	PAGE NO. 93



1 125 AMP SERVICE: 2" CONDUIT, 2 #1/0 THW AND 1 #4 WHITE THW
 200 AMP SERVICE: 2" CONDUIT, 2 250 KCMIL THW AND 1 #1/0 WHITE THW
 (0.82 DERATE HAS BEEN APPLIED FOR AMBIENT TEMPERATURE)

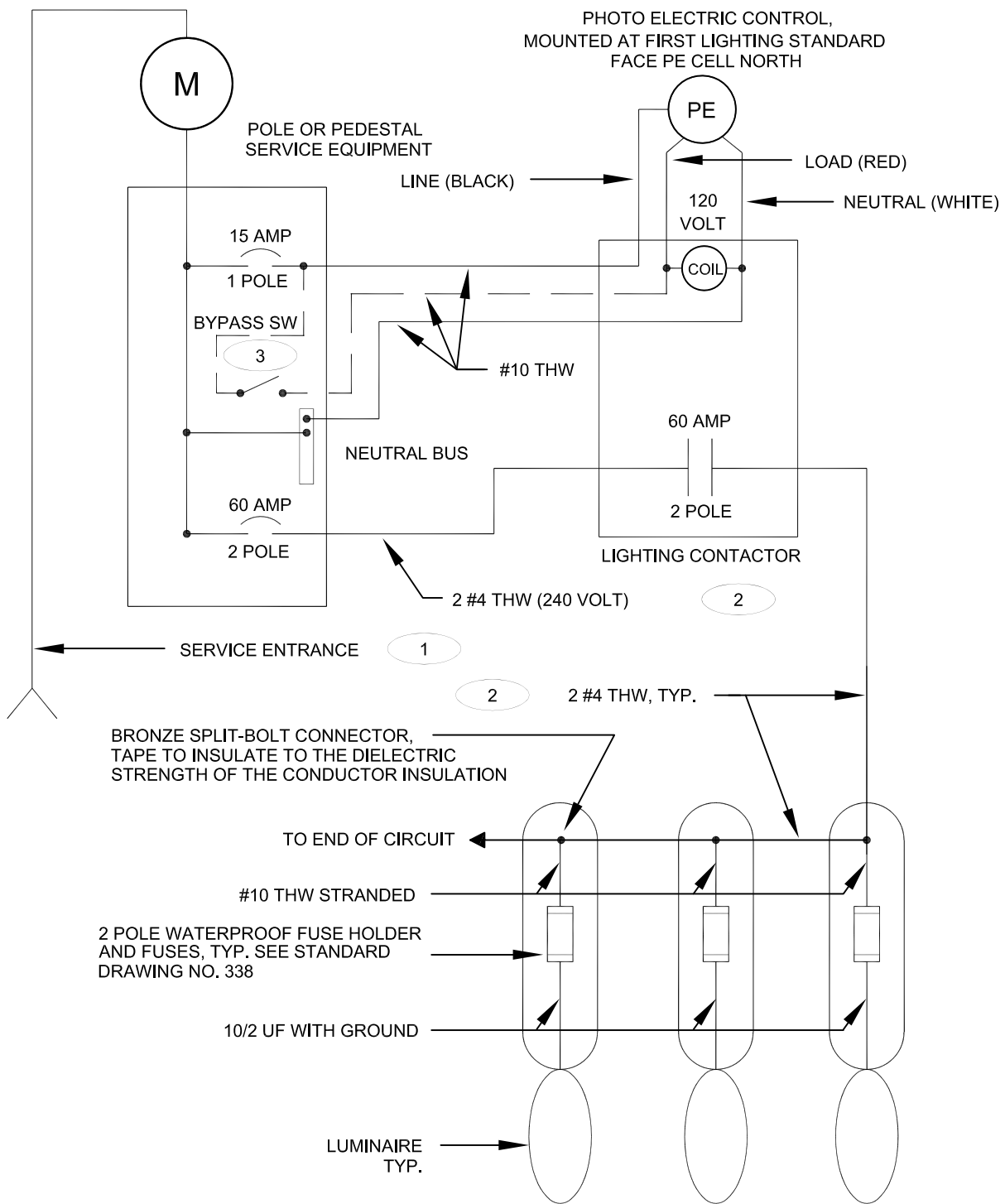
NOTES:

1. ALL WIRES TO BE COPPER; SEE PLANS FOR QUANTITY AND GAGES.
2. WITH ENGINEER'S APPROVAL, AN 8 FT. BY 5/8 IN. COPPER-CLAD GROUNDING ROD MAY BE USED.
3. ALL CONDUIT FITTINGS TO BE WATER-TIGHT.

SPECIFICATION REFERENCE

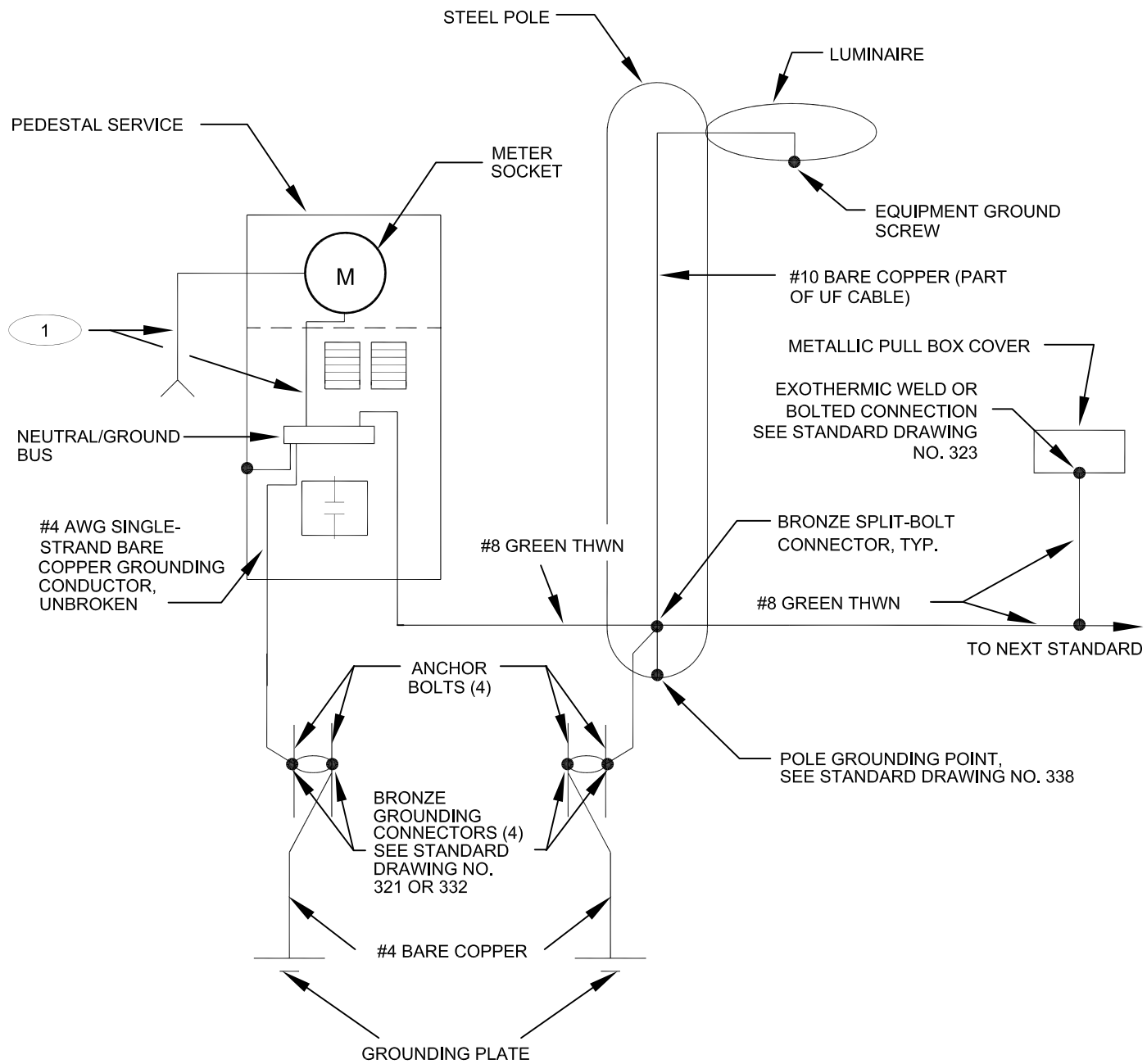
UNIFORM STANDARD DRAWINGS
 CLARK COUNTY AREA

TEMPORARY APPLICATION
 120/240 VAC SERVICE
 ON WOOD POLE
 OVERHEAD SERVICE



- 1 125 AMP SERVICE: 2" CONDUIT, 2 #1 THW AND 1 #6 WHITE THW
200 AMP SERVICE: 2" CONDUIT, 2 #3/0 THW AND 1 #2 WHITE THW
FOR POLE SERVICE, WIRE SIZES SHALL BE TEMPERATURE DERATED.
- 2 GROUNDING AND BONDING CONDUCTORS OMITTED FOR CLARITY,
SEE STANDARD DRAWING NOS. 336 AND 337.
- 3 SINGLE POLE, SINGLE THROW, ON-OFF, 10 AMP, 125 VAC SWITCH, SEALED, WITH 5 IN. LEADS

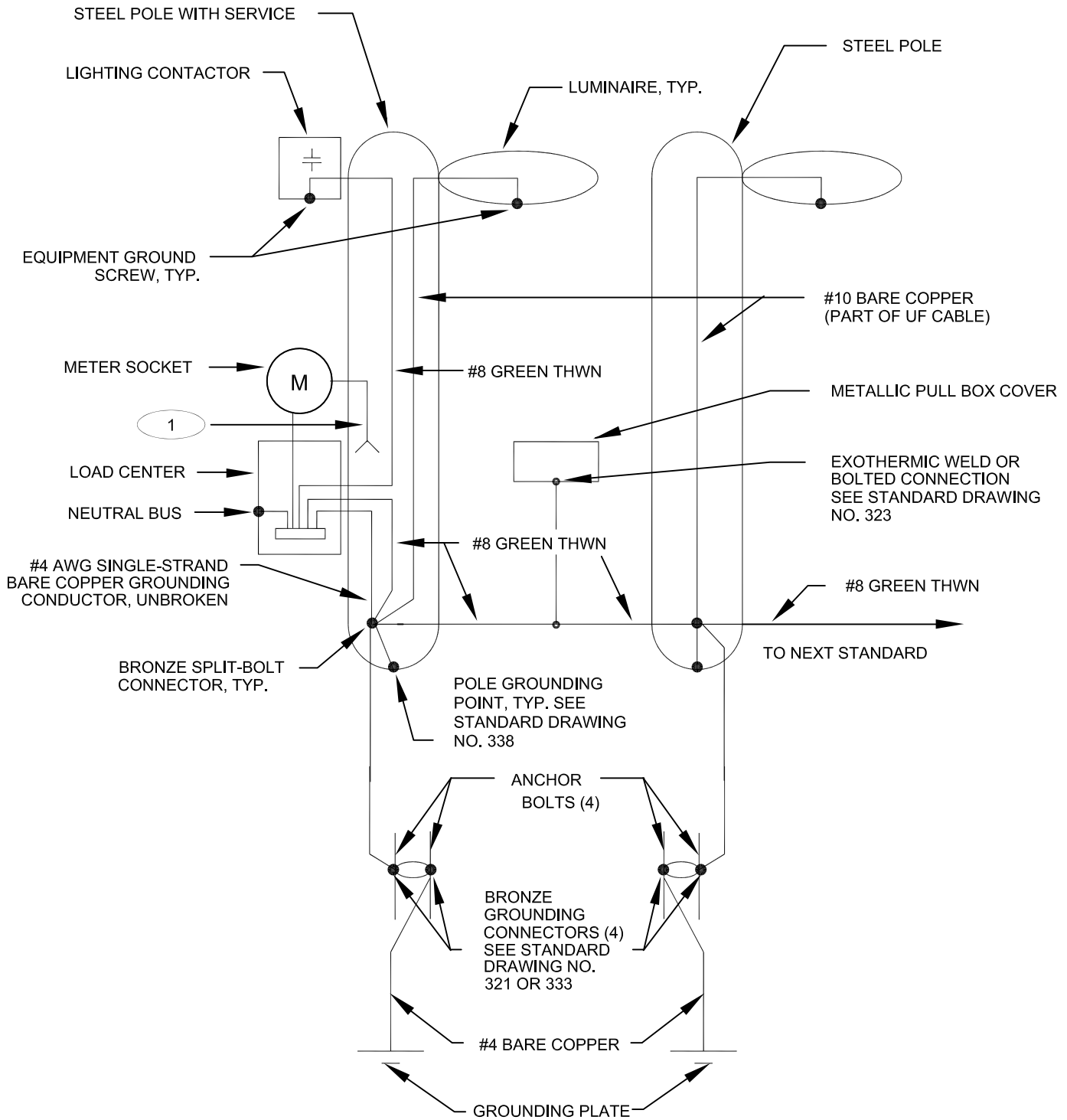
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	STREETLIGHT CIRCUIT ONE LINE DIAGRAM (EXCEPT CLARK COUNTY AND CITY OF LAS VEGAS)		
		DATE 2-10-00	DWG. NO. 335	PAGE NO. 95



1 FOR CONDUIT SIZE AND WIRING REQUIREMENTS FOR STREET LIGHT SERVICE, SEE STANDARD DRAWING NO. 332.1 FOR LAS VEGAS AND CLARK COUNTY ONLY AND 332 FOR ALL OTHER ENTITIES.

Effective 07/01/10 - 12/30/10

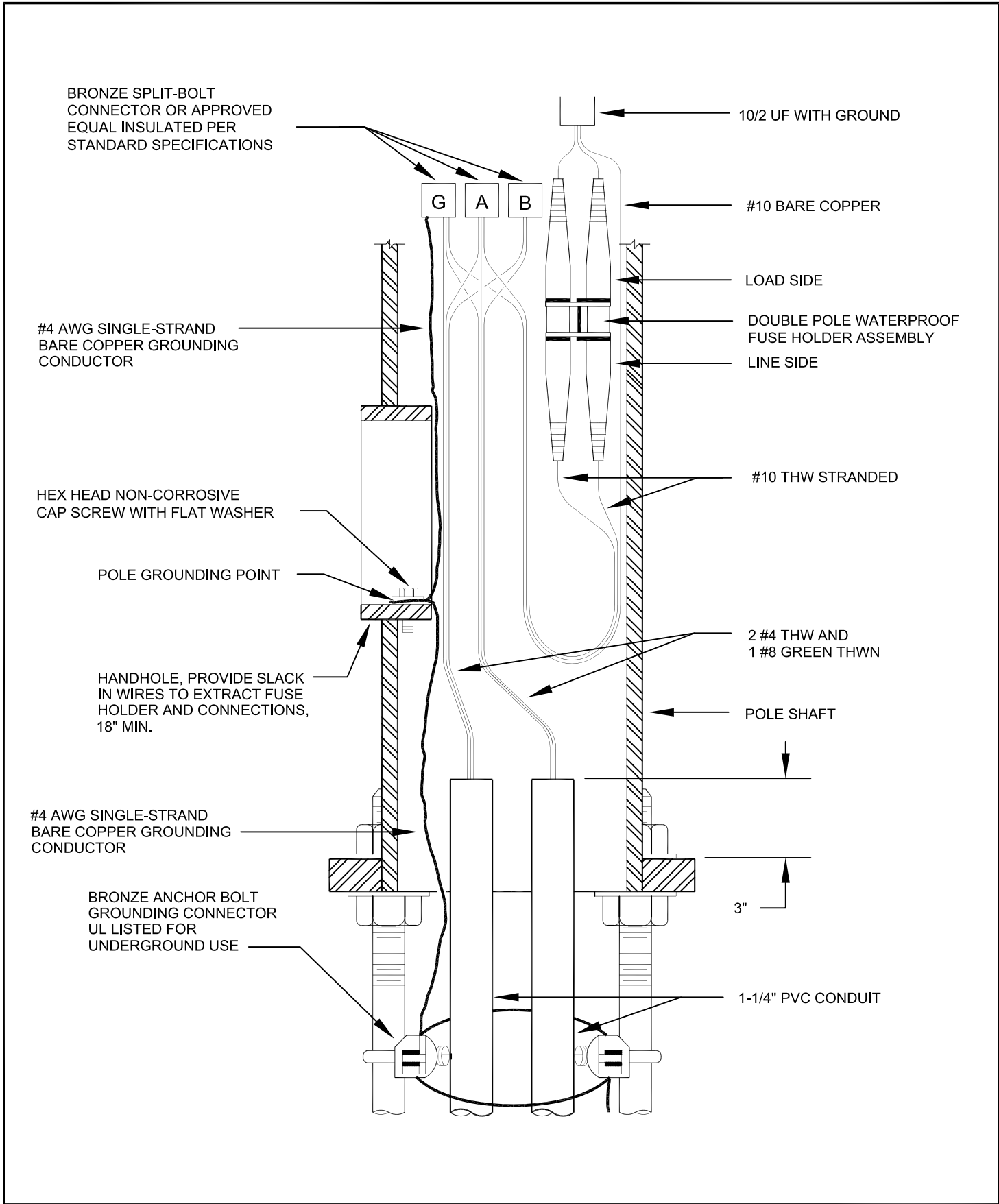
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	SYSTEM GROUNDING PLAN WITH PEDESTAL SERVICE		
		DATE 2-10-00	DWG. NO. 336	PAGE NO. 96



1 125 AMP SERVICE: 1 #4 WHITE THW
 200 AMP SERVICE: 1 #1/0 WHITE THW

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	SYSTEM GROUNDING PLAN WITH POLE MOUNTED SERVICE		
		DATE 8-12-99	DWG. NO. 337	PAGE NO. 97



SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
623	TRAFFIC SIGNALS & STREETLIGHTING	LIGHTING STANDARD WIRING DIAGRAM, 240 VOLT, 2 WIRE		
		DATE 12-12-96	DWG. NO. 338	PAGE NO. 98

Effective 07/01/10 - 12/30/10

BRONZE SPLIT-BOLT CONNECTOR OR APPROVED EQUAL INSULATED PER STANDARD SPECIFICATIONS

10/2 UF WITH GROUND, TYP.

#10 BARE COPPER, TYP.

G A B

#4 AWG SINGLE-STRAND BARE COPPER GROUNDING CONDUCTOR

LOAD SIDE, TYP.

DOUBLE POLE WATERPROOF FUSE HOLDER ASSEMBLY, TYP.

LINE SIDE, TYP.

HEX HEAD NON-CORROSIVE CAP SCREW WITH FLAT WASHER

#10 THW STRANDED, TYP.

POLE GROUNDING POINT

HANDHOLE, PROVIDE SLACK IN WIRES TO EXTRACT FUSE HOLDERS AND CONNECTIONS, 18" MIN.

2 #4 THW AND 1 #8 GREEN THWN

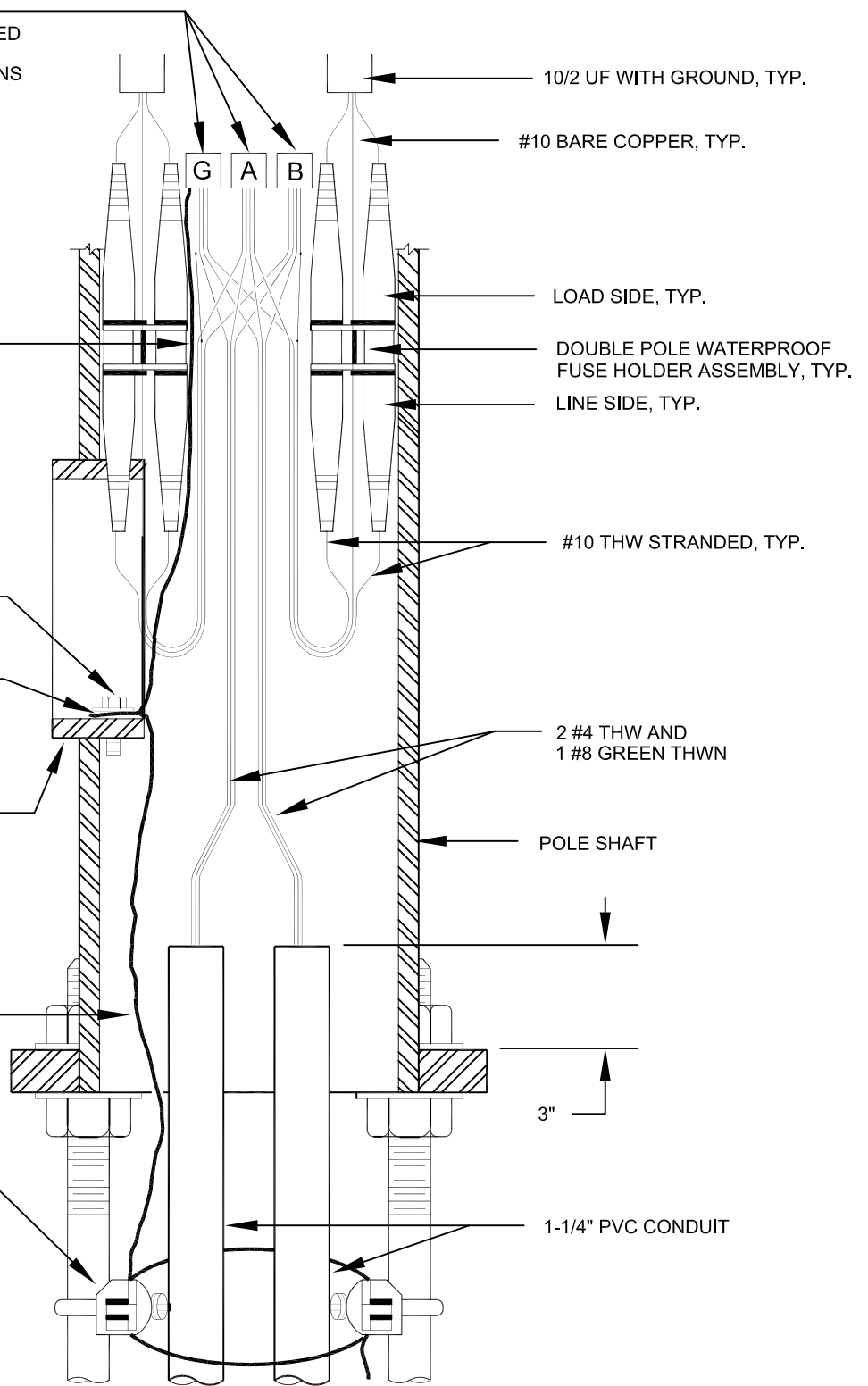
POLE SHAFT

#4 AWG SINGLE-STRAND BARE COPPER GROUNDING CONDUCTOR

3"

BRONZE ANCHOR BOLT GROUNDING CONNECTOR UL LISTED FOR UNDERGROUND USE

1-1/4" PVC CONDUIT



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

623 TRAFFIC SIGNALS & STREETLIGHTING

TWIN LIGHTING STANDARD WIRING
DIAGRAM, 240 VOLT, 2 WIRE

DATE 12-12-96

DWG. NO.

339

PAGE NO.

99

BRONZE SPLIT-BOLT CONNECTOR OR APPROVED EQUAL INSULATED PER STANDARD SPECIFICATIONS. ALTERNATE PHASE CONNECTIONS AT EACH POLE

10/2 UF WITH GROUND

G A N B

NEUTRAL

#10 BARE COPPER

LOAD SIDE

SINGLE POLE WATERPROOF FUSE HOLDER ASSEMBLY

LINE SIDE

#4 AWG SINGLE-STRAND BARE COPPER GROUNDING CONDUCTOR

#10 THW STRANDED

HEX HEAD NON-CORROSIVE CAP SCREW WITH FLAT WASHER

2 #4 WHITE THW

POLE GROUNDING POINT

3 #4 THW AND 1 #8 GREEN THWN

HANDHOLE, PROVIDE SLACK IN WIRES TO EXTRACT FUSE HOLDER AND CONNECTIONS, 18" MIN.

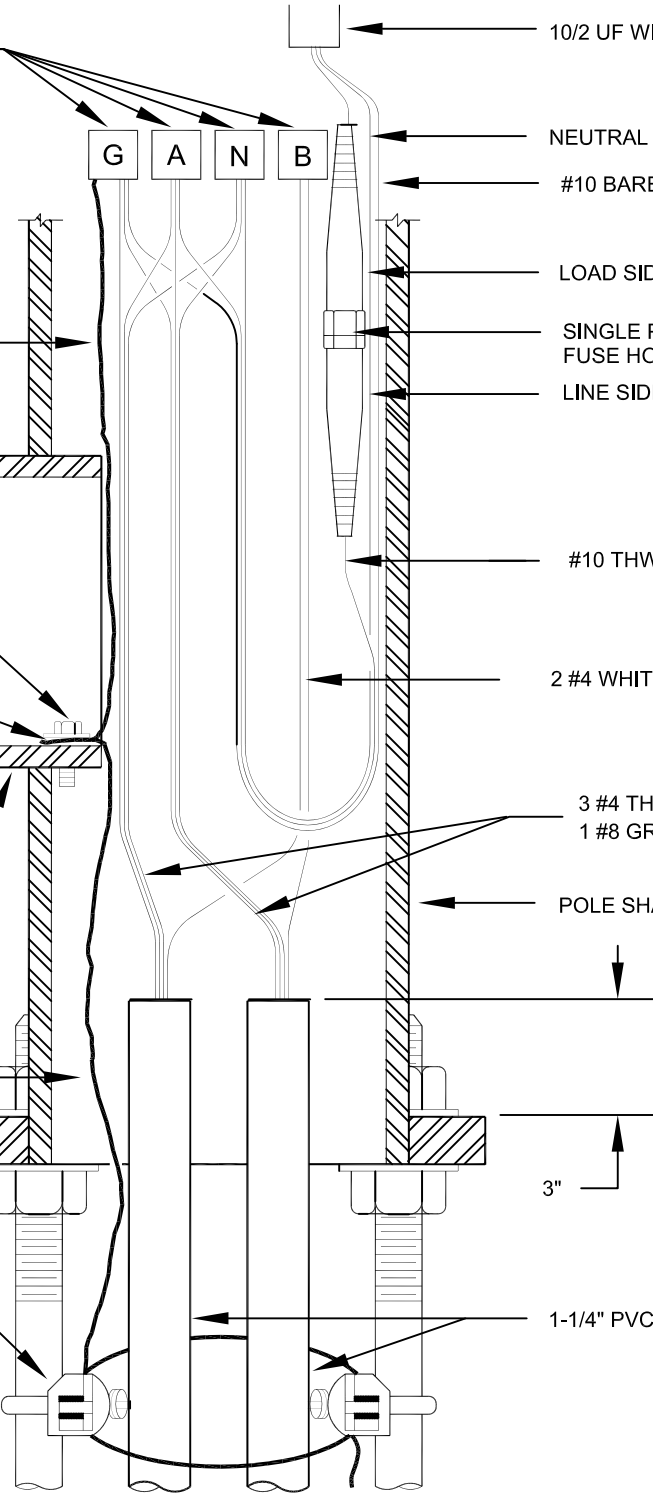
POLE SHAFT

#4 AWG SINGLE-STRAND BARE COPPER GROUNDING CONDUCTOR

3"

BRONZE ANCHOR BOLT GROUNDING CONNECTOR UL LISTED FOR UNDERGROUND USE

1-1/4" PVC CONDUIT



SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

623 TRAFFIC SIGNALS & STREETLIGHTING

LIGHTING STANDARD WIRING DIAGRAM
120 VOLT, 1 PHASE, 3 WIRE

DATE 12-12-96

DWG. NO. 340

PAGE NO. 100

MANHOLE NOTES:

1. MANHOLE MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF SECTION 609, "CATCH BASINS, MANHOLES AND INLETS" OF THE "STANDARD SPECIFICATIONS".
2. REINFORCING STEEL SHALL BE AS SHOWN, WIRED TIGHTLY AT ALL INTERSECTIONS AND EMBEDDED AT LEAST ONE (1) INCH CLEAR UNLESS OTHERWISE NOTED.
3. EXCAVATION SHALL BE AS NEARLY VERTICAL AS POSSIBLE (SHEET AND SHORE, IF SOIL CONDITIONS REQUIRE), IN EXISTING STREET SECTIONS, ALLEY SECTIONS AND CONFINED AREAS SUCH AS LIMITED EASEMENTS OR ADJACENT TO STRUCTURES. NATURAL ANGLE OF REPOSE WILL ALLOW IN ALL OTHER AREAS.
4. MANHOLE DESIGN FOR PIPE LARGER THAN SIXTY (60) INCHES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
5. MANHOLE DESIGN FOR DEPTHS EXCEEDING EIGHTEEN (18) FEET SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
6. TYPE AND SIZE OF MANHOLE TO BE CONSTRUCTED IN A PARTICULAR LOCATION SHALL BE DETERMINED BY THE PIPE SIZE, ALIGNMENT AND GRADE AS FOLLOWS:

TYPE I

FORTY-EIGHT (48) INCH SIZE

- A. ALL CASES FOR PIPE EIGHTEEN (18) INCHES AND SMALLER.
- B. TWENTY-FOUR (24) INCHES AND SMALLER PIPE ON TANGENT LINE AND GRADE.

SIXTY (60) INCH SIZE

- A. TWENTY-SEVEN (27) INCH THROUGH THIRTY-SIX (36) INCH PIPE ON TANGENT LINE AND GRADE.
- B. TWENTY-ONE (21) INCH THROUGH TWENTY-SEVEN (27) INCH PIPE AT ANGLE POINTS AND CHANGES IN GRADE OR PIPE SIZE.

TYPE I-A

USED IN PLACE OF TYPE I WHEN COVER ABOVE CONDUIT IS LIMITED, AND WHEN APPROVED BY THE ENGINEER.

TYPE II

FORTY-EIGHT (48) INCH SIZE

- A. THIRTY (30) INCH THROUGH SIXTY (60) INCH PIPE ON TANGENT LINE WITH A CHANGE IN GRADE OR PIPE SIZE.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	STORM DRAIN MANHOLES GENERAL NOTES		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 2-9-06	DWG. NO. 401	SHT 1 OF 2

MANHOLE NOTES (CONTINUED):

TYPE III

TANGENT

SIXTY (60) INCH SIZE

- A. THIRTY-NINE (39) INCH THROUGH SIXTY (60) INCH PIPE ON TANGENT LINE AND GRADE WITH NO CHANGE IN PIPE SIZE.

ANGLE POINT

SIXTY (60) INCH SIZE

- A. THIRTY (30) INCH THROUGH SIXTY (60) INCH PIPE AT THE ANGLE POINT IN LINE.

- 7. PRECAST MANHOLE COMPONENTS SHALL CONFORM TO ASTM C-478.
- 8. DISTANCE BETWEEN THE TOP OF MANHOLE AND FIRST STEP SHALL BE A MAXIMUM OF SIXTEEN (16) INCHES. MANHOLE STEPS SHALL BE GROUTED IN PLACE.
- 9. (CLARK COUNTY ONLY) DISTANCE BETWEEN MANHOLES SHALL BE A MAXIMUM OF FOUR HUNDRED (400) FEET.
- 10. MANHOLE SPACING SHALL BE REFERRED TO THE WASTE WATER COLLECTION STANDARDS.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	STORM DRAIN MANHOLES GENERAL NOTES		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 2-9-06	DWG. NO. 401	SHT 2 OF 2

DROP INLET NOTES:

1. ALL DROP INLETS, REGARDLESS OF TYPE, SHALL BE LOCATED SUCH THAT THE CURB OPENING (OR GRATE) IS A MINIMUM OF TEN (10) FEET FROM THE NEAREST P.C. OR P.T. OF THE CURRENT OR FUTURE CURB RETURN.

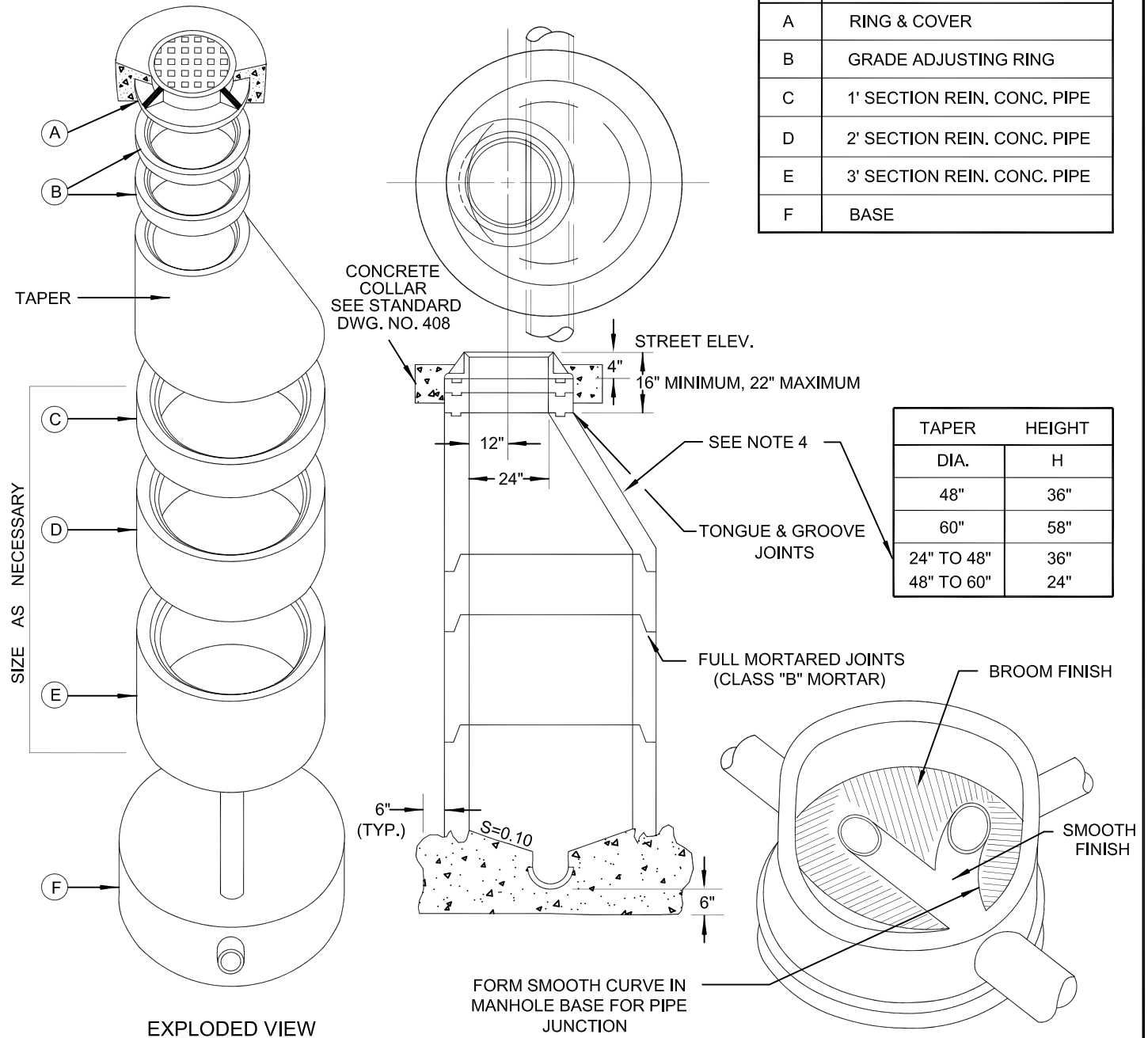
2. FOR CURB RETURNS WHERE A LOW POINT CONDITION EXIST OR IS PROPOSED AT THE P.C. OR P.T., THE CURB PROFILE SHALL BE DESIGNED SUCH THAT THE LOW POINT COINCIDES WITH THE CURB OPENING AS SPECIFIED ON NOTE NO. 1.

3. IF DRIVEWAYS OR UTILITIES EXIST, THE ENTITY ENGINEER SHALL APPROVE THE LOCATION OF THE DROP INLET.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	STORM DRAIN DROP INLET GENERAL NOTES		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 2-9-06	DWG. NO. 402	PAGE 102

SYM.	ITEM
A	RING & COVER
B	GRADE ADJUSTING RING
C	1' SECTION REIN. CONC. PIPE
D	2' SECTION REIN. CONC. PIPE
E	3' SECTION REIN. CONC. PIPE
F	BASE



EXPLODED VIEW

ISOMETRIC VIEW

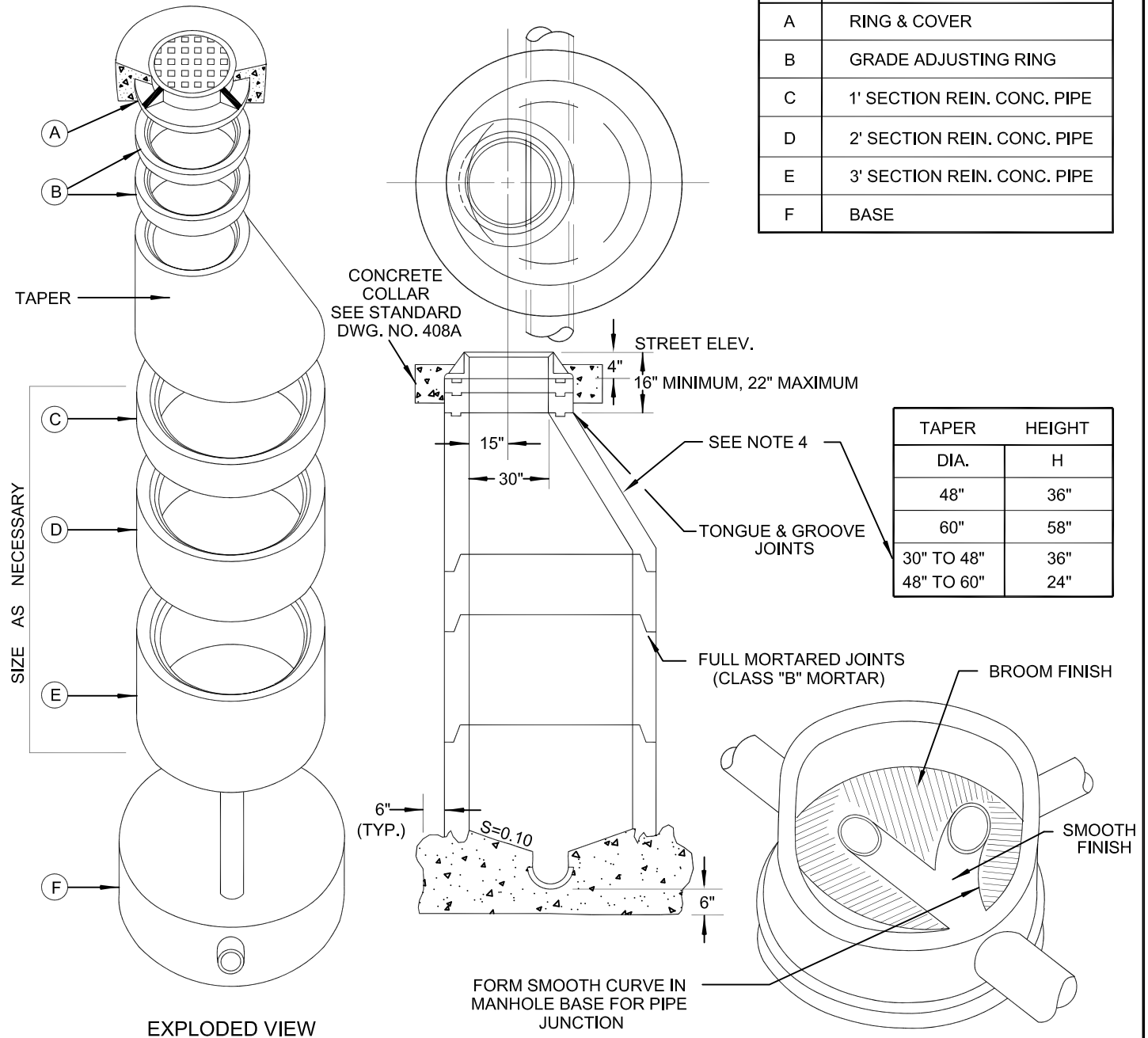
NOTES:

1. IN UNIMPROVED NON-TRAFFIC AREAS, TOP OF MANHOLE SHALL BE 6" TO 9" ABOVE GRADE.
2. PIPES SHALL NOT PROTRUDE MORE THAN 3" INSIDE OF MANHOLE SECTION. CONSTRUCT WATER TIGHT CONNECTION TO MANHOLE.
3. PIPE SECTION LENGTHS ARRANGED TO FIT DEPTH.
4. AN OPTIONAL TWO PIECE 24" TO 48" AND 48" TO 60" CONE MAY BE USED.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE & MORTAR	TYPE I MANHOLE		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 11-10-05	DWG. NO. 403	PAGE NO. 103

SYM.	ITEM
A	RING & COVER
B	GRADE ADJUSTING RING
C	1' SECTION REIN. CONC. PIPE
D	2' SECTION REIN. CONC. PIPE
E	3' SECTION REIN. CONC. PIPE
F	BASE



EXPLODED VIEW

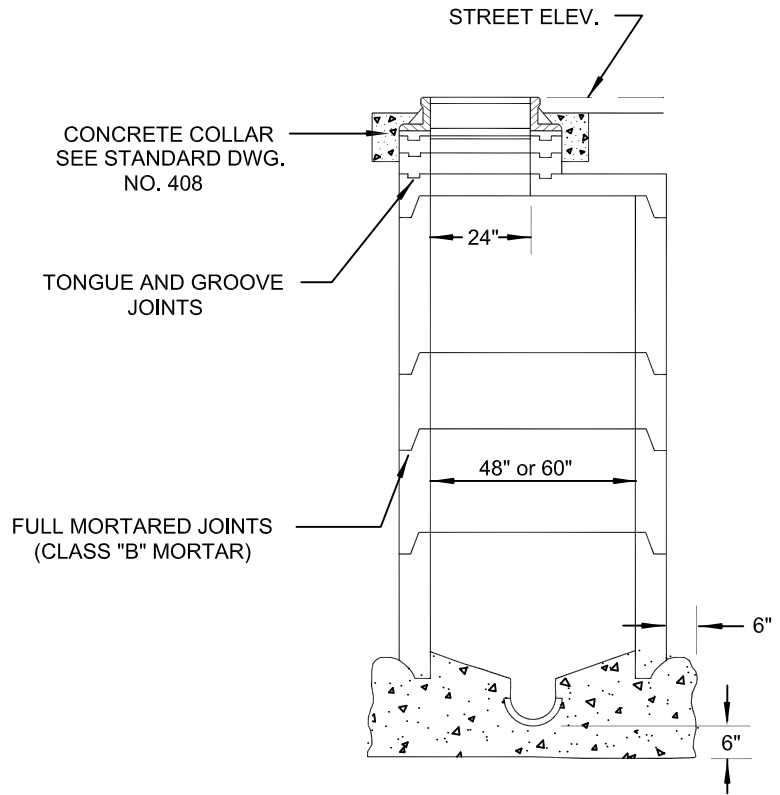
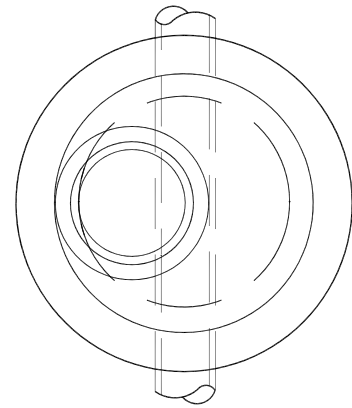
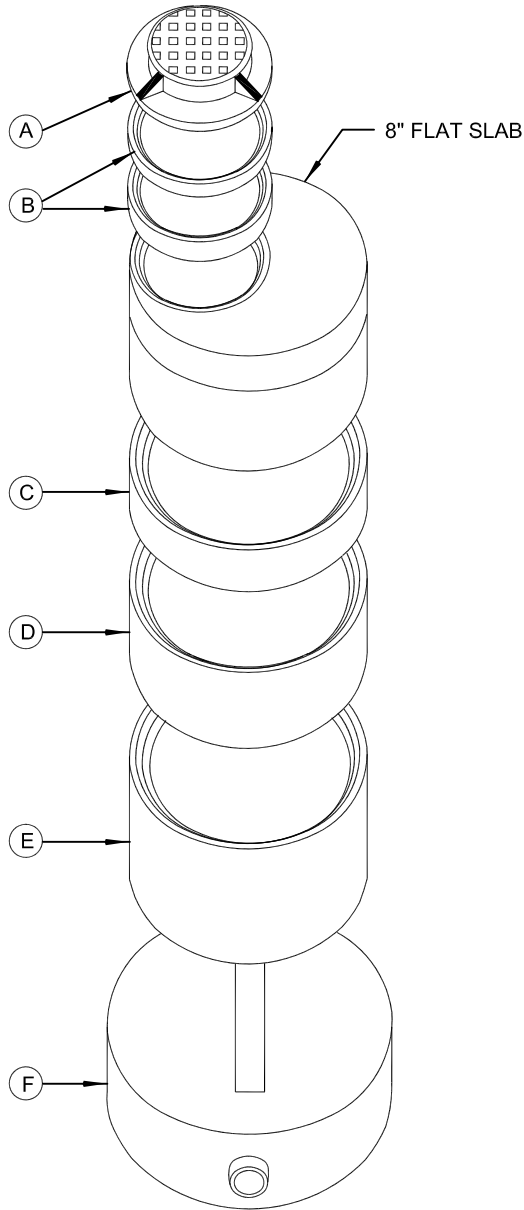
ISOMETRIC VIEW

NOTES:

1. IN UNIMPROVED NON-TRAFFIC AREAS, TOP OF MANHOLE SHALL BE 6" TO 9" ABOVE GRADE.
2. PIPES SHALL NOT PROTRUDE MORE THAN 3" INSIDE OF MANHOLE SECTION. CONSTRUCT WATER TIGHT CONNECTION TO MANHOLE.
3. PIPE SECTION LENGTHS ARRANGED TO FIT DEPTH.
4. AN OPTIONAL TWO PIECE 30" TO 48" AND 48" TO 60" CONE MAY BE USED.
5. THE USE OF A 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE & MORTAR	TYPE I MANHOLE 30" RING AND COVER		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 11-10-05	DWG. NO. 403A	PAGE NO. 103A



NOTE:

1. PIPE SECTION LENGTHS ARRANGED TO FIT DEPTH.

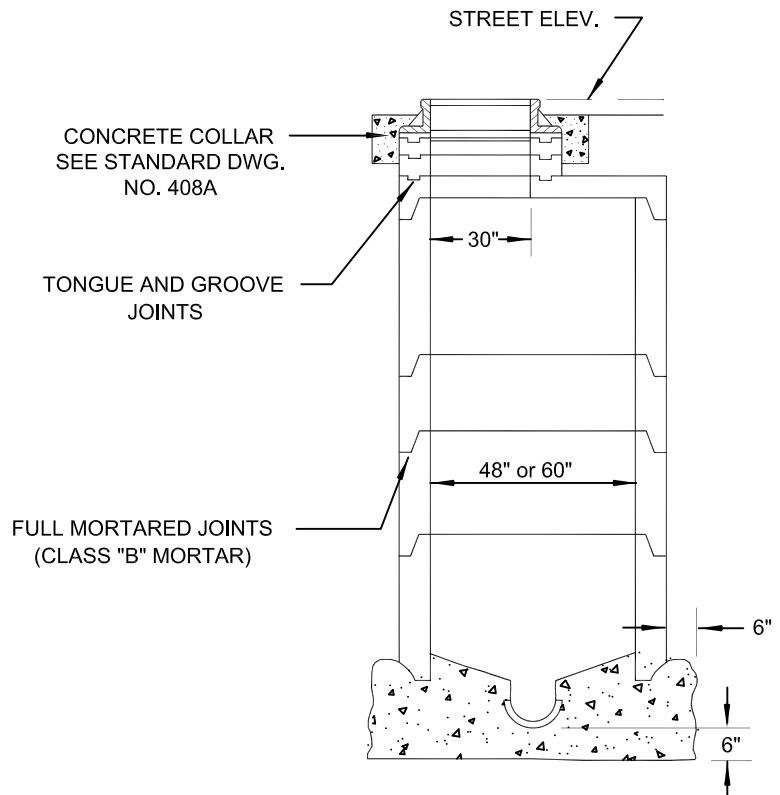
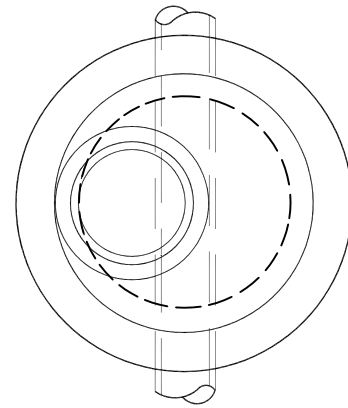
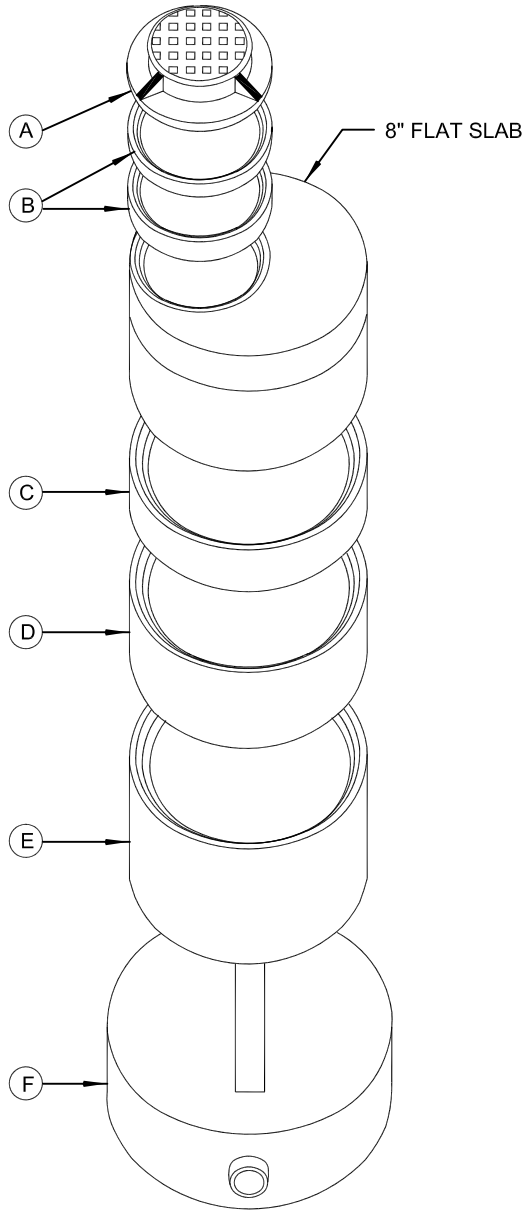
SYM.	ITEM
A	RING & COVER
B	GRADE ADJUSTING RING
C	1' SECTION REIN. CONC. PIPE
D	2' SECTION REIN. CONC. PIPE
E	3' SECTION REIN. CONC. PIPE
F	BASE

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501	CONCRETE & MORTAR
609	CATCH BASINS, MANHOLES & INLETS

TYPE IA MANHOLE



SYM.	ITEM
A	RING & COVER
B	GRADE ADJUSTING RING
C	1' SECTION REIN. CONC. PIPE
D	2' SECTION REIN. CONC. PIPE
E	3' SECTION REIN. CONC. PIPE
F	BASE

NOTE:

1. PIPE SECTION LENGTHS ARRANGED TO FIT DEPTH.
2. THE USE OF A 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

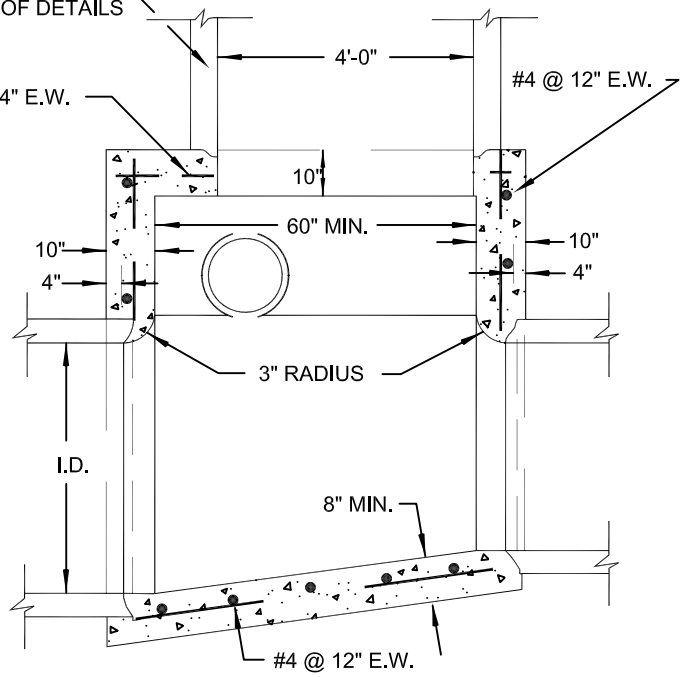
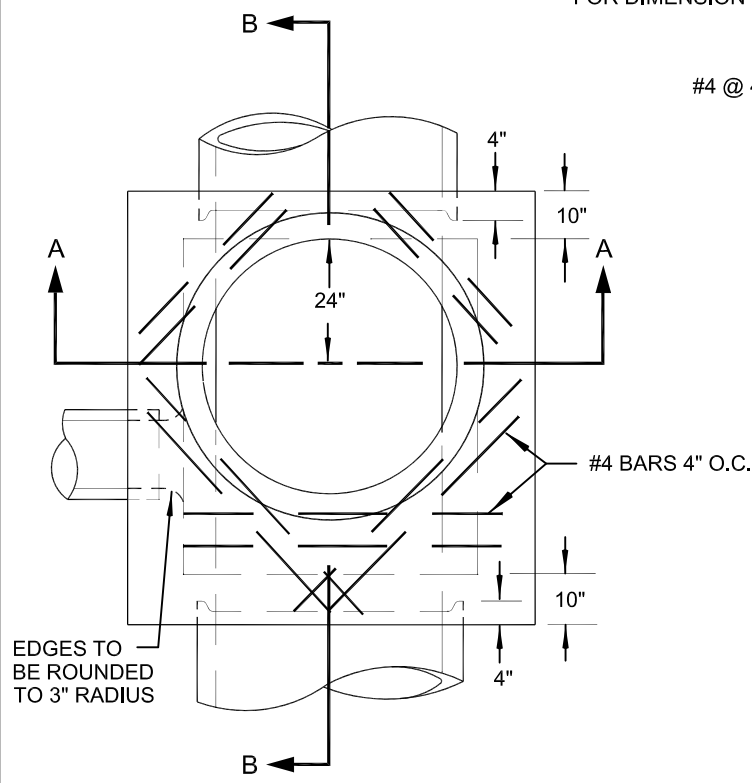
SPECIFICATION REFERENCE

501	CONCRETE & MORTAR
609	CATCH BASINS, MANHOLES & INLETS

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**TYPE IA MANHOLE
30" RING AND COVER**

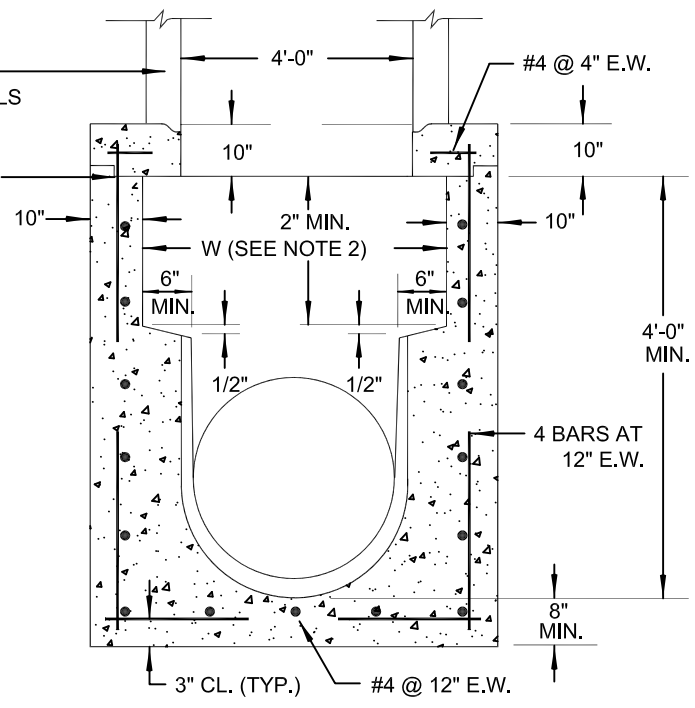
SEE TYPE I MANHOLE FOR DIMENSION OF DETAILS



SECTION B-B

SEE TYPE I MANHOLE FOR DIMENSIONS AND DETAILS

OPTIONAL CONSTRUCTION JOINT



SECTION A-A

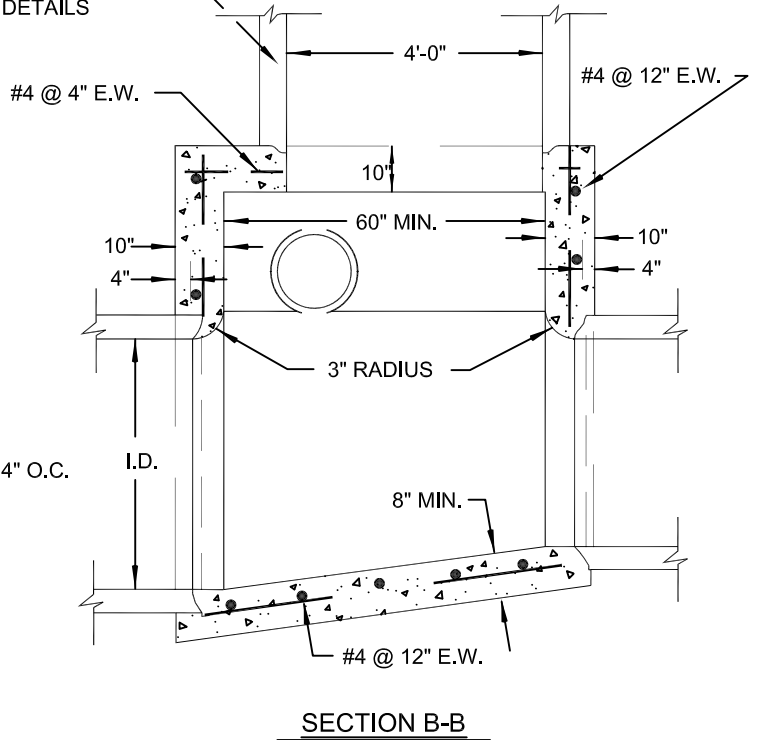
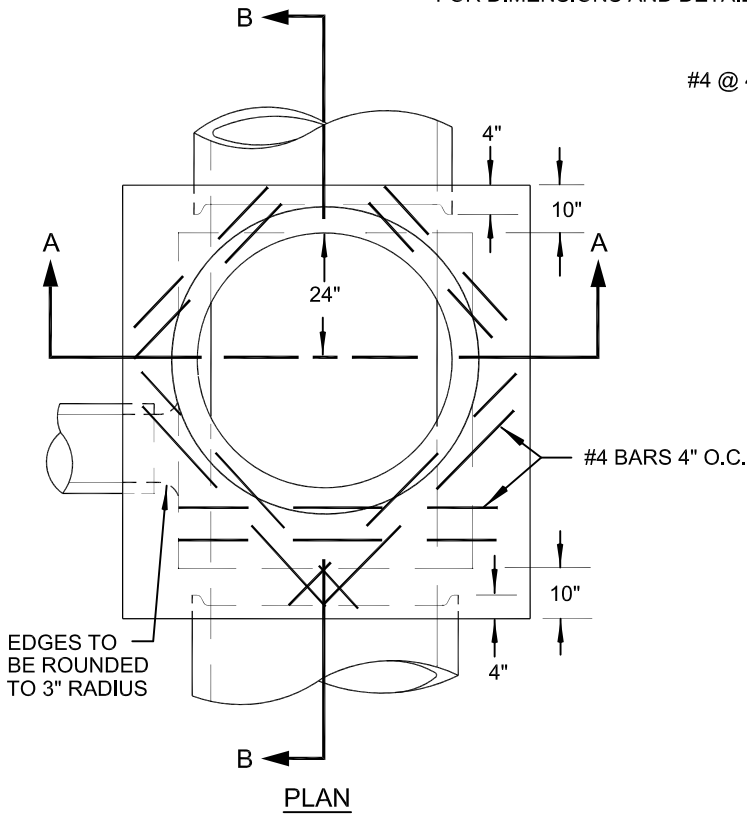
NOTES:

1. STEPS SHALL BE INSTALLED ON THE UPSTREAM WALL OF THE MANHOLE.
2. $W = I.D. + 12\text{-INCHES MIN.}$ BUT IN NO CASE SHALL W BE LESS THAN 60-INCHES.

Effective 07/01/10 - 12/30/10

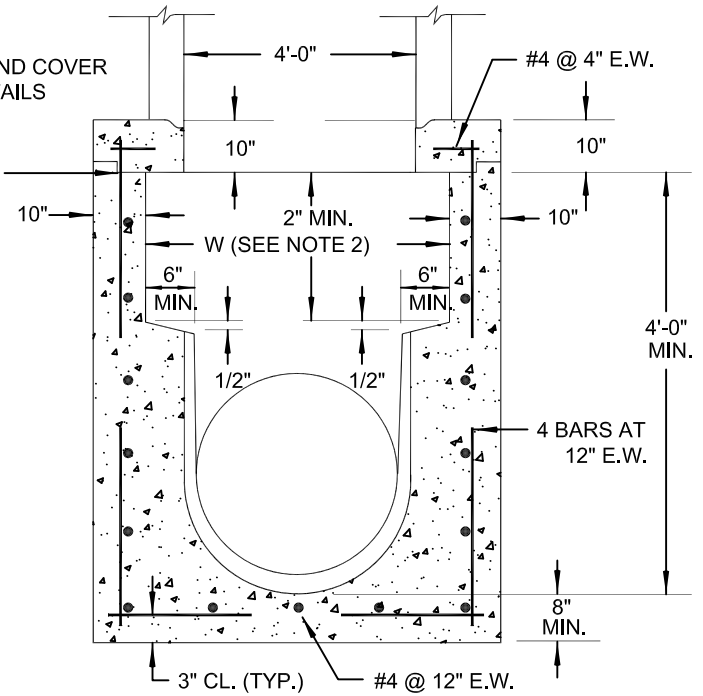
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE AND MORTAR	TYPE II MANHOLE		
505	REINFORCING STEEL			
609	CATCH BASINS, MANHOLES & INLETS			
DATE	DWG. NO.	405	PAGE NO.	105

SEE TYPE I MANHOLE 30" RING AND COVER FOR DIMENSIONS AND DETAILS



SEE TYPE I MANHOLE 30" RING AND COVER FOR DIMENSIONS AND DETAILS

OPTIONAL CONSTRUCTION JOINT



NOTES:

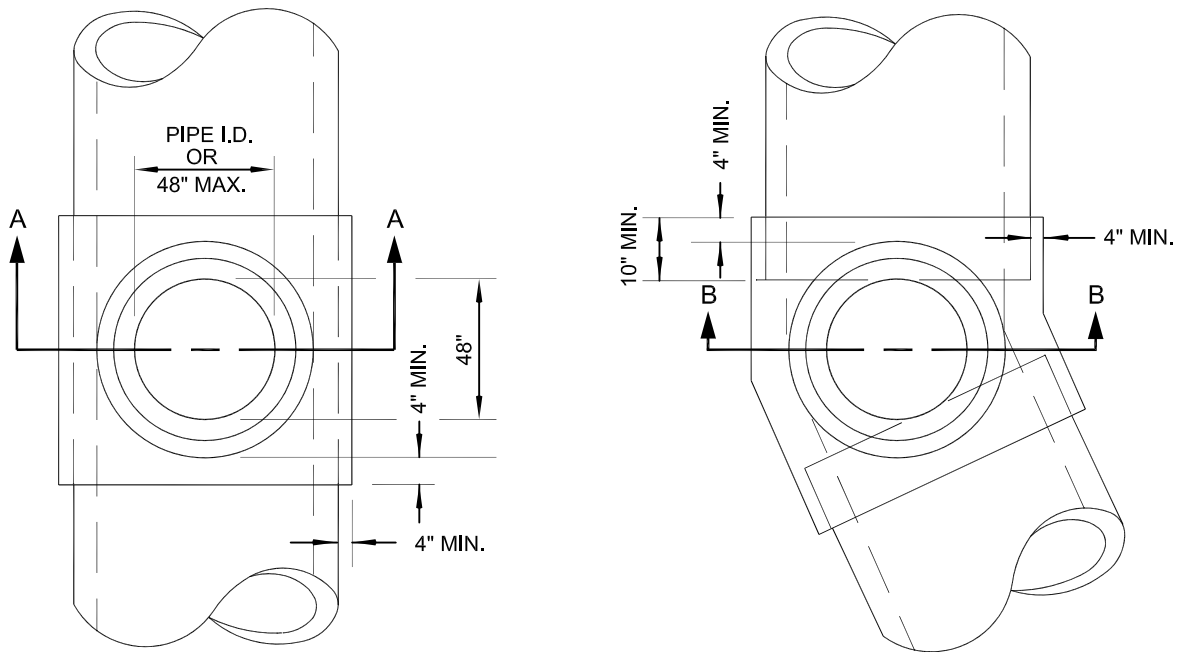
1. STEPS SHALL BE INSTALLED ON THE SIDE WALL OF THE MANHOLE.
2. $W = I.D. + 12\text{-INCHES MIN.}$ BUT IN NO CASE SHALL W BE LESS THAN 60-INCHES.
3. THE USE OF A 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

SPECIFICATION REFERENCE

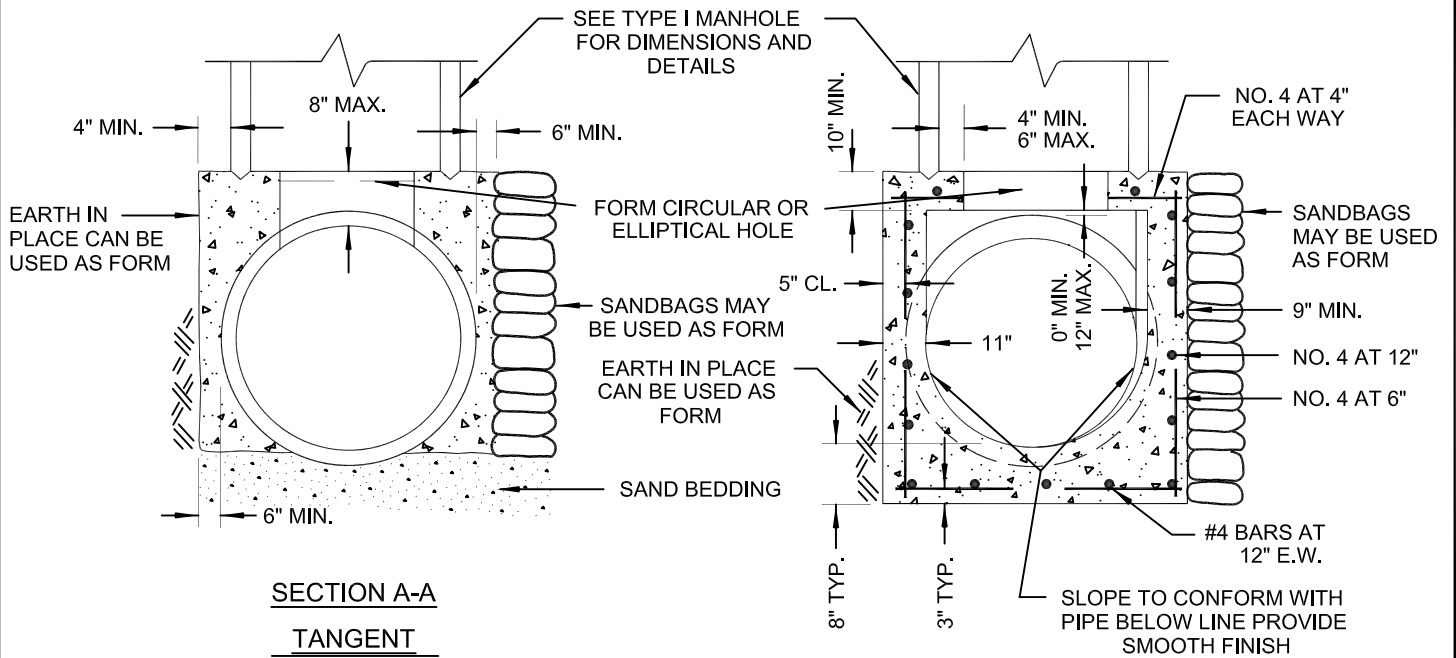
**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

501	CONCRETE AND MORTAR
505	REINFORCING STEEL
609	CATCH BASINS, MANHOLES & INLETS

**TYPE II MANHOLE
30" RING AND COVER**



PLAN



SECTION A-A
TANGENT

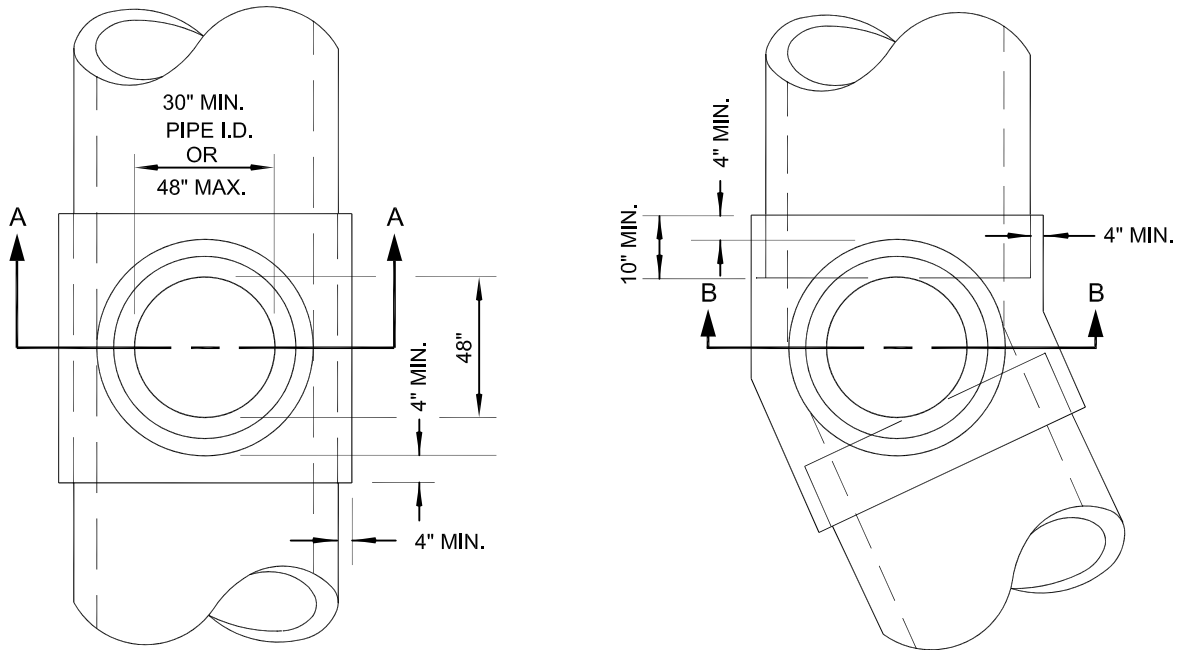
SECTION B-B
ANGLE POINT

NOTE:

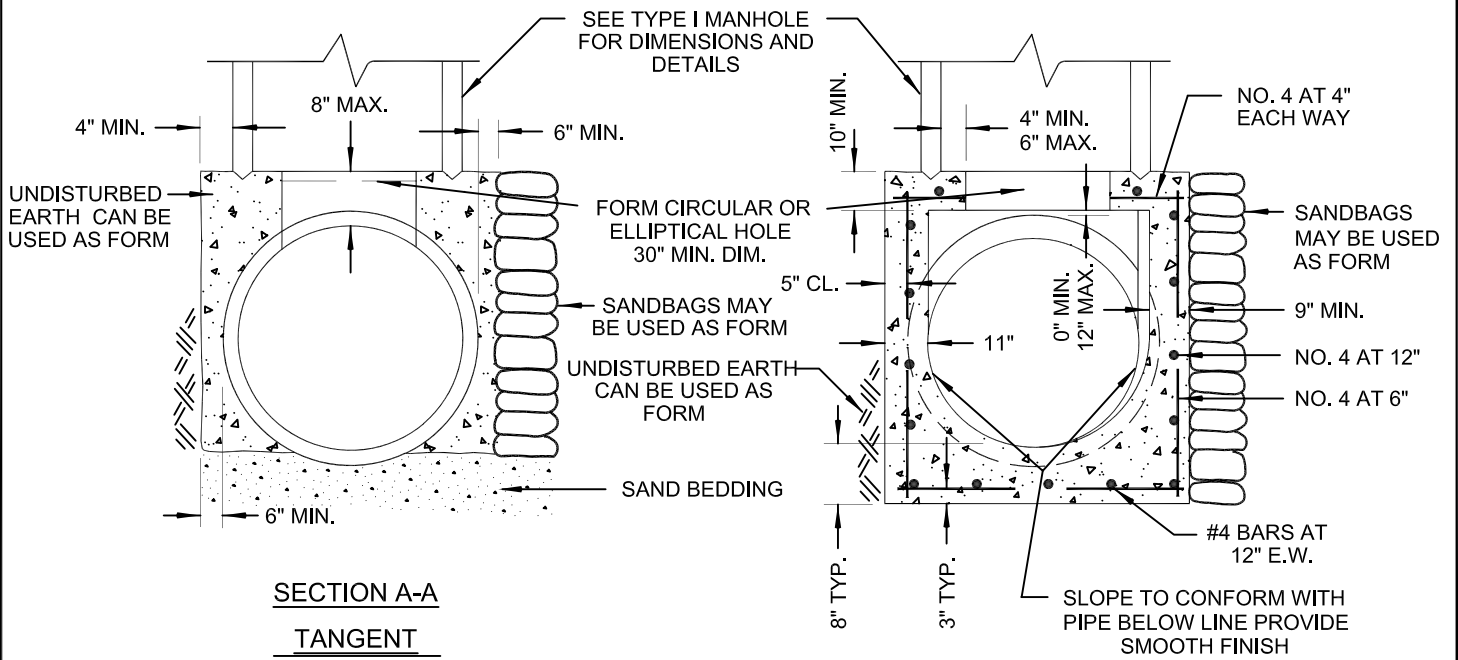
1. STEPS SHALL BE INSTALLED ON THE UPSTREAM WALL OF THE MANHOLE.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	TYPE III MANHOLE		
505	REINFORCING STEEL			
609	CATCH BASINS, MANHOLES & INLETS			
DATE	DWG. NO.	406	PAGE NO.	106



PLAN



NOTE:

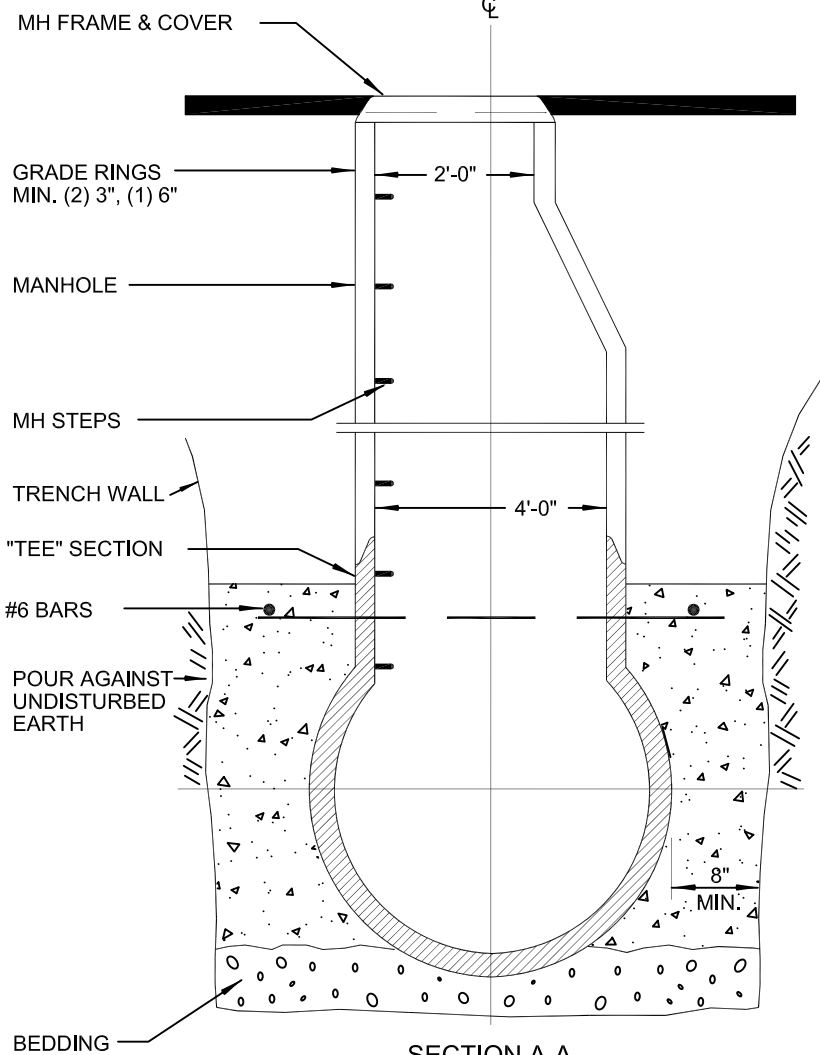
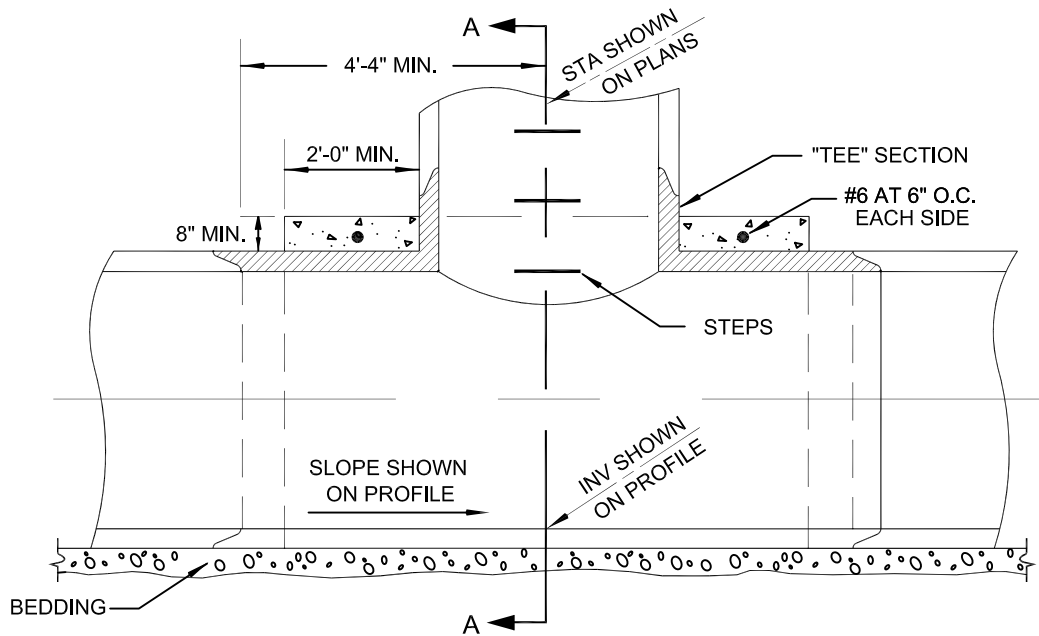
1. STEPS SHALL BE INSTALLED ON THE UPSTREAM WALL OF THE MANHOLE.
2. THE USE OF A 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

SPECIFICATION REFERENCE

501	CONCRETE
505	REINFORCING STEEL
609	CATCH BASINS, MANHOLES & INLETS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPE III MANHOLE
30" RING AND COVER

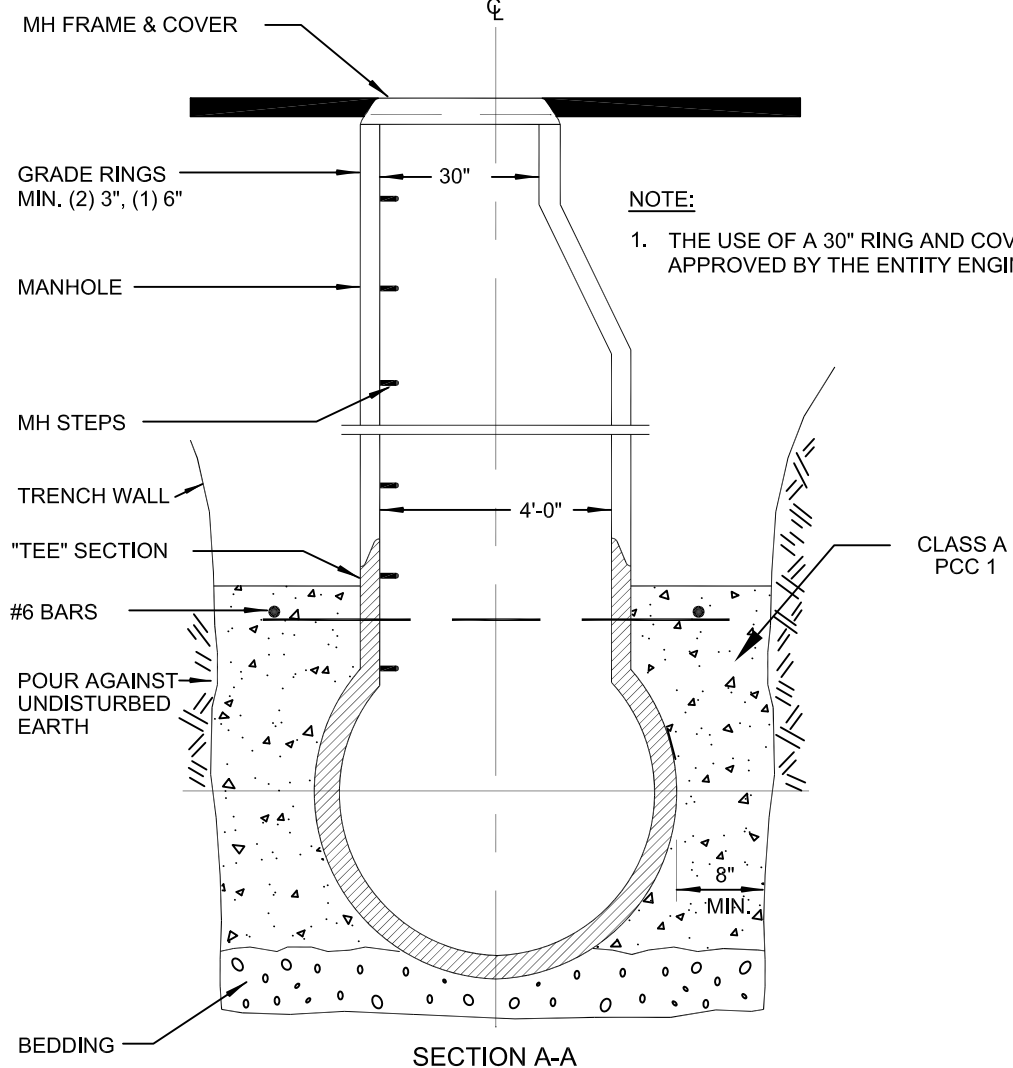
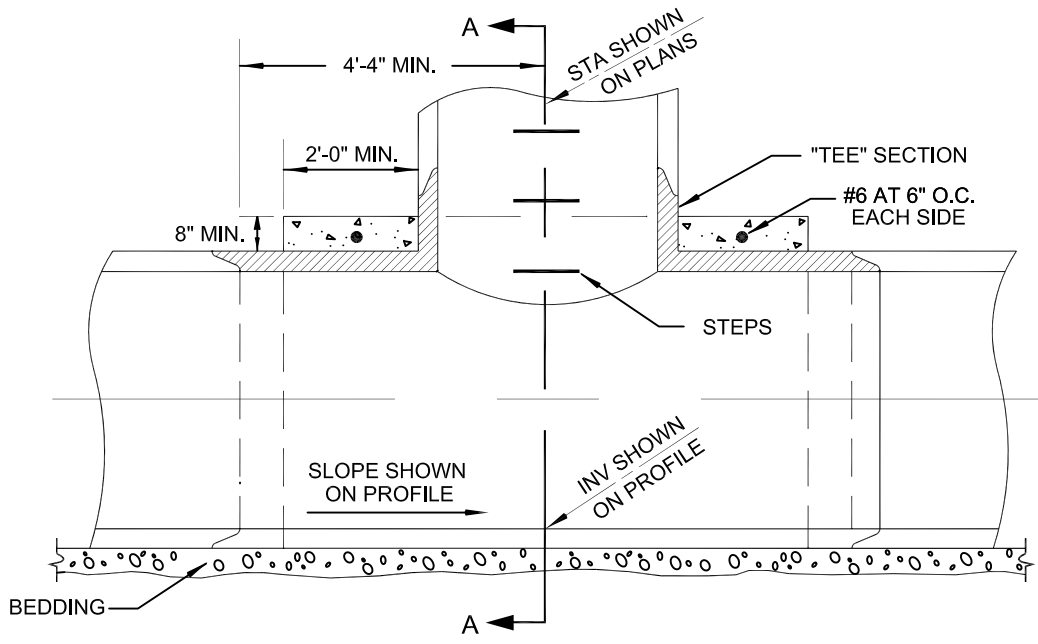


SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

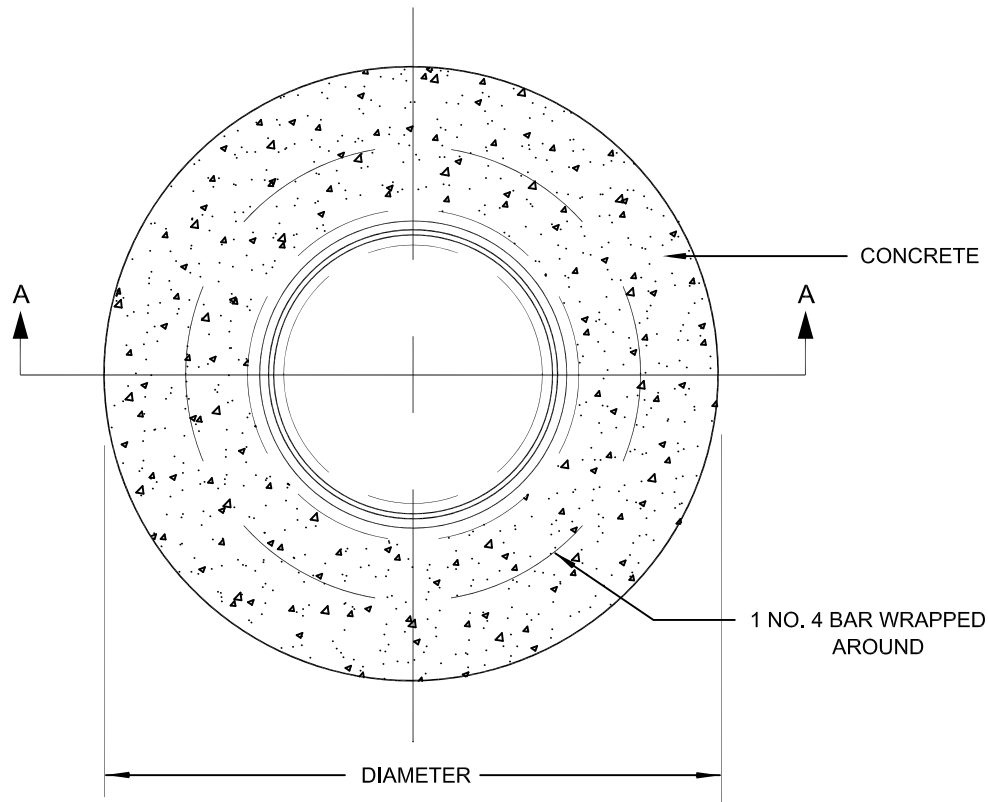
501	CONCRETE
609	CATCH BASINS, MANHOLES & INLETS

PRECAST MANHOLE TEES

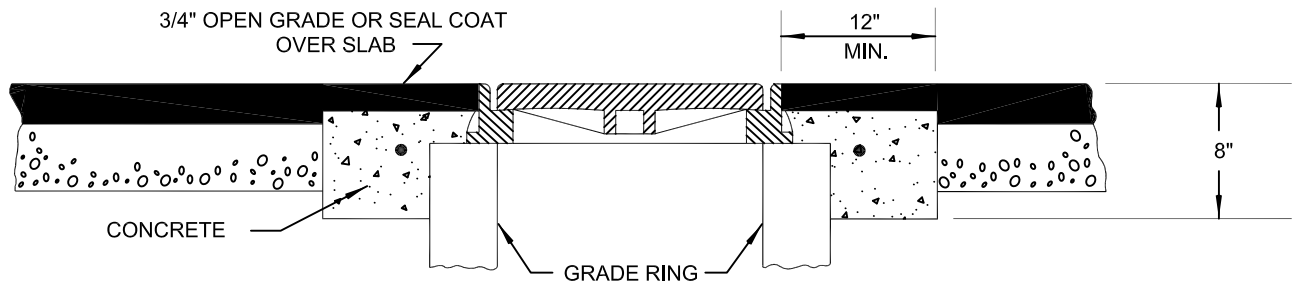


Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	<p style="text-align: center;">PRECAST MANHOLE TEES 30" RING AND COVER</p>		
609	CATCH BASINS, MANHOLES & INLETS			
		DATE 11-10-05	DWG. NO. 407A	PAGE NO. 107A



PLAN

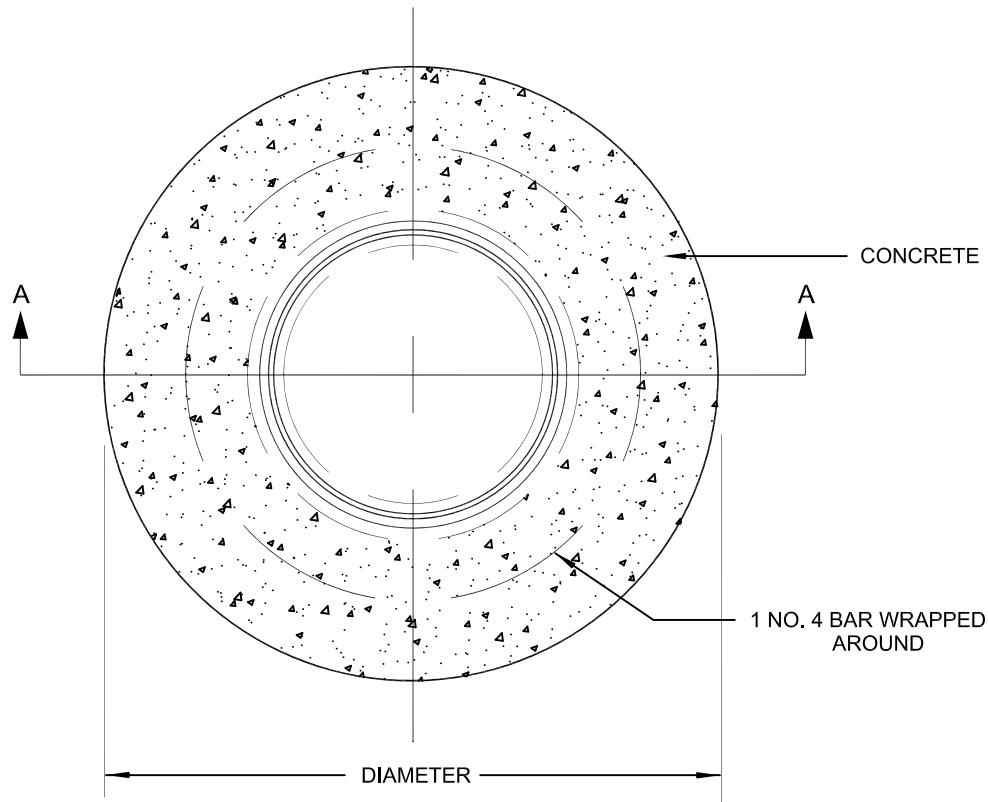


SECTION A-A

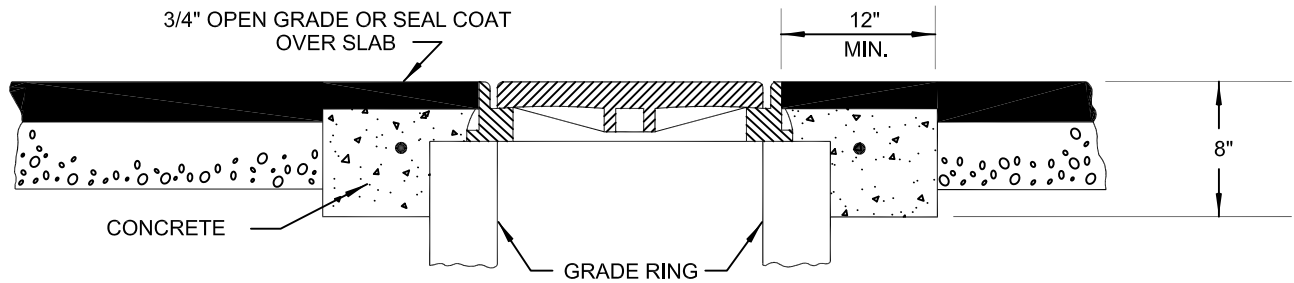
NOTES:

1. CONCRETE COLLAR TO BE CONSTRUCTED 1/8" BELOW SURFACE OF DENSE GRADE WHERE OPEN GRADE IS NOT USED.
2. CONCRETE COLLAR NOT REQUIRED IN UNINCORPORATED CLARK COUNTY RESIDENTIAL STREETS LESS THAN 80' R/W WIDTH.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	CONCRETE COLLAR AROUND MANHOLES		
505	REINFORCING STEEL			
		DATE 12-14-00	DWG. NO. 408	PAGE NO. 108



PLAN



SECTION A-A

NOTES:

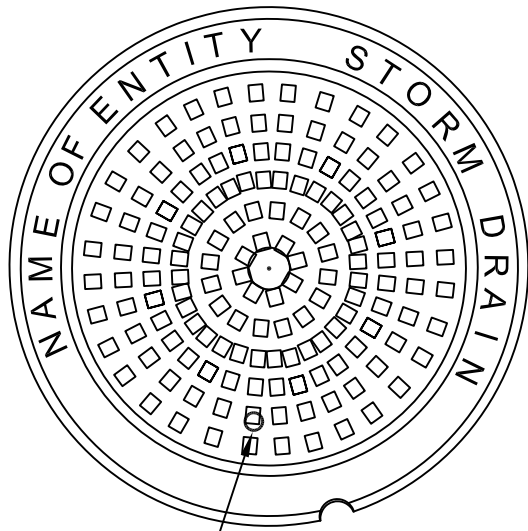
1. CONCRETE COLLAR TO BE CONSTRUCTED 1/8" BELOW SURFACE OF DENSE GRADE WHERE OPEN GRADE IS NOT USED.
2. CONCRETE COLLAR NOT REQUIRED IN UNINCORPORATED CLARK COUNTY RESIDENTIAL STREETS LESS THAN 80' R/W WIDTH.
3. THE USE OF 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

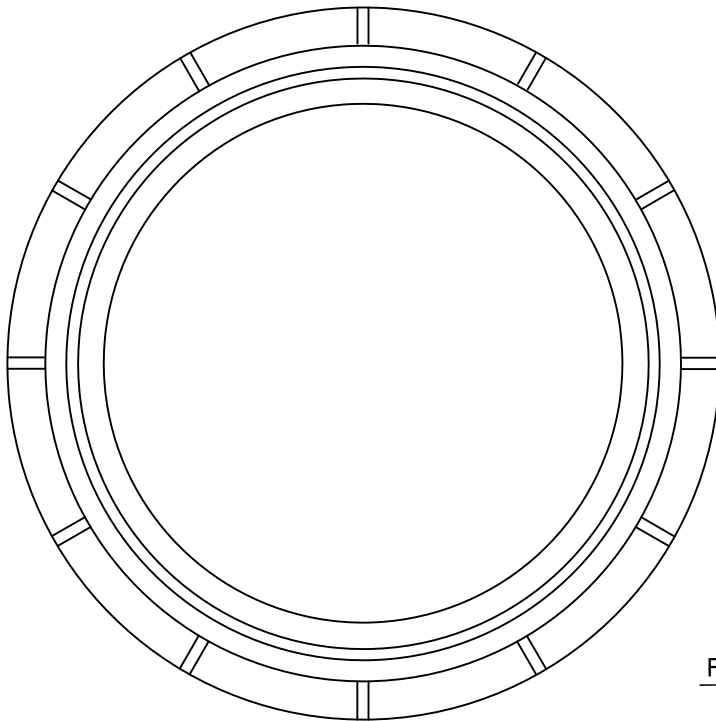
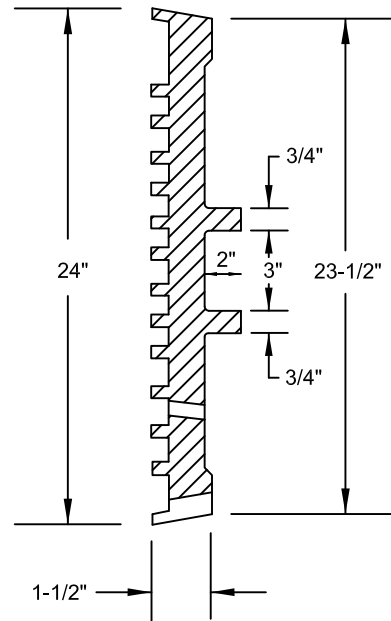
501	CONCRETE
505	REINFORCING STEEL

CONCRETE COLLAR AROUND
MANHOLES
30" RING AND COVER

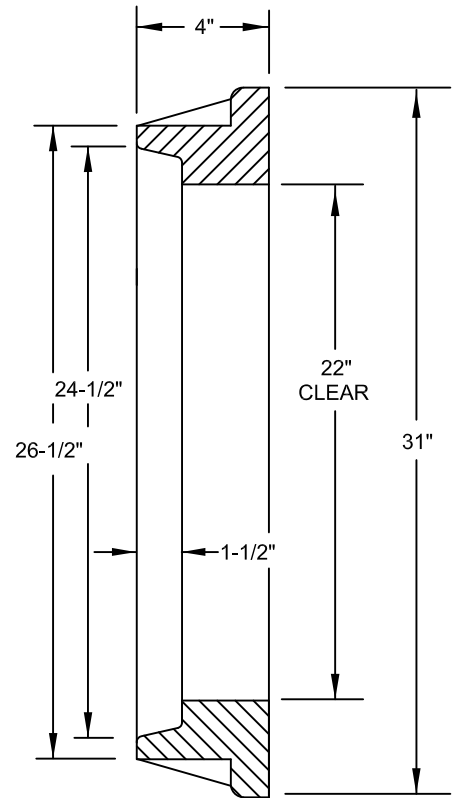


1" DIA. VENT HOLE

COVER



FRAME



NOTES:

1. FRAME AND COVER TO BE ALHAMBRA FOUNDRY COMPANY TYPE A1310 IN ACCORDANCE WITH ASTM A-48, CLASS 30, OR APPROVED EQUAL.
2. CAST IRON SHALL HAVE MINIMUM TENSILE STRENGTH OF 30,000 P.S.I.
3. FRAME AND COVER MACHINED TO FIT.
4. WEIGHT OF FRAME AND COVER 330 LBS. MINIMUM.

SPECIFICATION REFERENCE

712 MISCELLANEOUS METALS

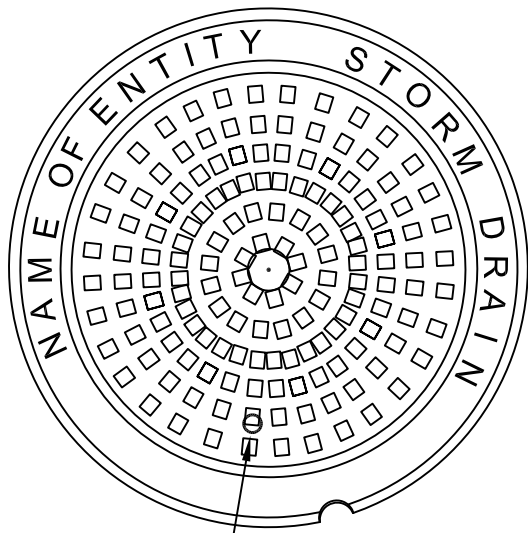
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STANDARD MANHOLE
COVER AND RING

DATE

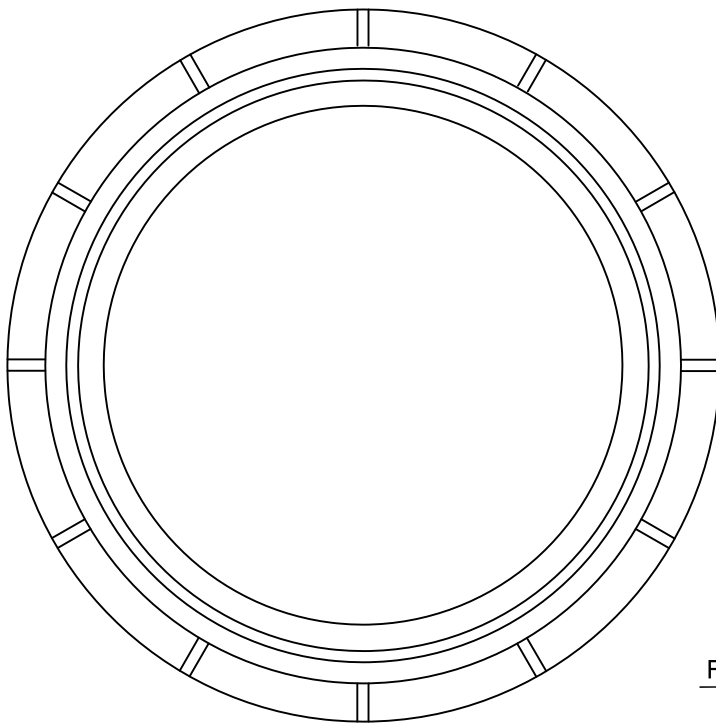
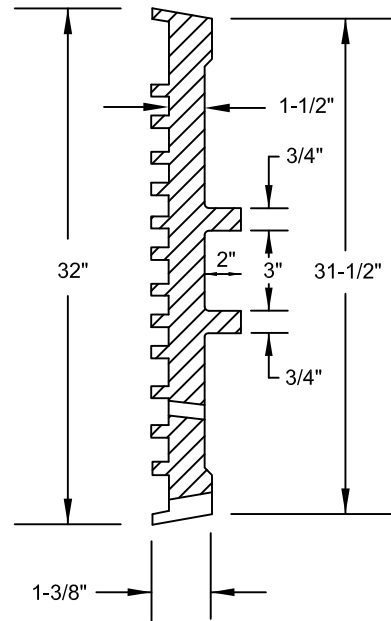
DWG. NO. 409

PAGE NO. 109

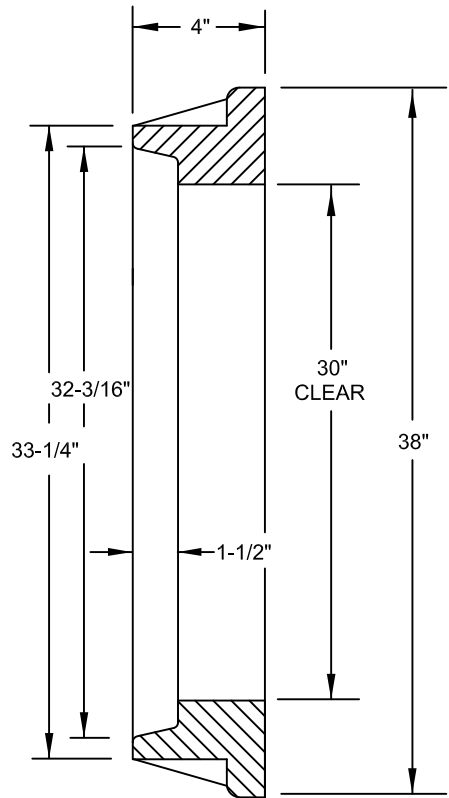


1" DIA. VENT HOLE

COVER



FRAME



NOTES:

1. FRAME AND COVER TO BE ALHAMBRA FOUNDRY COMPANY TYPE A1310 IN ACCORDANCE WITH ASTM A-48, CLASS 30, OR APPROVED EQUAL.
2. CAST IRON SHALL HAVE MINIMUM TENSILE STRENGTH OF 30,000 P.S.I.
3. FRAME AND COVER MACHINED TO FIT.
4. WEIGHT OF FRAME AND COVER 330 LBS. MINIMUM.
5. THE USE OF A 30" RING AND COVER SHALL BE APPROVED BY THE ENTITY ENGINEER.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

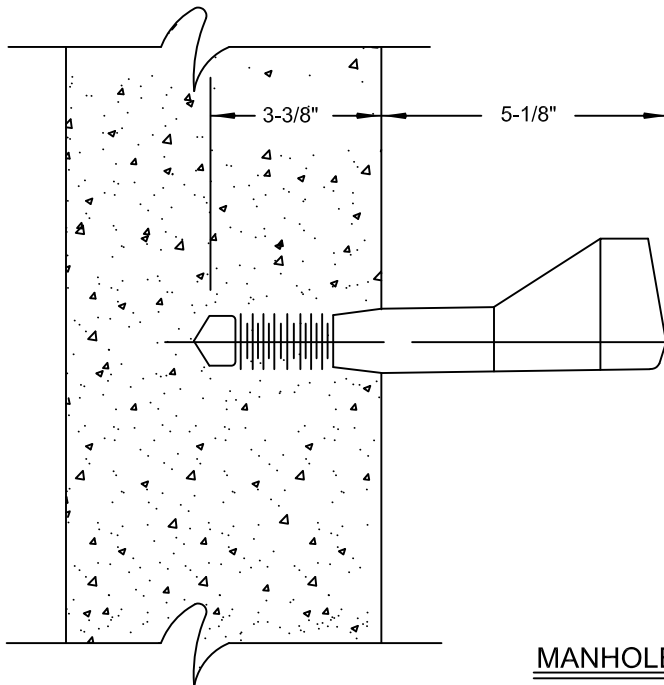
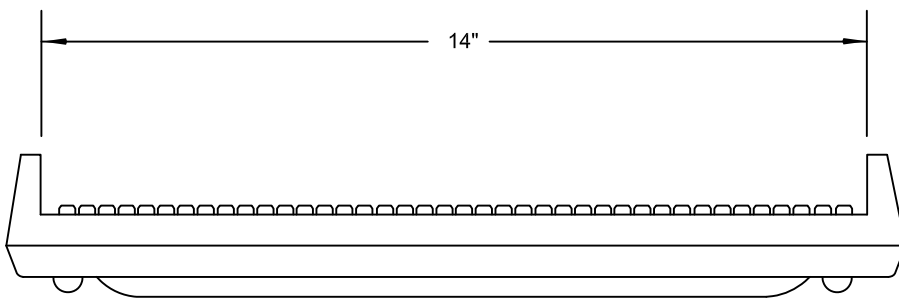
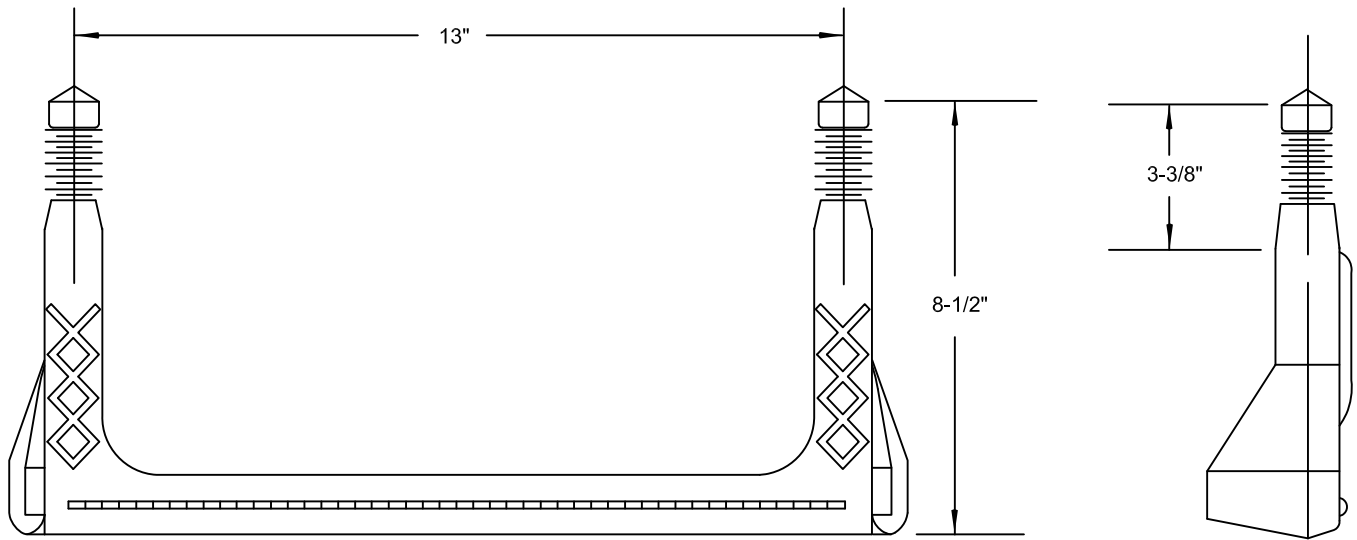
712 MISCELLANEOUS METALS

STANDARD MANHOLE
30" COVER AND RING

DATE 11/10/05

DWG. NO. 409A

PAGE NO. 109A



MANHOLE STEPS

NOTES:

1. MANHOLE STEP SHALL CONFORM TO A.S.T.M. C-478 AND C-497.
2. ALUMINUM STEPS SHALL BE SOLID, MADE FROM MATERIAL IN CONFORMANCE WITH A.S.T.M. B221 (ALLOY 6005-TS).
3. REINFORCED PLASTIC STEPS SHALL BE POLYPROPYLENE PLASTIC, WITH NO. 3 (MIN.) DEFORMED STEEL ROD (GRADE 60/A.S.T.M. A-615).
4. STEPS SHALL BE EVENLY SPACED FROM 12" TO 16".
5. ALL STEPS MUST BE EPOXIED IN PLACE DURING THE INSTALLATION PROCESS.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STANDARD
MANHOLE STEPS

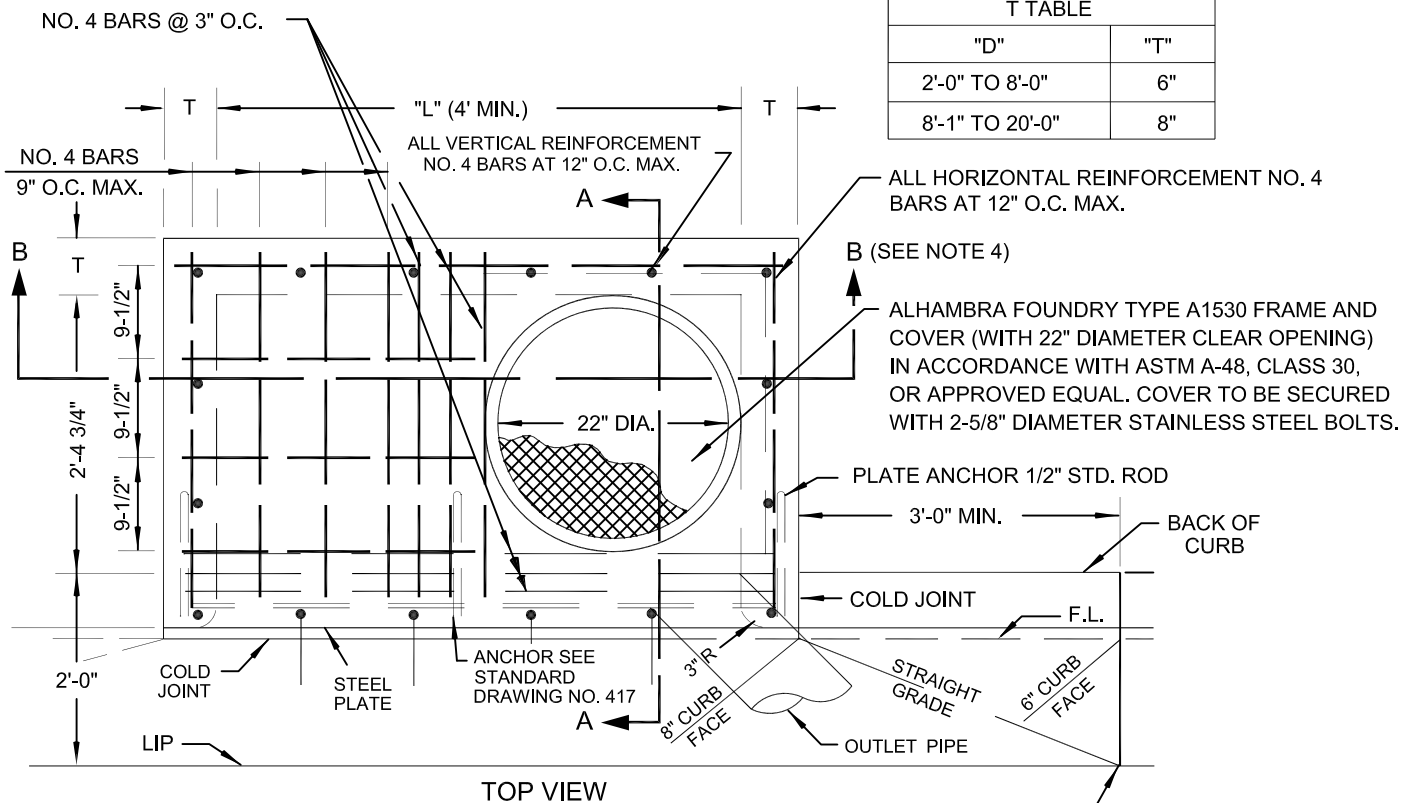
DATE 3-11-04

DWG. NO.

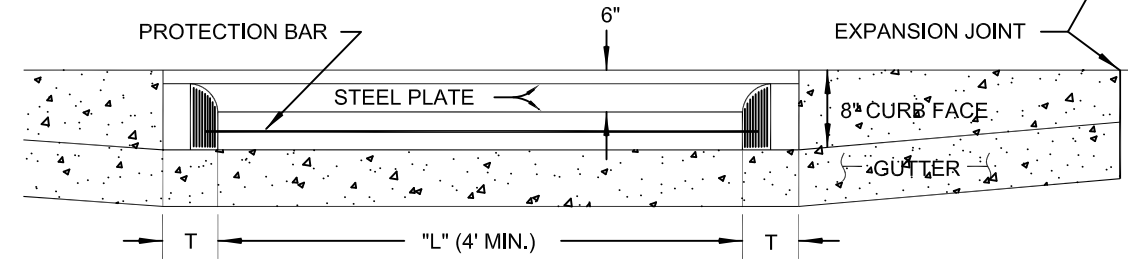
410

PAGE NO. 110

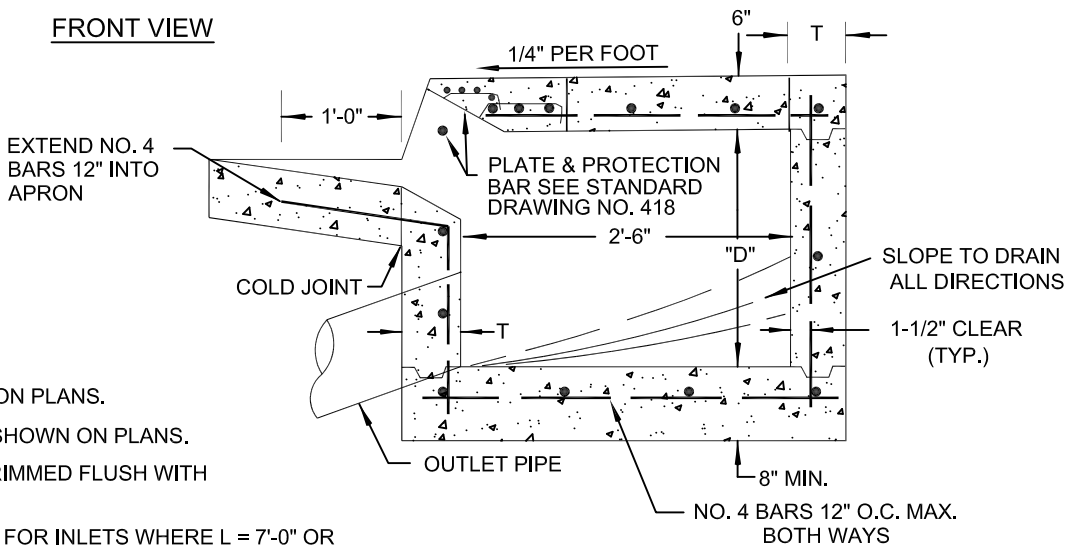
T TABLE	
"D"	"T"
2'-0" TO 8'-0"	6"
8'-1" TO 20'-0"	8"



TOP VIEW



FRONT VIEW



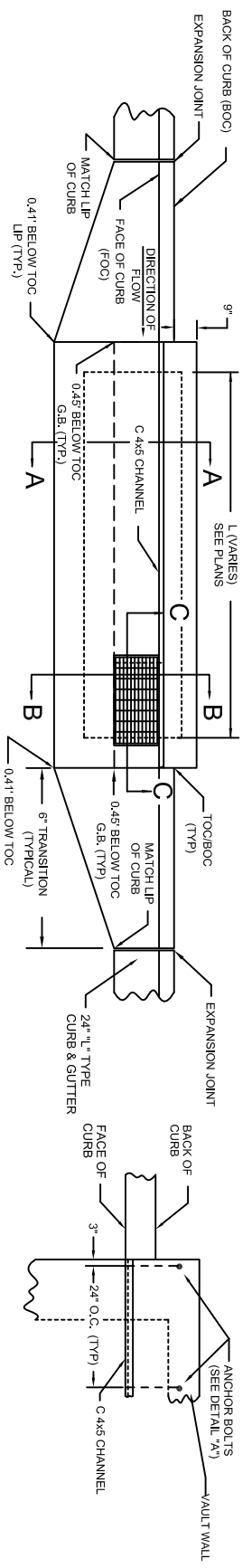
SECTION A-A

NOTES:

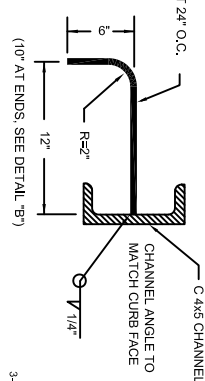
1. DEPTH "D" TO BE SHOWN ON PLANS.
2. OUTLET PIPE SIZE TO BE SHOWN ON PLANS.
3. OUTLET PIPE SHALL BE TRIMMED FLUSH WITH INSIDE FACE OF INLET.
4. SECTION B-B IS OPTIONAL FOR INLETS WHERE L = 7'-0" OR GREATER, AND D = 5'-0" OR GREATER, SEE STANDARD DRAWING NO. 415.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	<p>DROP INLET TYPE "A"</p>		
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
713	STEEL			
		DATE 4-11-02	DWG. NO. 411	PAGE NO. 111

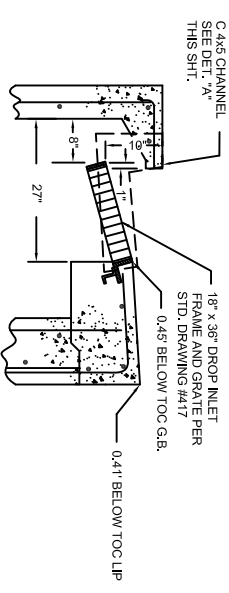
Effective 07/01/10 - 12/30/10



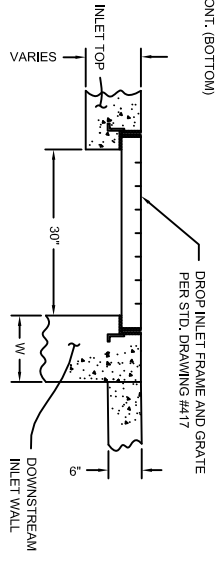
PLAN VIEW



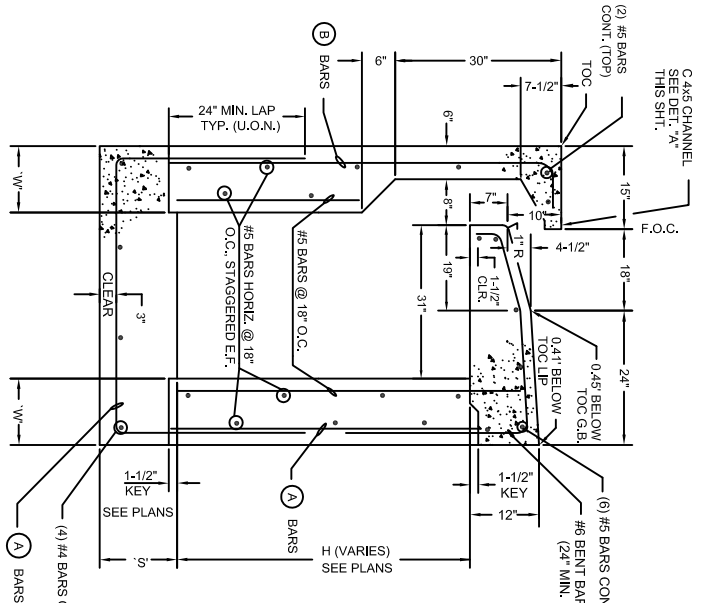
DETAIL "B"



SECTION B-B



SECTION C-C

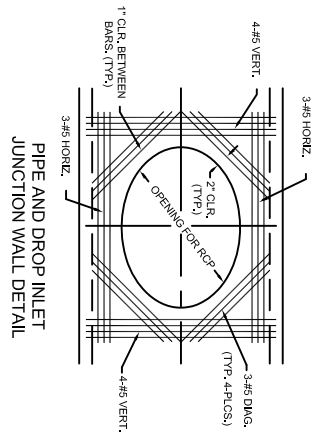


SECTION A-A

H (MAX)	SLAB DIM. 'S'	DIM. 'W'	WALLS
4'-0"	8"	8"	(A) BARS (B) BARS
6'-0"	8"	8"	#4 @ 10" O.C. #5 @ 12" O.C.
8'-0"	8"	8"	#5 @ 12" O.C. #5 @ 12" O.C.
10'-0"	8"	8"	#5 @ 12" O.C. #5 @ 12" O.C.

NOTES:

1. ALL REBAR SHALL HAVE 2-1/2" COVER U.O.N.
2. ALL CONCRETE SHALL BE CLASS DA 4000 PSL.
3. SEE PLANS FOR LENGTH (L) AND DEPTH (H) OF EACH INLET.
4. SEE PLANS FOR SIZE AND LOCATION OF OUTLET PIPE.
5. ALL EXPOSED STEEL SHALL BE HOT DIPPED GALVANIZED.
6. ALL DAMAGED GALVANIZED STEEL SHALL BE PAINTED WITH A MINIMUM 3.5 MIL. COAT OF ONE OF THE FOLLOWING ZINC RICH PAINTS: GALVINOX TYPE I SHERWIN-WILLIAMS ZINC CLAD I
7. ALL REBAR SHALL BE GRADE 60.
8. ALL STRUCTURAL STEEL SHALL BE A36.
9. ALL ANCHOR BOLTS SHALL BE A-307.
10. BEDDING FOR DROP INLET SHALL BE 6" MIN. OF TYPE II AGGREGATE BASE COMPACTED TO 95% MAXIMUM DENSITY PER ASTM D1557.



PIPE AND DROP INLET JUNCTION WALL DETAIL

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

MODIFIED TYPE "A"
DROP INLET

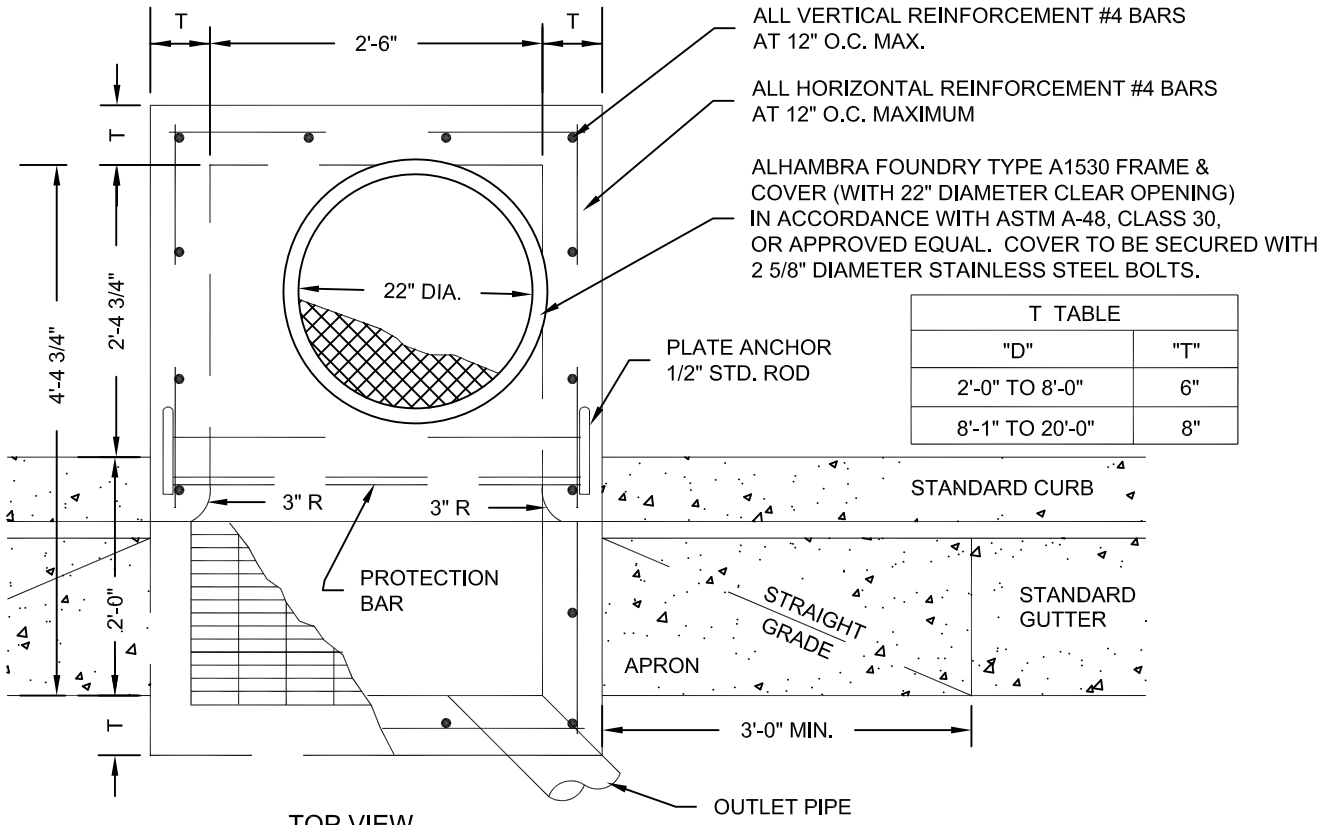
SPECIFICATION REFERENCE

501	CONCRETE
502	CONCRETE STRUCTURES
505	REINFORCING STEEL
713	STEEL

DATE 9-14-06

DWG. NO. 411A

PAGE NO.



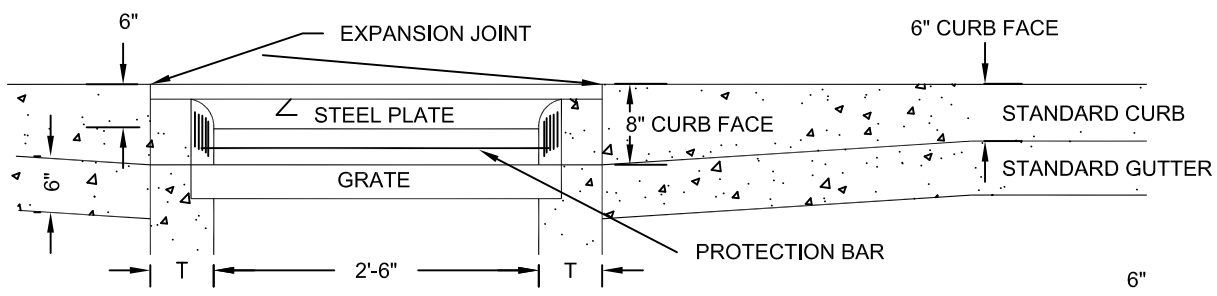
ALL VERTICAL REINFORCEMENT #4 BARS AT 12" O.C. MAX.

ALL HORIZONTAL REINFORCEMENT #4 BARS AT 12" O.C. MAXIMUM

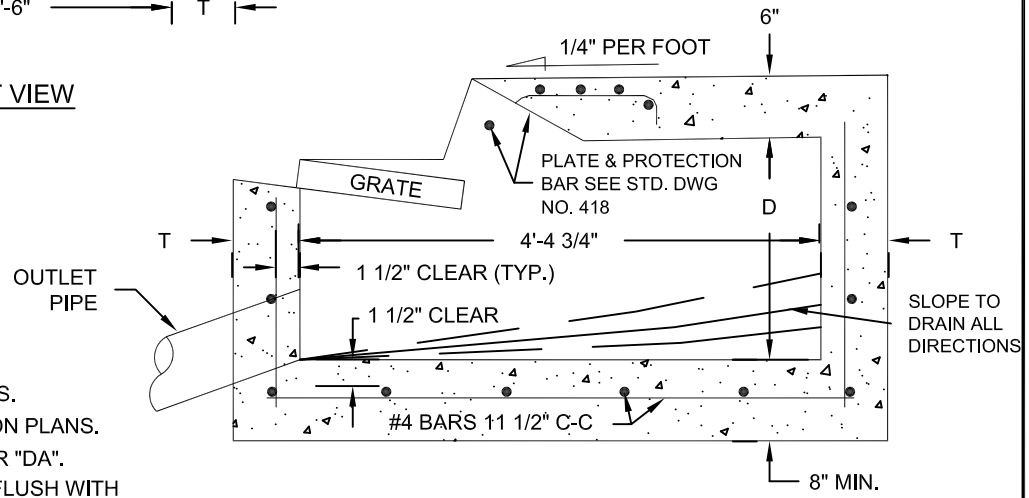
ALHAMBRA FOUNDRY TYPE A1530 FRAME & COVER (WITH 22" DIAMETER CLEAR OPENING) IN ACCORDANCE WITH ASTM A-48, CLASS 30, OR APPROVED EQUAL. COVER TO BE SECURED WITH 2 5/8" DIAMETER STAINLESS STEEL BOLTS.

T TABLE	
"D"	"T"
2'-0" TO 8'-0"	6"
8'-1" TO 20'-0"	8"

TOP VIEW



FRONT VIEW



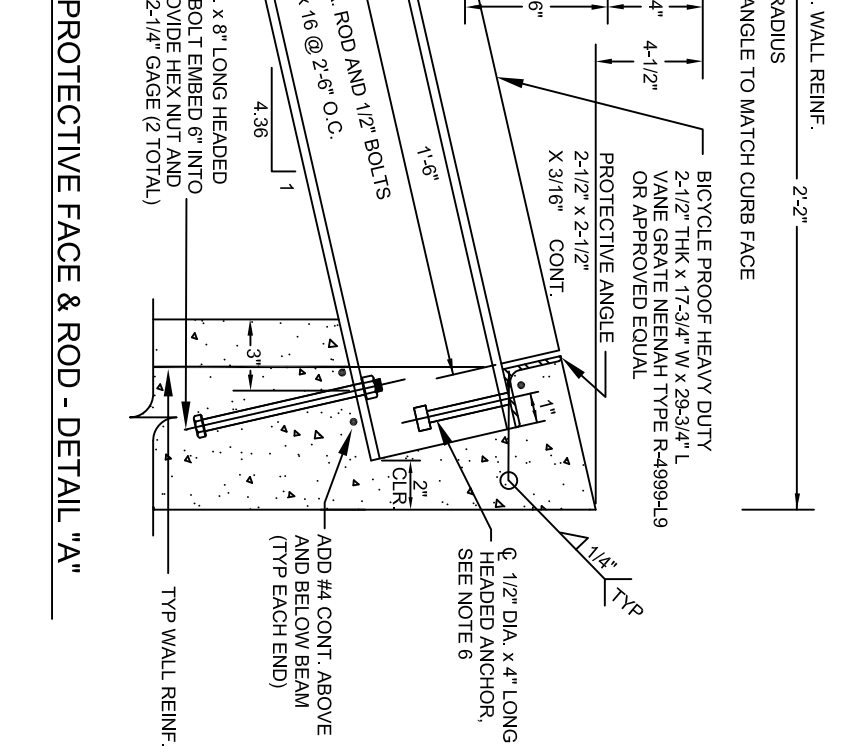
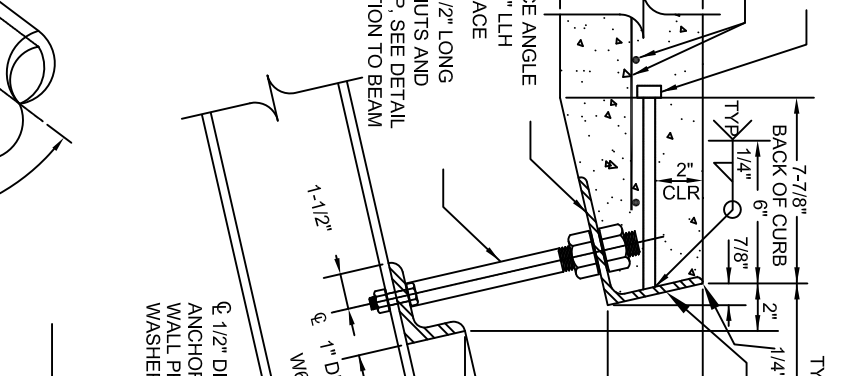
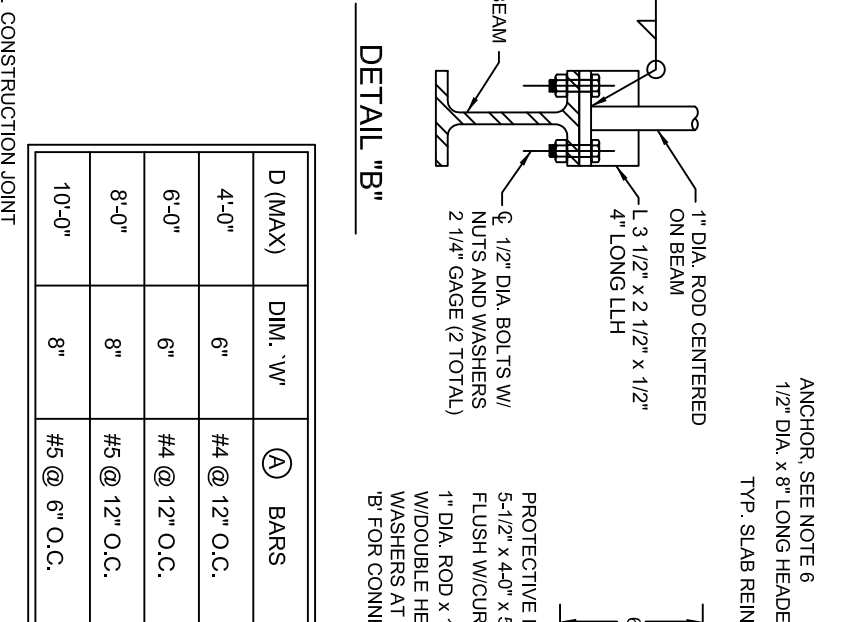
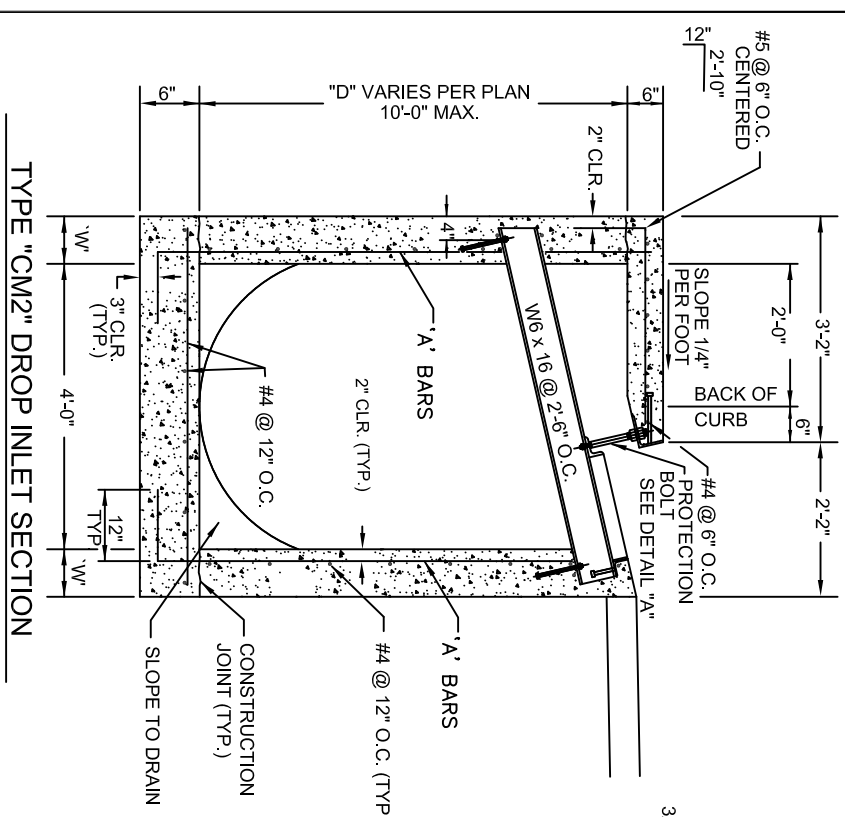
SIDE VIEW

NOTES:

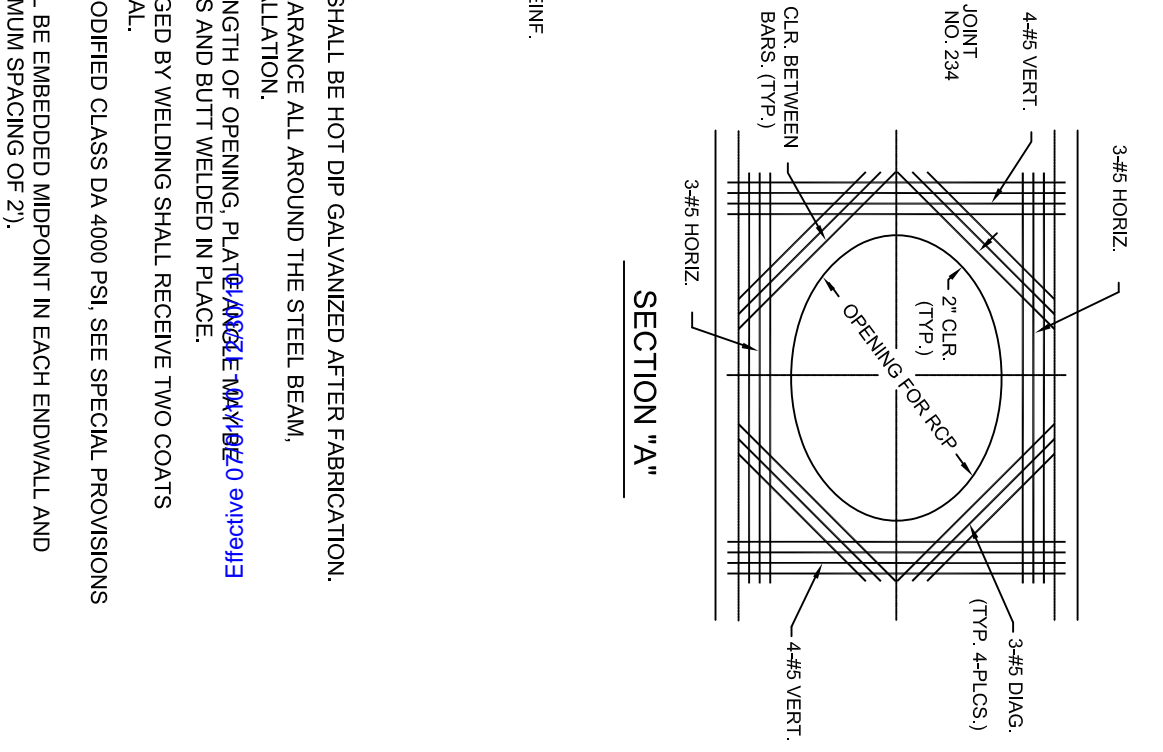
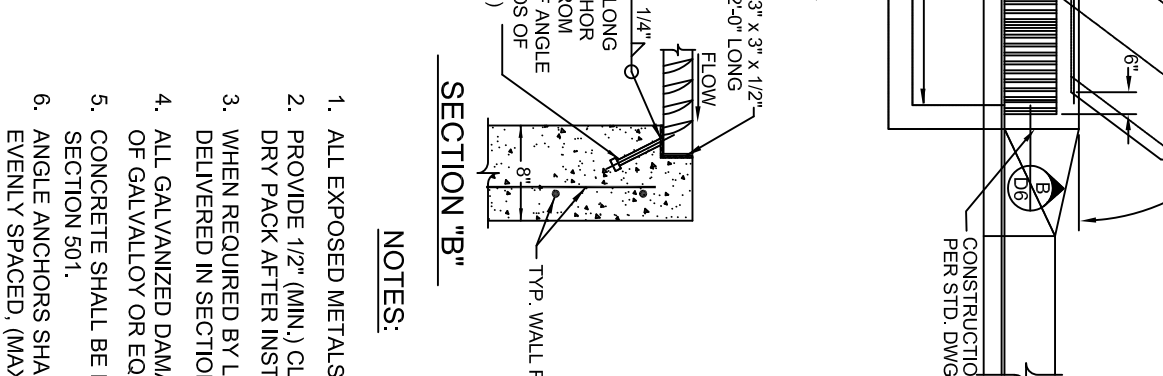
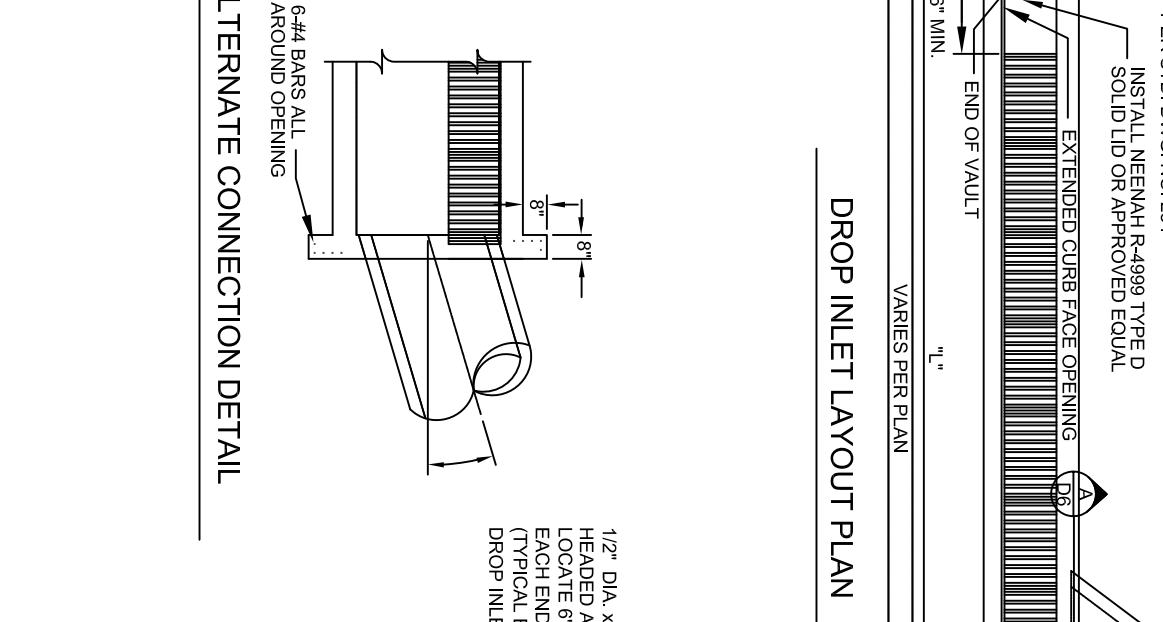
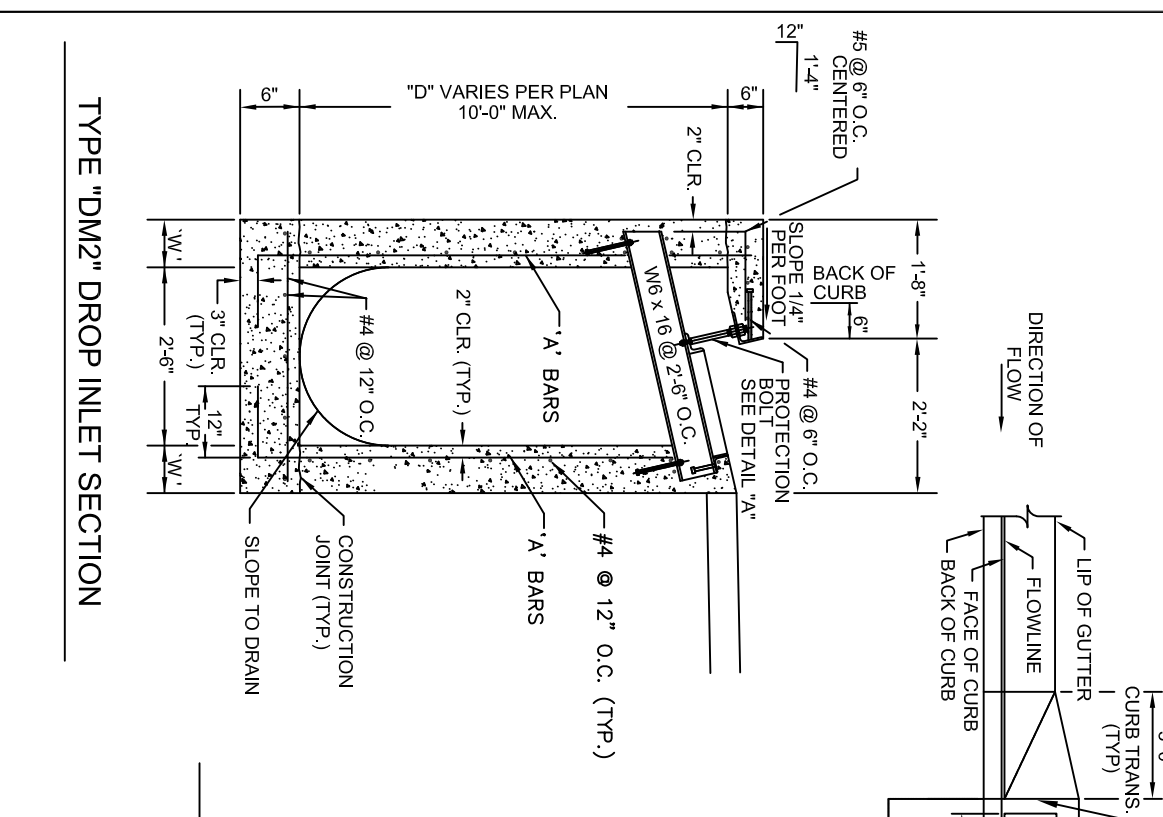
1. DEPTH "D" TO BE SHOWN ON PLANS.
2. OUTLET PIPE SIZE TO BE SHOWN ON PLANS.
3. CONCRETE SHALL BE CLASS "D" OR "DA".
4. OUTLET PIPE SHALL BE TRIMMED FLUSH WITH INSIDE FACE OF INLET.
5. FOR GRATE DETAIL SEE STANDARD DRAWING NO. 417

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
501	CONCRETE	DROP INLET TYPE "B"					
502	CONCRETE STRUCTURES						
505	REINFORCING STEEL						
713	STEEL						
		DATE	4-11-02	DWG. NO.	412	PAGE	112



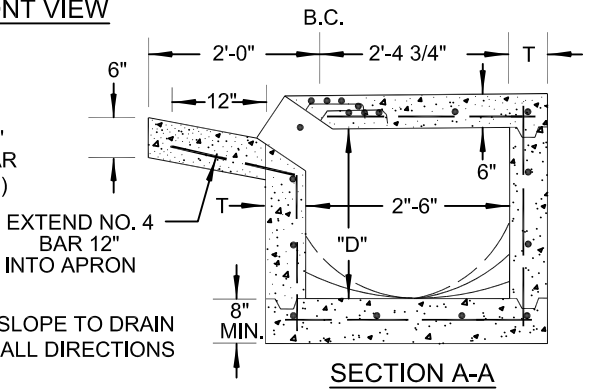
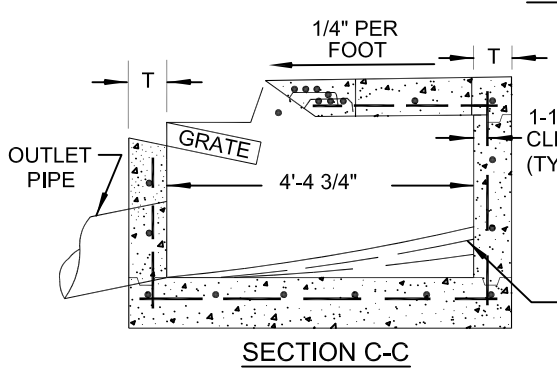
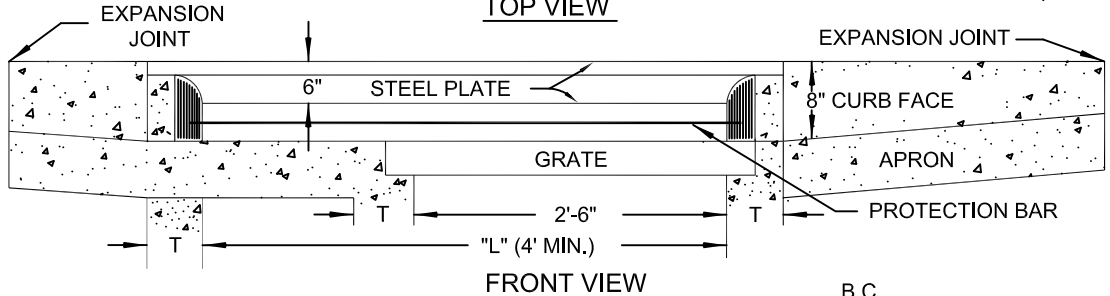
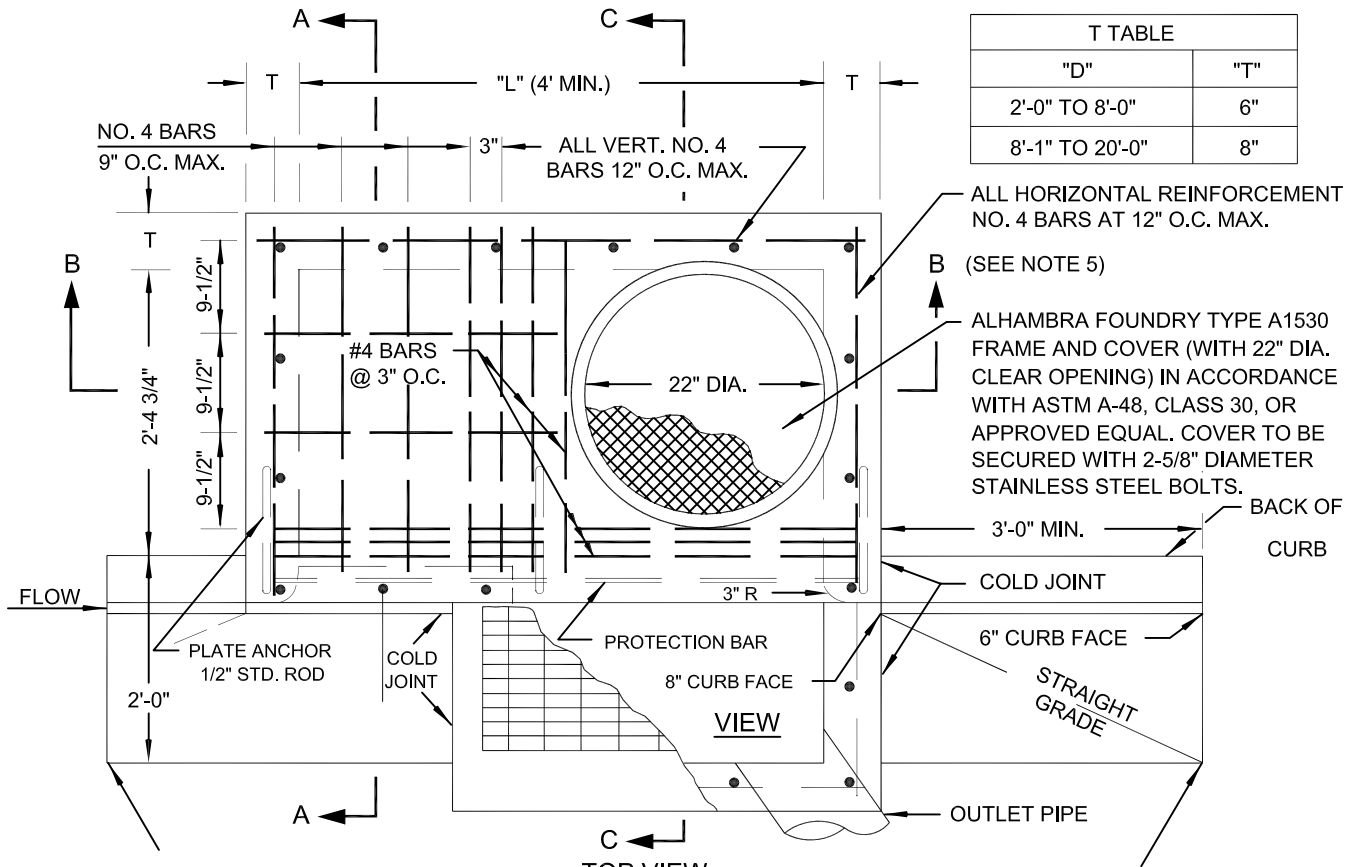
UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
TYPE C-D MODIFIED DROP INLET	
DATE : 10-9-08	DWG. NO. 412A



SPECIFICATION REFERENCE	
501	CONCRETE
502	CONCRETE STRUCTURES
505	REINFORCING STEEL
713	STEEL

- NOTES:**
1. ALL EXPOSED METALS SHALL BE HOT DIP GALVANIZED AFTER FABRICATION.
 2. PROVIDE 1/2" (MIN.) CLEARANCE ALL AROUND THE STEEL BEAM, DRY PACK AFTER INSTALLATION.
 3. WHEN REQUIRED BY LENGTH OF OPENING, PLATE ANCHORS SHALL BE DELIVERED IN SECTIONS AND BUTT WELDED IN PLACE.
 4. ALL GALVANIZED DAMAGED BY WELDING SHALL RECEIVE TWO COATS OF GALVALLOY OR EQUAL.
 5. CONCRETE SHALL BE MODIFIED CLASS DA 4000 PSI, SEE SPECIAL PROVISIONS SECTION 501.
 6. ANGLE ANCHORS SHALL BE EMBEDDED MIDPOINT IN EACH ENDWALL AND EVENLY SPACED. (MAXIMUM SPACING OF 2').

T TABLE	
"D"	"T"
2'-0" TO 8'-0"	6"
8'-1" TO 20'-0"	8"

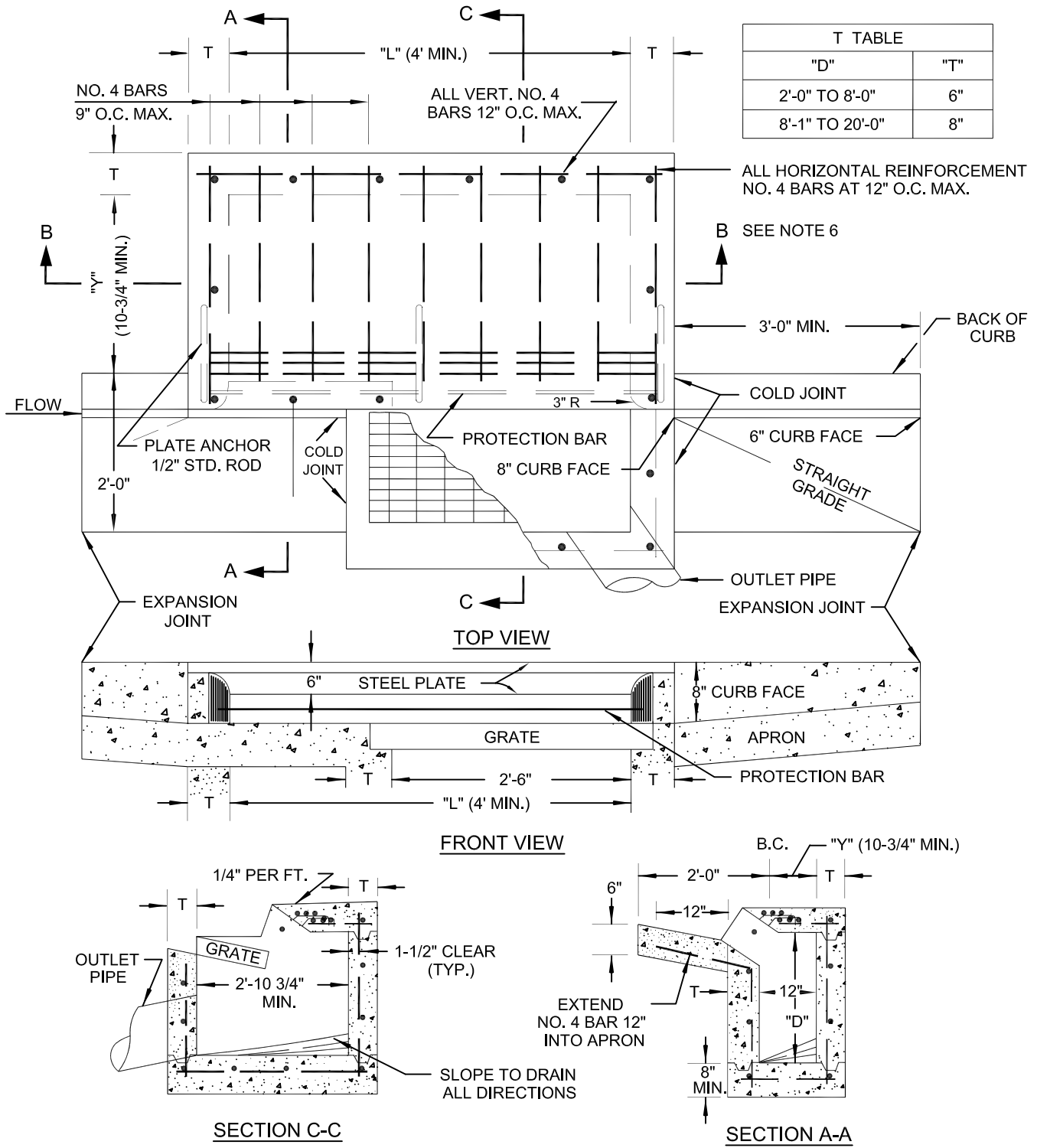


NOTES:

1. DEPTH "D" TO BE SHOWN ON PLANS.
2. OUTLET PIPE SIZE TO BE SHOWN ON PLANS.
3. WHEN LENGTH "L" EXCEEDS 4'-0" SUPPORT BOLTS REQUIRED, SEE STANDARD DRAWING NO. 418.
4. FOR GRATE DETAIL SEE STANDARD DRAWING NO. 417.
5. SECTION B-B IS OPTIONAL FOR INLETS WHERE L > 7'-0" AND D > 5'-0", SEE STANDARD DRAWING NO. 415.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	DROP INLET TYPE "C"		
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
713	STEEL			
		DATE 4-11-02	DWG. NO. 413	PAGE NO. 113

Effective 07/01/10 - 12/30/10

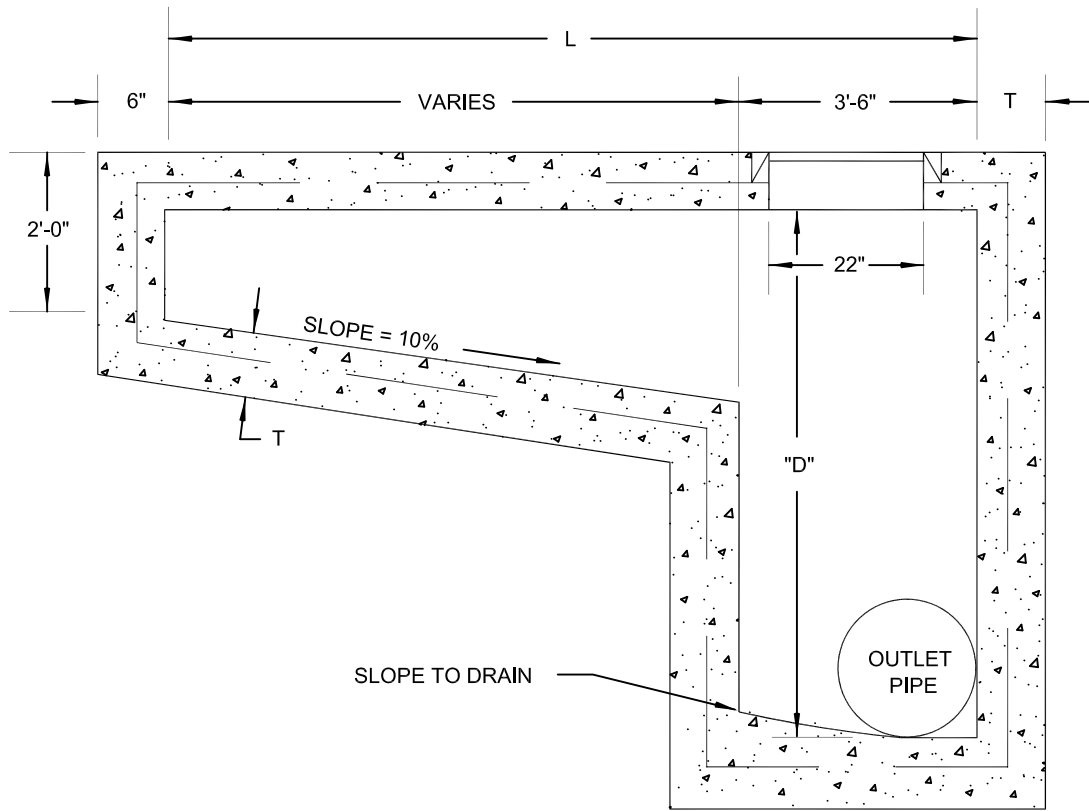


NOTES:

1. DROP INLET TYPE "D" TO BE USED WHEN CONFLICTING UTILITIES ARE LOCATED IN THE SIDEWALK AREA.
2. DEPTH "D" AND DISTANCE "Y" TO BE SHOWN ON PLANS.
3. OUTLET PIPE SIZE TO BE SHOWN ON PLANS.
4. WHEN LENGTH "L" EXCEEDS 4'-0" SUPPORT BOLTS REQUIRED, SEE STANDARD DRAWING NO. 418.
5. FOR GRATE DETAIL SEE STANDARD DRAWING NO. 417.
6. SECTION B-B IS OPTIONAL FOR INLETS WHERE L > 7'-0" AND D > 5'-0", SEE STANDARD DRAWING NO. 415.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
501	CONCRETE	DROP INLET TYPE "D"		
502	CONCRETE STRUCTURES			
505	REINFORCING STEEL			
713	STEEL			
		DATE 4-11-02	DWG. NO. 414	PAGE NO. 114

Effective 07/01/10 - 12/30/10



SECTION B-B

T TABLE	
"D"	"T"
2'-0" TO 8'-0"	6"
8'-1" TO 20'-0"	8"

NOTES:

1. DEPTH "D" TO BE SHOWN ON PLANS.
2. OUTLET PIPE SIZE TO BE SHOWN ON PLANS.
3. SECTION APPEARS AS SECTION B-B FOR DROP INLET TYPE "A", STANDARD DRAWING NO. 411 AND FOR DROP INLET TYPE "C" STANDARD DRAWING NO. 413.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

501 CONCRETE

502 CONCRETE STRUCTURES

DROP INLET TYPES "A" AND "B"
SPECIAL DESIGN

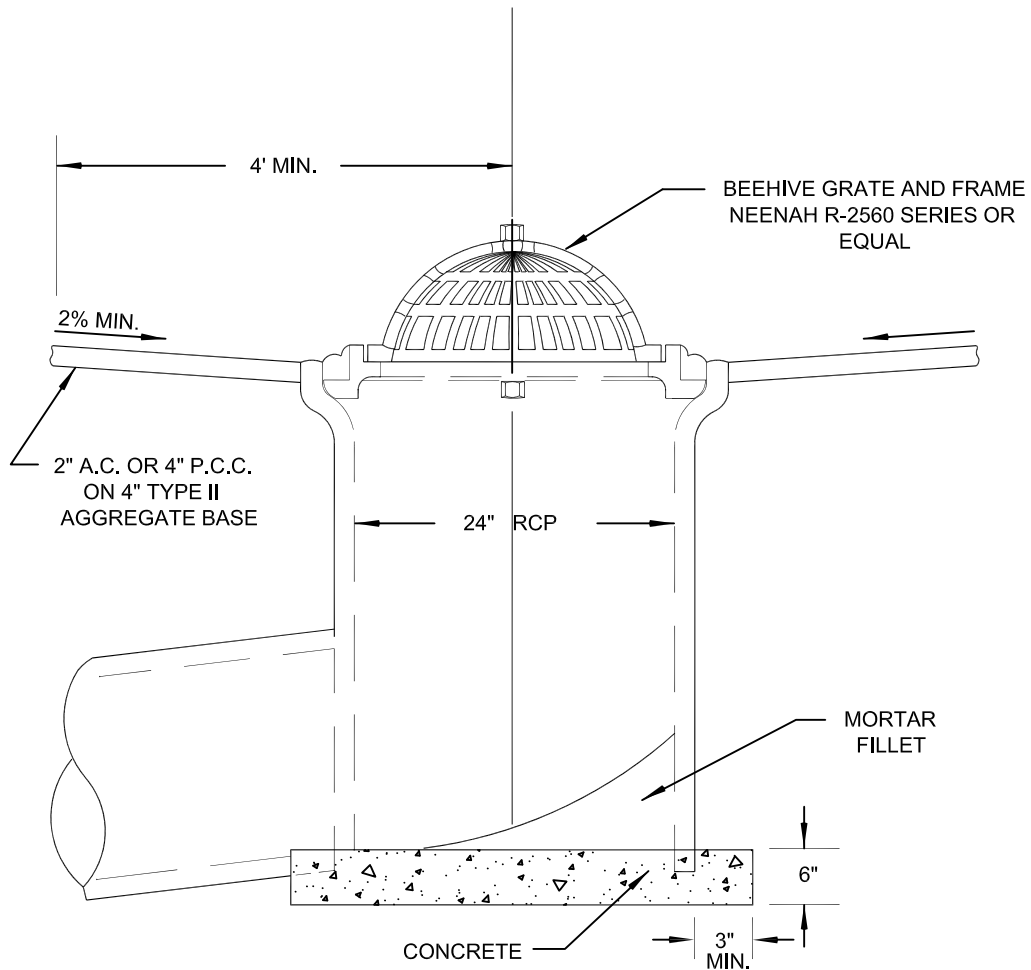
DATE

DWG. NO.

415

PAGE NO.

115



NOTE:

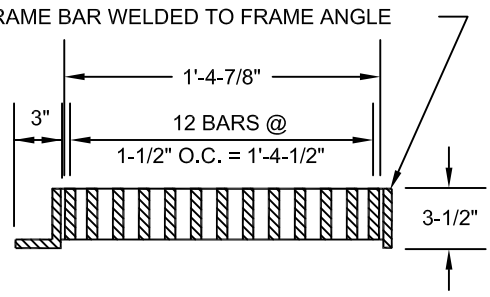
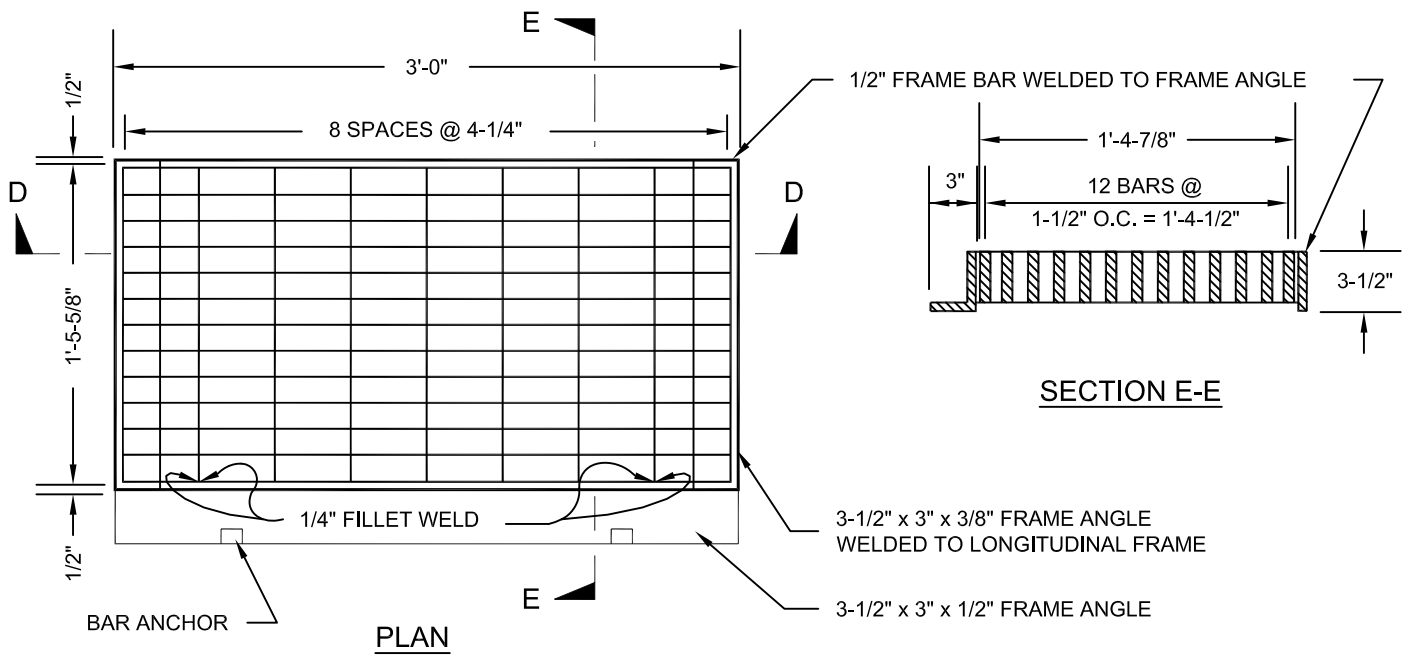
BEEHIVE DROP INLETS SHALL BE USED AT LOCATIONS APPROVED BY THE ENGINEER.

SPECIFICATION REFERENCE

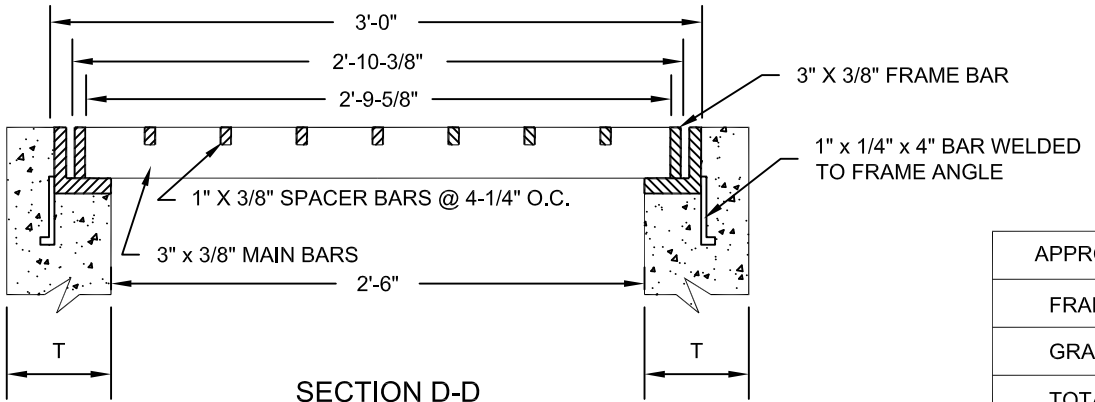
603	REINFORCED CONCRETE PIPE
712	MISCELLANEOUS METAL

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

BEEHIVE DROP INLET

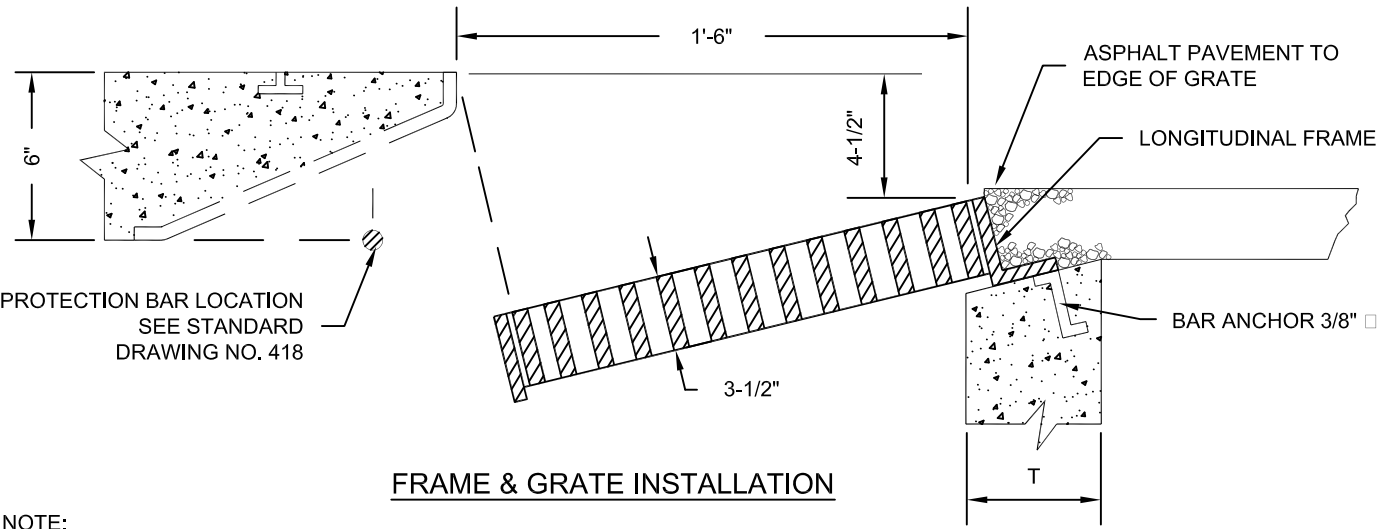


SECTION E-E



SECTION D-D

APPROXIMATE WEIGHT	
FRAME	78 LBS.
GRATE	157 LBS.
TOTAL	235 LBS.

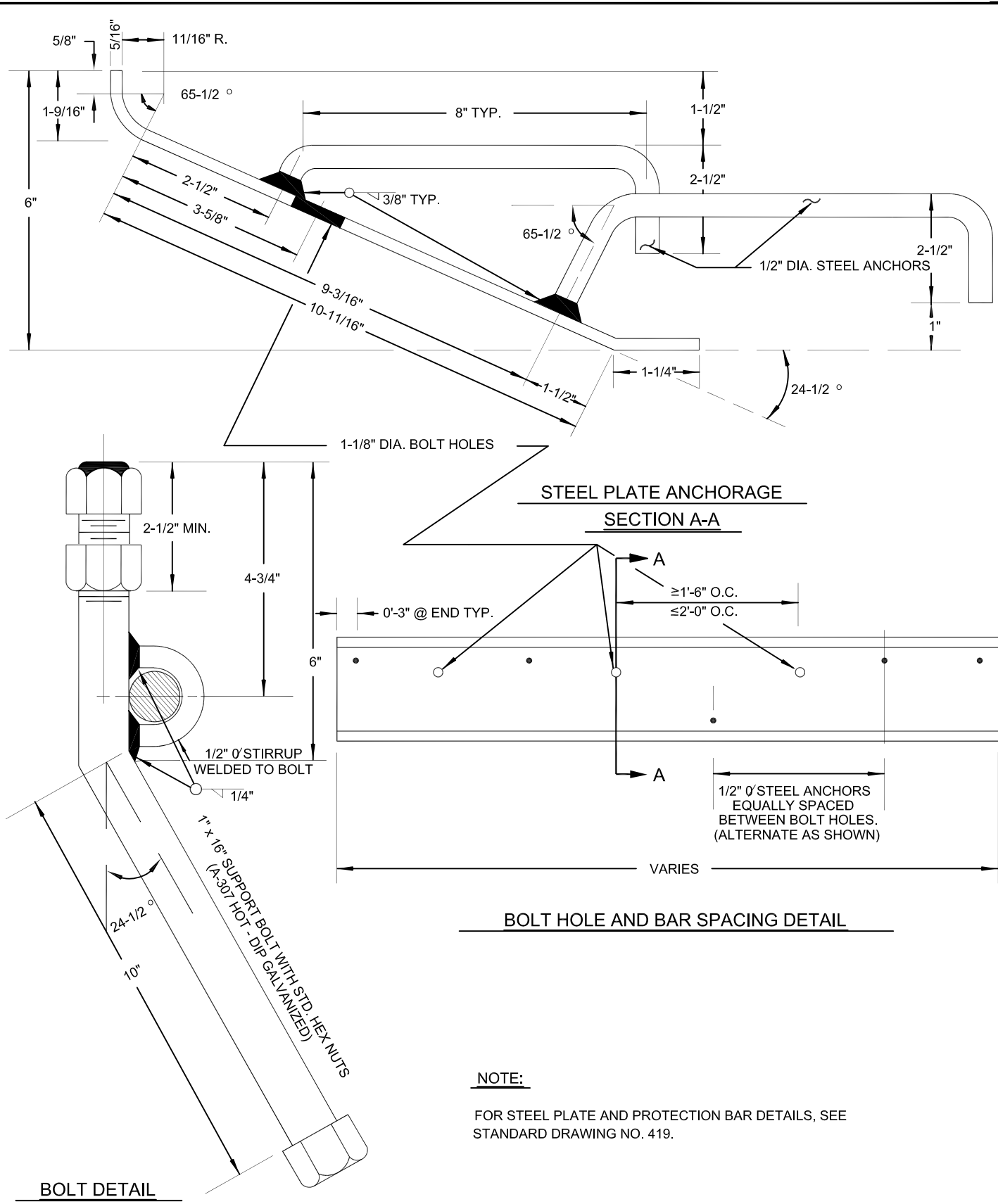


FRAME & GRATE INSTALLATION

NOTE:
 ALL EXPOSED METAL PARTS SHALL BE GALVANIZED AND ALL GALVANIZING DAMAGED BY FABRICATION OR INSTALLATION SHALL RECEIVE TWO COATS OF ALUMINUM PAINT (GALVONOX OR EQUAL).

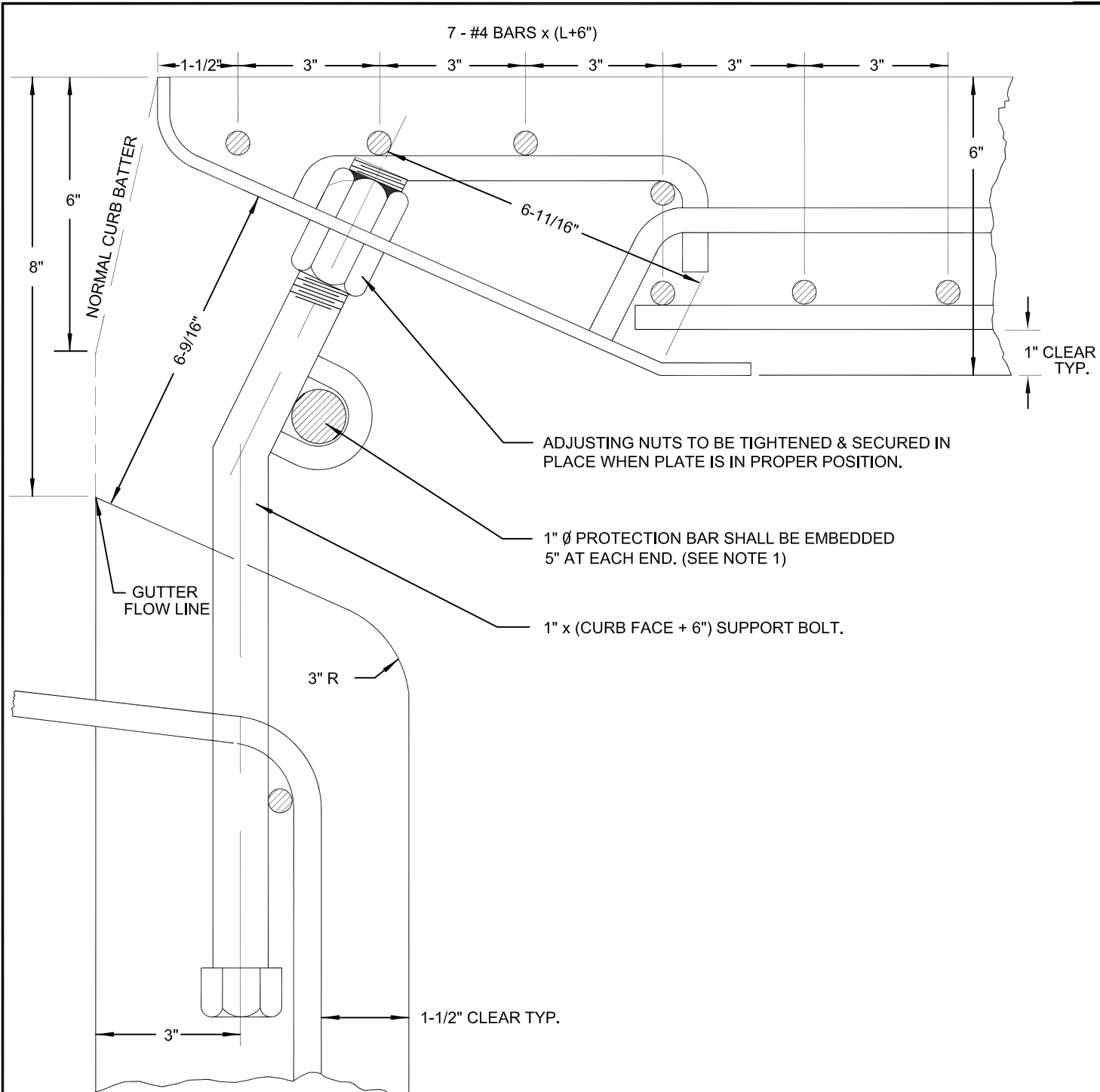
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
712	MISCELLANEOUS METAL	DROP INLET FRAME AND GRATE	
714	PAINT		
715	GALVANIZING		
		DATE 10-14-99	DWG. NO. 417
		PAGE 117	

Effective 07/01/10 - 12/30/10



Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
710	STRUCTURAL STEEL					
713	REINFORCEMENT STEEL	DROP INLET STEEL PLATE ANCHORAGE				
715	GALVANIZING					
		DATE	DWG. NO.	418	PAGE NO.	118

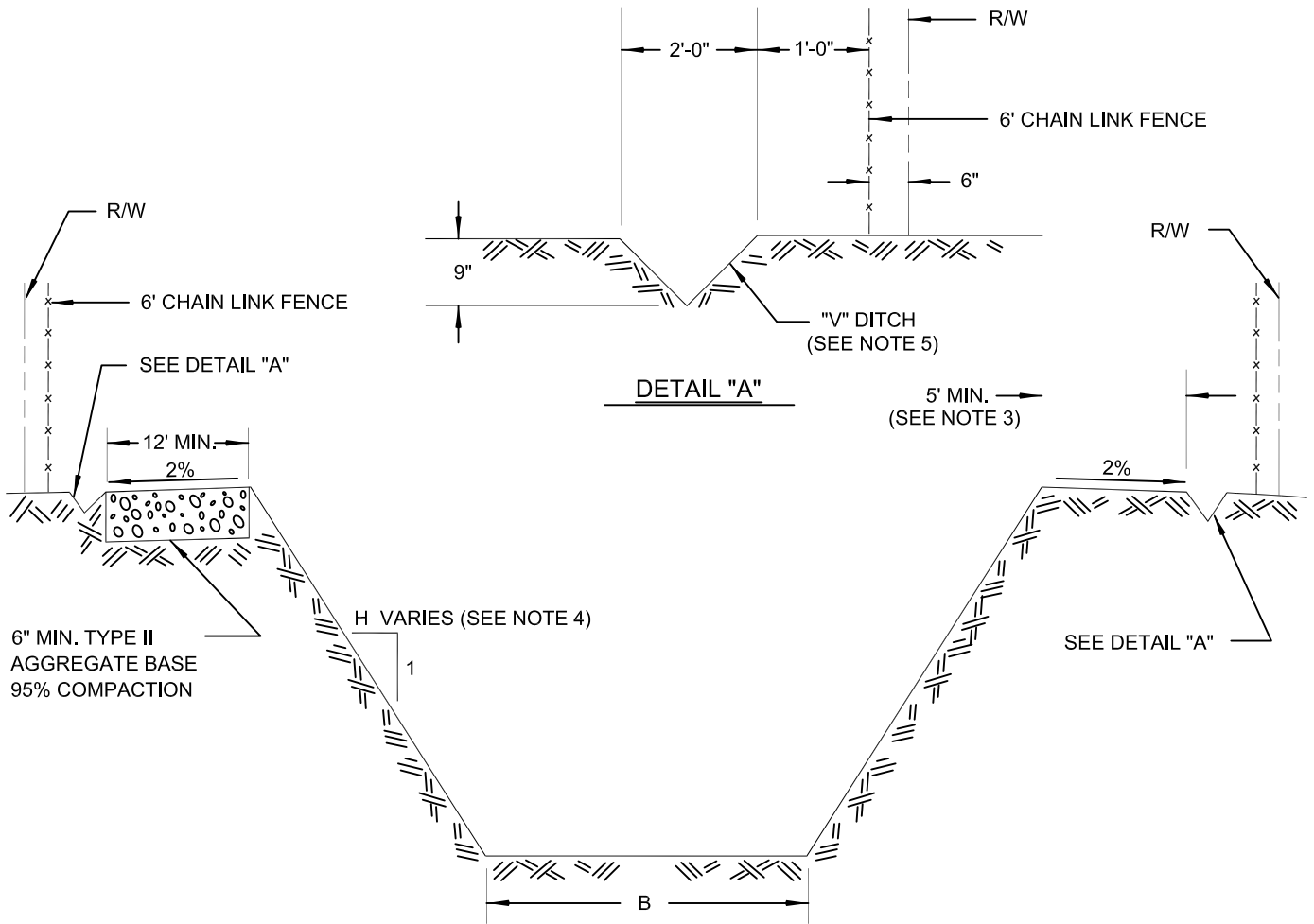


NOTES:

1. PROTECTION BAR SHALL BE REQUIRED ON ALL INLETS AND SHALL BE PLACED PARALLEL TO THE STEEL FACE PLATE.
2. SUPPORT BOLTS SHALL BE EQUALLY SPACED AT NOT MORE THAN 2'-0" O.C. AND NOT LESS THAN 1'-6" O.C.
3. ALL EXPOSED METAL PARTS SHALL BE GALVANIZED AND GALVANIZING DAMAGED BY FABRICATION OR INSTALLATION SHALL RECEIVE TWO COATS OF ALUMINUM PAINT (GALVONOX OR EQUAL).
4. FOR STEEL PLATE ANCHORAGE, SEE STANDARD DRAWING NO. 418.
5. #4 BARS x (L+6") SHALL BE IN ADDITION TO REINFORCING STEEL PER APPLICABLE DROP INLET STANDARD PLAN.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA				
713	REINFORCEMENT PLATES					
714	PAINT	DROP INLET STEEL PLATE AND PROTECTION BAR				
715	GALVANIZING					
		DATE	DWG. NO.	419	PAGE NO.	119

Effective 07/01/10 - 12/30/10



NOTES:

1. CONSTRUCT 14-FOOT WIDE CHAIN LINK GATE AT ALL STREET ACCESS POINTS, FOR ACCESS ONTO 12-FOOT ACCESS ROADS.
2. CONSTRUCT 3-FOOT WIDE CHAIN LINK GATE AT ALL STREET ACCESS POINTS FOR ACCESS ON THE 5-FOOT WIDTH ACCESS SIDE.
3. CONSTRUCT SECOND ACCESS ROAD (12-FOOT MINIMUM WIDTH WITH 6-INCH MIN. TYPE II AGGREGATE BASE) IF B EXCEEDS 30'.
4. FOR UNLINED CHANNELS $H \geq 2$.
5. "V" DITCH SHALL BE CONSTRUCTED TO PREVENT OVERLAND RUNOFF FROM ERODING SIDES OF BANK. AN ADEQUATE NUMBER OF INLETS ALONG THE "V" DITCH SHALL BE DESIGNED WITH A MINIMUM 12-INCH CMP LATERAL DISCHARGING INTO THE CHANNEL. APPROPRIATE BANK PROTECTION FOR LATERAL PIPE DISCHARGE SHALL BE PROVIDED. OTHER METHODS OF OVERLAND RUNOFF CONTROL MAY BE ACCEPTABLE IF APPROVED BY THE ENGINEER.

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL OPEN CHANNEL

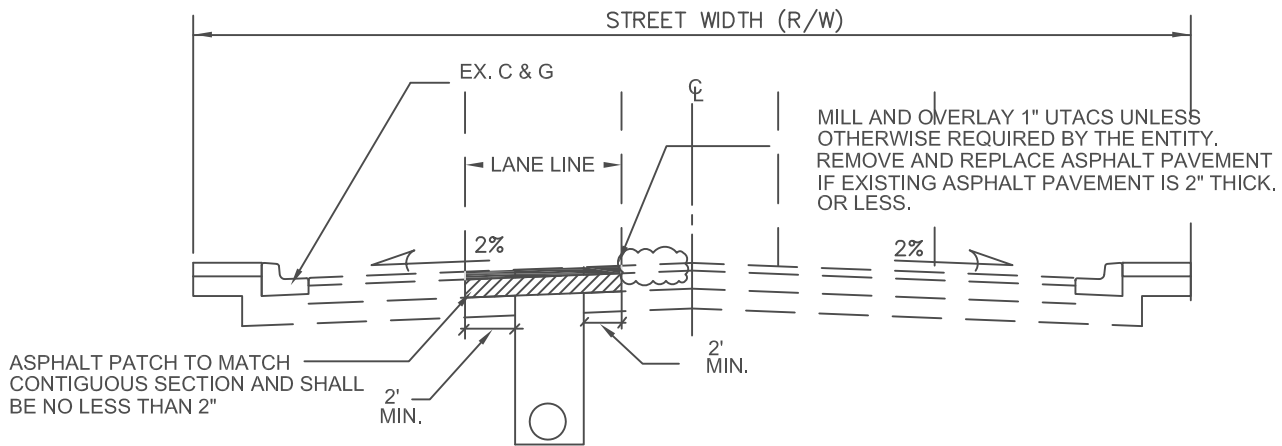
DATE

DWG. NO.

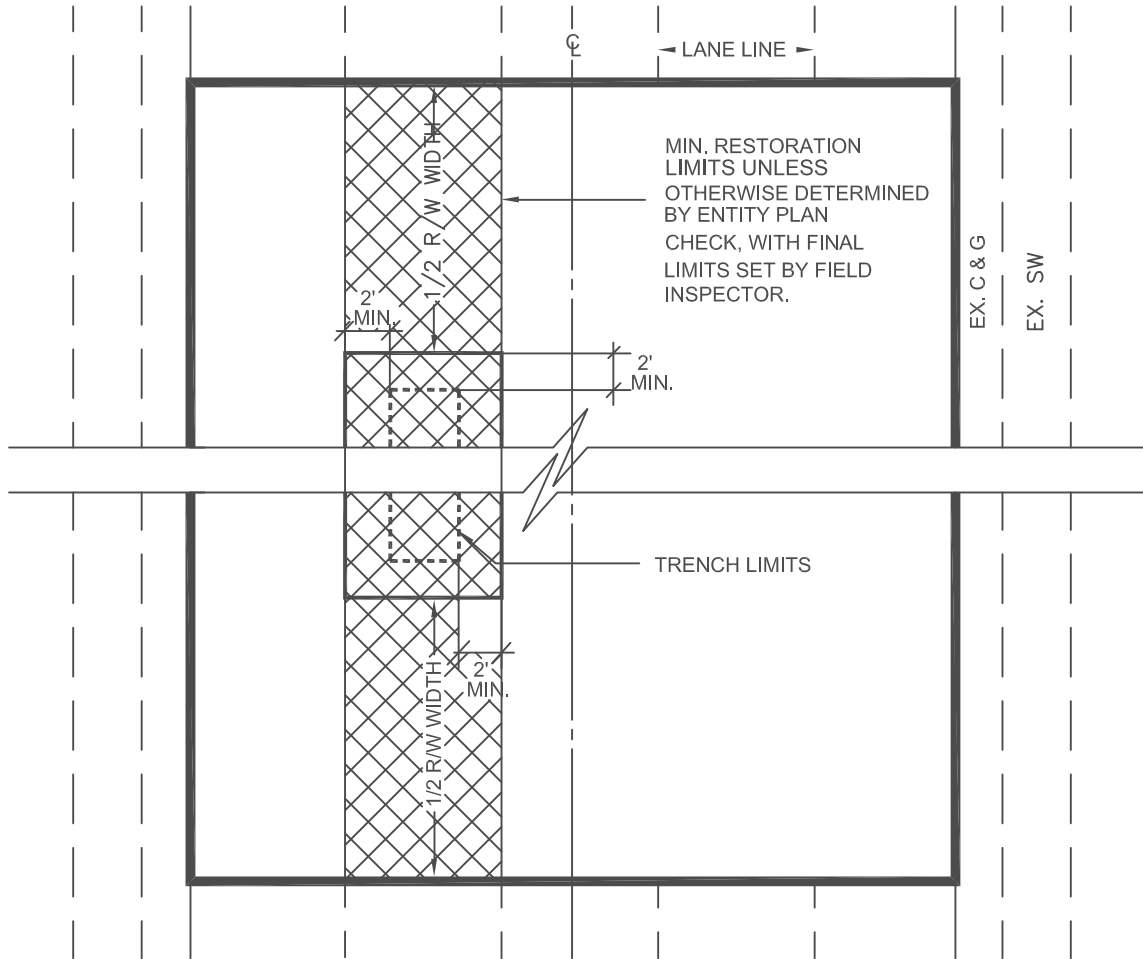
420

PAGE NO.

120



LONGITUDINAL CUT RESTORATION



PLAN VIEW

NOTES:

SEE DWG. 500AL SHEET 2 OF 2

Effective 07/01/10 - 12/30/10

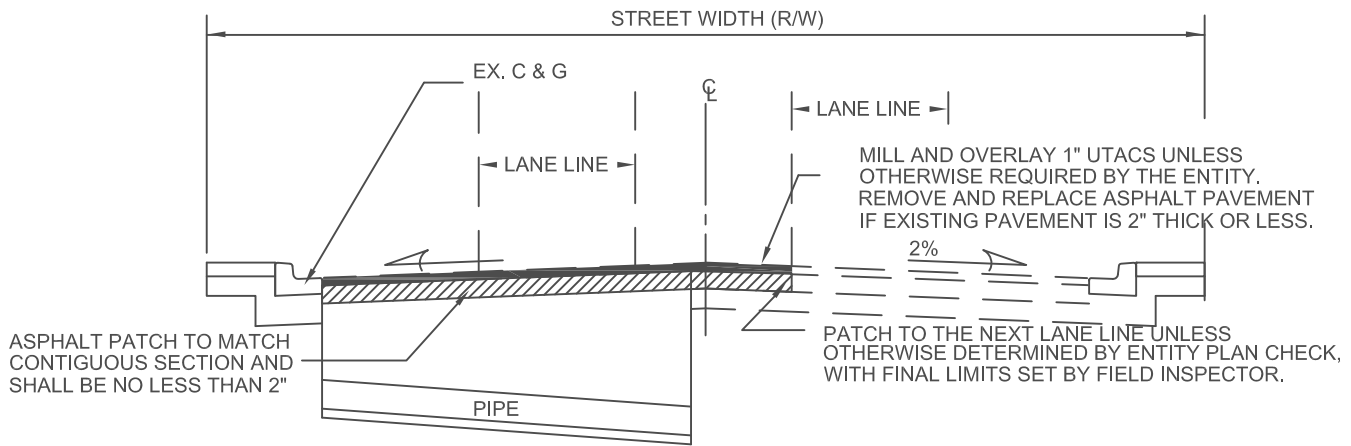
SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	0 TO 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE 6-12-08	DWG. NO. 500AL
		SHEET 1 OF 2	

NOTES:

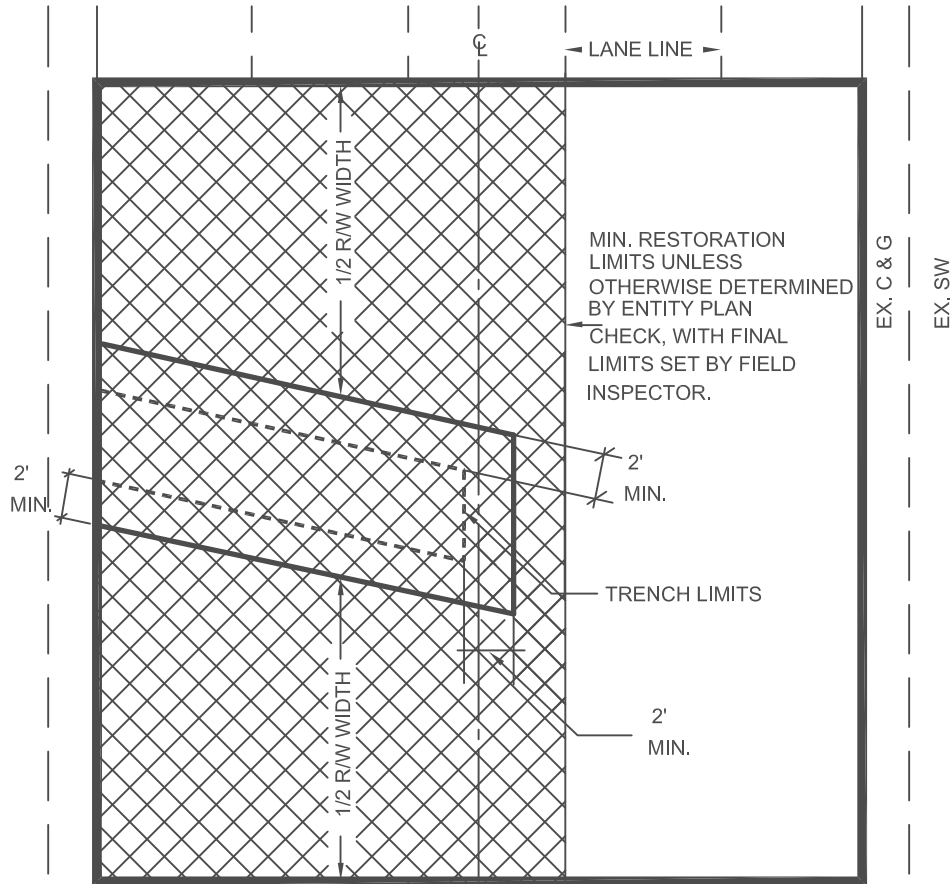
1. IF THERE IS A MEDIAN, RESTORATION MAY BE LIMITED TO THE AREA BETWEEN C & G AND THE MEDIAN CURB.
2. WHEN EXISTING PAVEMENT IS 2" THICK OR LESS, PAVEMENT WITHIN THE RESTORATION AREA SHALL BE REMOVED AND REPLACED IN KIND AS REQUIRED BY THE ENTITY.
3. IF SAWCUT LINE IS WITHIN FIVE FEET OF EDGE OF EXISTING ASPHALT CONCRETE SURFACE OR EXISTING SAWCUT LINE, MILL AND OVERLAY OR REPLACE TO THAT EDGE.
4. PAVEMENT RESTORATION AREA SAWCUT LINES SHALL NOT FALL WITHIN STREET INTERSECTION.
5. IF CUT IS WITHIN A LANE , PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.
6. THE ENTITY'S REQUIREMENTS TAKE PRECEDENCE OVER ANY MINIMUM REQUIREMENTS SHOWN HEREON.

Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	0 TO 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE 6-12-08	DWG. NO. 500AL
		SHEET 2 OF 2	



TRANSVERSE CUT RESTORATION

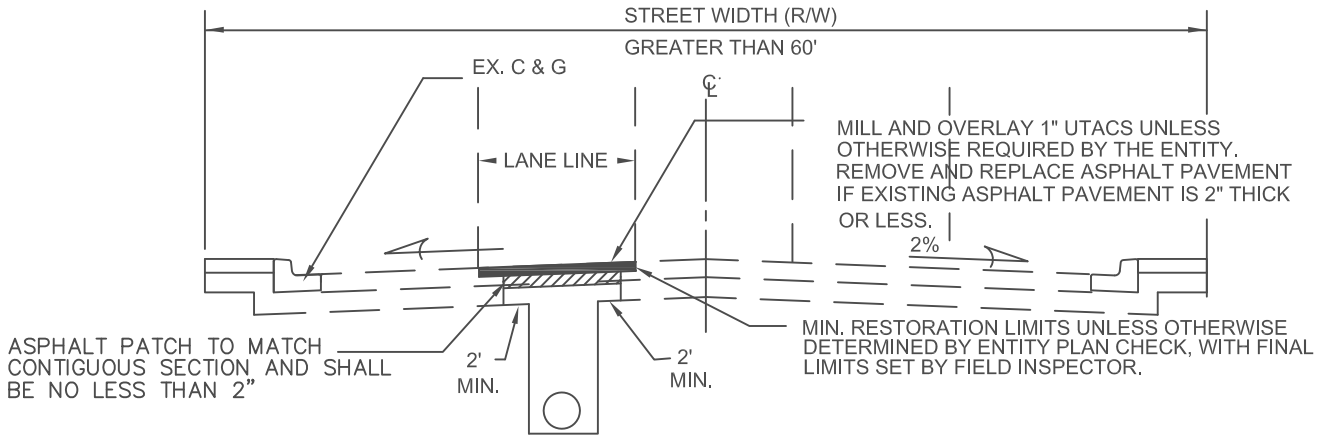


PLAN VIEW

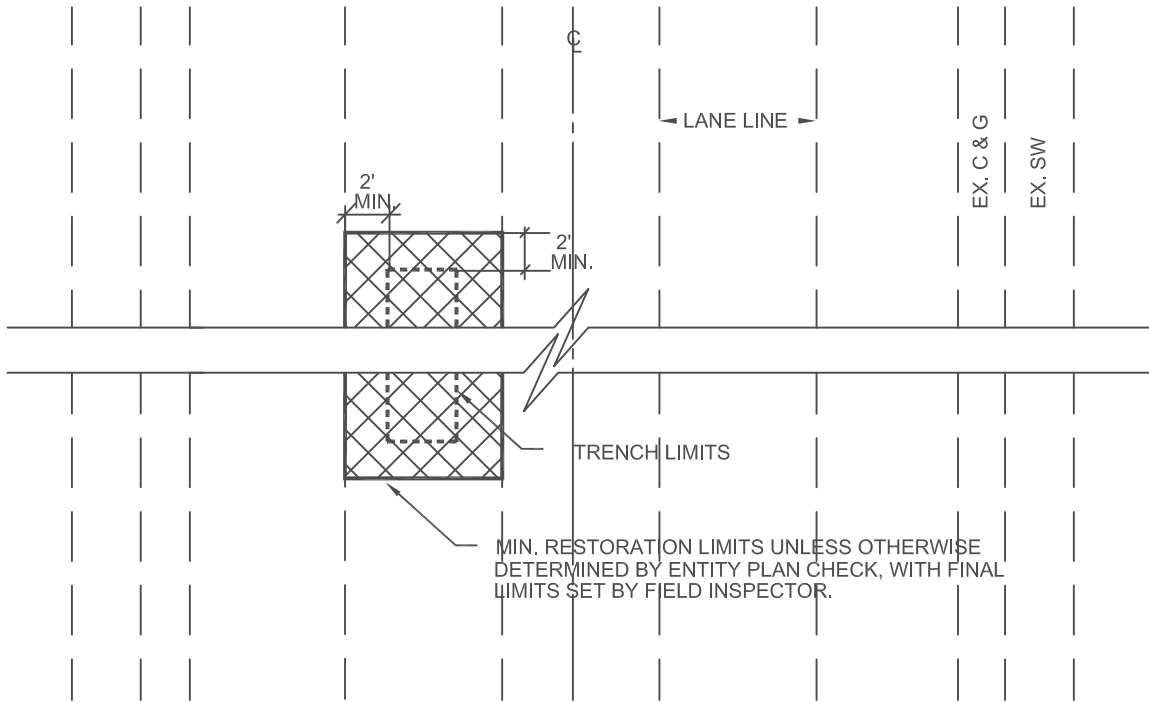
NOTES:

1. IF THERE IS MEDIAN, RESTORATION MAY BE LIMITED TO THE AREA BETWEEN C&G TO CURB OF MEDIAN.
2. IF SAWCUT LINE IS WITHIN FIVE FEET OF EDGE OF EXISTING ASPHALT CONCRETE SURFACE OR EXISTING SAWCUT LINE, MILL AND OVERLAY OR REPLACE TO THAT EDGE.
3. WHEN EXISTING PAVEMENT IS LESS 2" THICK OR LESS, PAVEMENT WITHIN THE RESTORATION AREA SHALL BE REMOVED AND REPLACED IN KIND AS REQUIRED BY THE ENTITY.
4. PAVEMENT RESTORATION AREA SAWCUT LINES SHALL NOT FALL WITHIN STREET INTERSECTION.
5. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.
6. THE ENTITY'S REQUIREMENTS TAKE PRECEDENCE OVER ANY MINIMUM REQUIREMENTS SHOWN HEREON.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	0 TO 5 YEARS PAVEMENT RESTORATION TRANSVERSE CUT	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE 6-12-08	DWG. NO. 500 AT



LONGITUDINAL CUT RESTORATION

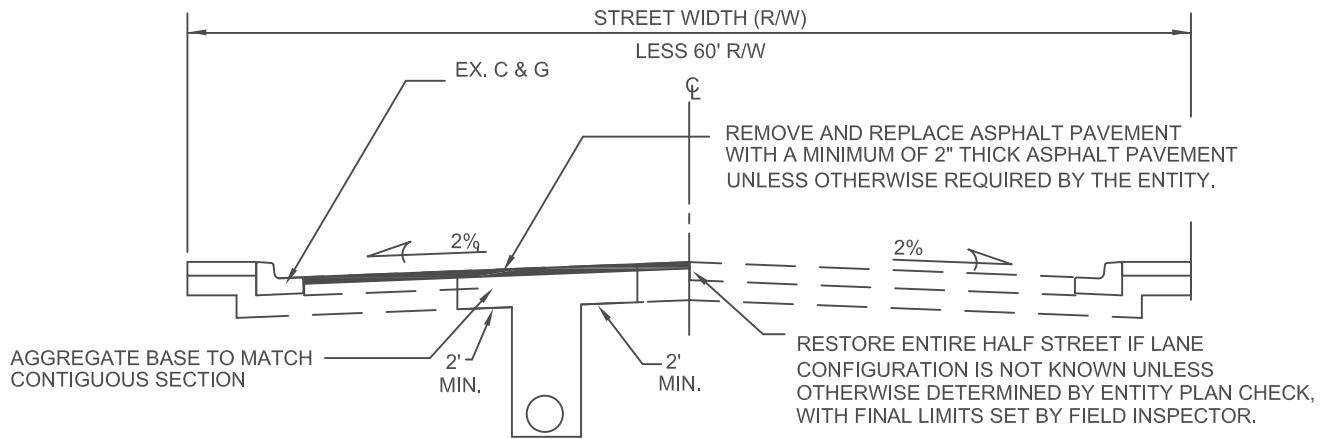


PLAN VIEW

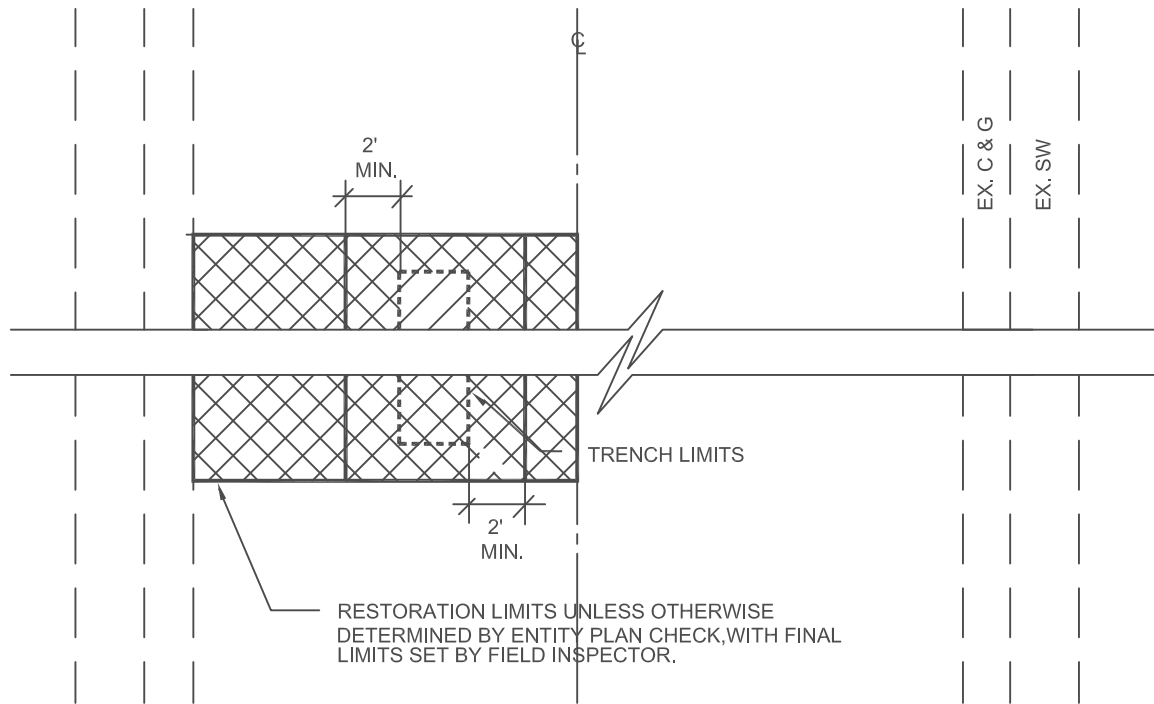
NOTES:

1. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.
2. THE ENTITY'S REQUIREMENTS TAKE PRECEDENCE OVER ANY MINIMUM REQUIREMENTS SHOWN HEREON.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	OVER 5 YEARS PAVEMENT RESTORATION - LONGITUDINAL CUT- GREATER THAN 60' R/W	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE 6-12-08	DWG. NO. 500BL1



LONGITUDINAL CUT RESTORATION

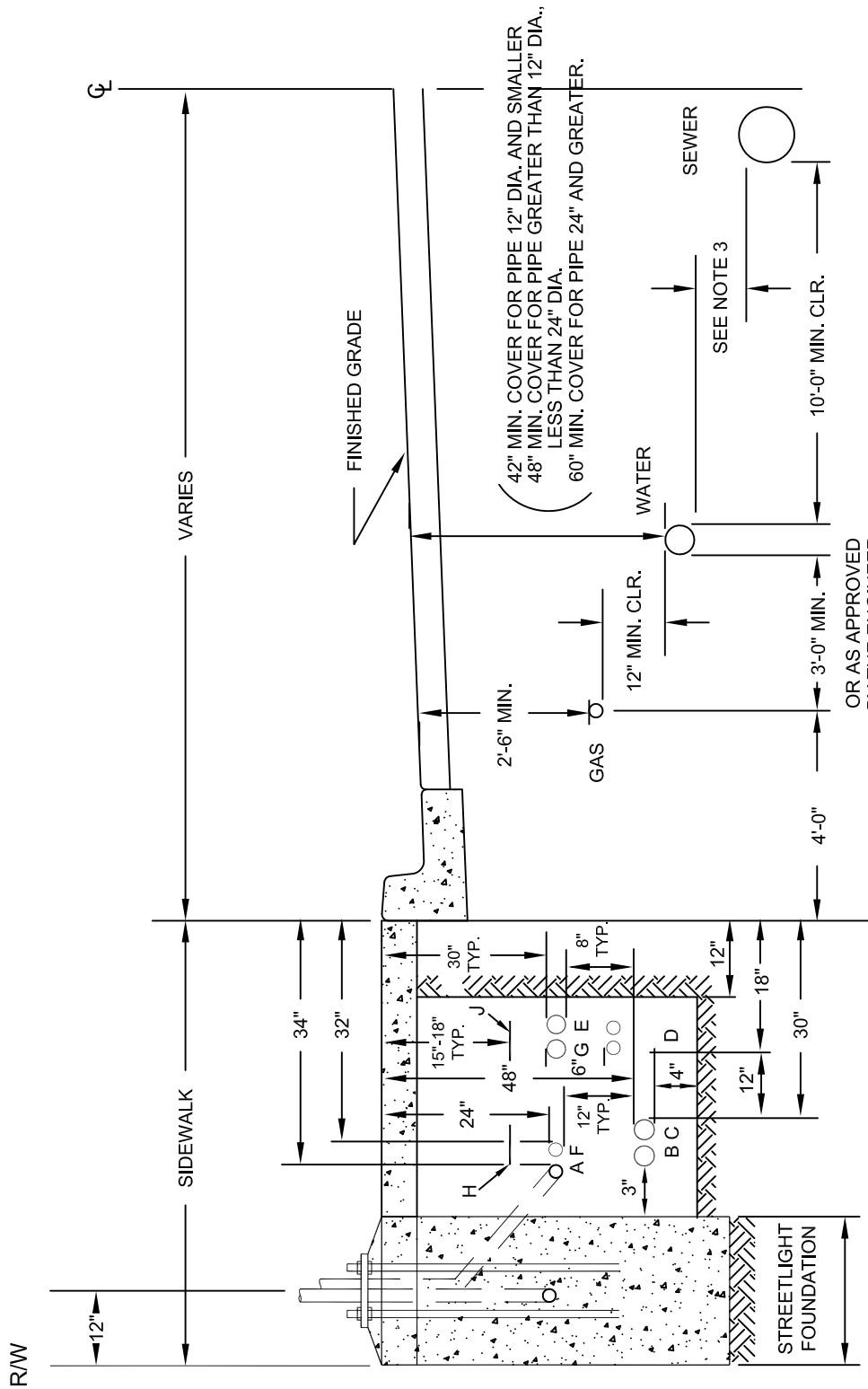


PLAN VIEW

NOTES:

1. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.
2. THE ENTITY'S REQUIREMENTS TAKE PRECEDENCE OVER ANY MINIMUM REQUIREMENTS SHOWN HEREON.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
302	AGGREGATE BASE	OVER 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT - 60' R/W OR LESS	
401	BITUMINOUS PAVEMENT		
406	PRIME COAT		
407	FOG SEAL		
501	CONCRETE		
		DATE 6-12-08	DWG. NO. 500BL2



OR AS APPROVED BY THE ENGINEER

NOTES:

1. SEWER MAY BE LOCATED ON OTHER SIDE OF CENTERLINE AS TERRAIN DICTATES.
2. STREETLIGHT FOUNDATIONS SHALL BE LOCATED BEHIND SIDEWALK FOR SIDEWALK WIDTHS LESS THAN 5 FEET PER STANDARD DRAWING NO. 320.
3. SEPARATION DISTANCE SHALL CONFORM TO UTILITY STANDARDS ADOPTED BY THE GOVERNING AGENCY FOR SEWER AND WATER FACILITIES.
4. STREET CONSTRUCTION SHALL CONFORM TO THE DESIGNED PLANS.
5. UTILITY CONSTRUCTION BACKFILL SHALL CONFORM TO SECTION 208.
6. UTILITY LINES SHALL BE RE-ROUTED IF DROP INLET IS IN CONFLICT.
7. WATER TRANSMISSION MAIN SEPARATION SHALL BE REFERRED TO WATER PURVEYOR GUIDELINES.

- A. STREETLIGHT CONDUIT
- B. POWER COMPANY SECONDARY
- C. POWER COMPANY PRIMARY
- D. TELEPHONE CONDUITS
- E. CABLE T.V. CONDUIT
- F. TRAFFIC SIGNAL CONDUIT
- G. OTHER COMMUNICATIONS CONDUIT
- H. POWER MARKING TAPE
- J. TELEPHONE MARKING TAPE

SPECIFICATION REFERENCE

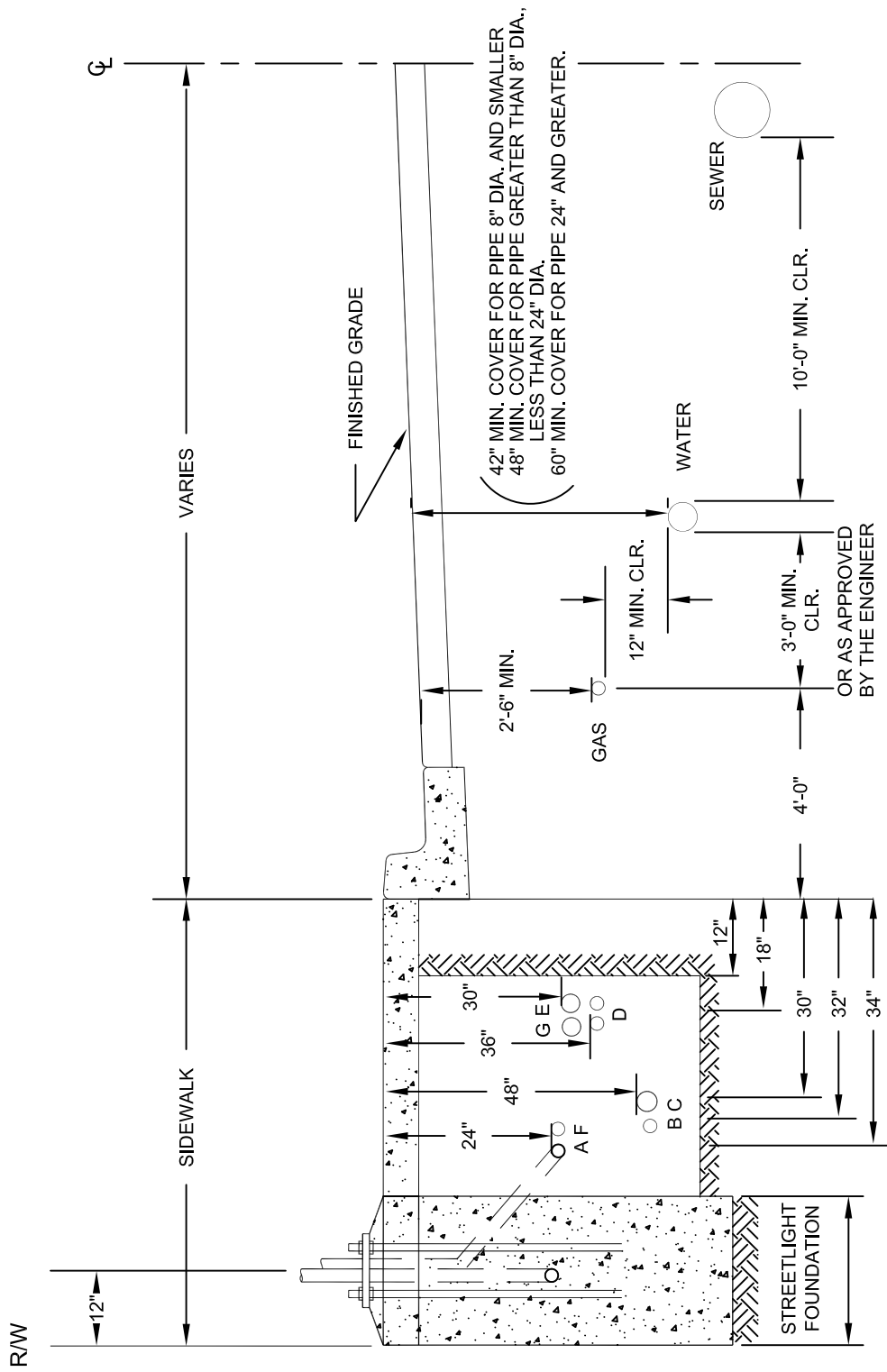
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL UNDERGROUND UTILITY
LOCATIONS IN RESIDENTIAL STREETS
WITH 60 FEET OR LESS RIGHT-OF-WAY

DATE 2-9-06

DWG. NO.

501



- A. STREETLIGHT CONDUIT
- B. POWER COMPANY SECONDARY
- C. POWER COMPANY PRIMARY
- D. TELEPHONE CONDUITS
- E. CABLE T.V. CONDUIT
- F. TRAFFIC SIGNAL CONDUIT
- G. OTHER COMMUNICATIONS CONDUIT

NOTE:

SEWER MAY BE OTHER SIDE OF CENTERLINE AS TERRAIN DICTATES.

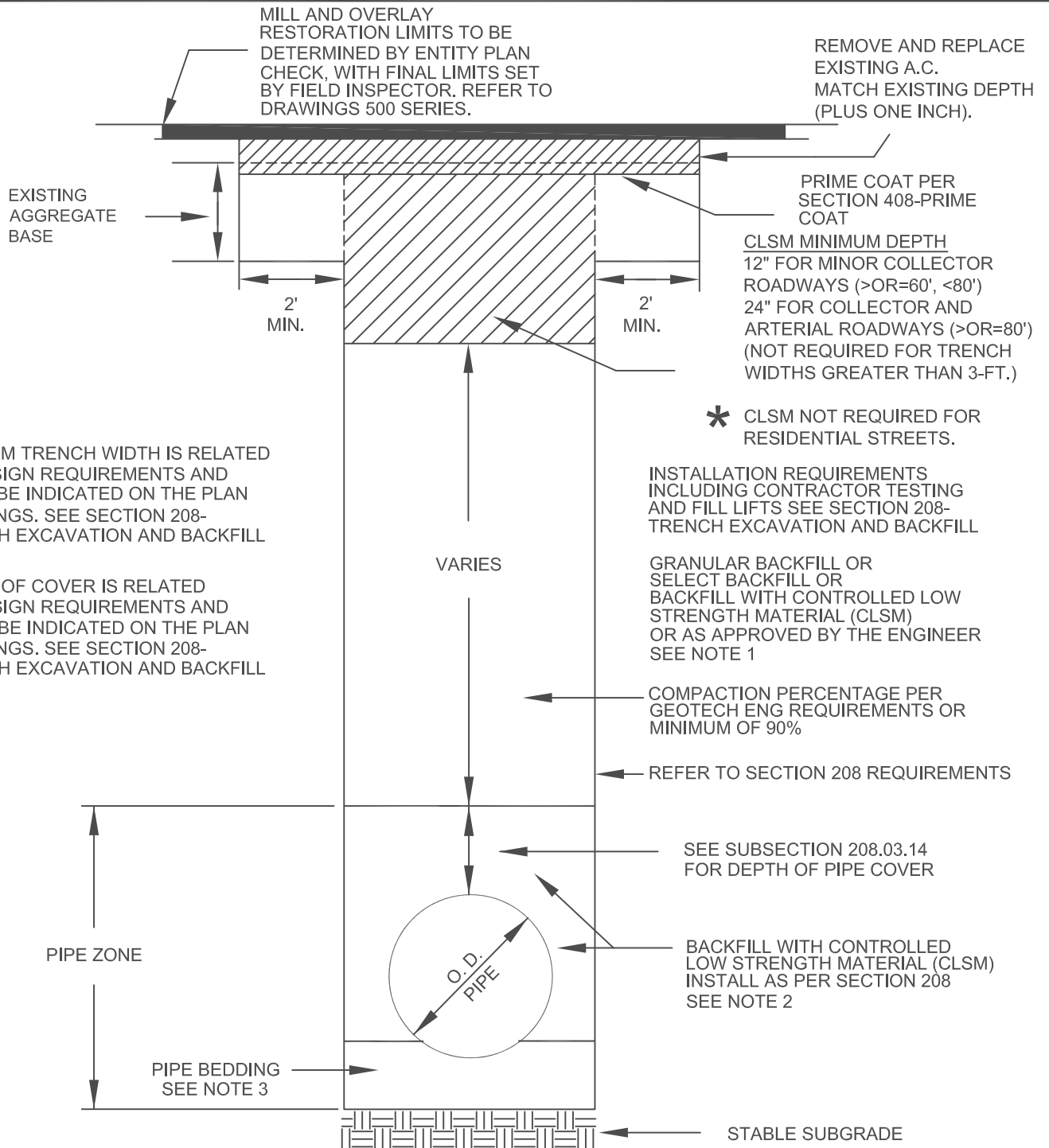
OR AS APPROVED BY THE ENGINEER

SPECIFICATION REFERENCE

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

TYPICAL UNDERGROUND
UTILITY LOCATIONS IN STREETS
WITH GREATER THAN 60 FEET RIGHT-OF-WAY

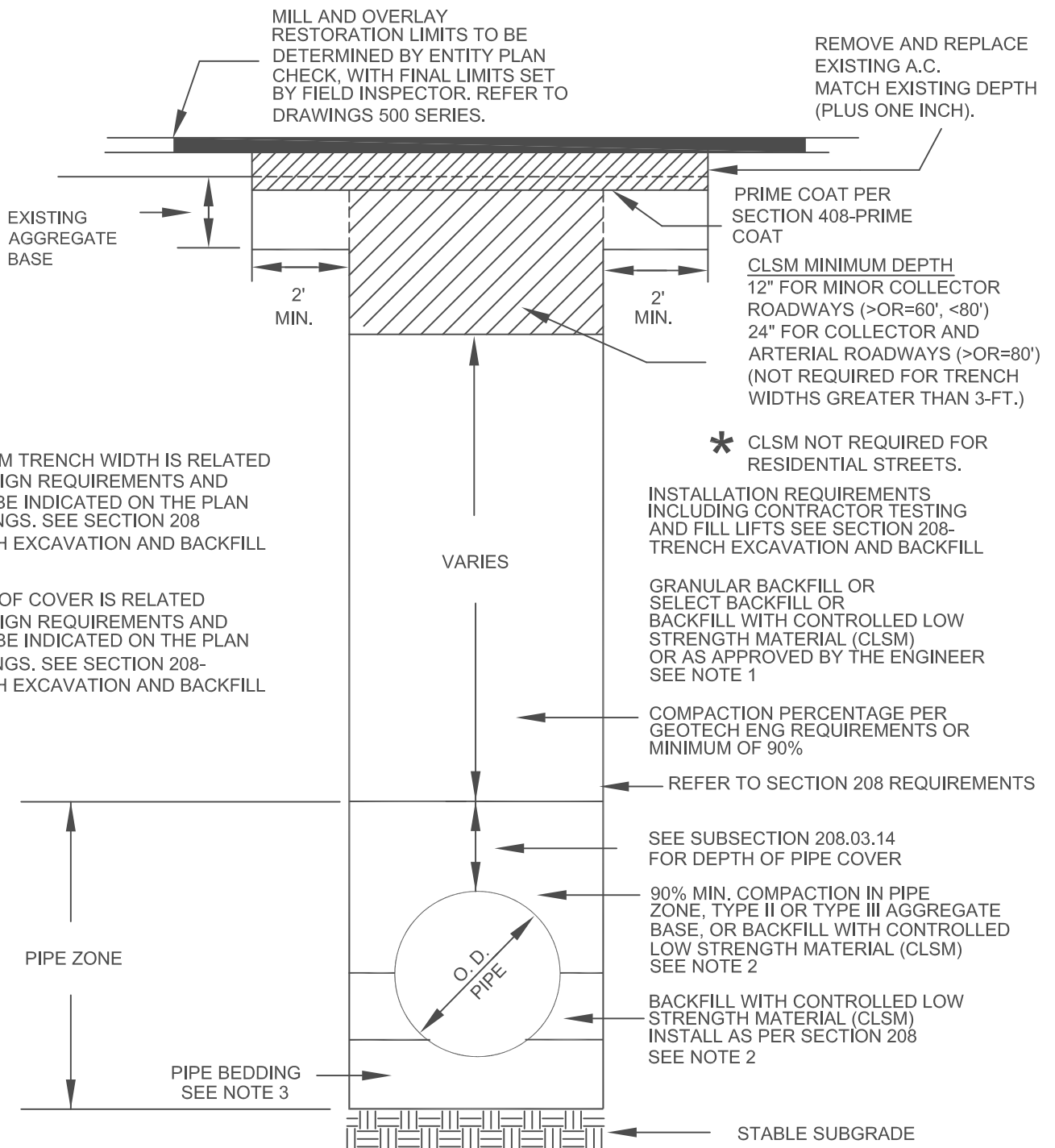
DATE 2-9-06 DWG. NO. 501A



NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
208	TRENCH EXCAVATION & BACKFILL	METHOD A FOR FLEXIBLE PIPE TRENCH BACKFILL - PAVED AREAS	
302	AGGREGATE BASE COURSES		
		DATE 06-11-09	DWG. NO. 503AF

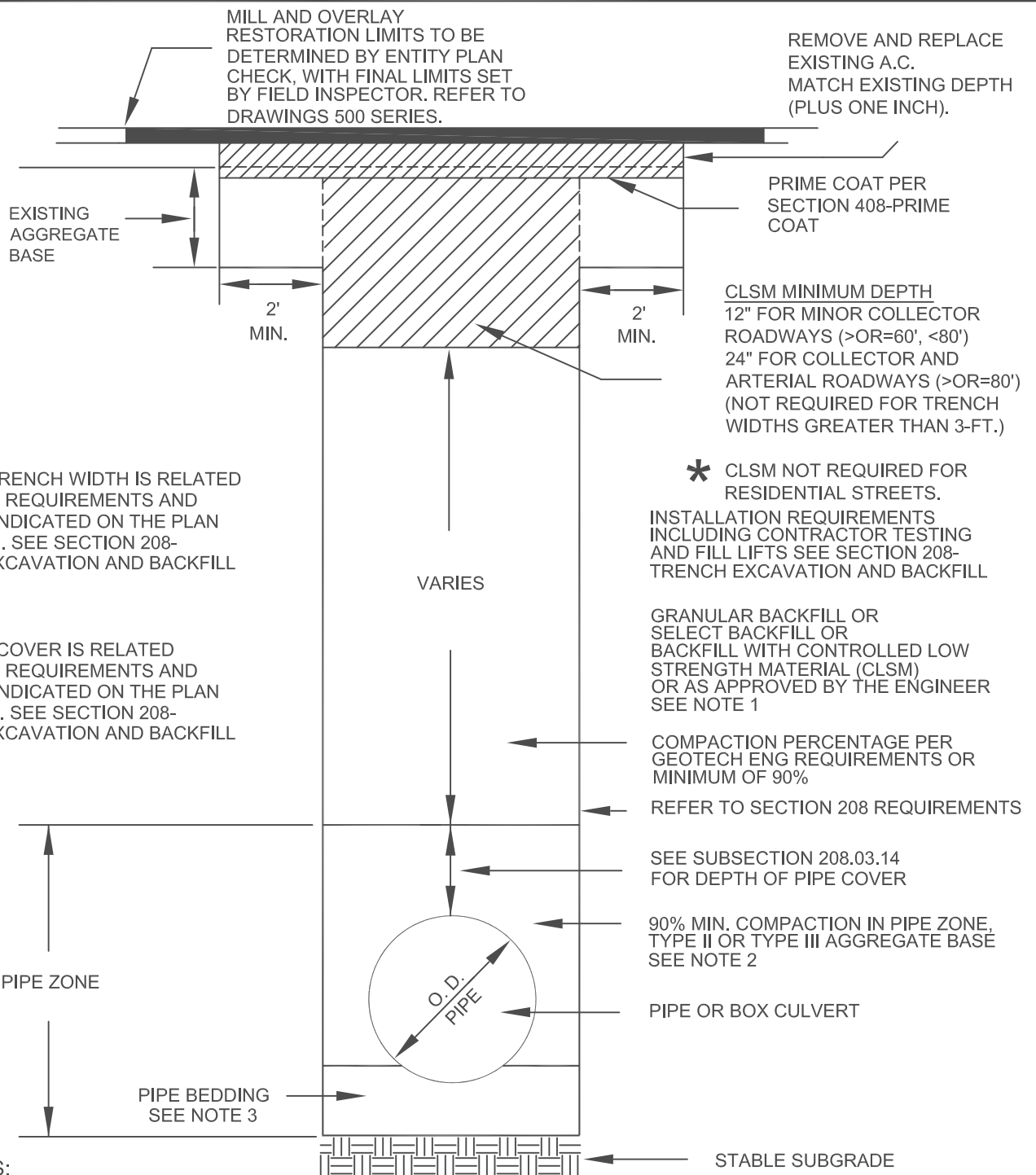


NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
208	TRENCH EXCAVATION & BACKFILL		
302	AGGREGATE BASE COURSES	METHOD A FOR RIGID PIPE TRENCH BACKFILL - PAVED AREAS	
		DATE 06-11-09	DWG. NO. 503AR

Effective 07/01/10 - 12/30/10



MINIMUM TRENCH WIDTH IS RELATED TO DESIGN REQUIREMENTS AND SHALL BE INDICATED ON THE PLAN DRAWINGS. SEE SECTION 208-TRENCH EXCAVATION AND BACKFILL

DEPTH OF COVER IS RELATED TO DESIGN REQUIREMENTS AND SHALL BE INDICATED ON THE PLAN DRAWINGS. SEE SECTION 208-TRENCH EXCAVATION AND BACKFILL

* CLSM NOT REQUIRED FOR RESIDENTIAL STREETS.
 INSTALLATION REQUIREMENTS INCLUDING CONTRACTOR TESTING AND FILL LIFTS SEE SECTION 208-TRENCH EXCAVATION AND BACKFILL

GRANULAR BACKFILL OR SELECT BACKFILL OR BACKFILL WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM) OR AS APPROVED BY THE ENGINEER SEE NOTE 1

COMPACTION PERCENTAGE PER GEOTECH ENG REQUIREMENTS OR MINIMUM OF 90%
 REFER TO SECTION 208 REQUIREMENTS

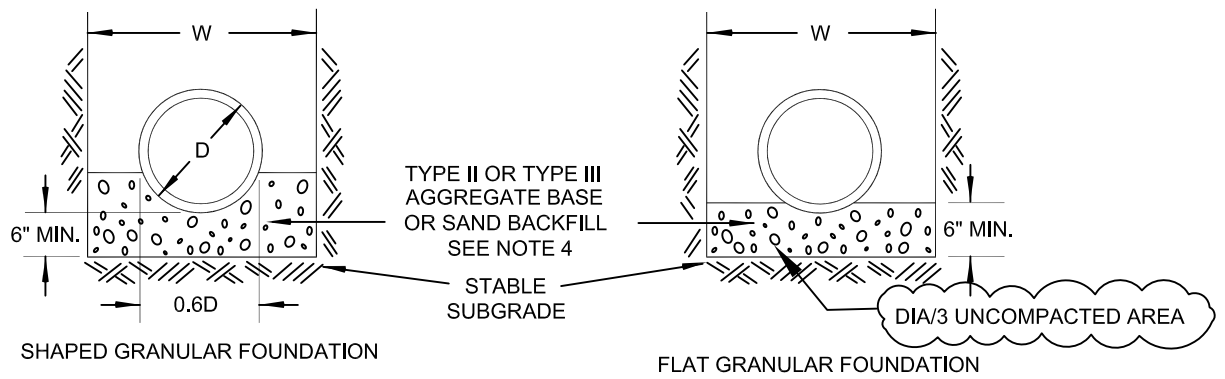
SEE SUBSECTION 208.03.14 FOR DEPTH OF PIPE COVER

90% MIN. COMPACTION IN PIPE ZONE, TYPE II OR TYPE III AGGREGATE BASE SEE NOTE 2

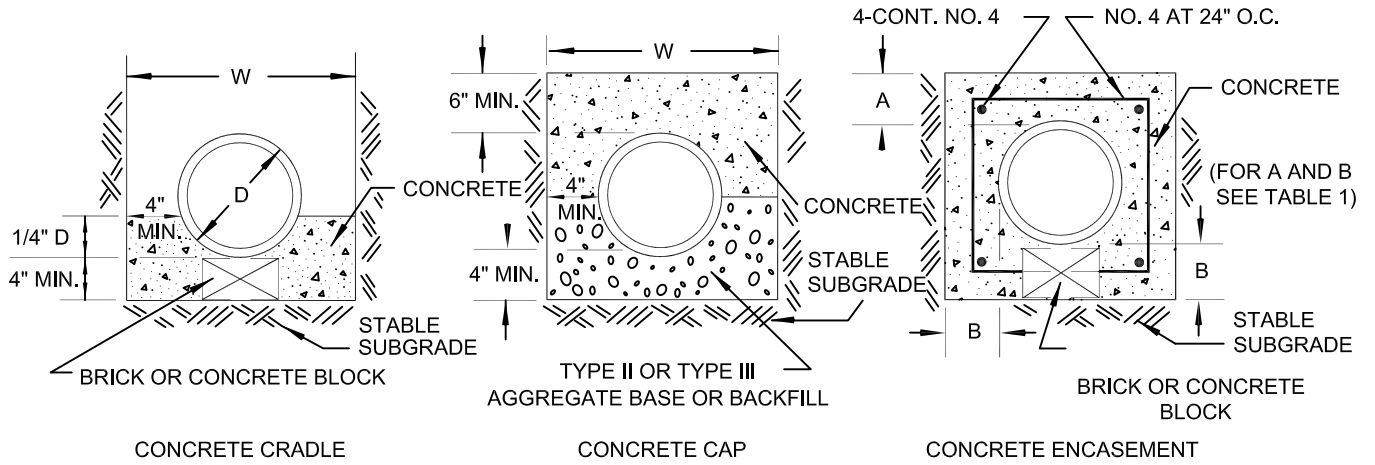
NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA	
208	TRENCH EXCAVATION & BACKFILL	METHOD B FOR RIGID AND FLEXIBLE PIPE TRENCH BACKFILL - PAVED AREAS	
302	AGGREGATE BASE COURSES		
		DATE 06-11-09	DWG. NO. 503B



NORMAL BEDDING



CONCRETE BEDDING

PIPE SIZE	PIPE		PIPE SIZE	PIPE	
	B	A		B	A
6"	4"	4"	24"	6"	6"
8"	4"	4"	27"	7"	6"
10"	4"	4"	30"	8"	6"
12"	4"	4"	33"	8"	6"
15"	4"	4"	36"	9"	6"
18"	5"	5"	39"	9"	6"
21"	5"	5"	42"	10"	6"

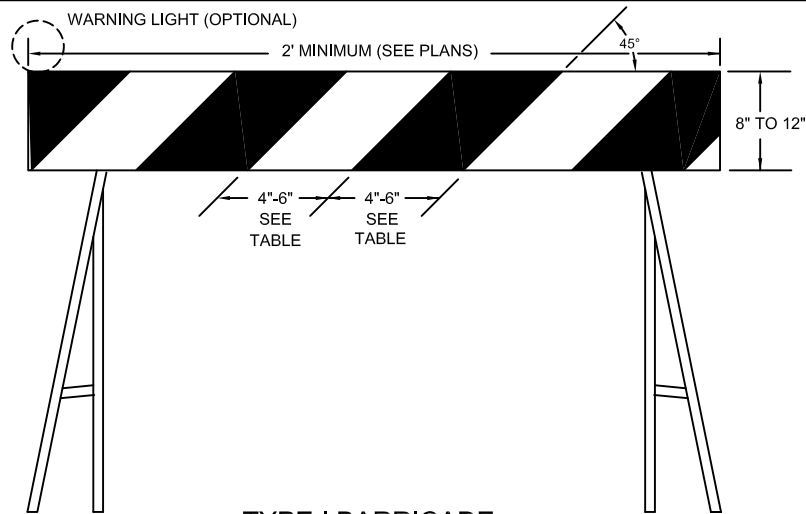
D = OUTSIDE DIAMETER OF PIPE
W = OUTSIDE DIAMETER OF PIPE + 24" MAXIMUM

NOTES:

1. PIPE BEDDING TO BE COMPACTED TO AT LEAST 90% OF MAXIMUM DENSITY. SUBGRADE SHALL CONFORM TO RESPECTIVE ENTITY REQUIREMENTS.
2. INDICATED THICKNESS OF BEDDING MATERIAL TO BE CONSTRUCTED UNDER THE BARREL. SUBGRADE TO BE EXCAVATED TO PROVIDE 2" CLEARANCE UNDER THE BELL.
3. OTHER BEDDING METHODS MAY BE SPECIFIED OR APPROVED.
4. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY.

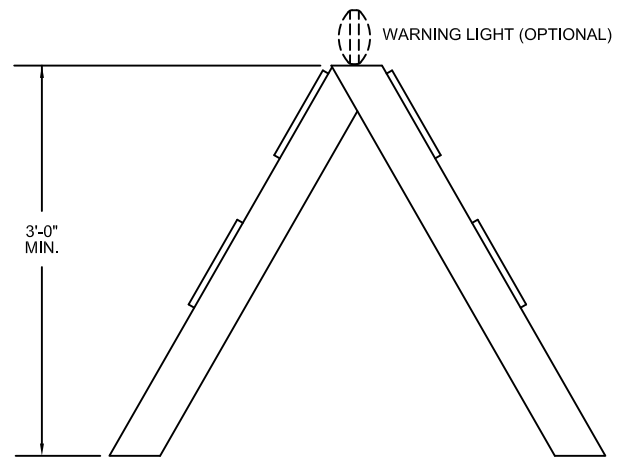
Effective 07/01/10 - 12/30/10

SPECIFICATION REFERENCE		UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA					
208	TRENCH EXCAVATION & BACKFILL	PIPE TRENCH BEDDING METHODS					
302	AGGREGATE BASE COURSES						
501	CONCRETE						
505	REINFORCEMENT STEEL						
		DATE	11-9-06	DWG. NO.	505	PAGE NO.	125



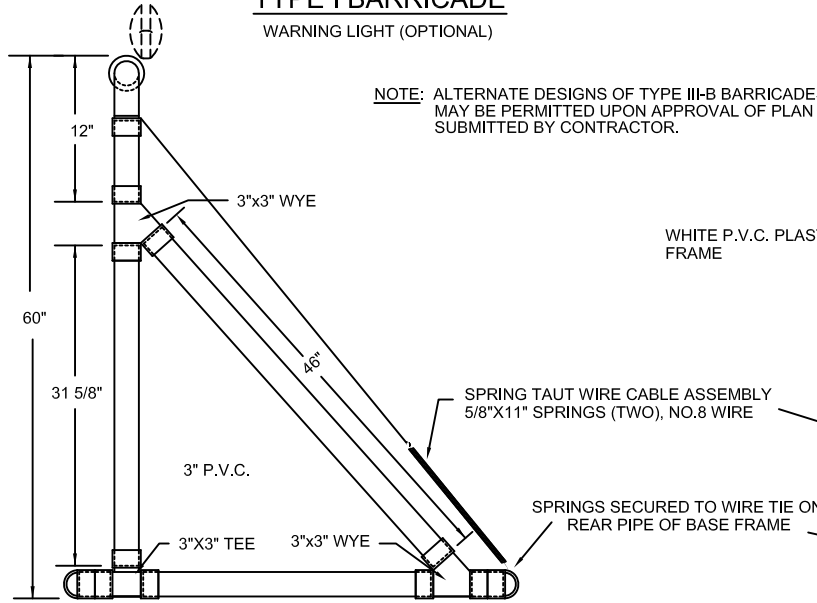
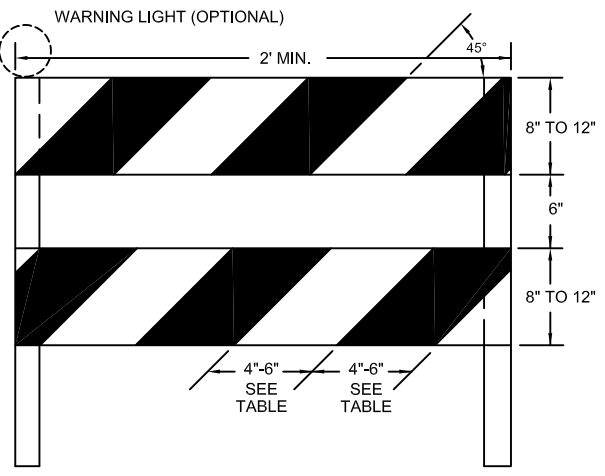
TYPE I BARRICADE

WARNING LIGHT (OPTIONAL)

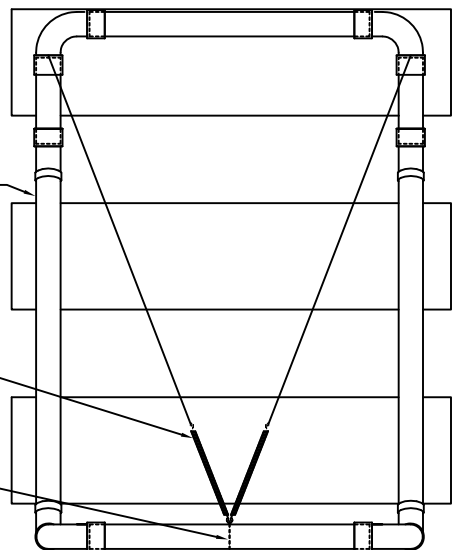


TYPE II BARRICADE

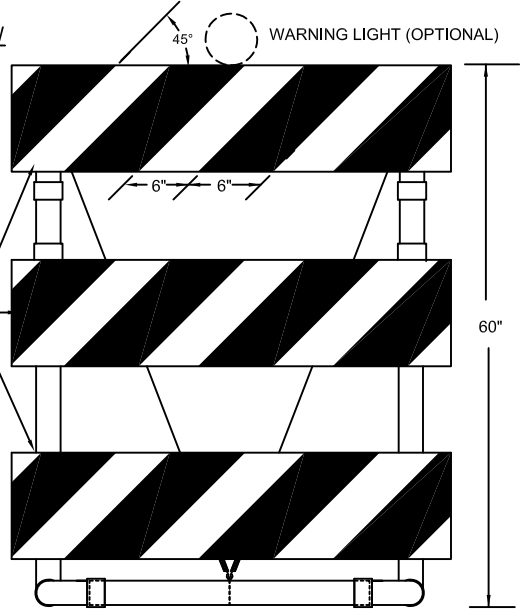
(FRAMEWORK TO BE WHITE OR ORANGE)



SIDE VIEW



BACK VIEW

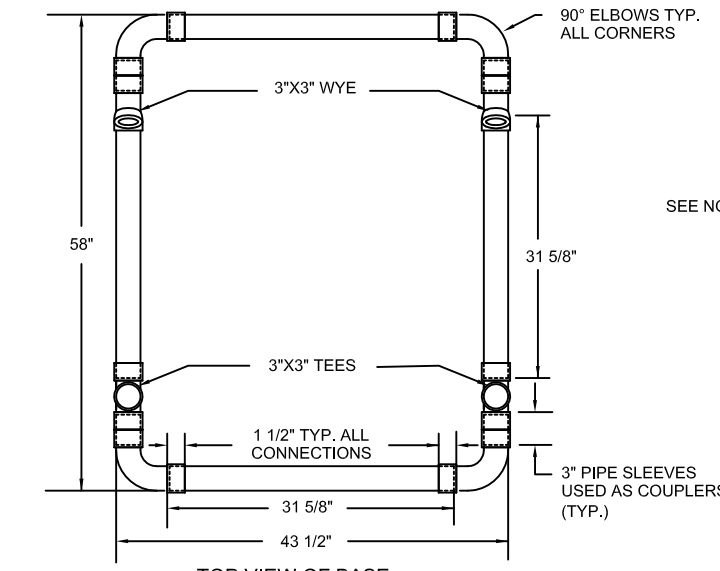


FRONT VIEW

Effective 07/01/10 - 12/30/10

TYPE III-B BARRICADE

(BARRICADE TO BE WEIGHTED DOWN WITH SANDBAGS)

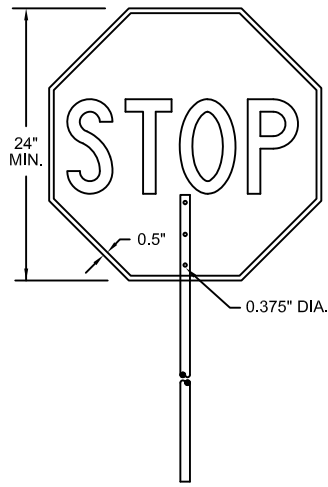
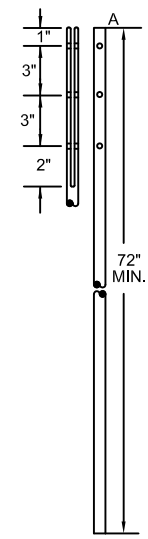


TOP VIEW OF BASE

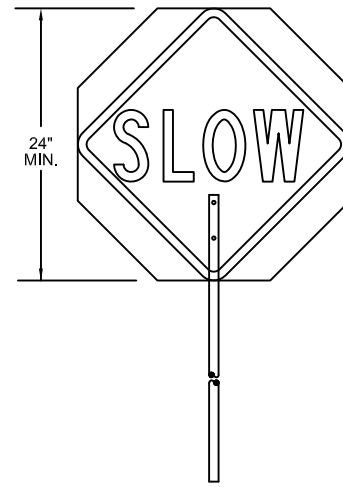
TYPE III-B BARRICADE

(BARRICADE TO BE WEIGHTED DOWN WITH SANDBAGS)

BARRICADE CHARACTERISTICS			
TYPE	I	II	III-B
WIDTH OF RAIL	8" TO 12"	8" TO 12"	8" TO 12"
LENGTH OF RAIL	2' MIN.	2' MIN.	4' MIN.
WIDTH OF STRIPES	RAIL LENGTH < 3'- 4" RAIL LENGTH ≥ 3' - 6"	RAIL LENGTH < 3'- 4" RAIL LENGTH ≥ 3' - 6"	6"
HEIGHT	3' MIN.	3' MIN.	5' MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION. 6 IF FACING TRAFFIC IN TWO DIRECTIONS



FRONT SIDE



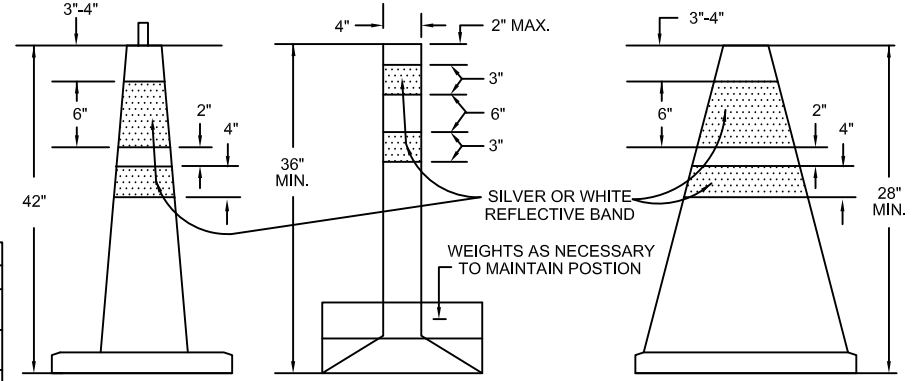
REVERSE SIDE

FLAGGER SIGN

1. THE STOP FACE SHALL CONSIST OF WHITE LETTERS AND BORDER ON A RED REFLECTORIZED BACKGROUND.
2. THE SLOW FACE SHALL CONSIST OF BLACK LETTERS AND BORDER ON AN ORANGE REFLECTORIZED BACKGROUND.
3. BOTH FACES SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
4. AREAS OUTSIDE SIGN BORDERS SHALL BE BLACK.
5. THE SIGN BLANK SHALL BE OCTAGONAL.
6. THE PORTION OF THE STAFF WITHIN THE SIGN FACE SHALL MATCH THE SIGN COLORS.
7. ALL COLORS AND LETTERS SHALL MEET APPLICABLE FEDERAL STANDARDS.
8. THE STAFF SHALL CONSIST OF TWO SECTIONS JOINED BY A COUPLING LOCATED 60 IN. FROM THE BOTTOM OF THE STAFF. ALTERNATE DESIGNS MAY BE USED WHEN APPROVED BY THE TRAFFIC ENGINEER. ALL MATERIALS SHALL BE SUBSTANTIAL AND DURABLE.
9. THIS SIGN SHALL BE FURNISHED BY THE CONTRACTOR AND SHALL BE USED BY THE FLAGGER IN LIEU OF FLAGS OR OTHER SIGNALING DEVICES.
10. STOP - SIX (6) INCH SERIES C LETTERS.
SLOW - SIX (6) INCH SERIES B LETTERS.
11. PADDLES SHALL CONFORM TO SECTION 6E-4 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
12. THE PADDLE MAY BE SUPPLEMENTED BY ONE OR TWO SYMMETRICALLY POSITIONED ALTERNATELY FLASHING WHITE HIGH INTENSITY LAMPS ON EACH SIDE.

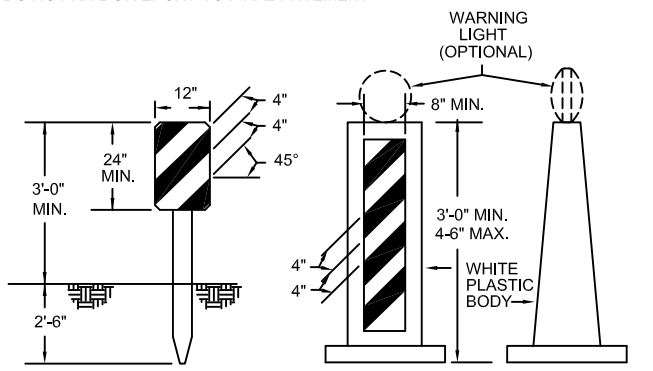
BARRICADE NOTES

1. ALL REFLECTIVE SHEETING ON BARRICADES, VERTICAL PANELS & VERTICAL PANEL POST HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SUBSECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
2. BARRICADES MAY BE WEIGHTED WITH SANDBAGS, BUT SUCH SANDBAGS SHALL NOT BE PLACED SO AS TO OBSCURE ANY RETRO REFLECTIVE SURFACE, NOR SHALL THEY BE LOCATED HIGHER THAN 24" ABOVE THE PAVEMENT.
3. WARNING LIGHTS MAY BE PLACED ON ALL BARRICADES WHEN USED DURING NIGHTTIME HOURS.
4. NAME & PHONE NUMBER OF OWNER MAY BE SHOWN ON NON-REFLECTIVE SURFACE; MUST BE OF ONE COLOR, NON-REFLECTIVE, WITH MAXIMUM 1" LETTERS ON THE FACE.
5. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE SURFACE.



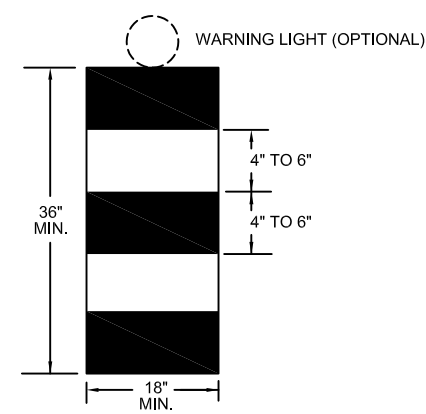
TRAFFIC CONES

1. CONES TO BE PREDOMINATELY ORANGE.
2. CONES TO BE USED DURING HOURS OF DARKNESS SHALL BE REFLECTORIZED AS SHOWN ABOVE.
3. CONES, DRUMS AND VERTICAL PANEL POST SHALL HAVE WEIGHTED BASES; HOWEVER IF THE CONTRACTOR WISHES IN LIEU OF WEIGHTED BASES, THE DEVICES MAY BE NAILED OR EPOXIED IN PLACE. DO NOT NAIL OR EPOXY TO FINAL PAVEMENT.



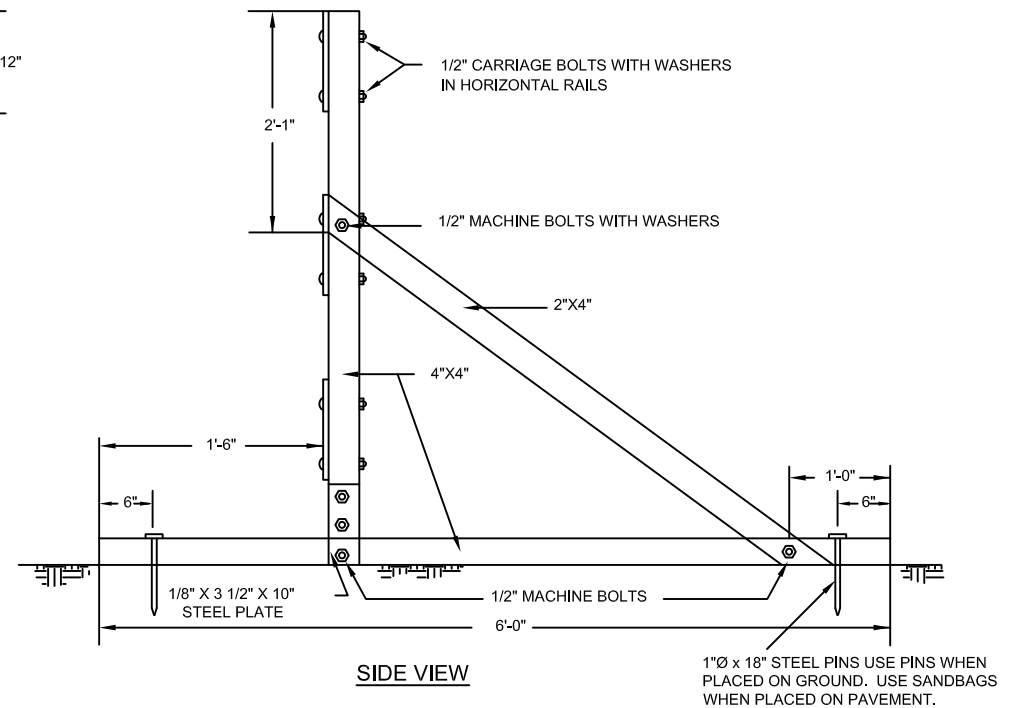
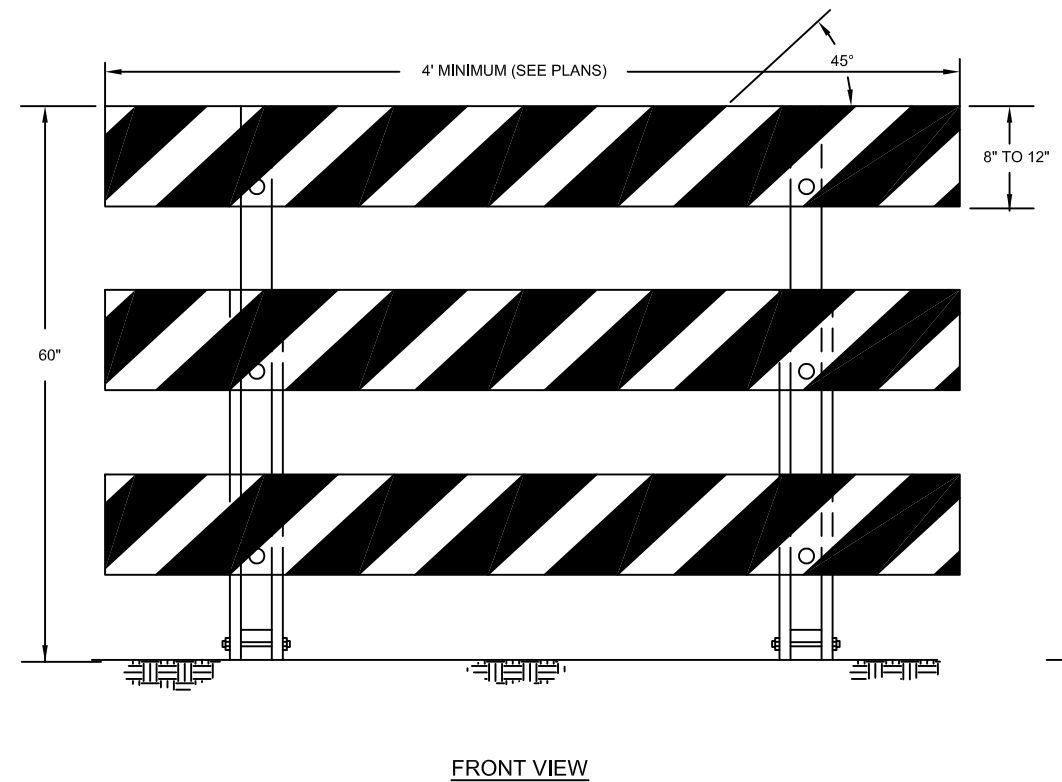
VERTICAL PANEL

VERTICAL PANEL POST

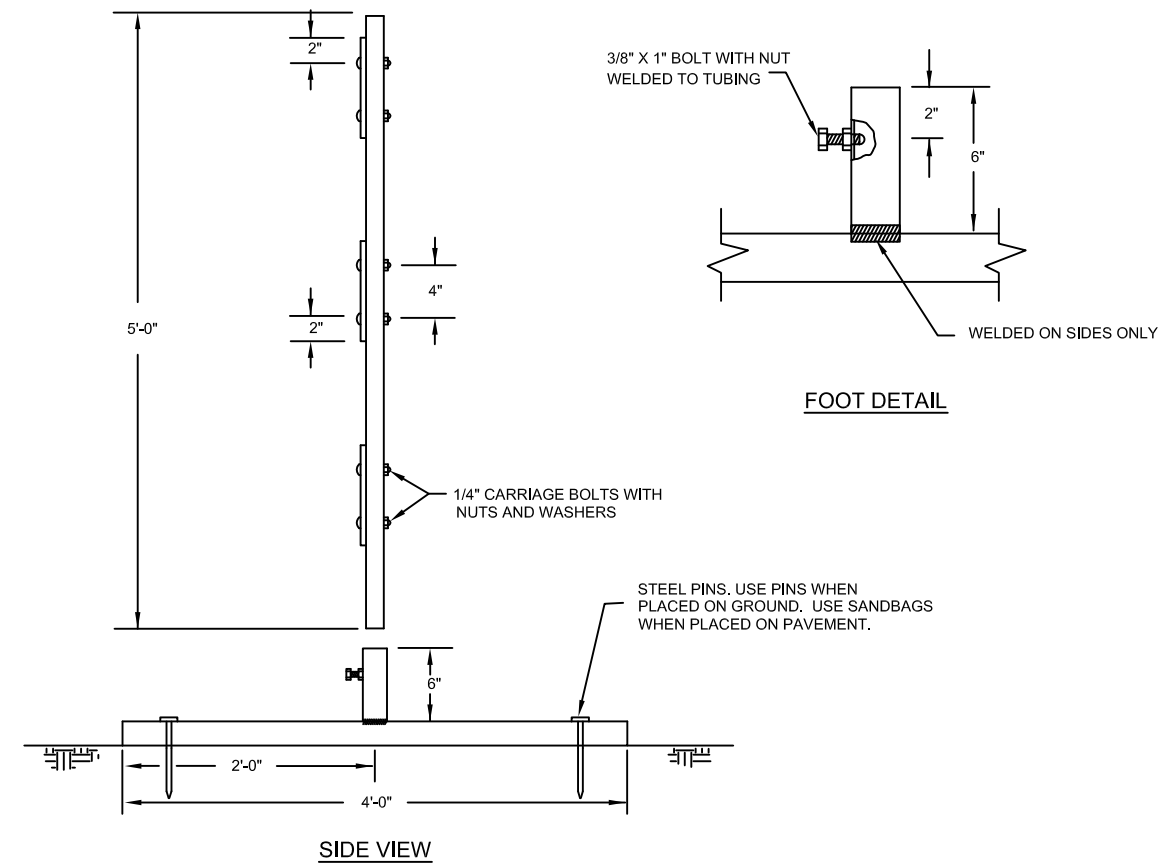
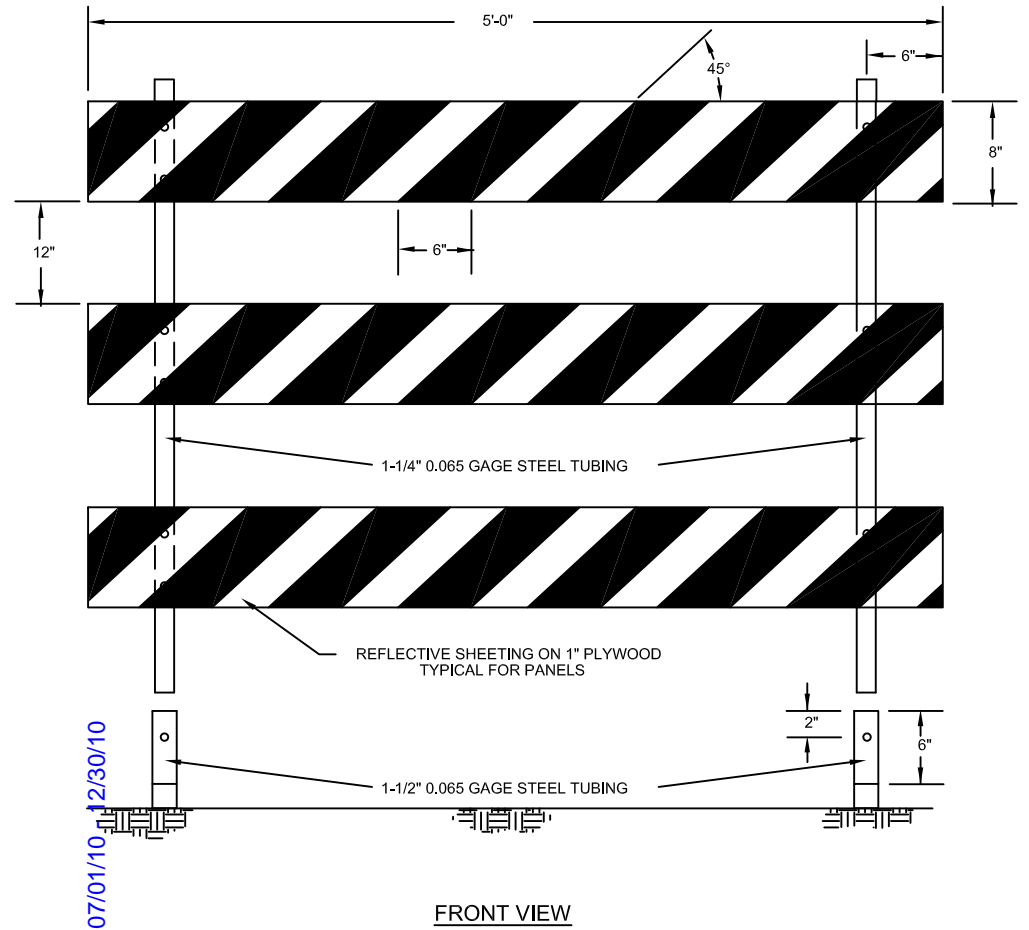


TRAFFIC DRUM

TYPE IIIA BARRICADE
N.T.S.



PORTABLE TYPE IIIA BARRICADE
N.T.S.



Effective 07/01/10 12/30/10

TRAFFIC CONTROL PLAN FOR HIGHWAY WORK ZONE	SPECIFICATION REFERENCE		TYPICAL APPLICATION FOR BARRICADES & FLAGGER TRAFFIC CONTROL SIGN STANDARD DESIGNS	UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA		
				DATE 1-9-97	DWG. NO. 601	(2 OF 2)
				PAGE 140A		

PORTABLE CONCRETE BARRIER RAIL SHALL BE USED TO PROTECT ANY WORK AREA IN WHICH IS ESTABLISHED A CONDITION SHOWN IN FIGURES A & B OR TABLES 1,2 & 3 (BELOW) WARRANTING A BARRIER

THE TABLES AND FIGURES SHOWN ABOVE ARE TAKEN IN THEIR ENTIRETY FROM THE ROADSIDE DESIGN GUIDE, AMERICAN ASSOCIATION OF STATE HIGHWAY & TRANSPORTATION OFFICIALS (AASHTO), WASHINGTON, D.C. 1989 AND USED TO ESTIMATE CLEAR ZONE DIMENSION. REFER TO ROADSIDE DESIGN GUIDE FOR ADDITIONAL INFORMATION AND GUIDELINES WHICH SHOULD BE CONSIDERED.

TABLE 2. TYPICAL WARRANTS FOR NONTRAVERSABLE AND FIXED OBJECT HAZARDS

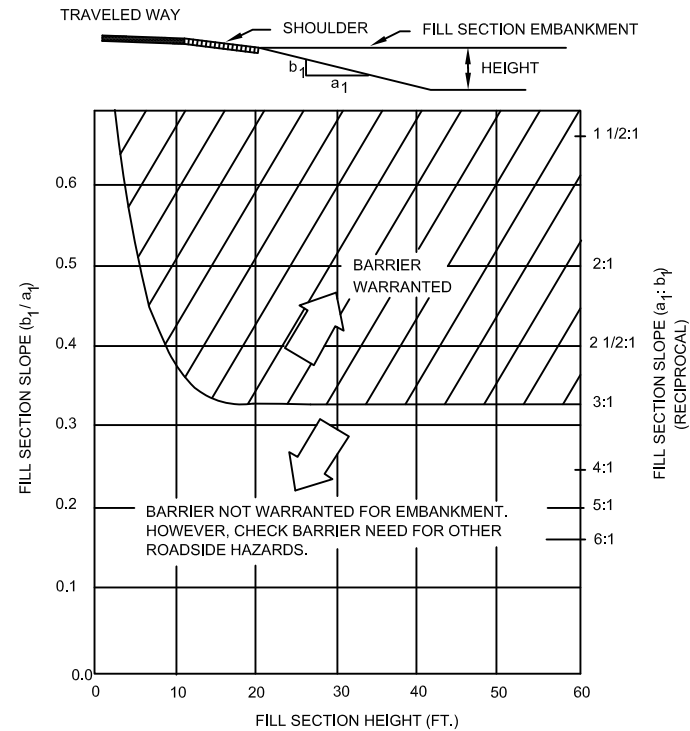


FIGURE A. WARRANTS FOR FILL SECTION EMBANKMENTS

TABLE 1. CLEAR ZONE DISTANCES (IN FEET FROM EDGE OF DRIVING LANE)

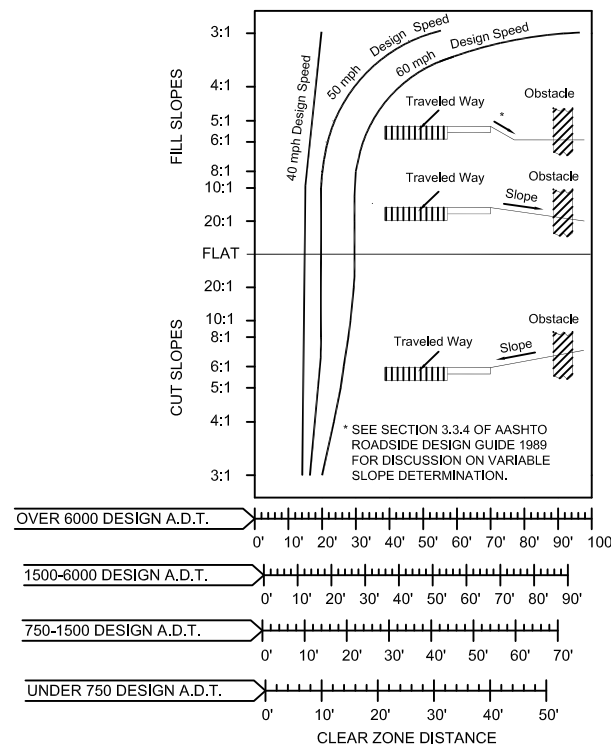
DESIGN SPEED	DESIGN ADT	FILL SLOPES			CUT SLOPES		
		6:1 OR FLATTER	5:1 TO 4:1	3:1	3:1	5:1 TO 4:1	6:1 OR FLATTER
40 MPH OR LESS	UNDER 750	7-10	7-10	**	7-10	7-10	7-10
	750-1500	10-12	12-14	**	10-12	10-12	10-12
	1500-6000	12-14	14-16	**	12-14	12-14	12-14
	OVER 6000	14-16	16-18	**	14-16	14-16	14-16
45-50 MPH	UNDER 750	10-12	12-14	**	8-10	8-10	10-12
	750-1500	12-14	16-20	**	10-12	12-14	14-16
	1500-6000	16-18	20-26	**	12-14	14-16	16-18
	OVER 6000	18-20	24-28	**	14-16	18-20	20-22
55 MPH	UNDER 750	12-14	14-18	**	8-10	10-12	10-12
	750-1500	16-18	20-24	**	10-12	14-16	16-18
	1500-6000	20-22	24-30	**	14-16	16-18	20-22
	OVER 6000	22-24	26-32*	**	16-18	20-22	22-24
60 MPH	UNDER 750	16-18	20-24	**	10-12	12-14	14-16
	750-1500	20-24	26-32*	**	12-14	16-18	20-22
	1500-6000	26-30	32-40*	**	14-18	18-22	24-26
	OVER 6000	30-32*	36-44*	**	20-22	24-26	26-28
65-70 MPH	UNDER 750	18-20	20-26	**	10-12	14-16	14-16
	750-1500	24-26	28-36*	**	12-16	18-20	20-22
	1500-6000	28-32*	34-42*	**	16-20	22-24	26-28
	OVER 6000	30-34*	38-46*	**	22-24	26-30	28-30

* MAY BE LIMITED TO 30 FEET BASED SITE SPECIFIC INVESTIGATION. SEE ROADSIDE DESIGN GUIDE FOR FURTHER DISCUSSION.

** FIXED OBJECTS SHOULD NOT BE PRESENT IN THE VICINITY OF THE TOE OF THESE SLOPES. RECOVERY OF HIGH SPEED VEHICLES THAT ENCROACH BEYOND THE EDGE OF SHOULDER MAY BE EXPECTED TO OCCUR BEYOND THE TOE OF THE SLOPE. SEE ROADSIDE DESIGN GUIDE FOR FURTHER DISCUSSION.

Effective 07/01/10 - 12/30/10

FIGURE B. CLEAR ZONE DISTANCE CURVES



(FOR SLOPES GREATER THAN 4:1 CONSULT AASHTO ROADSIDE DESIGN GUIDE 1989)

BRIDGE PIERS, ABUTMENTS AND RAILING ENDINGS	SHIELDING GENERALLY REQUIRED
BOULDERS	A JUDGEMENT DECISION BASED ON NATURE OF HAZARD AND LIKELIHOOD OF IMPACT
CULVERTS, PIPES, HEADWALLS	A JUDGEMENT DECISION BASED ON SIZE, SHAPE AND LOCATION OF HAZARD
CUT SLOPES (SMOOTH)	SHIELDING NOT GENERALLY REQUIRED
CUT SLOPES (ROUGH)	A JUDGEMENT DECISION BASED ON LIKELIHOOD OF IMPACT
DITCHES (PARALLEL)	REFER TO ROADSIDE DESIGN GUIDE SECTION 3.2.4
DITCHES (TRANSVERSE)	SHIELDING GENERALLY REQUIRED IF LIKELIHOOD OF HEAD-ON IMPACT IS HIGH
EMBANKMENT	A JUDGEMENT DECISION BASED ON FILL HEIGHT AND SLOPE (SEE FIGURE A)
RETAINING WALLS	A JUDGEMENT DECISION BASED ON RELATIVE SMOOTHNESS OF WALL AND ANTICIPATED MAXIMUM ANGLE OF IMPACT
SIGN/LUMINAIRE SUPPORTS ³	SHIELDING GENERALLY REQUIRED FOR NON-BREAKAWAY SUPPORTS
TRAFFIC SIGNAL SUPPORTS ⁴	ISOLATED TRAFFIC SIGNALS WITHIN CLEAR ZONE ON HIGH-SPEED RURAL FACILITIES MAY WARRANT SHIELDING
TREES	A JUDGEMENT DECISION BASED ON SITE SPECIFIC CIRCUMSTANCES
UTILITY POLES	SHIELDING MAY BE WARRANTED ON A CASE-BY-CASE BASIS
PERMANENT BODIES OF WATER	A JUDGEMENT DECISION BASED ON LOCATION AND DEPTH OF WATER AND LIKELIHOOD OF ENCROACHMENT

- SHIELDING A NON-TRAVERSABLE OR FIXED OBJECT HAZARD IS USUALLY WARRANTED ONLY WHEN THE HAZARD IS WITHIN THE CLEAR ZONE AND CANNOT PRACTICALLY OR ECONOMICALLY BE REMOVED, RELOCATED OR MADE BREAKAWAY, AND IT IS DETERMINED THAT THE BARRIER IS A LESSER HAZARD THAN THE UNSHIELDED CONDITION.
- MARGINAL SITUATIONS, WITH RESPECT TO PLACEMENT OR OMISSION OF A BARRIER, WILL USUALLY BE DECIDED BY ACCIDENT EXPERIENCE, EITHER AT THE SITE OR AT A COMPARABLE SITE.
- WHERE FEASIBLE, ALL SIGN AND LUMINAIRE SUPPORTS SHOULD BE A BREAKAWAY DESIGN REGARDLESS OF THEIR DISTANCE FROM THE ROADWAY IF THERE IS A REASONABLE LIKELIHOOD OF THEIR BEING HIT BY AN ERRANT MOTORIST.
- IN PRACTICE, RELATIVELY FEW TRAFFIC SIGNAL SUPPORTS, INCLUDING FLASHING LIGHT SIGNALS AND GATES USED AT RAILROAD CROSSING, ARE SHIELDED. IF SHIELDING IS DEEMED UNNECESSARY, HOWEVER, CRASH CUSHIONS ARE SOMETIMES USED IN LIEU OF A LONGITUDINAL BARRIER INSTALLATION.

TABLE 3. HORIZONTAL CURVE ADJUSTMENTS

DEGREE OF CURVE	K_c (CURVE CORRECTION FACTOR)						
	DESIGN SPEED						
	40	45	50	55	60	65	70
2.0	1.08	1.10	1.12	1.15	1.19	1.22	1.27
2.5	1.10	1.12	1.15	1.19	1.23	1.28	1.33
3.0	1.11	1.15	1.18	1.23	1.28	1.33	1.40
3.5	1.13	1.17	1.22	1.26	1.32	1.39	1.46
4.0	1.15	1.19	1.25	1.30	1.37	1.44	1.51
4.5	1.17	1.22	1.28	1.34	1.41	1.49	1.56
5.0	1.19	1.24	1.31	1.37	1.46	1.54	1.61
6.0	1.23	1.29	1.36	1.45	1.54	1.61	1.68
7.0	1.26	1.34	1.42	1.52	1.61	1.68	1.75
8.0	1.30	1.38	1.48	1.57	1.66	1.73	1.80
9.0	1.34	1.43	1.53	1.62	1.71	1.78	1.85
10.0	1.37	1.47	1.57	1.66	1.75	1.82	1.89
15.0	1.54	1.64	1.74	1.83	1.92	2.00	2.08

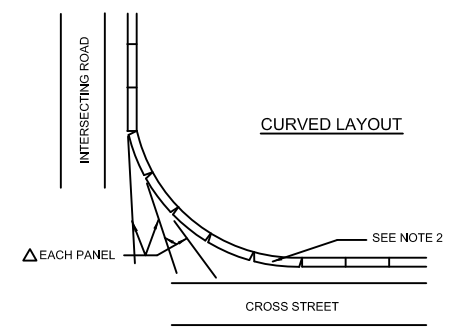
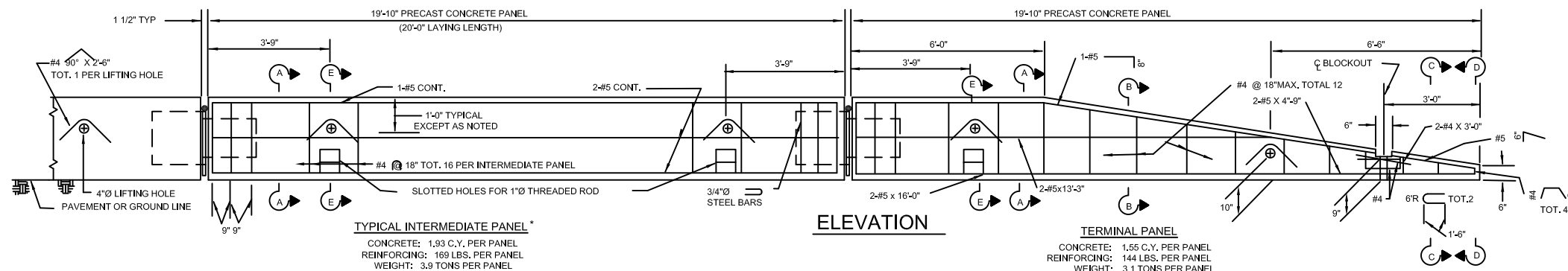
$$CZ_c = (L_c)(K_{CZ})$$

WHERE: CZ_c = CLEAR ZONE ON OUTSIDE OF CURVATURE, FT.

L_c = CLEAR ZONE DISTANCE, FT., FIGURE B.

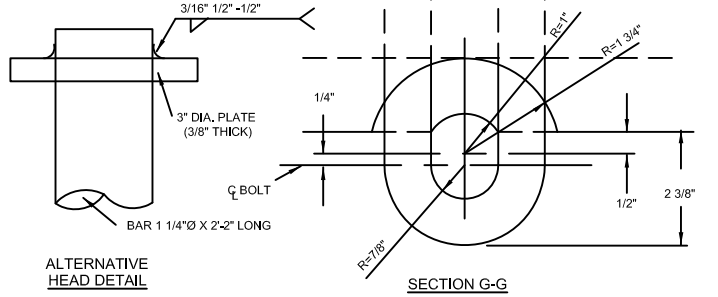
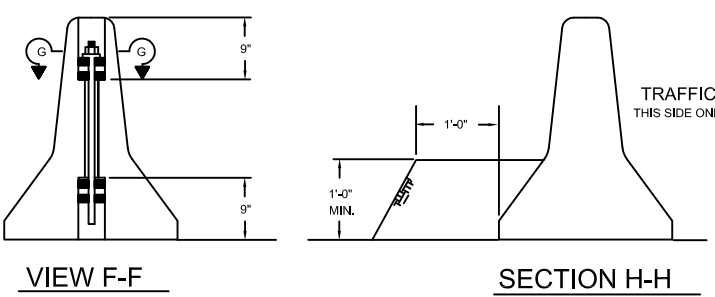
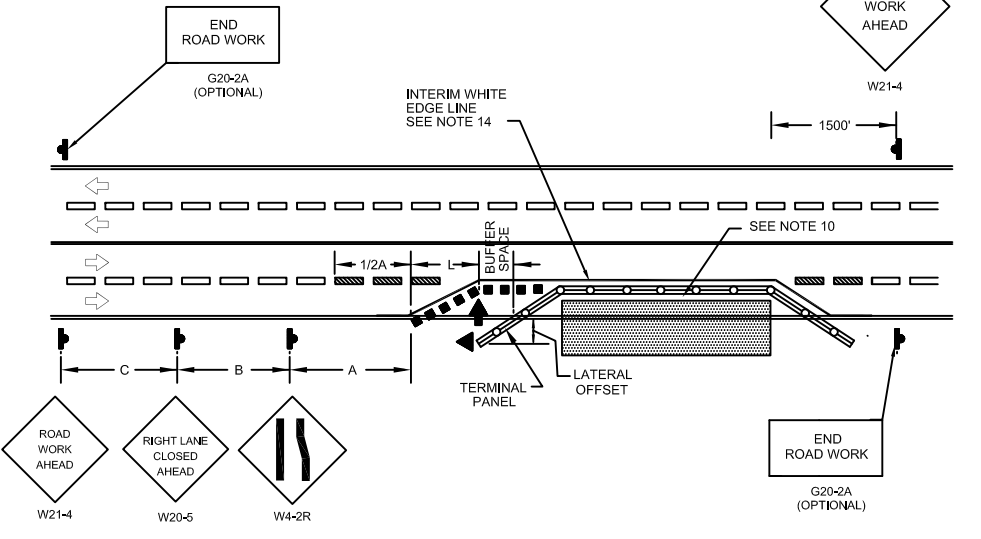
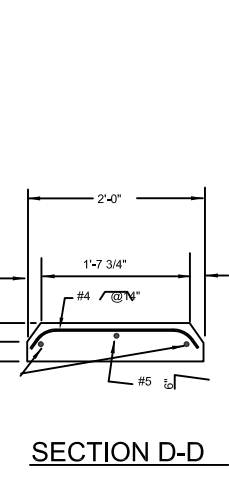
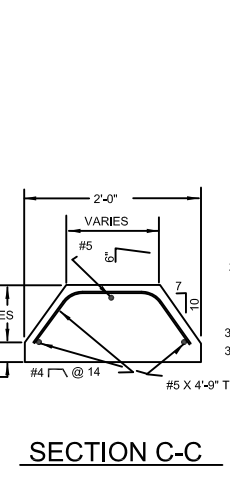
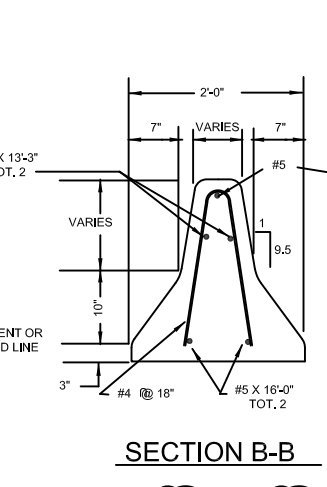
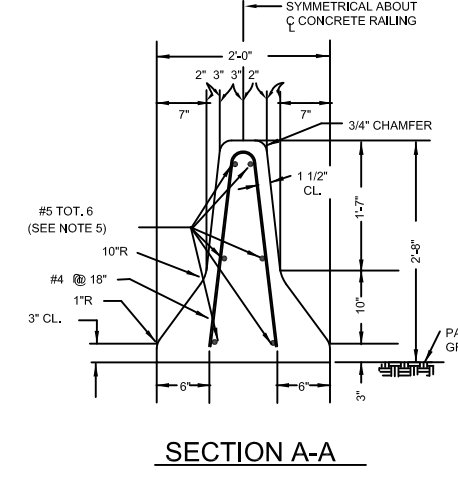
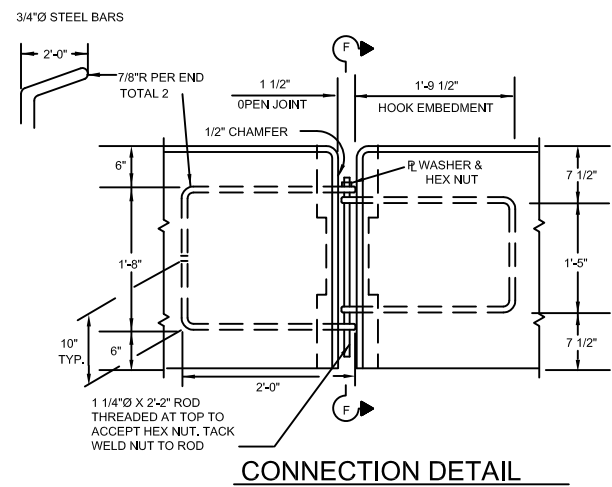
K_{CZ} = CURVE CORRECTION FACTOR

NOTE: CLEAR ZONE CORRECTION FACTOR IS APPLIED TO OUTSIDE CURVES ONLY. CURVES FLATTER THAN 2° DO NOT REQUIRE AN ADJUSTED CLEAR ZONE.



TYPICAL INTERMEDIATE PANEL*
 CONCRETE: 1.93 C.Y. PER PANEL
 REINFORCING: 169 LBS. PER PANEL
 WEIGHT: 3.9 TONS PER PANEL
 *ALTERNATE LENGTHS MAY BE USED IF APPROVED BY THE ENGINEER.

TERMINAL PANEL
 CONCRETE: 1.55 C.Y. PER PANEL
 REINFORCING: 144 LBS. PER PANEL
 WEIGHT: 3.1 TONS PER PANEL
 NOTE: #4 @ 18" AND #4 @ 14" MAY BE REPLACED BY WELDED WIRE FABRIC OF EQUIVALENT CROSS-SECTIONAL AREA. FOR DETAILS NOT SHOWN IN SECTIONS B-B, C-C, & D-D, SEE SECTION A-A



SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

SPEED* (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

SPEED (M.P.H.)	MAXIMUM TAPER	MINIMUM LATERAL OFFSET (FT.)
70	15:1	10.0
65	14:1	9.0
60	13:1	8.0
55	12:1	7.25
50	11:1	6.5
45	10:1	5.75
40	9:1	5.0
35	8:1	4.25
30	7:1	3.5
25	6:1	2.75
20	5:1	2.0

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

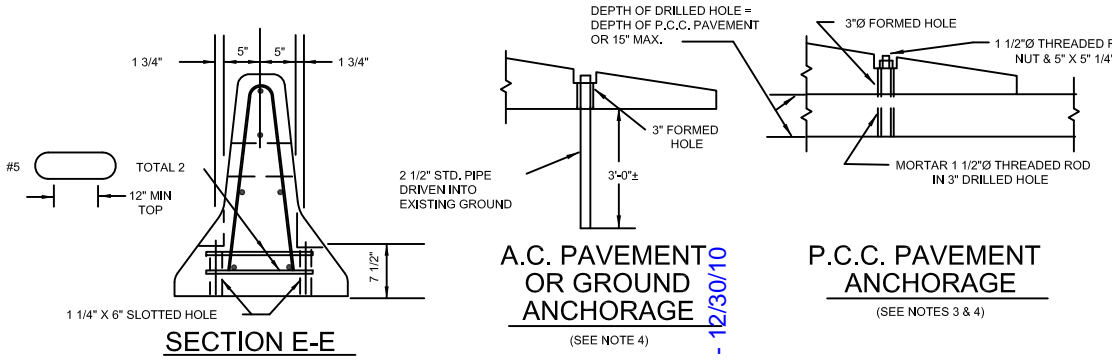
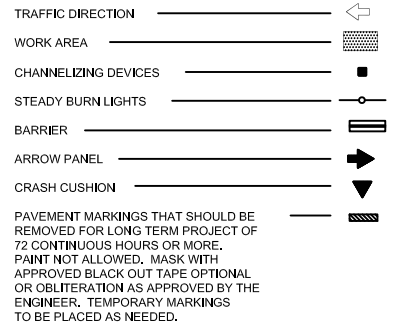
PIN CONNECTION DETAIL

NOTES:

- THE MINIMUM LENGTH OF PORTABLE BARRIER SYSTEM SHOULD BE 100 FT. IN ACCORDANCE WITH CURRENT EDITION AASHTO ROADSIDE DESIGN GUIDE.
- WHERE BARRIERS ARE PLACED ON CURVES AND RADII THAT ARE TOO SEVERE TO MATCH UP JOINTS, BARRIERS ARE TO BE BACKED CONTINUOUSLY WITH EARTH FILL. SEE SECTION H-H.
- BOLT UNITS TO DECK SLABS WHEN REQUIRED BY BRIDGE PLANS.
- ATTACH UNITS TO PAVEMENT WHEN REQUIRED IN THE PLANS.
- THE TWO #5 BARS SHALL BE EQUALLY SPACED FROM THE VERTEX OF THE STIRRUP BARS. ONE #5 BAR SHALL BE TIGHTLY WIRED TO THE STIRRUP BARS AND THE SECOND #5 BAR SHALL BE TACK WELDED TO THE STIRRUP BARS. EACH PROCESS SHALL BE CONTINUOUS FOR EACH #5 BAR.
- THE USE OF A BARRIER SHALL BE BASED ON THE NEED DETERMINED IN SHEET 1 OF THIS DRAWING, IN CONJUNCTION WITH THE ROADSIDE DESIGN GUIDE, CURRENT EDITION, OR WHEN CALLED FOR ON THE PLANS.
- BARRIERS SHALL BE FLARED BEYOND THE SHOULDER OR HAVE A CRASH CUSHION TO PROTECT TRAFFIC FROM THE END OF THE BARRIER. TERMINAL PANELS MAY BE USED AT ENDS ON PORTABLE BARRIER SYSTEM WHICH FACE ONCOMING TRAFFIC FOR SPEEDS LESS THAN 40 MPH.
- ALTERNATE STRUCTURAL DETAILS MAY BE PERMITTED IF DOCUMENTED WITH DRAWINGS SUBMITTED BY THE CONTRACTOR AND APPROVED BY THE TRAFFIC ENGINEER.
- EVERY PRECAST CONCRETE PANEL SHALL BE JOINED WITH EACH PANEL ADJACENT TO IT. THE CONNECTION DETAILS TO CONFORM WITH THOSE SHOWN ABOVE, OR CONFORMING WITH ALTERNATE DETAIL DESIGN SUBMITTED BY THE CONTRACTOR AND APPROVED BY THE TRAFFIC ENGINEER. OPENINGS IN BARRIER SHOULD BE AVOIDED.
- PROVIDE AT LEAST 3 FT. BETWEEN THE BARRIER AND THE WORK AREA.
- IF LESS THAN 3 FT., BARRIER SHALL BE ANCHORED IN ACCORDANCE WITH CURRENT EDITION OF AASHTO ROADSIDE DESIGN GUIDE.
- CORNERS OF BARRIERS MAY BE BEVELED TO ALLOW PLACEMENT ON CURVES AND SHOULD BE FREE FROM JAGGED EDGES TO AVOID SNAGGING OF VEHICLES.
- WHEN TEMPORARY BARRIERS ARE INSTALLED ON BOTH SIDES OF TRAFFIC, BEGINNINGS OF BARRIERS SHOULD BE STAGGERED.
- FOR BETTER NIGHT VISIBILITY, STEADY BURN LIGHTS MAY BE MOUNTED ALONG BARRIER.
- AN INTERIM WHITE REFLECTORIZED EDGE LINE, 6" WIDE MINIMUM SHOULD BE INSTALLED FROM THE START OF THE TAPER TO A POINT BEYOND THE WORK AREA, REJOINING THE PERMANENT EDGE LINE. THE EDGE LINE SHALL BE REMOVABLE, NON-FOLI TAPE.
- THE BARRIER SHALL NOT BE PLACED ALONG MERGING TAPER. THE LANE SHALL FIRST BE CLOSED USING CHANNELIZING DEVICES AND PAVEMENT MARKINGS. THE BARRIER IS THEN PLACED ON A FLARE BEGINNING BEYOND THE DOWNSTREAM END OF THE MERGING TAPER.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
- WATER-FILLED BARRIER RAIL MAY BE USED ON SHORT TERM MOVING OPERATIONS WHEN APPROVED BY THE TRAFFIC ENGINEER.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

SYMBOLS

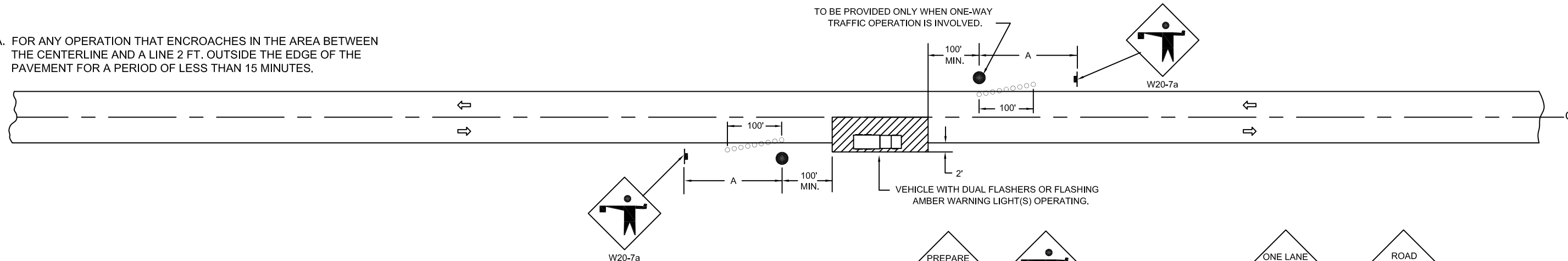


TRAFFIC CONTROL PLAN
FOR
HIGHWAY WORK ZONE

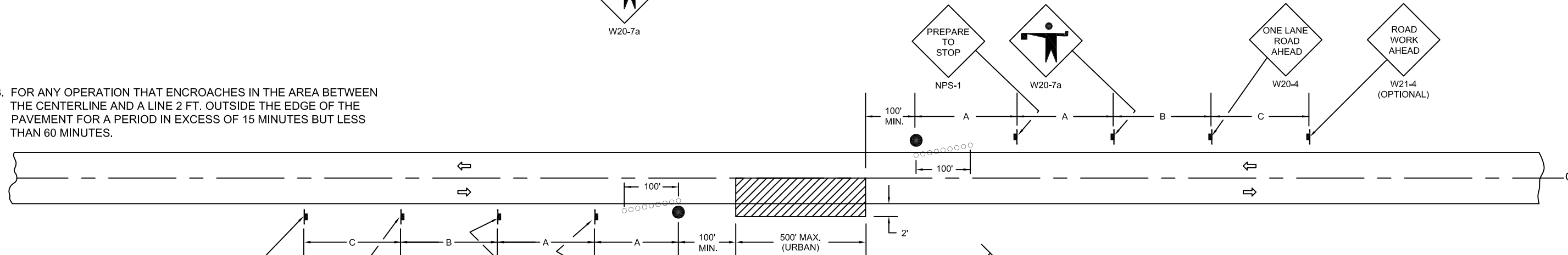
TYPICAL APPLICATION FOR
PORTABLE CONCRETE BARRIER RAIL

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA
DATE 1-9-97 DWG. NO. 602 (2 OF 2) PAGE 141A

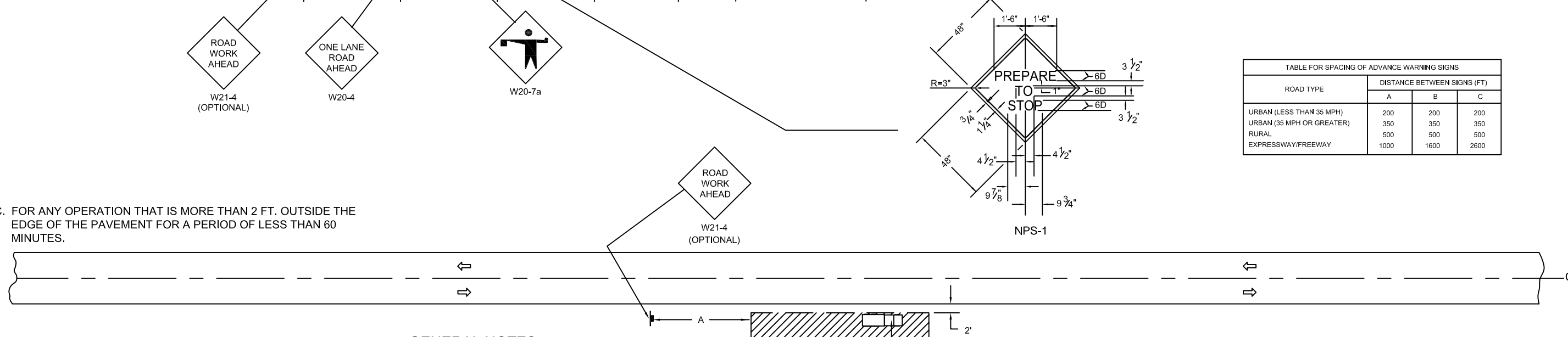
A. FOR ANY OPERATION THAT ENCROACHES IN THE AREA BETWEEN THE CENTERLINE AND A LINE 2 FT. OUTSIDE THE EDGE OF THE PAVEMENT FOR A PERIOD OF LESS THAN 15 MINUTES.



B. FOR ANY OPERATION THAT ENCROACHES IN THE AREA BETWEEN THE CENTERLINE AND A LINE 2 FT. OUTSIDE THE EDGE OF THE PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES BUT LESS THAN 60 MINUTES.



C. FOR ANY OPERATION THAT IS MORE THAN 2 FT. OUTSIDE THE EDGE OF THE PAVEMENT FOR A PERIOD OF LESS THAN 60 MINUTES.



ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

GENERAL NOTES

- CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE. ON TWO-LANE ROADS, AT LEAST 500 FT. OF BOTH TRAFFIC LANES SHALL BE AVAILABLE FOR TRAFFIC MOVEMENT AT INTERVALS NOT GREATER THAN 1,000 FT. AND A COMPLETE TRAFFIC CONTROL PLAN MUST BE APPROVED FOR ANY PROJECT EXPECTED TO EXCEED 1,000 FT. IN LENGTH.
- THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES.
- ALL SIGNS ARE TO BE REMOVED AT COMPLETION OF EACH OPERATION.
- FOR THREE LANE ROADWAYS, THE FLAGGER SHOWN FOR TRAFFIC APPROACHING FROM THE OPPOSITE DIRECTION MAY BE DELETED, IF ONE LANE IS MAINTAINED IN EACH DIRECTION, AS DIRECTED BY THE TRAFFIC ENGINEER, THE ADVANCE WARNING SIGN FOR TRAFFIC APPROACHING FROM THE OPPOSITE DIRECTION OMITTED, AND THE "ONE LANE ROAD AHEAD" SIGN CHANGED TO "RIGHT (LEFT) LANE CLOSED AHEAD". FOR MULTILANE ROADWAYS WITH TWO LANES IN EACH DIRECTION, THE FLAGGER MAY BE DELETED IF DEEMED UNNECESSARY BY THE TRAFFIC ENGINEER.

- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGERS MAY BE VARIED FROM THAT SHOWN.
 - ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
 - ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
 - IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.
- | | | | |
|--------------|----------|-----------------|----------|
| BOULDER CITY | 293-9200 | LAS VEGAS | 229-6331 |
| CLARK COUNTY | 455-6100 | MESQUITE | 346-5295 |
| HENDERSON | 565-2140 | NORTH LAS VEGAS | 642-2462 |

- IF THE WORK PLACE IS IN THE MEDIAN OF A DIVIDED HIGHWAY, AN ADVANCE WARNING SIGN SHOULD ALSO BE PLACED ON THE LEFT SIDE OF THE DIRECTIONAL ROADWAY.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
- FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION FOR USE DURING HOURS OF DARKNESS.

TYPICAL APPLICATIONS

- MARKING PATCHES
- FIELD SURVEY
- STRING LINE
- UTILITY OPERATION
- CLEANING UP DEBRIS ON PAVEMENT

SYMBOLS

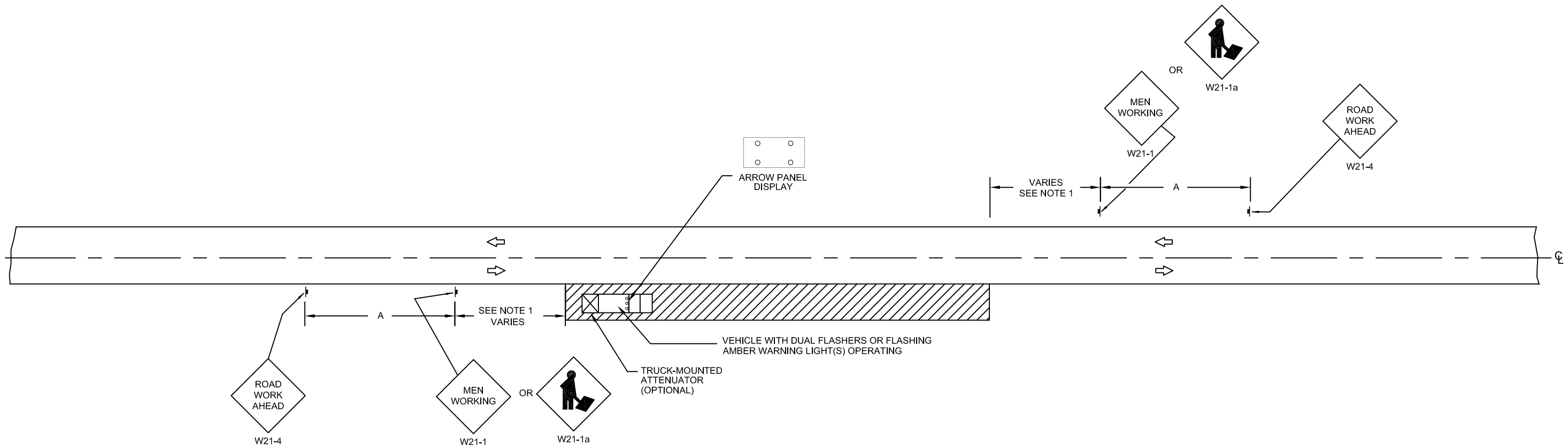
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH TRAFFIC CONTROL SIGN
- TRAFFIC CONES
- TRAFFIC DIRECTION

TRAFFIC CONTROL PLAN FOR HIGHWAY WORK ZONE

SPECIFICATION REFERENCE	

TYPICAL APPLICATION FOR SHORT TIME, DAY OR NIGHT OPERATIONS

UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA



GENERAL NOTES

1. MINIMUM DISTANCE IS 200 FT. MAXIMUM DISTANCE TO BE DETERMINED BY THE TRAFFIC ENGINEER BUT SHOULD NOT EXCEED 1/2 THE LENGTH REQUIRED FOR ONE NORMAL WORKING DAY'S OPERATION. IN SITUATIONS WHERE MULTIPLE WORK LOCATIONS IN A LIMITED DISTANCE MAKE IT PRACTICABLE TO PLACE STATIONARY SIGNS, THE MAXIMUM SPACING FOR THE ADVANCE WARNING SIGN IS 5 MILES IN ADVANCE OF THE WORK.
2. IF THE WORK OPERATION DOES NOT EXCEED 60 MINUTES, TRAFFIC CONTROL MAY BE IN CONFORMANCE WITH STANDARD DRAWING NO. 603.
3. ALL SIGNS ARE TO BE REMOVED AT COMPLETION OF THE DAY'S OPERATION.
4. FOR DIVIDED ROADWAYS THE REQUIRED ADVANCE WARNING SIGNS SHALL BE POSTED ON BOTH THE RIGHT AND MEDIAN SIDE OF THE AFFECTED APPROACH.
5. FOR MULTILANE ROADWAYS THE ADVANCE WARNING SIGNS MAY BE REQUIRED FOR TRAFFIC APPROACHING FROM THE OPPOSITE DIRECTION.
6. WORKER SIGNS ARE TO BE REMOVED WHEN NO WORK IS BEING PERFORMED. ANY UNATTENDED OBSTACLE OR EXCAVATION IN THE WORK AREA, WHICH IN THE OPINION OF THE TRAFFIC ENGINEER CONSTITUTES A HAZARD, SHALL BE PROTECTED BY BARRICADES WITH FLASHING LIGHTS AT NIGHT AT THE POINTS OF HAZARD. STEADY BURNING LIGHTS SHALL BE USED FOR DELINEATION AND LONG LINE GUIDANCE. BARRICADES SHALL BE PLACED ACCORDING TO MAXIMUM SPACING VALUES LISTED IN THE TABLE BELOW.

7. IF THE WORK OPERATION REQUIRES ANY WORK VEHICLE TO ENTER OR LEAVE THE THROUGH TRAFFIC LANES, A FLAGGER SHALL BE PROVIDED AND THE FLAGGER SIGN SHALL BE SUBSTITUTED FOR THE WORKER SIGN. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO STATION TO PROTECT THE FLAGGER. FLAGGER IS NOT REQUIRED FOR 25 MPH OR LESS RESIDENTIAL STREETS.
8. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
9. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
10. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

13. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

TYPICAL APPLICATIONS

- SHOULDER WORK
- MOWING
- UTILITY OPERATIONS

11. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

SYMBOLS

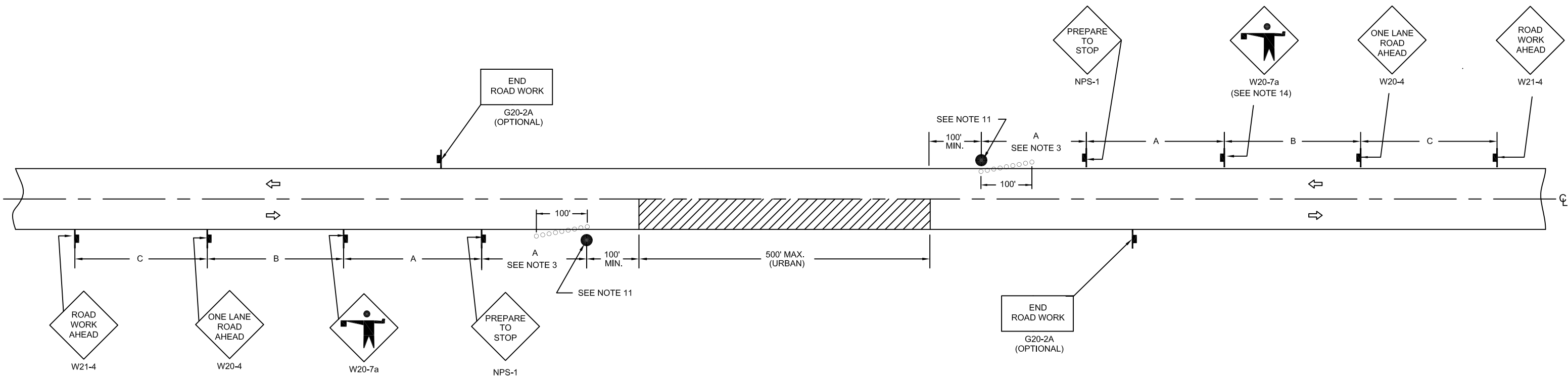
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TRAFFIC DIRECTION

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

12. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		

Effective 07/01/10 - 12/30/10



SHALL BE USED WHEN PRIME COAT IS APPLIED TO PAVEMENT AND SHALL REMAIN UNTIL NO TRACKING. INSTALL A MINIMUM OF 500 FT. PRECEDING START OF PRIME.



- CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE, LEAVING THE OPPOSITE LANE OPEN TO TRAFFIC. AT LEAST 500ft. OF BOTH TRAFFIC LANES SHALL BE AVAILABLE FOR TRAFFIC MOVEMENT AT INTERVALS NOT GREATER THAN 1,000 FT. A SPECIAL TRAFFIC CONTROL DETAIL MUST BE APPROVED FOR ANY PROJECT EXPECTED TO EXCEED 1,000 FT. IN LENGTH.
 - THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIME.
 - MAXIMUM DISTANCE TO BE DETERMINED BY THE TRAFFIC ENGINEER BUT SHOULD NOT EXCEED 1/2 THE LENGTH REQUIRED FOR ONE NORMAL WORKING DAY'S OPERATION OR 1,000 FT., WHICHEVER IS LESS.
 - IF THE WORK OPERATION DOES NOT EXCEED 60 MINUTES, TRAFFIC CONTROL WILL BE IN CONFORMANCE WITH STANDARD DRAWING NO. 603.
 - ALL SIGNS ARE TO BE REMOVED AT COMPLETION OF THE DAY'S OPERATIONS.
 - FOR DIVIDED ROADWAYS THE REQUIRED ADVANCE WARNING SIGNS SHALL BE POSTED ON BOTH THE RIGHT AND LEFT SIDE OF THE AFFECTED APPROACH.
 - FOR MULTILANE ROADWAYS, THE FLAGGER AND THE ADVANCE WARNING SIGNS SHOWN MAY BE REQUIRED FOR TRAFFIC APPROACHING FROM THE OPPOSITE DIRECTION. "RIGHT LANE CLOSED AHEAD" SIGNS SHALL BE SUBSTITUTED FOR THE "ONE LANE ROAD AHEAD" SIGNS.
 - THIS CASE ALSO APPLIES WHEN WORK IS BEING PERFORMED IN LANES ADJACENT TO THE CENTERLINE OF AN UNDIVIDED MULTILANE HIGHWAY OR ADJACENT TO THE MEDIAN ON A DIVIDED HIGHWAY. UNDER THESE CONDITIONS, "LEFT LANE CLOSED AHEAD" SIGNS SHALL BE SUBSTITUTED FOR "RIGHT LANE CLOSED AHEAD" SIGNS.
- THIS CASE DOES NOT APPLY WHEN WORK IS BEING PERFORMED IN THE MIDDLE LANE(S) OF A SIX OR MORE LANE HIGHWAY. SPECIAL TRAFFIC CONTROL DETAILS APPROVED BY THE TRAFFIC ENGINEER WILL BE REQUIRED.

Effective 07/01/10 - 12/30/10

GENERAL NOTES

- "ONE LANE ROAD AHEAD" AND FLAGGER SIGNS SHALL BE REMOVED OR COVERED WHEN NO WORK IS BEING PERFORMED.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGERS MAY BE VARIED FROM THAT SHOWN. IF A CURVED SECTION OF ROADWAY IS INVOLVED FLAGGER SHOULD BE PLACED AT THE BEGINNING OF CURVE (P.C.).
- ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- IN MULTILANE SITUATIONS, THE FLAGGER AND FLAGGER WARNING SIGNS ON THE SIDE OPPOSITE TO THE WORK AREA ARE REQUIRED ONLY WHEN TRAFFIC IN ANY DIRECTION IS MADE TO CROSS THE ROAD CENTER LINE.
- TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600
- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462

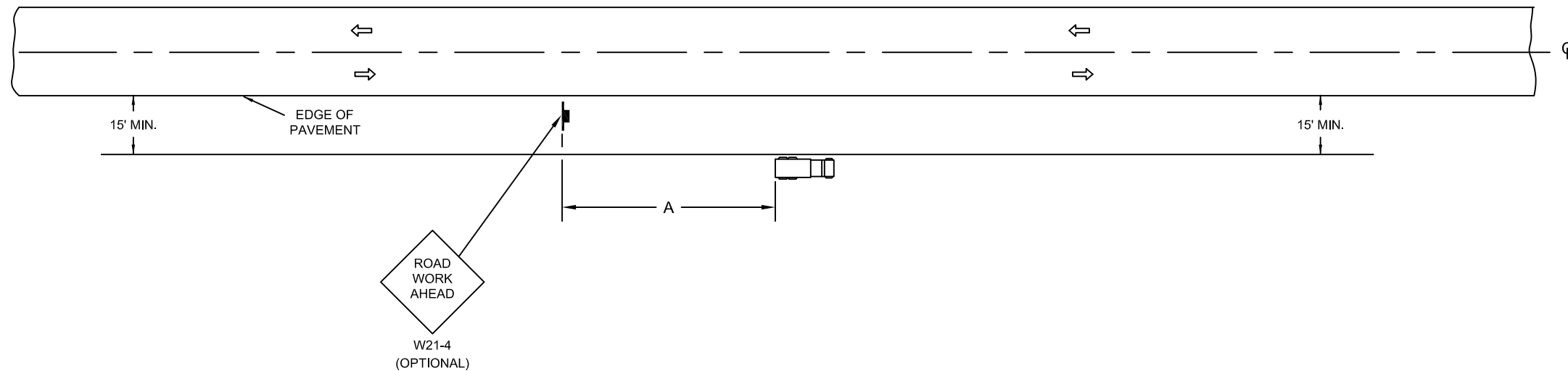
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

TYPICAL APPLICATIONS

- BITUMINOUS RESURFACING
- CRACK POURING
- UTILITY OPERATIONS

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH TRAFFIC CONTROL SIGN
- TRAFFIC CONE
- TRAFFIC DIRECTION





GENERAL NOTES

1. NO SPECIAL SIGNING IS REQUIRED.
2. IF THE WORK OPERATION REQUIRES ANY WORK VEHICLES TO CROSS THE 15 FT. CLEAR ZONE, TRAFFIC CONTROL SHALL CONFORM WITH STANDARD DRAWING NO. 607.
3. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
4. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.

SYMBOLS

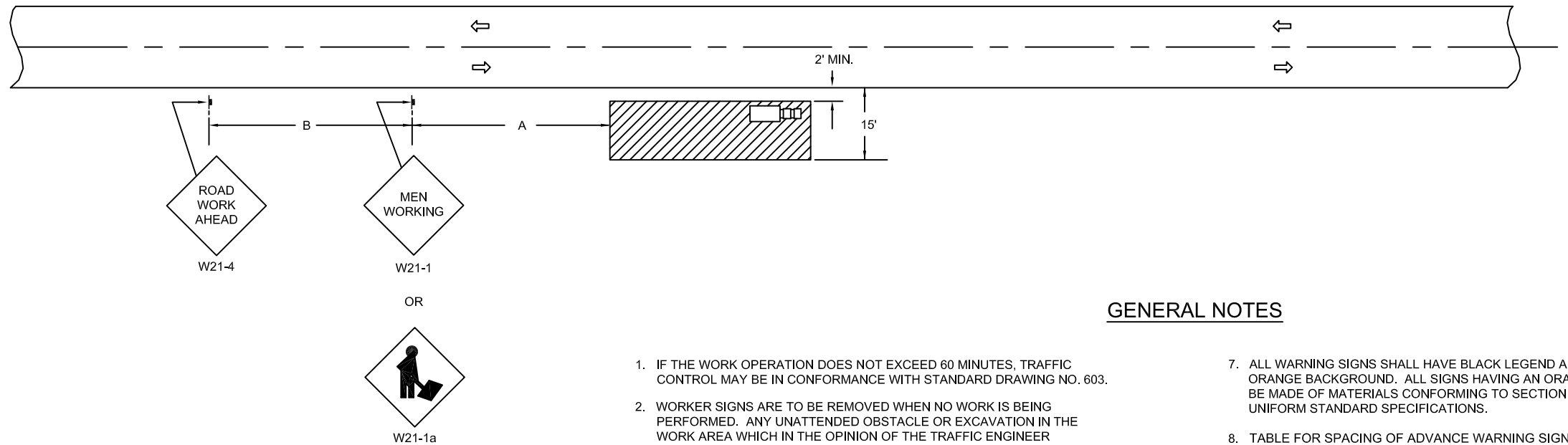
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TRAFFIC DIRECTION

TYPICAL APPLICATIONS

- LANDSCAPING WORK
- UTILITY WORK
- FENCING CONTRACTS AND MAINTENANCE
- CLEANING CULVERTS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

Effective 07/01/10 - 12/30/10



TYPICAL APPLICATIONS

- UTILITY OPERATION
- CULVERT EXTENSIONS
- SIDE SLOPE CHANGES
- GUARD RAIL INSTALLATION AND MAINTENANCE
- DELINEATOR INSTALLATION AND MAINTENANCE
- LANDSCAPING OPERATIONS
- CLEANING DITCHES AND DRAINAGE STRUCTURES
- SIGN INSTALLATION AND MAINTENANCE
- SHOULDER REPAIR

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TRAFFIC DIRECTION

GENERAL NOTES

1. IF THE WORK OPERATION DOES NOT EXCEED 60 MINUTES, TRAFFIC CONTROL MAY BE IN CONFORMANCE WITH STANDARD DRAWING NO. 603.
2. WORKER SIGNS ARE TO BE REMOVED WHEN NO WORK IS BEING PERFORMED. ANY UNATTENDED OBSTACLE OR EXCAVATION IN THE WORK AREA WHICH IN THE OPINION OF THE TRAFFIC ENGINEER CONSTITUTES A HAZARD SHALL BE PROTECTED BY BARRICADES WITH FLASHING LIGHTS AT NIGHT AT THE POINTS OF HAZARD. STEADY BURNING LIGHTS SHALL BE USED FOR DELINEATION AND LONG LINE GUIDANCE. BARRICADES SHALL BE PLACED ACCORDING TO MAXIMUM SPACING VALUES LISTED IN THE TABLE BELOW.

TAPER LENGTHS FOR LANE CLOSURES - DISTANCE L					
SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

3. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
4. IF THE WORK OPERATION REQUIRES ANY WORK VEHICLES TO ENTER OR LEAVE THROUGH TRAFFIC LANES, A FLAGGER SHALL BE PROVIDED AND A FLAGGER SIGN SHALL BE SUBSTITUTED FOR THE WORKER SIGN. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO STATION TO PROTECT THE FLAGGER. FLAGGER IS NOT REQUIRED FOR 25 MPH OR LESS RESIDENTIAL STREETS.
5. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
6. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.

7. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

8. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

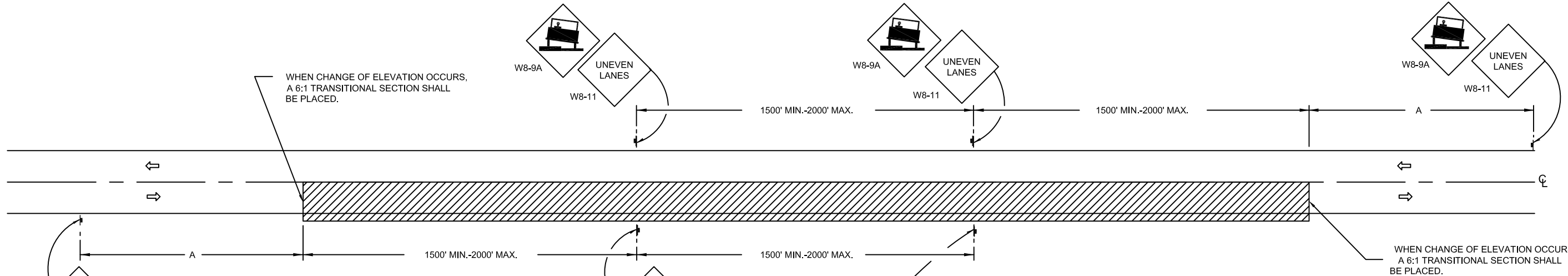
ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

9. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462

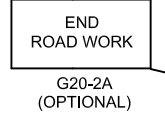
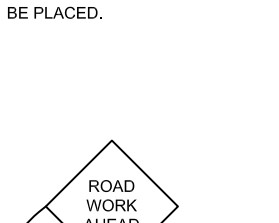
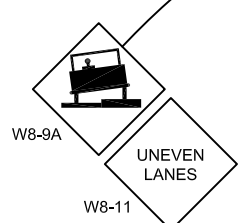
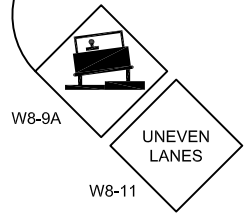
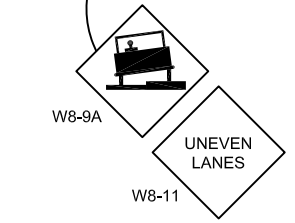
10. FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
11. IF WORKSPACE IS IN THE MEDIAN OF A DIVIDED HIGHWAY, AN ADVANCE WARNING SIGN SHOULD ALSO BE PLACED ON THE LEFT SIDE OF THE DIRECTIONAL ROADWAY.
12. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

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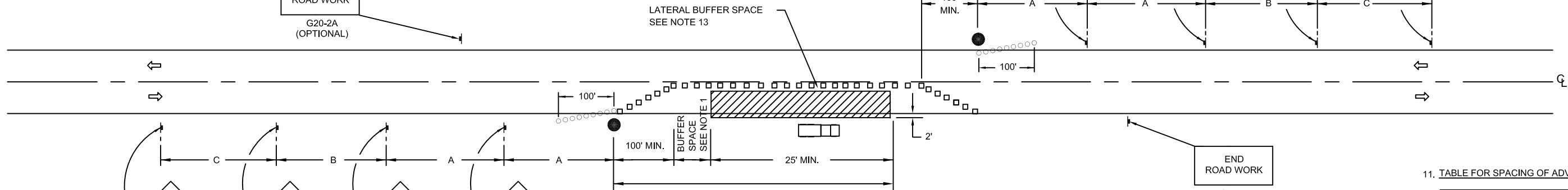


TYPICAL PLACEMENT OF DROP OFF & UNEVEN LANE SIGNS

PLACED WHEN DROP OFF OF LESS THAN 3 IN. EXIST DURING WORKING OR NON-WORKING HOURS. ANY LARGER DROP REQUIRES CONFORMANCE WITH APPROPRIATE TRAFFIC CONTROL PLANS INVOLVING PORTABLE CONCRETE BARRIER RAIL (STANDARD DRAWING NO. 602 SHEETS 1 & 2) OR OTHER POSITIVE PROTECTION.



LATERAL BUFFER SPACE
SEE NOTE 13



DEVICES SPACED IN ACCORDANCE WITH NOTES 1 & 2. IF CONES ARE USED, DOUBLE THE NUMBER OF DEVICES.

BUFFER SPACE	
SPEED (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

2. ALL DEVICES ESTABLISHING A TAPER OR A TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.

GENERAL NOTES

- CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE, LEAVING THE OPPOSITE LANE OPEN TO TRAFFIC. AT LEAST 500 FT. OF BOTH TRAFFIC LANES SHALL BE AVAILABLE FOR TRAFFIC MOVEMENT AT INTERVALS NOT GREATER THAN 1000 FT.
- IF THE WORK OPERATION DOES NOT EXCEED 60 MINUTES, TRAFFIC CONTROL MAY BE IN CONFORMANCE WITH STANDARD DRAWING NO. 603.
- THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES.
- WHEN NO WORK IS BEING PERFORMED, THE FLAGGERS WILL NOT BE REQUIRED. IF THE FLAGGERS ARE NOT PRESENT, THE FLAGGER SIGNS SHALL BE REMOVED OR COVERED.
- ALL SIGNS, CONES, BARRICADES, AND DRUMS SHALL BE REMOVED AT COMPLETION OF OPERATIONS DURING DAYLIGHT AND THE WORK AREA OPEN TO TRAFFIC.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGER MAY BE VARIED FROM SHOWN.
- ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS), AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

11. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT.)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

12. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200
CLARK COUNTY	455-6100
HENDERSON	565-2140
LAS VEGAS	229-6331
MESQUITE	346-5295
NORTH LAS VEGAS	642-2462

13. A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.

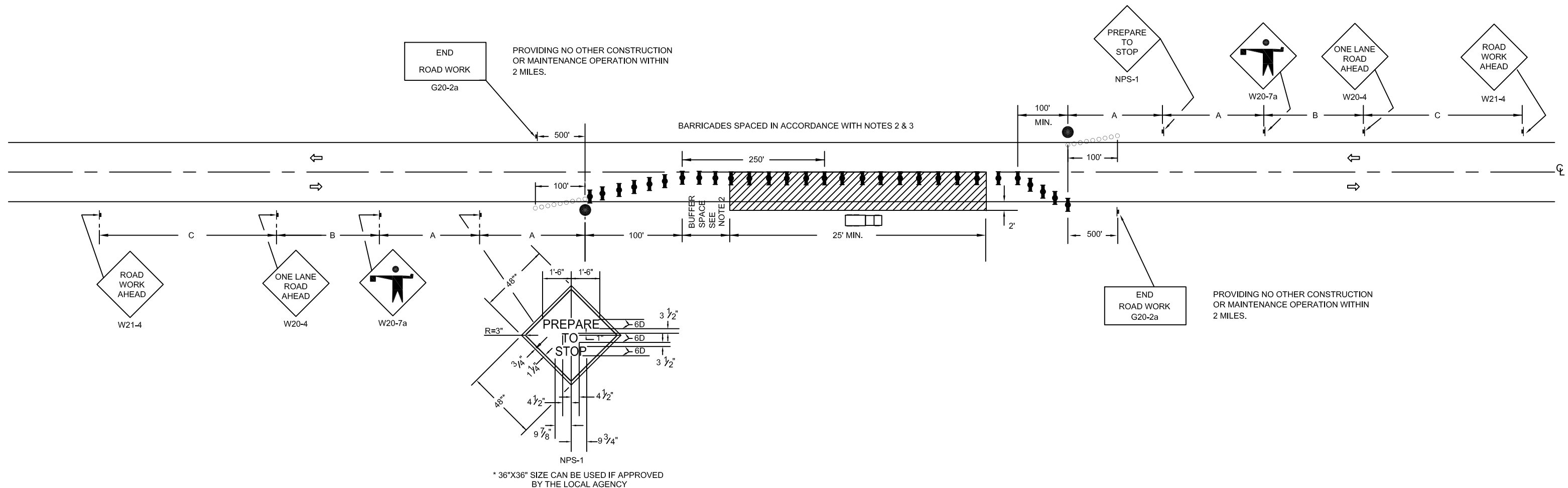
14. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

TYPICAL APPLICATIONS

UTILITY OPERATIONS
PAVEMENT PATCH

- SYMBOLS**
- WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - FLAGGER WITH TRAFFIC CONTROL SIGN
 - CONE, BARRICADE OR DRUM
 - TRAFFIC CONE
 - TRAFFIC DIRECTION

Effective 07/01/10 - 12/31/10



TYPICAL APPLICATIONS

- ISOLATED PATCH
- INSTALLATION OF DRAINAGE STRUCTURE
- UTILITY OPERATIONS

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH TRAFFIC CONTROL SIGN
- TRAFFIC CONE
- TRAFFIC DIRECTION
- BARRICADE OR DRUM WITH STEADY BURNING LIGHT

- CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE, LEAVING THE OPPOSITE LANE OPEN TO TRAFFIC.
-

BUFFER SPACE	
SPEED * (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

TAPER LENGTHS FOR LANE CLOSURES - DISTANCE L					
SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

Effective 07/01/10 - 12/30/11

GENERAL NOTES

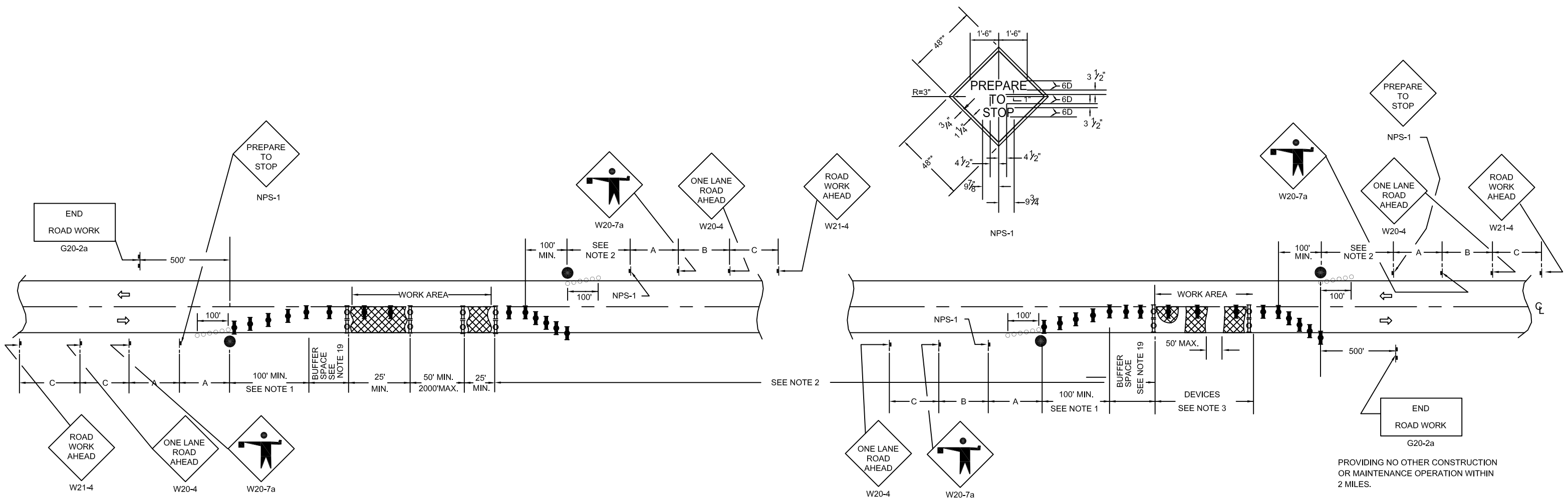
- ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
- THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES.
- FLAGGERS SHALL BE REQUIRED AT ALL ENTRY POINTS INTO THE WORK AREA.
- ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGERS MAY BE VARIED FROM THAT SHOWN.
- ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL BARRICADE LIGHTS SHALL BE BIDIRECTIONAL, EXCEPT LIGHTS ON TAPER BARRICADES, WHICH SHALL BE MONODIRECTIONAL.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

12. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

TABLE FOR SPACING OF ADVANCE WARNING SIGNS			
ROAD TYPE	SPACING (FT.)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
- FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
- A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.



GENERAL NOTES

- ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
- WHEN THE DISTANCE BETWEEN SUCCESSIVE PATCHES IS LESS THAN 2,000 FT., THE ENTIRE OPERATION MAY BE CONSIDERED AS ONE WORK AREA FOR SIGNING PURPOSES. WHEN SINGLE-SIGNING SET-UP IS USED FOR WORK SITES UP TO 2,000 FT. APART, LANE CLOSURE SHOULD BE CONTINUED THROUGH ENTIRE WORK. WHEN THE DISTANCE BETWEEN SUCCESSIVE PATCHES EXCEEDS 2,000 FT., ADDITIONAL WARNING SIGNS AND TAPER SHALL BE PLACED AS REQUIRED FOR DISTANCES LESS THAN 2,000 FT. AT THE DISCRETION OF THE TRAFFIC ENGINEER.
- WHERE SUCCESSIVE PATCHES ARE MORE THAN 50 FT. BUT LESS THAN 2,000 FT. APART, BARRICADES WITH FLASHING LIGHTS SHALL BE PLACED ON THE PAVEMENT BEYOND AND IN ADVANCE OF THE WORK AREAS. ALL BARRICADES WILL HAVE THE REFLECTIVE SURFACE FACING TRAFFIC AT NIGHT.
- THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIME AND SHALL BE POSITIONED TO PROTECT THE WORKERS. THE FIRST FLAGGER SHALL BE A MINIMUM OF 200 FT. AND A MAXIMUM DISTANCE OF 1/2 DAY'S NORMAL OPERATION BEYOND THE FLAGGER SIGN AND A MINIMUM OF 100 FT. IN ADVANCE OF THE WORK PARTY.
- FLAGGERS SHALL BE REQUIRED AT ALL ENTRY POINTS INTO THE WORK AREA.
- WHEN NO WORK IS BEING PERFORMED, THE FLAGGERS WILL NOT BE REQUIRED. IF THE FLAGGERS ARE NOT PRESENT, THE FLAGGER SIGNS SHALL BE REMOVED OR COVERED, AND ALL BARRICADES OR CONES SHALL BE REMOVED.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES FOR DAY OPERATIONS.
- WHEN THE DISTANCE BETWEEN PATCHES IS LESS THAN 500 FT., THE BARRICADE ADJACENT TO THE EDGE OF PAVEMENT ON THE FAR SIDE OF THE PATCH MAY BE OMITTED.

- ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGERS MAY BE VARIED FROM THAT SHOWN.
- ALL BARRICADE LIGHTS SHALL BE BIDIRECTIONAL, EXCEPT LIGHTS ON TAPER BARRICADES, WHICH SHALL BE MONODIRECTIONAL.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE, LEAVING THE OPPOSITE LANE OPEN TO TRAFFIC. AT LEAST 500 FT. OF BOTH TRAFFIC LANES SHALL BE AVAILABLE FOR TRAFFIC MOVEMENT AT INTERVALS NOT GREATER THAN 1000 FT. TWO FLAGGERS SHALL BE REQUIRED FOR EACH SEPARATE CONSTRUCTION OPERATION. ALL WORK AREAS SHALL BE PROTECTED DURING PERIODS WHEN WORKERS ARE PRESENT BY CONES OR BARRICADES ALONG THE CENTER LINE.
- DURING DAYTIME OPERATIONS, CONES MAY BE SUBSTITUTED FOR BARRICADES AT HALF THE BARRICADE SPACING.

17. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST 2 WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200
CLARK COUNTY	455-6100
HENDERSON	565-2140
LAS VEGAS	229-6331
MESQUITE	346-5295
NORTH LAS VEGAS	642-2462

19.

BUFFER SPACE	
SPEED (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

- A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.
- FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF IF RE-ROUTING NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

TAPER LENGTHS FOR LANE CLOSURES - DISTANCE L					
SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

TYPICAL APPLICATIONS

- CABLE LAYING
- OVERHEAD STRINGING OF CABLE
- PATCHING

SYMBOLS

- PATCH
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH TRAFFIC CONTROL SIGN
- BARRICADE OR DRUM WITH OPTIONAL FLASHING LIGHT
- BARRICADE OR DRUM WITH STEADY BURNING LIGHT
- TRAFFIC CONES
- TRAFFIC DIRECTION

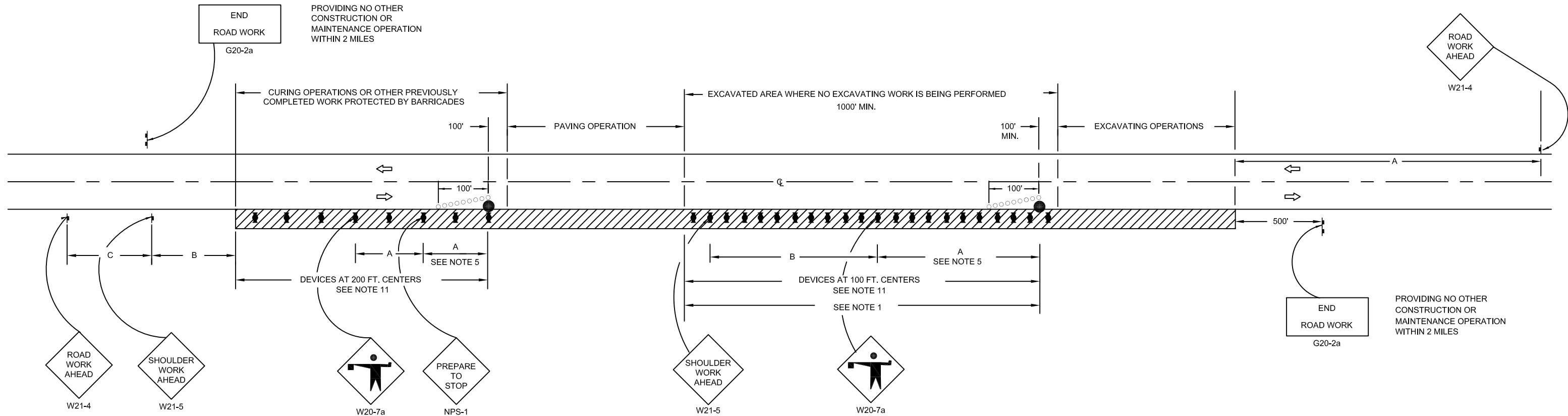


TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

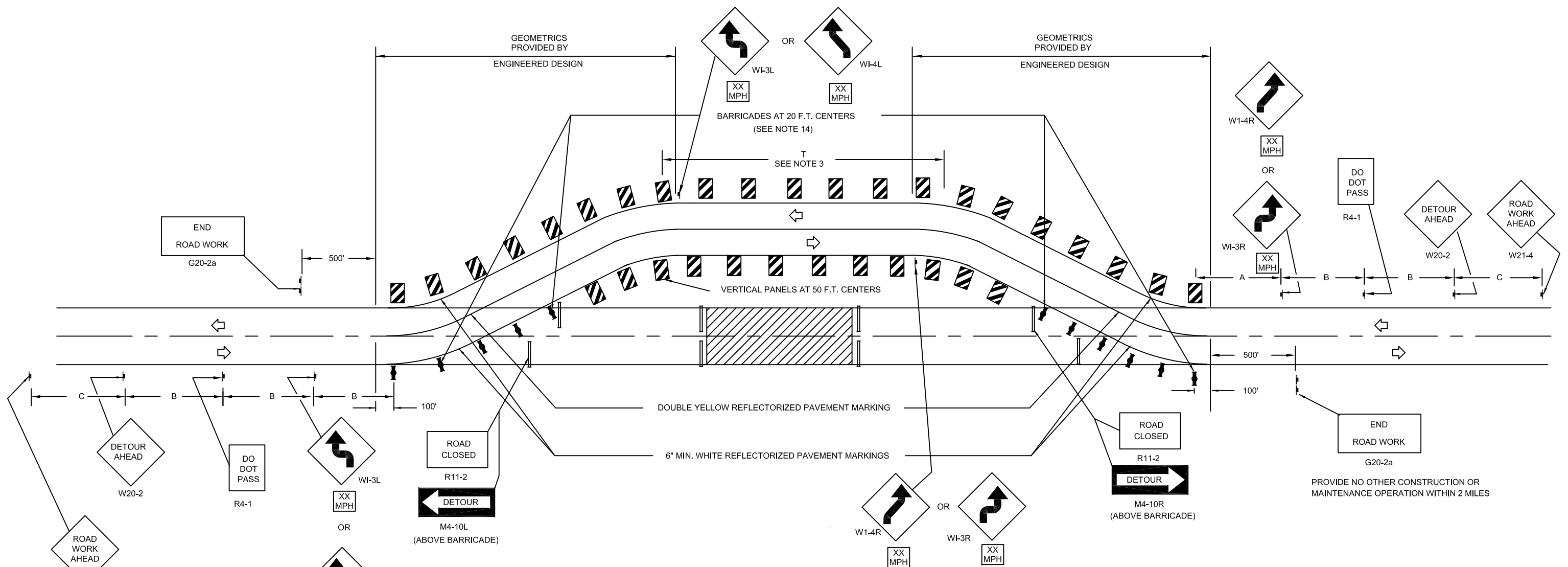
- WHERE THE DISTANCE BETWEEN PAVING AND EXCAVATING OPERATIONS IS LESS THAN 2,000 FT. THE ENTIRE OPERATION MAY BE CONSIDERED AS ONE WORK AREA FOR SIGNING PURPOSES. WHEN THE DISTANCE BETWEEN OPERATIONS EXCEEDS 2,000 FT. ADDITIONAL WARNING SIGNS SHALL BE PLACED AS SHOWN. UNDER RESTRICTED SIGHT DISTANCE CONDITIONS, SUCH ADDITIONAL DEVICES MAY ALSO BE REQUIRED FOR DISTANCES LESS THAN 2,000 FT. AT THE DISCRETION OF THE TRAFFIC ENGINEER.
- ONE FLAGGER SHALL BE REQUIRED FOR EACH SEPARATE CONSTRUCTION OPERATION. FOR RESIDENTIAL STREETS 25 MPH OR LESS, FLAGGERS MAY BE REQUIRED AS DIRECTED BY THE TRAFFIC ENGINEER.
- THE FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES.
- NO PAVING OR EXCAVATING OPERATIONS SHALL BE PERFORMED AT NIGHT UNLESS AUTHORIZED BY THE TRAFFIC ENGINEER.
- MAXIMUM DISTANCE TO BE DETERMINED BY THE TRAFFIC ENGINEER BUT IN NO CASE TO EXCEED THE LENGTH OF 1/2 DAY'S NORMAL OPERATION.
- ALL SIGNS SHALL BE GROUND-MOUNTED IF THE WORKING TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.

GENERAL NOTES

- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGER MAY BE VARIED FROM THAT SHOWN.
- ALL VEHICLES, EQUIPMENT, WORKERS, FLAGGERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE ROADWAY UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462

- SYMBOLS**
- WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - FLAGGER WITH TRAFFIC CONTROL SIGN
 - BARRICADE OR DRUM WITH STEADY BURNING LIGHT
 - TRAFFIC CONE
 - TRAFFIC DIRECTION
- FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
 - ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.



GENERAL NOTES

- ALL TEMPORARY BYPASSES SHALL BE PAVED WHEN DURATION EXCEEDS TIME LIMITS ESTABLISHED BY THE ENTITY. GRADED & COMPACTED GRAVEL ACCEPTABLE FOR DURATIONS ESTABLISHED BY THE ENTITY. SEE SHEET 2 OF THIS DRAWING FOR UNPAVED TEMPORARY BYPASS.
- ON PAVED BYPASSES, REFLECTIVE EDGE LINES AND A CENTER LINE REFLECTORIZED, REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE SHALL BE USED FOR MARKING THE EDGE LINES AND CENTER LINE ON EXISTING PAVEMENT. (REFLECTORIZED PAVEMENT MARKING PAINT MAY BE USED FOR MARKINGS ON THE PAVED BYPASSES.) RAISED REFLECTIVE PAVEMENT MARKERS CONFORMING TO CLARK COUNTY STANDARDS MAY BE USED IN LIEU OF TAPE OR PAINT WHERE THE PAVEMENT MARKING IS TO BE PLACED ADJACENT TO BARRICADES OR VERTICAL PANELS. ALL EXISTING PAVEMENT MARKING WHICH CONFLICT WITH THE REVISED TRAFFIC PATTERN SHALL BE REMOVED. IF STRIPING IS TO BE PLACED ON FINAL PAVEMENT ONLY REMOVABLE, NON-FOIL TAPE SHALL BE USED. EDGE LINE MARKINGS SHALL BE A MINIMUM OF 6 IN. WIDE.
- WHEN (T) IS GREATER THAN 800 FEET, 4 FOOT LENGTHS OF SINGLE YELLOW REFLECTORIZED, REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE AT 40 FOOT CENTERS MAY BE USED WITHIN THE TANGENT SECTION (ONLY IF PASSING ZONES CAN BE SAFELY ALLOWED IN ACCORDANCE WITH MUTCD SECTS. 3B-3 THRU. 3B-5).
- A CURVE SIGN WILL BE REQUIRED AT EXIT END OF THE BYPASS IF (T) IS EQUAL TO OR GREATER THAN 1,000 FEET.
- THE ADVISORY SAFE SPEED TO BE SHOWN BELOW THE REVERSE CURVE (TURN) SIGNS SHALL BE DETERMINED AT THE SITE AND APPROVED BY THE TRAFFIC ENGINEER.
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES FOR DAY OPERATIONS.
- CONES MAY BE SUBSTITUTED FOR BARRICADES AT HALF THE SPACING DURING DAY OPERATIONS.
- ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
- ALL BARRICADE LIGHTS SHALL BE BIDIRECTIONAL.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- TABLE FOR SPACING OF ADVANCE WARNING SIGNS
- ALL DEVICES INDICATED SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED.
- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.







BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
- WHERE THE TEMPORARY PAVEMENT AND OLD PAVEMENT ARE DIFFERENT COLORS, THE TEMPORARY PAVEMENT SHOULD START ON THE TANGENT OF THE EXISTING PAVEMENT AND END ON THE TANGENT OF THE EXISTING PAVEMENT.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
- IF THE DETOUR IS SHORT AND HAS SHARP CURVES (30 MPH OR LESS), REVERSE TURN (W1-3) SIGN SHOULD BE USED.
- PAVEMENT MARKINGS THAT ARE NO LONGER APPLICABLE SHALL BE MASKED WITH APPROVED BLACKOUT TAPE OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER.

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

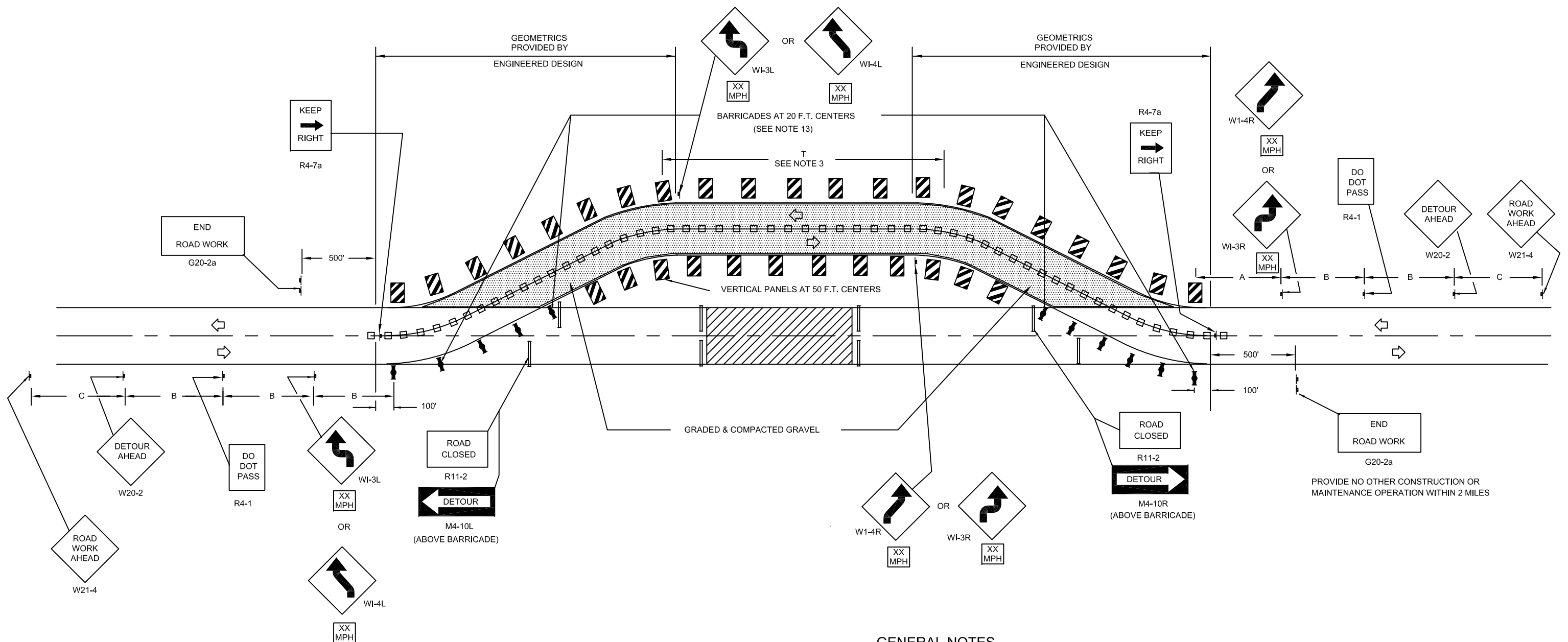
TYPICAL APPLICATIONS

BRIDGE CONSTRUCTION
CULVERT CONSTRUCTION

SYMBOLS

-  WORK AREA
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  BARRICADE OR DRUM WITH STEADY BURNING LIGHT
-  VERTICAL PANEL (SEE STANDARD DRAWING NO. 601 SHEET 1)
-  TYPE III BARRICADE
-  TRAFFIC DIRECTION

Effective 07/01/10 - 12/30/10



TYPICAL APPLICATIONS

BRIDGE CONSTRUCTION
CULVERT CONSTRUCTION

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE OR DRUM WITH STEADY BURNING LIGHT
- VERTICAL PANEL (SEE STANDARD DRAWING NO. 601 SHEET 1)
- TYPE III BARRICADE
- TRAFFIC DIRECTION
- TRAFFIC CONE OR VERTICAL PANEL (SEE NOTE 2)

1. TEMPORARY UNPAVED BYPASSES SHALL BE GRADED AND COMPACTED GRAVEL AND ARE ACCEPTABLE FOR TIME LIMITS ESTABLISHED BY THE ENTITY.
2. REFLECTORIZED 28 IN. MIN. TRAFFIC CONES OR VERTICAL PANELS SHALL BE USED FOR CENTERLINE DELINEATION FOR SHORT-TERM PROJECTS. FOR LONG TERM PROJECTS OF 72 CONTINUOUS HOURS OR MORE AND PROJECTS PERFORMED AT NIGHT, VERTICAL PANELS SHALL BE USED. SEE STANDARD DRAWING 601 SHEET 1 FOR DETAILS OF CONES AND PANELS.
3. A CURVE SIGN WILL BE REQUIRED AT EXIT END OF THE BYPASS IF (T) IS EQUAL TO OR GREATER THAN 1,000 FEET.
4. THE ADVISORY SAFE SPEED TO BE SHOWN BELOW THE REVERSE CURVE (TURN) SIGNS SHALL BE DETERMINED AT THE SITE AND APPROVED BY THE TRAFFIC ENGINEER.
5. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES FOR DAY OPERATIONS.
6. CONES MAY BE SUBSTITUTED FOR BARRICADES AT HALF THE SPACING DURING DAY OPERATIONS.
7. ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.

GENERAL NOTES

8. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
9. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
10. ALL BARRICADE LIGHTS SHALL BE BIDIRECTIONAL.
11. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

12. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

13. ALL DEVICES INDICATED SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED.
14. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
15. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
16. IF THE DETOUR IS SHORT AND HAS SHARP CURVES (30 MPH OR LESS), REVERSE TURN (W1-3) SIGN SHOULD BE USED.
17. PAVEMENT MARKINGS THAT ARE NO LONGER APPLICABLE SHALL BE MASKED WITH APPROVED BLACKOUT TAPE OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER.

Effective 07/01/10 - 12/30/10

TRAFFIC CONTROL PLAN
FOR
HIGHWAY WORK ZONE

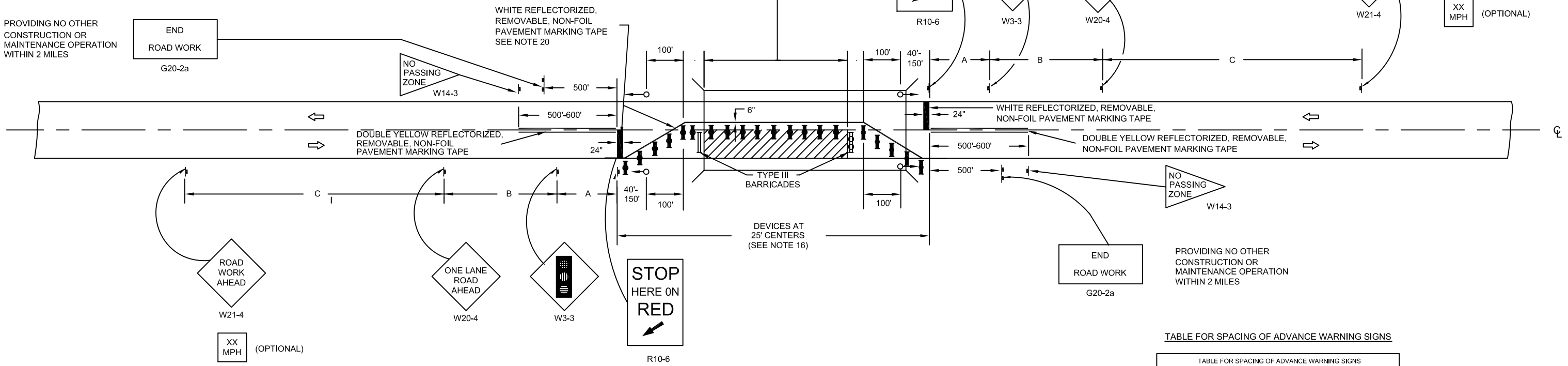
SPECIFICATION REFERENCE

TYPICAL APPLICATION FOR
2-LANE, 2-WAY, RURAL OR SUBURBAN, DAY OR
NIGHT TEMPORARY UNPAVED BYPASS

UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

SEQUENCE OF OPERATIONS						
PHASE	A		B		CONFLICT	
INTERVAL	1	2	3	4	5	6
NORTHBOUND OR EASTBOUND	G	Y	R	R	R	RED FLASH
SOUTHBOUND OR WESTBOUND	R	R	R	G	Y	RED FLASH

IF A.D.T. IS	MAXIMUM DISTANCE (FT.)
> 8000	300
> 5000 < 8000	500
< 5000	700



GENERAL NOTES

- THE TRAFFIC ENGINEER MUST BE NOTIFIED AT LEAST 72 HOURS PRIOR TO PLACING THE TEMPORARY SIGNALS IN OPERATION SO THAT ARRANGEMENTS CAN BE MADE TO INSPECT THE INSTALLATION AND SET THE TIMING OF THE SIGNALS.
- AT ANY TIME THAT THE SIGNALS ARE NOT OPERATING THE SIGNAL HEAD SHALL BE HOODED AND THE SIGNAL AHEAD SIGN COVERED OR REMOVED.
- THE LEFT SIGNAL HEAD SHALL NORMALLY BE MOUNTED AT A HEIGHT OF 10 FEET ABOVE THE ROAD SURFACE MEASURED TO THE BOTTOM OF THE SIGNAL HEAD. THE RIGHT HEAD SHALL NORMALLY BE MOUNTED AT A HEIGHT OF 14 FEET ABOVE THE ROAD SURFACE. BAFFLE OR BACK PLATES WILL BE REQUIRED ON ALL SIGNALS. A MAST ARM SIGNAL SHOULD BE USED ON THE RIGHT SIDE WHEN EVER POSSIBLE.
- ALL LENSES SHALL BE 12 INCH NOMINAL DIAMETER. THE RIGHT SIGNAL HEAD SHALL BE AIMED SO THE CENTERS OF THE LIGHT BEAMS OF THE INDICATIONS ARE DIRECTED TOWARD A POINT IN THE CENTER OF THE APPROACH LANE 500 FEET IN ADVANCE OF THE SIGNAL. THE LEFT INDICATION SHALL BE AIMED AT A POINT IN THE CENTER OF THE APPROACH LANE 100 FEET IN ADVANCE OF THE STOP LINE.
- EACH SIGNAL SHALL BE WIRED TO ALLOW FOR FLASH RED.
- IF FLAGGERS ARE USED INSTEAD OF TRAFFIC SIGNALS, THE TRAFFIC CONTROL DEVICES SHALL CONFORM TO STANDARD DRAWING NO. 601. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO FLAGGER STATION TO PROTECT THE FLAGGER. FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
- DURING DAYTIME OPERATIONS CONES MAY BE SUBSTITUTED FOR BARRICADES WITH STEADY BURNING LIGHTS AT HALF THE BARRICADE SPACING.
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED FOR DAY
- BIDIRECTIONAL LIGHTS SHALL BE USED AT NIGHT ALONG THE CENTER LINE WHETHER THE WORK AREA IS SEPARATED FROM THE TRAVELLED LANE USING BARRICADES OR BY USING OTHER DEVICES. MONODIRECTIONAL LIGHTS SHALL BE USED AT NIGHT ON ALL OTHER BARRICADES.
- ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
- ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462
- ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
- ADEQUATE AREA ILLUMINATION TO CLEARLY IDENTIFY BOTH ENDS OF THE WORK SPACE AT NIGHT FOR LONG-TERM OPERATIONS OF 72 CONTINUOUS HOURS OR MORE SHOULD BE PROVIDED.
- A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
- EDGE LINE SHALL BE A MINIMUM OF 6 IN. WIDE AND SHOULD BE INSTALLED FROM THE START OF THE TAPER TO A POINT BEYOND THE WORK AREA REJOINING THE PERMANENT EDGE LINE.
- FOR LONG TERM PROJECTS OF 72 CONTINUOUS HOURS OR MORE, CONFLICTING PAVEMENT MARKINGS BETWEEN ACTIVITY AREA AND STOP LINE SHALL BE REMOVED.

TABLE FOR SPACING OF ADVANCE WARNING SIGNS

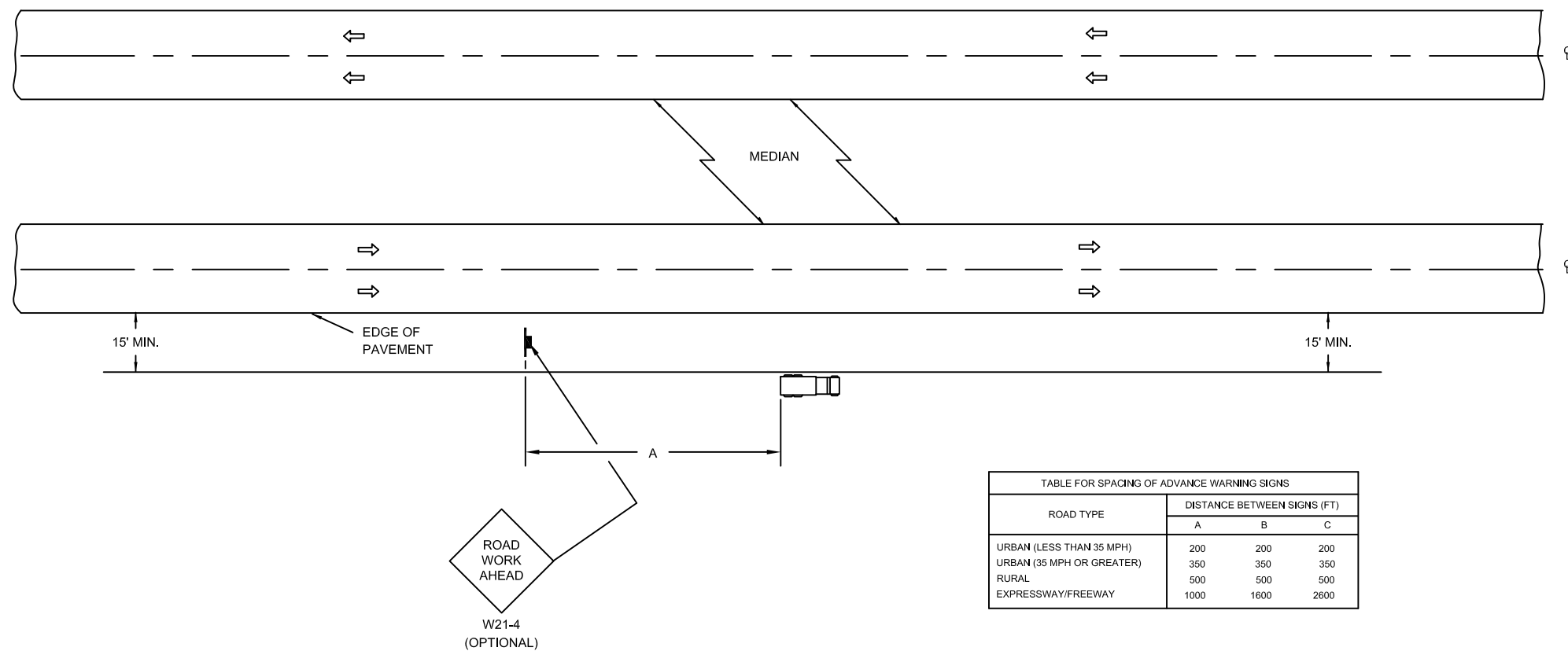
ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

TYPICAL APPLICATIONS

BRIDGE DECK REPAIR
ONE-HALF ROAD CONSTRUCTION

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE OR DRUM WITH FLASHING LIGHT
- BARRICADE OR DRUM WITH STEADY BURNING LIGHT
- BARRICADE OR DRUM
- TRAFFIC SIGNAL
- TRAFFIC DIRECTION





ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

TYPICAL APPLICATIONS
 LANDSCAPING WORK
 UTILITY WORK
 FENCING CONTRACTS AND MAINTENANCE
 CLEANING CULVERTS

GENERAL NOTES

- NO SPECIAL SIGNING IS REQUIRED.
- IF THE WORK OPERATION REQUIRES TWO OR MORE WORK VEHICLES CROSS THE 15 FT. CLEAR ZONE IN ANY ONE HOUR TRAFFIC CONTROL WILL BE IN CONFORMANCE WITH STANDARD DRAWING NO. 615.
- THIS CASE ALSO APPLIES TO WORK PERFORMED IN THE MEDIAN MORE THAN 15 FT. FROM EITHER PAVEMENT.
- IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.

SYMBOLS
 SIGN ON PORTABLE OR PERMANENT SUPPORT
 TRAFFIC DIRECTION

Effective 07/01/10 - 12/30/10

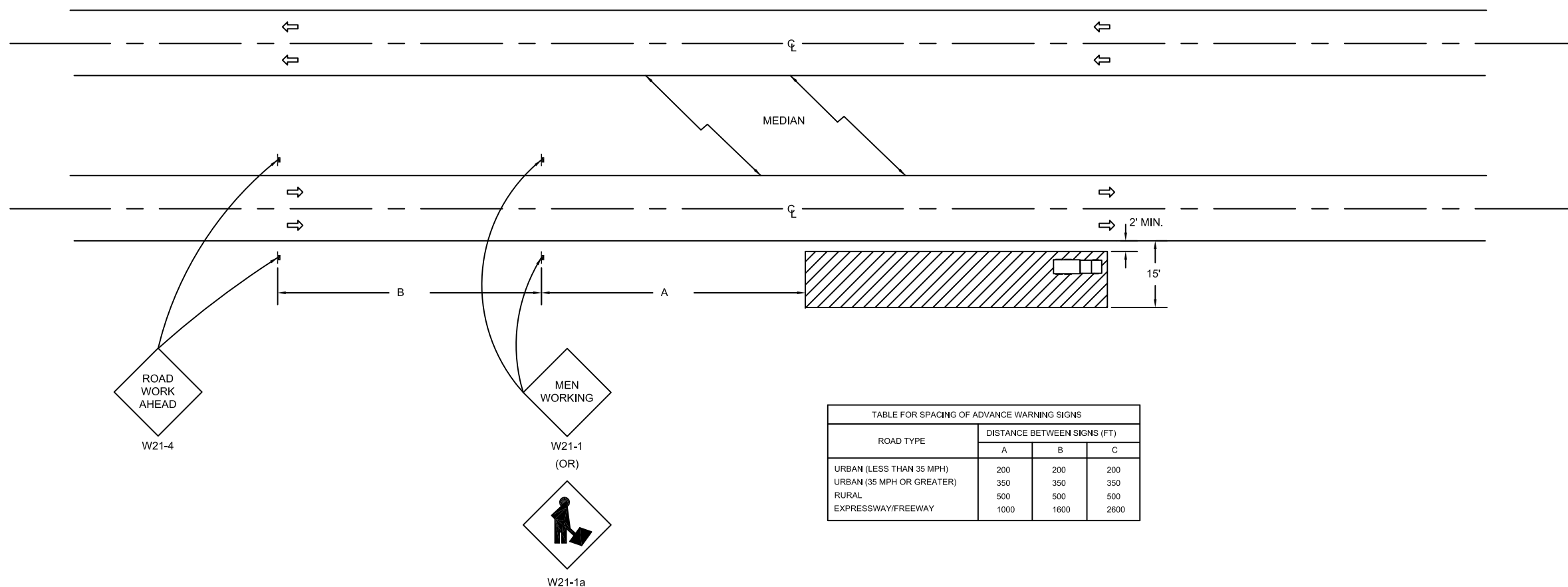


TABLE FOR SPACING OF ADVANCE WARNING SIGNS			
ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- SIGN ON PORTABLE OR PERMANENT SUPPORT

TYPICAL APPLICATIONS

- UTILITY OPERATIONS
- CULVERT EXTENSIONS
- SIDE SLOPE CHANGES
- GUARD RAIL INSTALLATION AND MAINTENANCE
- DELINEATOR INSTALLATION AND MAINTENANCE
- LANDSCAPING OPERATIONS
- CLEANING DITCHES AND DRAINAGE STRUCTURES
- SIGN INSTALLATION AND MAINTENANCE
- SHOULDER REPAIR

1. WORKER SIGNS ARE TO BE REMOVED WHEN NO WORK IS BEING PERFORMED. ANY UNATTENDED OBSTACLE OR EXCAVATION IN THE WORK AREA WHICH IN THE OPINION OF THE TRAFFIC ENGINEER CONSTITUTES A HAZARD SHALL BE PROTECTED BY BARRICADES WITH FLASHING LIGHTS AT NIGHT AT THE POINTS OF HAZARD. STEADY BURNING LIGHTS SHALL BE USED FOR DELINEATION AND LONG LINE GUIDANCE. BARRICADE SHALL BE PLACED ACCORDING TO MAXIMUM SPACING VALUES LISTED IN THE TABLE BELOW.

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
	20	70	75		
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

Effective 07/01/10 - 12/30/10

GENERAL NOTES

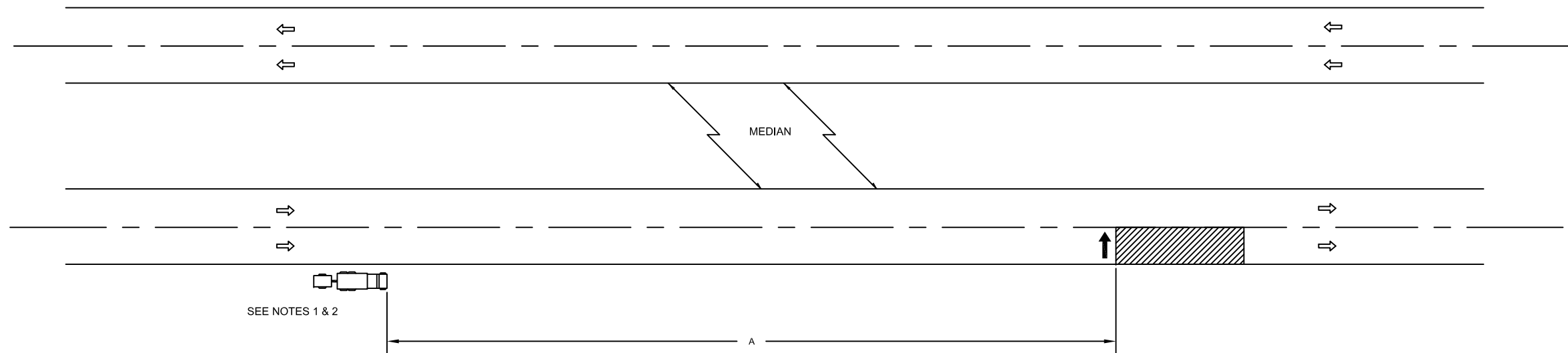
2. IF THE WORK OPERATION REQUIRES THAT FOUR OR MORE WORK VEHICLES ENTER THROUGH TRAFFIC LANES IN A ONE HOUR PERIOD, A FLAGGER SHALL BE SUBSTITUTED FOR THE WORKER SIGN. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO FLAGGER STATION TO PROTECT THE FLAGGER. FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
3. THIS CASE ALSO APPLIES WHEN WORK IS BEING PERFORMED ON A MULTILANE UNDIVIDED HIGHWAY. UNDER THESE CONDITIONS THE SIGNS NORMALLY MOUNTED IN THE MEDIAN SHALL BE OMITTED.
4. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS
5. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.

8. REQUIRED PROTECTION FOR OPEN EXCAVATIONS DURING NON-WORKING HOURS: OPEN TRENCHES SHALL BE COMPLETELY FENCED (ALL FENCES TO BE SIX(6) FOOT HIGH, NON-CLIMBABLE FENCE), BACKFILLED OR PLATED IN ANY AREA OF THE CITY OF LAS VEGAS THAT ARE WITHIN 300 FT. OF ANY BUILDING OR ROADWAY, AFTER WORKING HOURS. BEYOND 300', TRENCH MAY BE PROTECTED BY A THREE (3) FOOT MOUND OF EARTH COMPLETELY AROUND THE TRENCH AND TYPE II BARRICADES WITH FLASHERS SPOTTED AROUND THE TOP AFTER WORKING HOURS.

9. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

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CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		

10. IF THE WORK SPACE IS IN THE MEDIAN OF A DIVIDED HIGHWAY, AN ADVANCE WARNING SIGN SHOULD ALSO BE PLACED ON THE LEFT SIDE OF THE DIRECTIONAL ROADWAY.
11. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND



GENERAL NOTES

- HIGHWAY, THE PROTECTION VEHICLE SHALL FOLLOW ON THE LEFT SHOULDER AND THE BOTTOM LINE SHALL READ "USE RIGHT LANE".
- IF WORK IS BEING PERFORMED ON THE CENTER LANE OF THE ROADWAY, TRAFFIC SHALL BE DIVERTED TO EITHER LEFT OR RIGHT LANE. AT NO TIME IS TRAFFIC PERMITTED TO PASS ON BOTH SIDES OF MOVING OPERATION. CENTER LANE OPERATIONS SHALL NOT BE PERFORMED DURING PEAK TRAVEL TIMES.
- THE LIGHTS ON THE TRAILER SHALL FLASH IN PAIRS ALTERNATING BETWEEN THE TWO OUTSIDE LIGHTS AND THE TWO INSIDE LIGHTS OR SEQUENTIAL FLASHING.
- ALL STRIPING SHALL HAVE ALTERNATING WHITE AND ORANGE STRIPES AT 45° FROM THE VERTICAL. ALL STRIPES SHALL BE 6" IN WIDTH.
- THE SIGN PANELS SHALL HAVE THE MINIMUM DIMENSIONS SHOWN AND HAVE BLACK LEGEND ON AN ORANGE REFLECTORIZED BACKGROUND CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- PAVEMENT STRIPING AND CONE PICKUP WILL BE CONSIDERED AS TWO SEPARATE OPERATIONS.
- WHERE WORK OPERATIONS ARE MORE THAN 2 FT. FROM THE EDGE OF THE PAVEMENT, PROTECTION VEHICLES MAY BE OMITTED.
- THIS CASE DOES NOT APPLY WHEN WORK IS BEING PERFORMED IN THE MIDDLE LANE(S) OF A SIX OR MORE LANE HIGHWAY. SPECIAL PLANS APPROVED BY THE TRAFFIC ENGINEER ARE REQUIRED.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.
- ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL WARNING SIGNS HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

12. TABLE FOR SPACING OF FOLLOWING VEHICLE

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

13. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200
CLARK COUNTY	455-6100
HENDERSON	565-2140
LAS VEGAS	229-6331
MESQUITE	346-5295
NORTH LAS VEGAS	642-2462

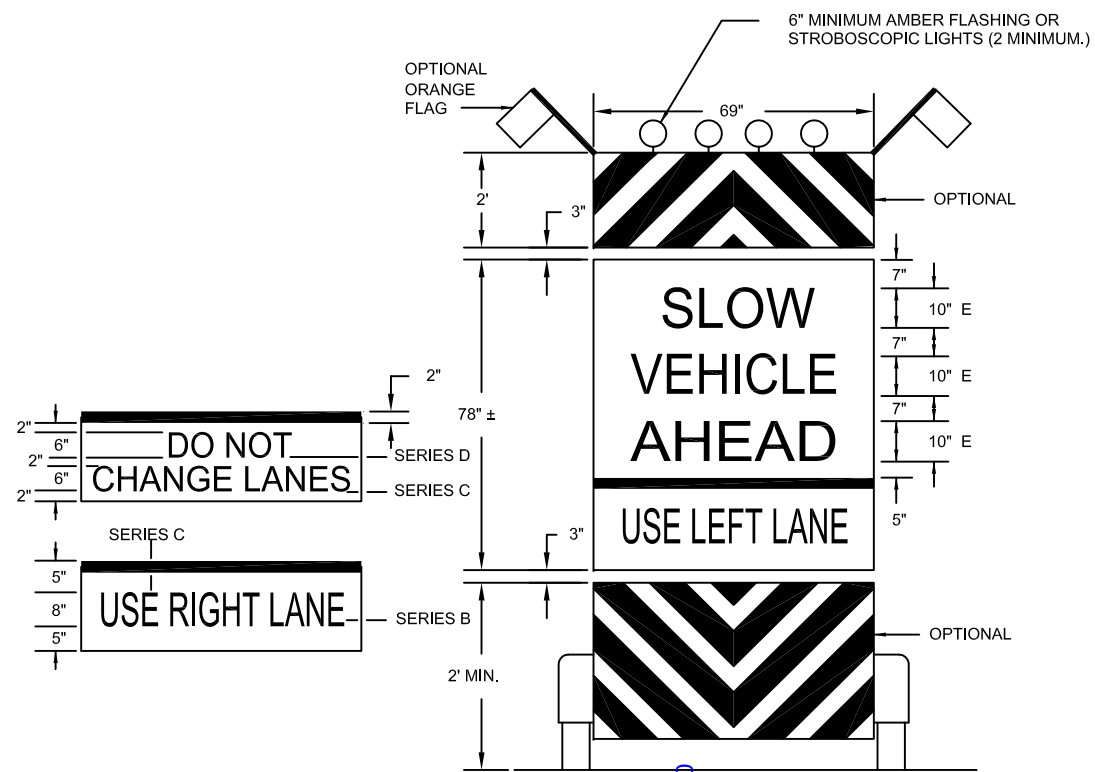
14. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

SYMBOLS

- WORK AREA
- VEHICLE MOUNTED ARROW PANEL
- TRAFFIC DIRECTION

TYPICAL APPLICATIONS

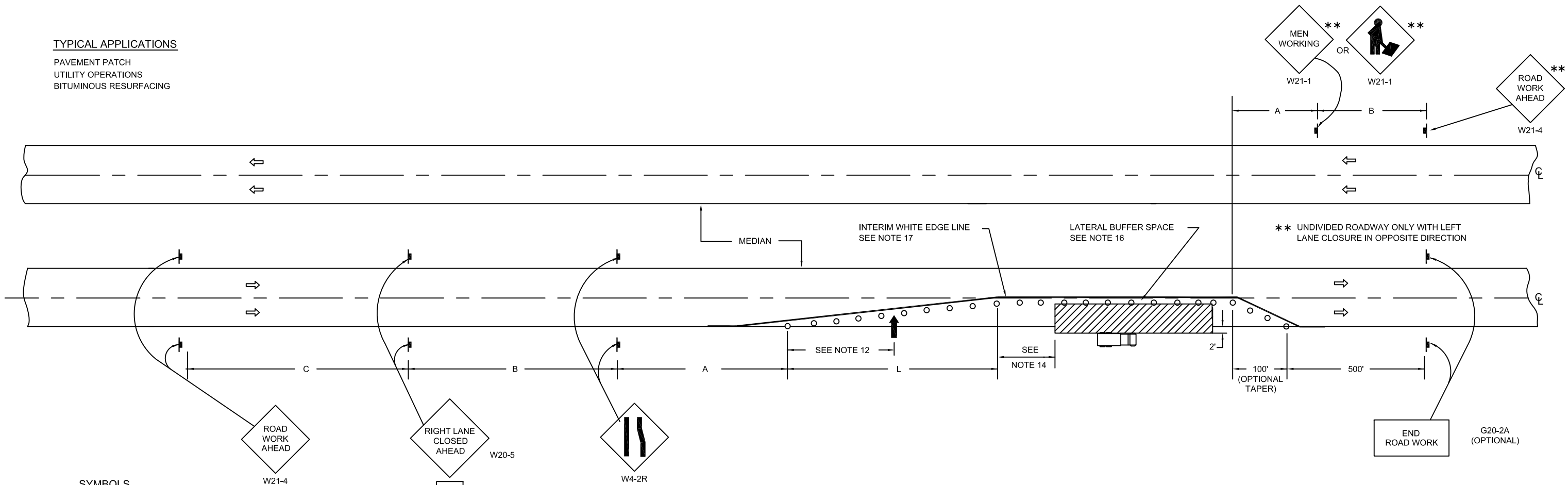
- PAVEMENT STRIPING
- WEED SPRAYING
- ROADMETER MEASUREMENTS



Effective 07/01/10 - 12/30/10

TYPICAL APPLICATIONS

PAVEMENT PATCH
UTILITY OPERATIONS
BITUMINOUS RESURFACING



** UNDIVIDED ROADWAY ONLY WITH LEFT LANE CLOSURE IN OPPOSITE DIRECTION

SYMBOLS

- ARROW BOARD
- WORK AREA
- CONE, DRUM OR BARRICADE
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TRAFFIC DIRECTION

1. THE "L" DISTANCE EQUALS:

FORMULAS FOR TAPER LENGTH

SPEED	FORMULA
40 M.P.H. OR UNDER	$L = WS^2/60$
40 M.P.H. OR OVER	$L = WS$

WHERE:
L = TAPER LENGTH
W = WIDTH OF LANE OR OFFSET
S = POSTED SPEED, OR OFF-PEAK 85TH PERCENTILE PRIOR TO WORK STARTING OR ANTICIPATED OPERATING SPEED

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

2. WHEN EQUIPMENT ENTERS OR EXITS THE WORK AREA DIRECTLY FROM THE ADJOINING LANE CARRYING TRAFFIC, A FLAGGER WILL BE REQUIRED. IF THE FLAGGER IS PRESENT, THE FLAGGER SIGN SHALL BE PLACED AT DISTANCE "A" PRIOR TO THE FLAGGER AND "PREPARE TO STOP" SIGN (NPS-1) SHALL BE PLACED AT DISTANCE "A" PRIOR TO FLAGGER SIGN. FLAGGERS SHALL NOT BE REQUIRED FOR SPEEDS OF 25 MPH OR LESS. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO FLAGGER STATION TO PROTECT THE FLAGGER. FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
 3. THIS CASE ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE LEFT LANE. UNDER THESE CONDITIONS, LEFT LANE CLOSED SIGNS SHALL BE SUBSTITUTED FOR RIGHT LANE CLOSED SIGNS. ON INDIVIDUAL HIGHWAYS, SIGNS SHALL BE ADDED IN THE OPPOSITE DIRECTION AS SHOWN AND CONES SHALL BE PLACED ALONG THE CENTERLINE THROUGHOUT THE TAPER AND WORK AREA.
 4. ALL SIGNS, CONES, BARRICADES AND DRUMS ARE TO BE REMOVED AT COMPLETION OF THE DAY'S OPERATIONS AND THE WORK AREA OPENED TO TRAFFIC.
- THIS CASE DOES NOT APPLY WHEN WORK IS BEING PERFORMED IN THE MIDDLE LANE(S) OF A SIX OR MORE LANE HIGHWAY. SPECIAL PLANS APPROVED BY THE TRAFFIC ENGINEER WILL BE REQUIRED.
- CONES SHALL BE A MINIMUM OF 28 IN. IN HEIGHT.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGERS, IF NECESSARY, MAY BE VARIED.

GENERAL NOTES

8. ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGERS) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
 9. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
 10. ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
 11. **TABLE FOR SPACING OF ADVANCE WARNING SIGNS**
- | ROAD TYPE | DISTANCE BETWEEN SIGNS (FT) | | |
|---------------------------|-----------------------------|------|------|
| | A | B | C |
| URBAN (LESS THAN 35 MPH) | 200 | 200 | 200 |
| URBAN (35 MPH OR GREATER) | 350 | 350 | 350 |
| RURAL | 500 | 500 | 500 |
| EXPRESSWAY/FREEWAY | 1000 | 1600 | 2600 |
12. ARROWBOARD PANELS SHALL BE USED ON HIGH SPEED ROADWAYS WITH SPEED LIMITS OVER 35 MPH OR AS DIRECTED BY THE TRAFFIC ENGINEER. ARROWBOARD SHOULD BE PLACED AS CLOSE TO THE BEGINNING OF THE TAPER AS POSSIBLE, AS SOON AS THERE IS ADEQUATE SPACE.
 13. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.
- | | | | |
|--------------|----------|-----------------|----------|
| BOULDER CITY | 293-9200 | LAS VEGAS | 229-6331 |
| CLARK COUNTY | 455-6100 | MESQUITE | 346-5295 |
| HENDERSON | 565-2140 | NORTH LAS VEGAS | 642-2462 |

14. BUFFER SPACE SHALL BE:

SPEED* (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

15. WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERCTED, AS NEEDED.
16. A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.
17. FOR LONG TERM PROJECTS OF 72 CONTINUOUS HOURS OR MORE, A 6 IN. WIDE INTERIM WHITE EDGE LINE SHOULD BE INSTALLED FROM THE START OF THE TAPER TO A POINT BEYOND THE WORK AREA, REJOINING THE PERMANENT EDGE LINE. INTERIM EDGE LINE SHALL BE REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE. FOR LEFT LANE CLOSURE, THE EDGE LINE SHALL BE YELLOW. PAVEMENT MARKINGS THAT ARE NO LONGER APPLICABLE SHALL BE MASKED WITH APPROVED BLACKOUT TAPE OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER FROM THE END OF THE ENTRANCE TAPER TO A POINT LOCATED 1/2A FROM THE TAPER BEGINNING.
18. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

**TRAFFIC CONTROL PLAN
FOR
HIGHWAY WORK ZONE**

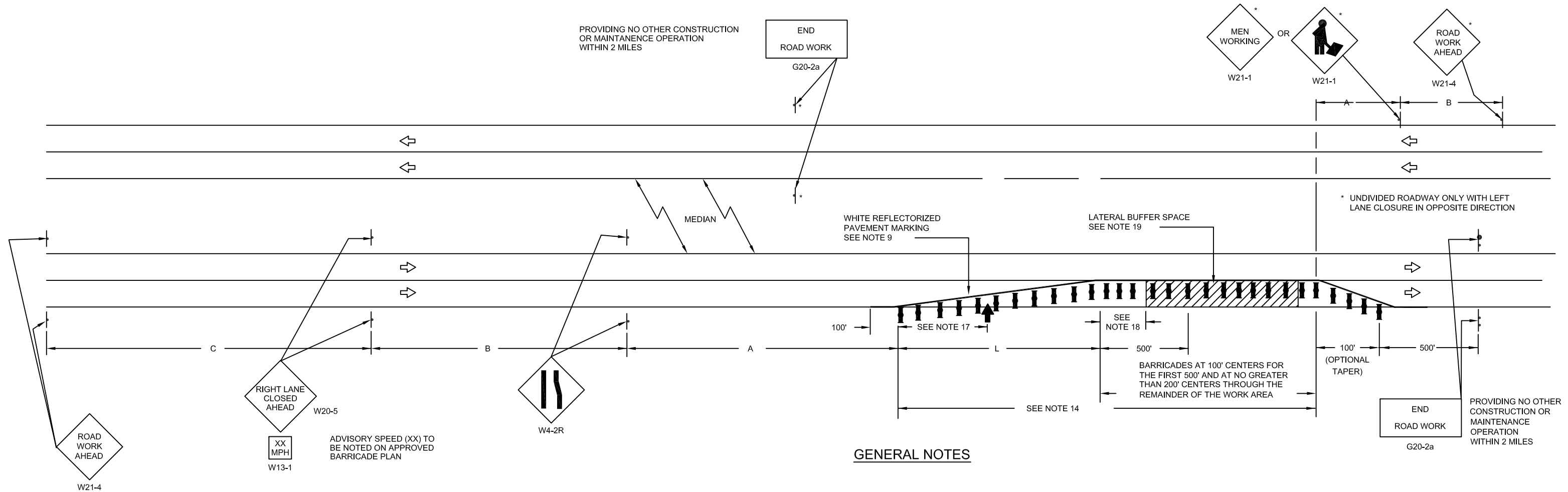
SPECIFICATION REFERENCE

TYPICAL APPLICATION FOR

MULTILANE, DIVIDED OR UNDIVIDED, RURAL OR SUBURBAN, DAY OPERATIONS WHERE ACTIVITIES WILL ENCROACH ON LANE ABUTTING SHOULDER OR ON THE SHOULDER WITHIN 2 FT. OUTSIDE THE PAVEMENT EDGE

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

Effective 07/01/10 - 12/30/10



GENERAL NOTES

- WHEN EQUIPMENT ENTERS OR EXITS THE WORK AREA DIRECTLY FROM THE ADJOINING LANE CARRYING TRAFFIC, A FLAGGER WILL BE REQUIRED. IF THE FLAGGER IS PRESENT, THE FLAGGER SIGN SHALL BE PLACED AT DISTANCE "A" PRIOR TO THE FLAGGER AND "PREPARE TO STOP" SIGN (NPS-1) SHALL BE PLACED AT DISTANCE "A" PRIOR TO THE FLAGGER SIGN. FLOODLIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED. A 100 FT. CONE TAPER SHALL BE PROVIDED PRIOR TO STATION TO PROTECT THE FLAGGER. FLAGGERS SHALL NOT BE REQUIRED FOR SPEEDS OF 25 MPH OR LESS.
- THIS CASE ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE LEFT LANE. UNDER THESE CONDITIONS, LEFT LANE CLOSED SIGNS SHALL BE SUBSTITUTED FOR RIGHT LANE CLOSED SIGNS. ON UNDIVIDED HIGHWAYS, SIGNS SHALL BE ADDED IN OPPOSITE DIRECTION AS SHOWN.
- THIS CASE DOES NOT APPLY WHEN WORK IS BEING PERFORMED IN THE MIDDLE LANE(S) OF A SIX OR MORE LANE HIGHWAY. SPECIAL PLANS APPROVED BY THE TRAFFIC ENGINEER WILL BE REQUIRED.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS CONES SHALL BE A MINIMUM OF 28" IN HEIGHT.
- STEADY BURNING LIGHTS SHALL NOT BE REQUIRED ON BARRICADES FOR DAY OPERATIONS.
- ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.
- TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- FOR LONG TERM PROJECTS OF 72 CONTINUOUS HOURS OR MORE, A 6 IN. WIDE INTERIM WHITE EDGE LINE SHOULD BE INSTALLED FROM THE START OF THE TAPER TO A POINT BEYOND THE WORK AREA, REJOINING THE PERMANENT EDGE LINE. INTERIM EDGE LINE SHALL BE REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE. FOR LEFT LANE CLOSURE, THE EDGE LINE SHALL BE YELLOW. PAVEMENT MARKINGS THAT ARE NO LONGER APPLICABLE SHALL BE MASKED WITH APPROVED BLACKOUT TAPE OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER FROM THE END OF THE ENTRANCE TAPER TO A POINT LOCATED 1/2A FROM THE TAPER BEGINNING.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER. THE LATERAL PLACEMENT OF THE FLAGGER, IF NECESSARY, MAY BE VARIED.
- ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE TRAFFIC ENGINEER.
- ALL BARRICADE LIGHTS SHALL BE MONODIRECTIONAL IF THE ROADWAY IS UNDIVIDED.
- ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
- TABLE FOR SPACING OF ADVANCE WARNING SIGNS
- ARROWBOARD PANELS SHALL BE USED ON HIGH SPEED ROADWAYS WITH SPEED LIMITS OVER 35 MPH OR AS DIRECTED BY THE TRAFFIC ENGINEER. ARROWBOARD SHOULD BE PLACED AS CLOSE TO THE BEGINNING OF THE TAPER AS POSSIBLE, AS SOON AS THERE IS ADEQUATE SPACE.
- BUFFER SPACE SHALL BE:
- A LATERAL BUFFER SPACE MAY BE REQUIRED TO SEPARATE WORK SPACE FROM TRAFFIC SPACE. THE WIDTH SHALL BE DETERMINED BY THE TRAFFIC ENGINEER.
- WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERCTED, AS NEEDED.
- ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

SPEED* (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	290
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

SYMBOLS

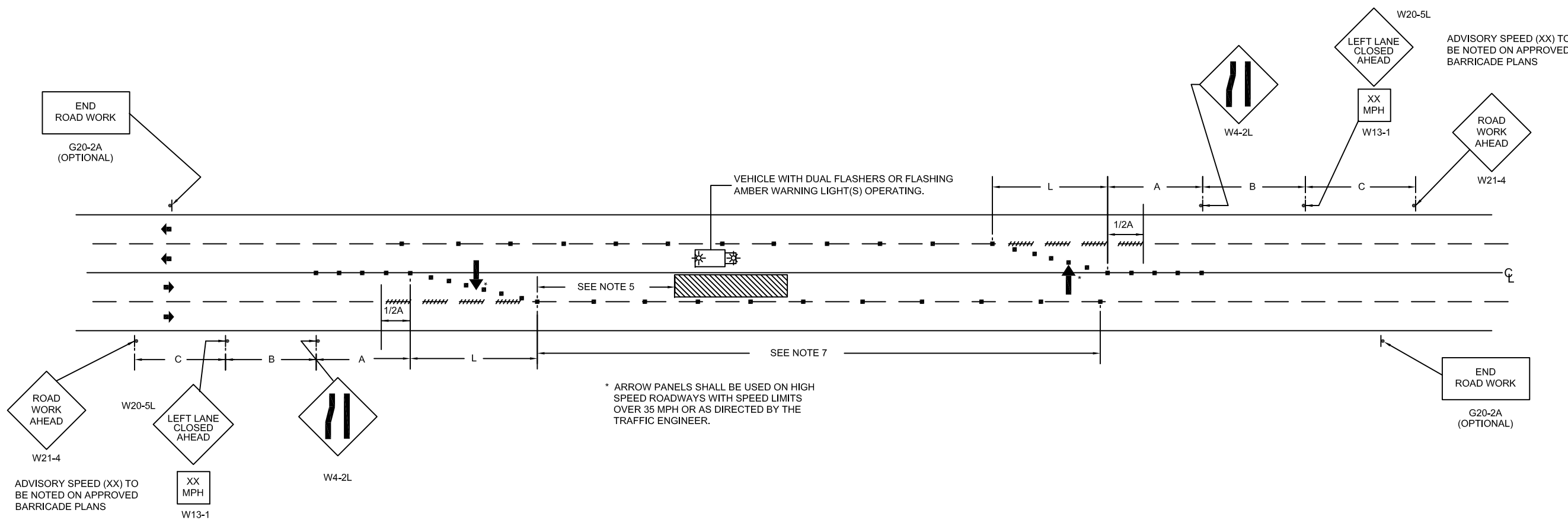
- WORK AREA
- ARROW BOARD (IN LINE OF LANE)
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TRAFFIC DIRECTION
- BARRICADE OR DRUM WITH STEADY BURNING LIGHT

1. FORMULAS FOR TAPER LENGTH

SPEED	FORMULA
40 M.P.H. OR UNDER	L = WS ² /60
40 M.P.H. OR OVER	L = WS

WHERE:
 L = TAPER LENGTH
 W = WIDTH OF LANE OR OFFSET
 S = POSTED SPEED, OR OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR ANTICIPATED OPERATING SPEED

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70



GENERAL NOTES

1. TAPER FORMULA: $L = S \times W$ FOR SPEEDS OF 45 MPH OR MORE
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS

WHERE: L = MINIMUM LENGTH OF TAPER
 S = POSTED SPEED, 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR ANTICIPATED OPERATING SPEED
 W = WIDTH OF OFFSET

TAPER LENGTHS FOR LANE CLOSURES - DISTANCE L					
SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

2. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHALL BE AS SPECIFIED IN TABLE IN NOTE 1.
 3. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
 4. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

5. A BUFFER SPACE SHOULD BE REQUIRED AS FOLLOWS:

BUFFER SPACE	
SPEED* (M.P.H.)	BUFFER SPACE (FT.)
70	585
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

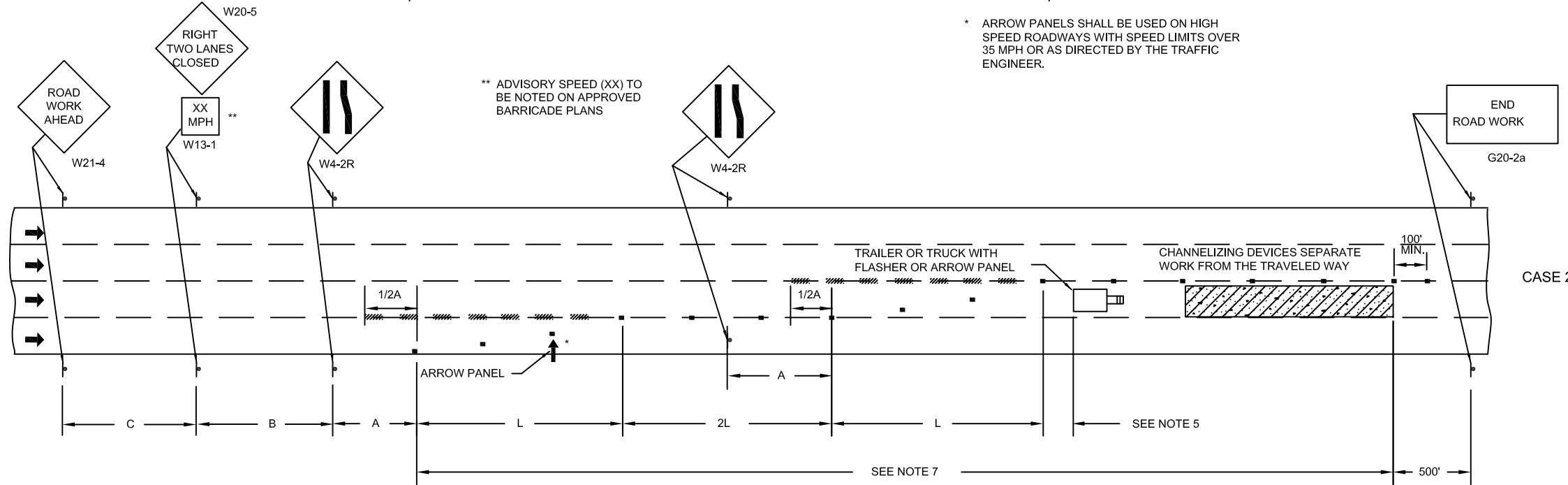
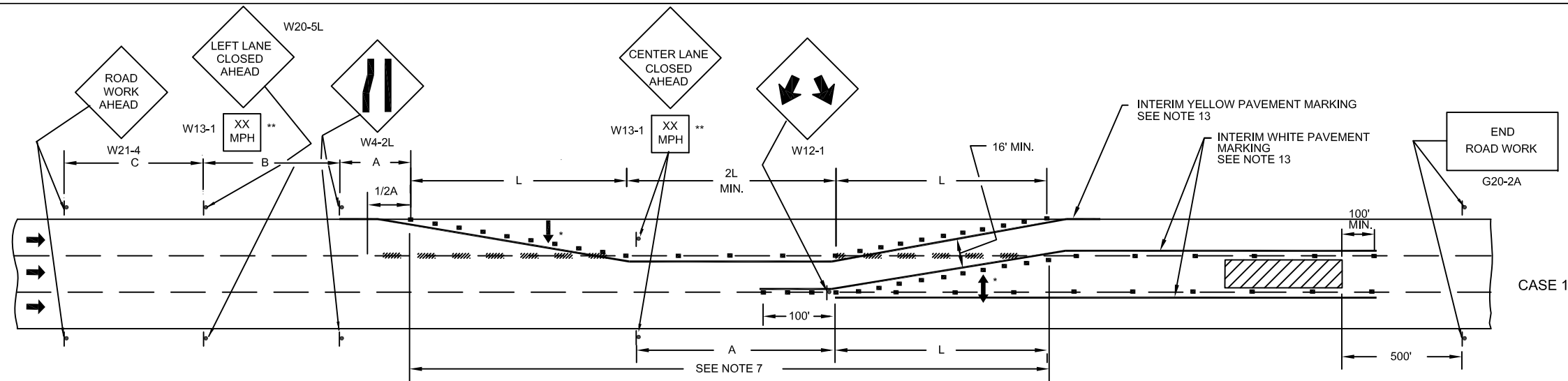
6. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

7. ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.
 8. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.
 BOULDER CITY 293-9200 MESQUITE 346-5295
 CLARK COUNTY 455-6100 NORTH LAS VEGAS 642-2462
 HENDERSON 565-2140
 LAS VEGAS 229-6331
 9. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
 10. DURING HOURS OF DARKNESS, STEADY BURNING WARNING LIGHTS SHALL BE USED ON ALL CHANNELIZING DEVICES.

SYMBOLS

- ARROW PANEL
- FLASHING VEHICLE LIGHT
- PAVEMENT MARKINGS THAT SHOULD BE REMOVED FOR A LONG TERM PROJECT OF 72 CONTINUOUS HOURS OR MORE. PAINT NOT ALLOWED. MASK WITH APPROVED BLACK OUT TAPE OPTIONAL OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER. TEMPORARY MARKINGS TO BE PLACED AS NEEDED.
- BARRICADE, DRUM, VERTICAL PANEL OR CONE.
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TRAFFIC DIRECTION
- WORK AREA



SYMBOLS

- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- BARRICADE DRUM, VERTICAL PANEL OR CONE
- PAVEMENT MARKINGS THAT SHOULD BE REMOVED FOR A LONG TERM PROJECT OF 72 CONTINUOUS HOURS OR MORE. PAINT NOT ALLOWED. MASK WITH APPROVED BLACK OUT TAPE OPTIONAL OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER.
- TEMPORARY MARKINGS TO BE PLACED AS NEEDED.
- TRAFFIC DIRECTION

* ARROW PANELS SHALL BE USED ON HIGH SPEED ROADWAYS WITH SPEED LIMITS OVER 35 MPH OR AS DIRECTED BY THE TRAFFIC ENGINEER.

** ADVISORY SPEED (XX) TO BE NOTED ON APPROVED BARRICADE PLANS

GENERAL NOTES

1. TAPER FORMULA: $L = S \times W$ FOR SPEEDS OF 45 MPH OR MORE
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS

WHERE: L = MINIMUM LENGTH OF TAPER
 S = POSTED SPEED, 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR ANTICIPATED OPERATING SPEED
 W = WIDTH OF OFFSET

- 3. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.
- 4. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
- 5. A BUFFER SPACE SHOULD BE REQUIRED AS FOLLOWS:

BUFFER SPACE	
SPEED* (M.P.H.)	BUFFER SPACE (FT.)
70	685
65	485
60	415
55	335
50	280
45	220
40	170
35	120
30	85
25	55
20	35

* POSTED SPEED, OFF-PEAK 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED.

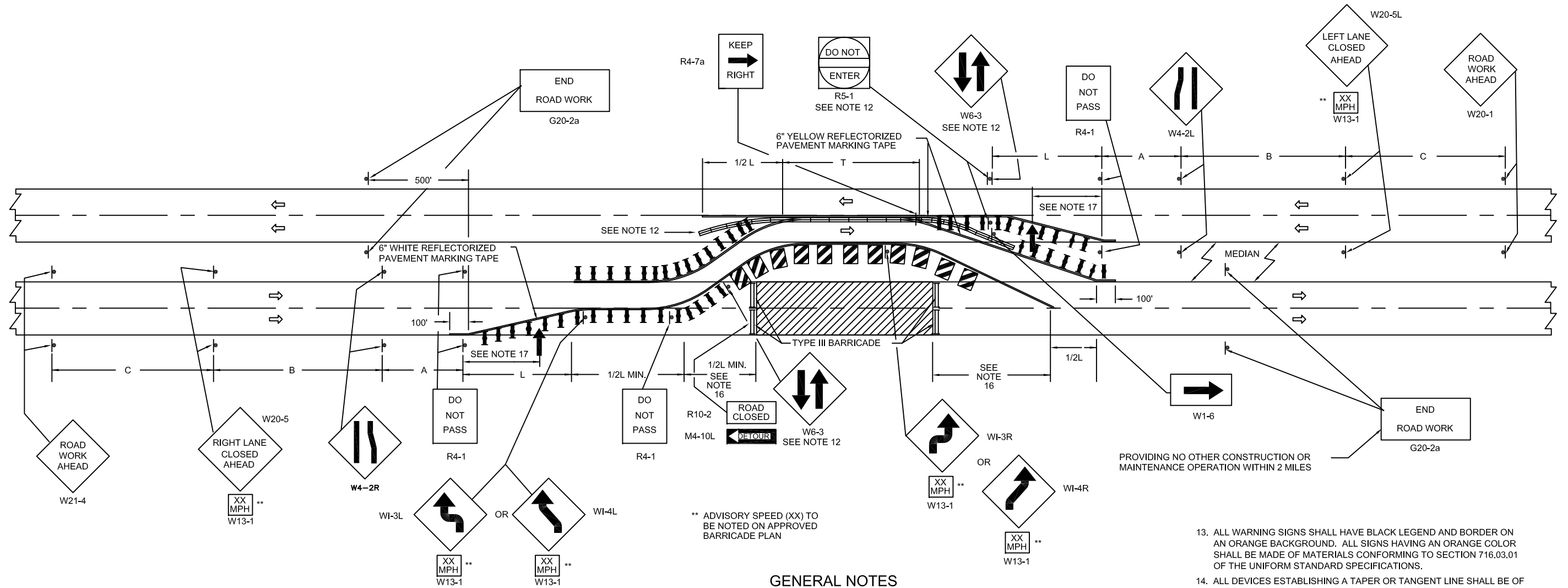
ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

- 9. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.
- | | | | |
|--------------|----------|-----------------|----------|
| BOULDER CITY | 293-9200 | MESQUITE | 346-5295 |
| CLARK COUNTY | 455-6100 | NORTH LAS VEGAS | 642-2462 |
| HENDERSON | 565-2140 | | |
| LAS VEGAS | 229-6331 | | |

- 10. FOR CASE 1, A BUFFER SPACE SHOULD BE USED AT THE UPSTREAM END OF THE CLOSED INTERIOR LANE. FOR LONG-TERM OPERATIONS OF 72 CONTINUOUS HOURS OR MORE, A BARRIER SHOULD BE USED TO PROTECT THE OPERATION IN THE CLOSED INTERIOR LANE.
- 11. WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERECTED, AS NEEDED.
- 12. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.
- 13. INTERIM PAVEMENT MARKINGS SHALL BE REFLECTORIZED, REMOVABLE, NON-FOIL TAPE AND A MINIMUM OF 6 IN. WIDE AND SHOULD BE REQUIRED FOR OPERATIONS OF 72 CONTINUOUS HOURS OR MORE.

2. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHALL BE AS SPECIFIED IN THE TABLE IN NOTE 1.

Effective 07/01/10 - 12/30/10



** ADVISORY SPEED (XX) TO BE NOTED ON APPROVED BARRICADE PLAN

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

FORMULAS FOR TAPER LENGTH

SPEED	FORMULA
40 M.P.H. OR UNDER	$L = WS^2/60$
40 M.P.H. OR OVER	$L = WS$

WHERE:

L = TAPER LENGTH
 W = WIDTH OF LANE OR OFFSET
 S = POSTED SPEED, 85TH PERCENTILE SPEED PRIOR TO WORK STARTING OR ANTICIPATED OPERATING SPEED

SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	475	540	13	45
50	500	520	600	13	50
55	550	575	660	13	55
60	600	620	720	13	60
65	650	675	780	13	65
70	700	720	840	13	70

2. TWO WAY TRAFFIC SIGN(S) SHALL BE REPEATED EVERY ONE-QUARTER MILE IN EACH DIRECTION THROUGH THE TANGENT DISTANCE.

3. WHEN (T) IS GREATER THAN 600 FT., 4-FOOT LENGTHS OF DOUBLE YELLOW REFLECTORIZED, REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE AT 40 FOOT CENTERS MAY BE USED.

4. A CURVE SIGN SHALL BE REQUIRED AT THE EXIT END OF THE BYPASS IF (T) IS EQUAL TO OR GREATER THAN 1,000 FEET.

5. ON PAVED CROSSOVERS, REFLECTIVE EDGE LINES AND A CENTERLINE SHALL BE USED WHEN THE CLOSURE TIME IS 72 CONTINUOUS HOURS OR MORE OR WHEN THE NORMAL POSTED SPEED OUTSIDE THE AREA OF OPERATIONS EXCEEDS 50 M.P.H. REFLECTORIZED, REMOVABLE, NON-FOIL PAVEMENT MARKING TAPE SHALL BE USED FOR MARKING THE EDGE LINES AND CENTER LINE ON THE EXISTING PAVEMENT. REFLECTORIZED PAVEMENT MARKING PAINT MAY BE USED FOR MARKINGS ON PAVED CROSSOVERS. RAISED REFLECTIVE PAVEMENT MARKERS CONFORMING TO UNIFORM STANDARD SPECS. & DRAWINGS MAY BE USED IN LIEU OF TAPE OR PAINT WHERE THE PAVEMENT MARKING IS TO BE PLACED ADJACENT TO BARRICADES OR VERTICAL PANELS. ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE REVISED TRAFFIC PATTERN SHALL BE REMOVED. IF STRIPING IS TO BE PLACED ON FINAL PAVEMENT ONLY REMOVABLE, NON-FOIL TAPE SHALL BE USED.

6. CONES MAY BE SUBSTITUTED FOR BARRICADES AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 IN. IN HEIGHT.

7. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES FOR DAY OPERATIONS.

8. ALL SIGNS SHALL BE GROUND-MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS AND AS REQUIRED BY SECTION 625 OF THE UNIFORM STANDARD SPECIFICATIONS.

9. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.

10. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS BY THE TRAFFIC ENGINEER.

11. ALL BARRICADE LIGHTS SHALL BE MONODIRECTIONAL.

12. PORTABLE CONCRETE BARRIER RAIL SHOULD BE REQUIRED FOR LONG-TERM PROJECTS OF 72 CONTINUOUS HOURS OR MORE. SHALL CONFORM WITH STANDARD DRAWING NO. 602, AND BE USED WHEN CALLED UPON THEREIN. WHEN PORTABLE CONCRETE BARRIER RAIL IS REQUIRED, THE W6-3 & R5-1 SIGNS MAY BE OMITTED THROUGH THE TANGENT DISTANCE (T).

13. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.

14. ALL DEVICES ESTABLISHING A TAPER OR TANGENT LINE SHALL BE OF ONE TYPE; DEVICES SHALL NOT BE MIXED BY TYPE.

15. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

16. ENGINEERED GEOMETRIC DESIGN TO BE PROVIDED BY ENGINEER.

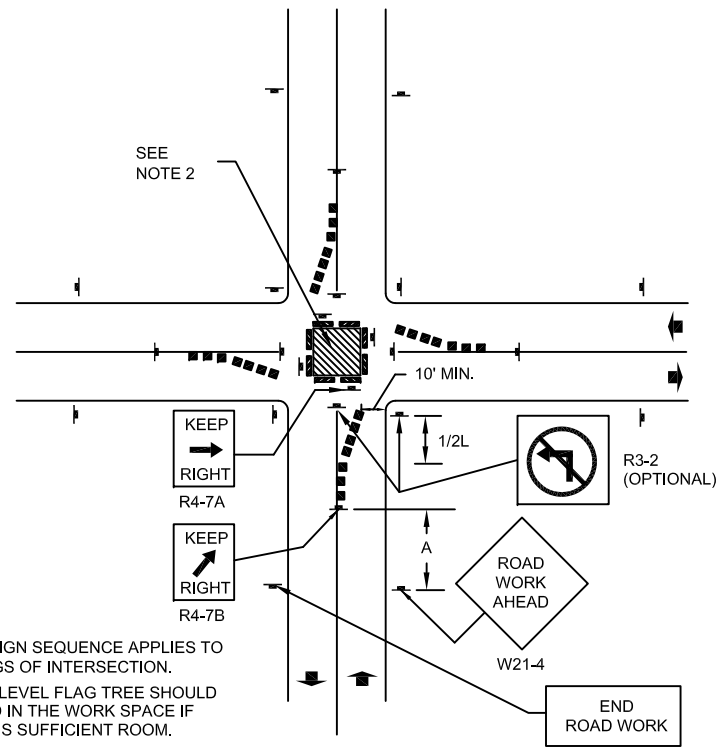
17. ARROW BOARD SHOULD BE PLACED AS CLOSE TO THE BEGINNING OF THE TAPER AS POSSIBLE, AS SOON AS THERE IS ADEQUATE SPACE.

18. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462

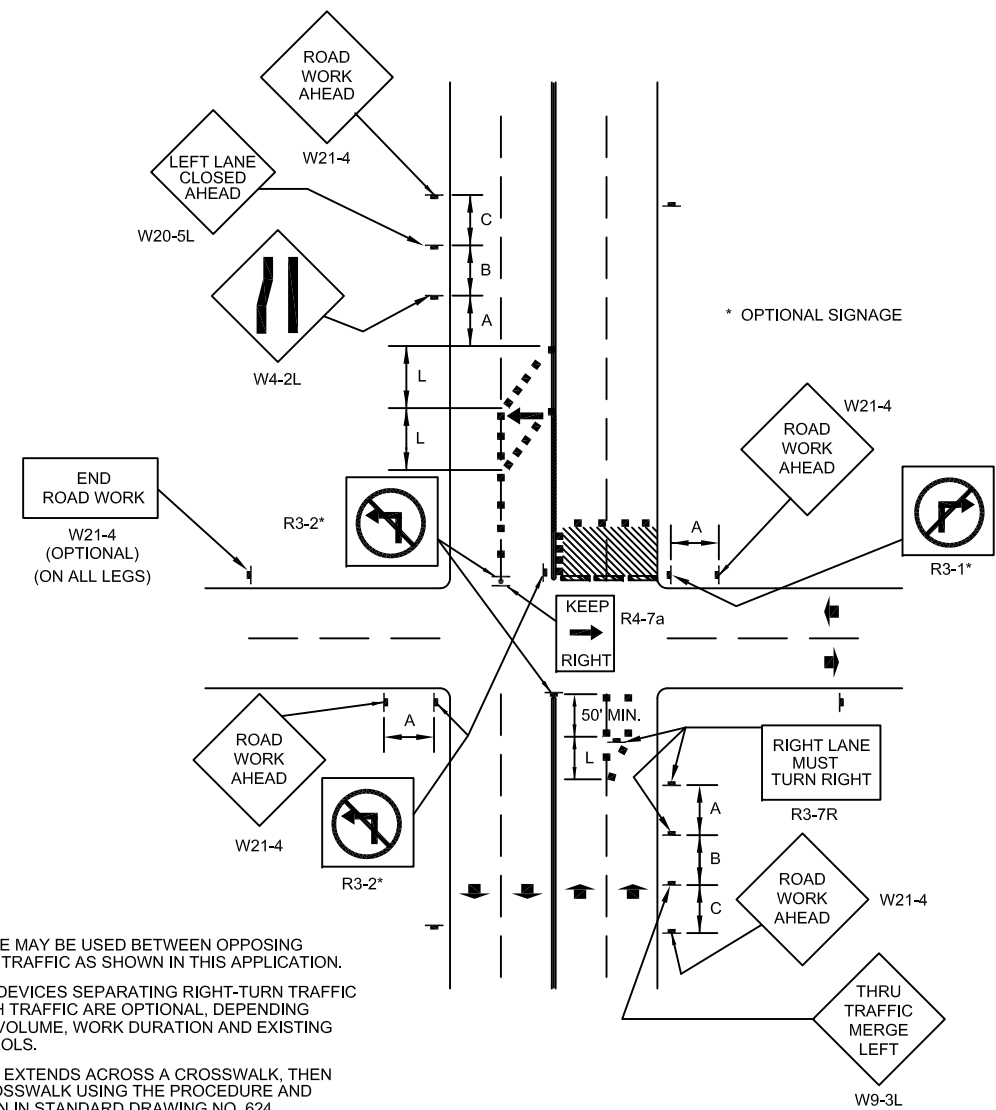
19. WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERRECTED, AS NEEDED.

20. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.



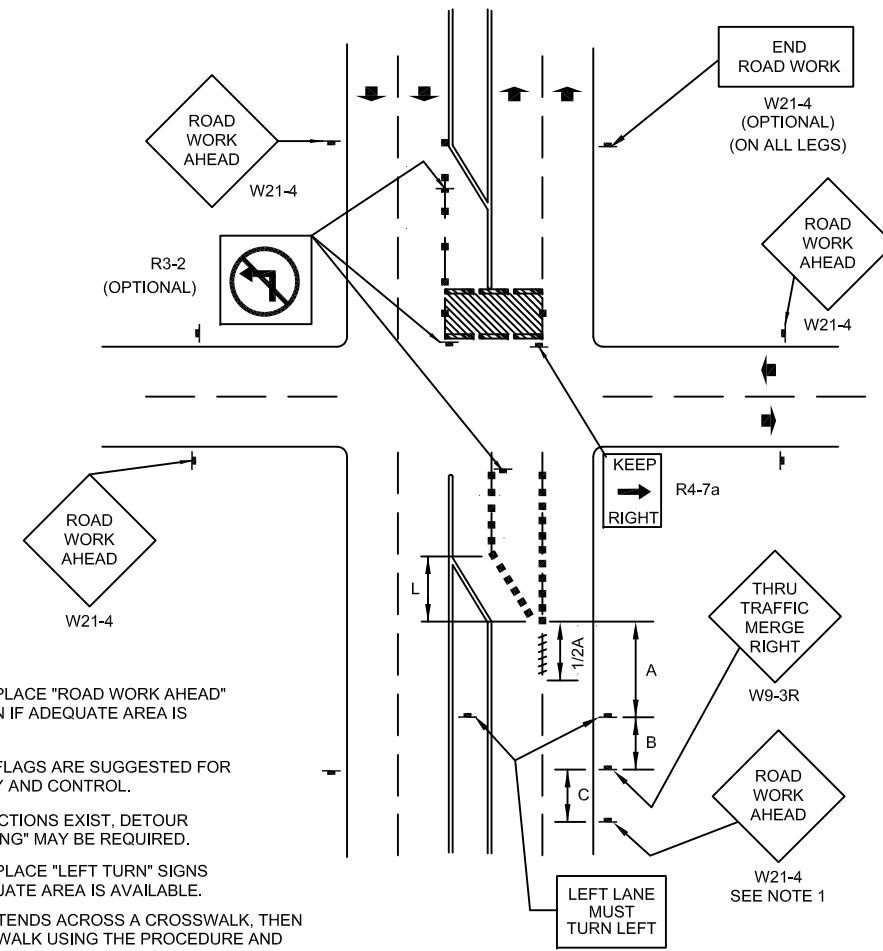
- NOTE:**
1. SAME SIGN SEQUENCE APPLIES TO ALL LEGS OF INTERSECTION.
 2. A HIGH-LEVEL FLAG TREE SHOULD BE PLACED IN THE WORK SPACE IF THERE IS SUFFICIENT ROOM.
 3. A MINIMUM OF SIX CHANNELIZING DEVICES SHALL BE USED FOR EACH TAPER.

A. WORK AREA IS IN THE CENTER OF AN INTERSECTION



- NOTES:**
1. A BUFFER SPACE MAY BE USED BETWEEN OPPOSING DIRECTIONS OF TRAFFIC AS SHOWN IN THIS APPLICATION.
 2. CHANNELIZING DEVICES SEPARATING RIGHT-TURN TRAFFIC FROM THROUGH TRAFFIC ARE OPTIONAL, DEPENDING UPON TRAFFIC VOLUME, WORK DURATION AND EXISTING TRAFFIC CONTROLS.
 3. IF WORK SPACE EXTENDS ACROSS A CROSSWALK, THEN CLOSE THE CROSSWALK USING THE PROCEDURE AND DEVICES SHOWN IN STANDARD DRAWING NO. 624.

B. WORK AREA NEAR AN INTERSECTION, ALLOWING RIGHT TURNS.



- NOTES:**
1. IF MEDIAN EXISTS, PLACE "ROAD WORK AHEAD" SIGN IN THE MEDIAN IF ADEQUATE AREA IS AVAILABLE.
 2. LARGE SIGNS AND FLAGS ARE SUGGESTED FOR MAXIMUM VISIBILITY AND CONTROL.
 3. IF TURNING RESTRICTIONS EXIST, DETOUR ROUTE "TRAILBLAZING" MAY BE REQUIRED.
 4. IF MEDIAN EXISTS, PLACE "LEFT TURN" SIGNS IN MEDIAN IF ADEQUATE AREA IS AVAILABLE.
 5. IF WORK SPACE EXTENDS ACROSS A CROSSWALK, THEN CLOSE THE CROSSWALK USING THE PROCEDURE AND DEVICES SHOWN IN STANDARD DRAWING NO. 624.

C. WORK AREA NEAR AN INTERSECTION, PROVIDING ACCESS TO LEFT-TURN LANE.

GENERAL NOTES

1. FORMULAS FOR TAPER LENGTH

SPEED	FORMULA
40 M.P.H. OR UNDER	$L = WS^2/60$
40 M.P.H. OR OVER	$L = WS$

WHERE:
 L = TAPER LENGTH
 W = WIDTH OF LANE OR OFFSET
 S = POSTED SPEED, 85TH PERCENTILE SPEED PRIOR TO STARTING WORK OR ANTICIPATED OPERATING SPEED

TAPER LENGTHS FOR LANE CLOSURES - DISTANCE L					
SPEED MILES PER HOUR 85th PERCENTILE	TAPER LENGTH LANE WIDTH IN FEET			NUMBER OF CHANNELIZING DEVICES FOR TAPER	MAX. SPACING OF DEVICES ALONG TAPER IN FEET
	10	11	12		
20	70	75	80	5	20
25	105	115	125	6	25
30	150	165	180	7	30
35	205	225	245	8	35
40	270	295	320	9	40
45	450	495	540	13	45
50	500	550	600	13	50
55	550	605	660	13	55
60	600	660	720	13	60
65	650	715	780	13	65
70	700	770	840	13	70

2. ADDITIONAL ADVANCE WARNING MAY BE NECESSARY
3. PROHIBIT TURNS AS REQUIRED BY TRAFFIC CONDITIONS

4. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

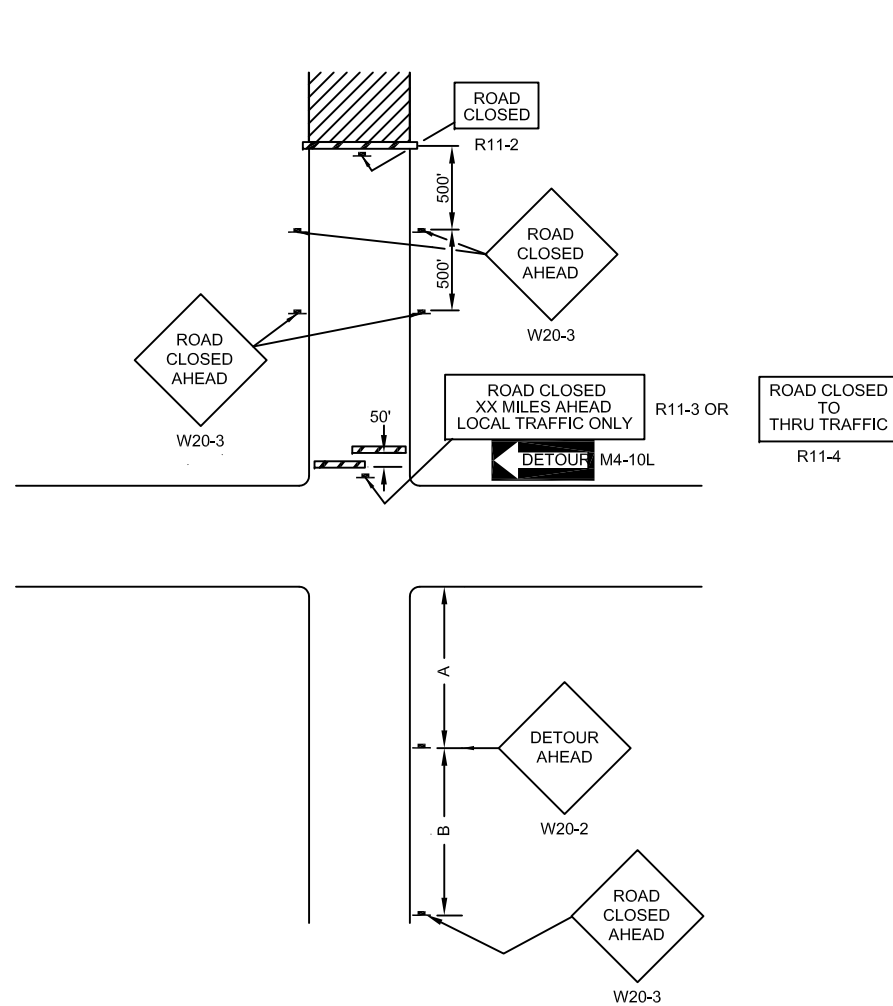
8. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.

5. ALL DEVICES ESTABLISHING A TAPER OF TANGENT LINE SHALL BE OF ONE TYPE, DEVICES SHALL NOT BE MIXED BY TYPE.
6. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	LAS VEGAS	229-6331
CLARK COUNTY	455-6100	MESQUITE	346-5295
HENDERSON	565-2140	NORTH LAS VEGAS	642-2462
7. FLASHING WARNING LIGHTS AND/OR FLAGS MAY BE USED TO CALL ATTENTION TO THE ADVANCE WARNING SIGNS.

SYMBOLS

- CHANNELIZING DEVICES
- ////// PAVEMENT MARKINGS THAT SHOULD BE REMOVED FOR A LONG TERM PROJECT OF 72 CONTINUOUS HOURS OR MORE. PAINT NOT ALLOWED. MASK WITH APPROVED BLACK OUT TAPE OPTIONAL OR OBLITERATED AS APPROVED BY THE TRAFFIC ENGINEER. TEMPORARY MARKINGS TO BE PLACED AS NEEDED.
- ↑ TRAFFIC DIRECTION
- ▨ TYPE III BARRICADE
- ▩ WORK AREA






A. TYPICAL APPLICATION-ROADWAY CLOSED BEYOND DETOUR POINT.

GENERAL NOTES

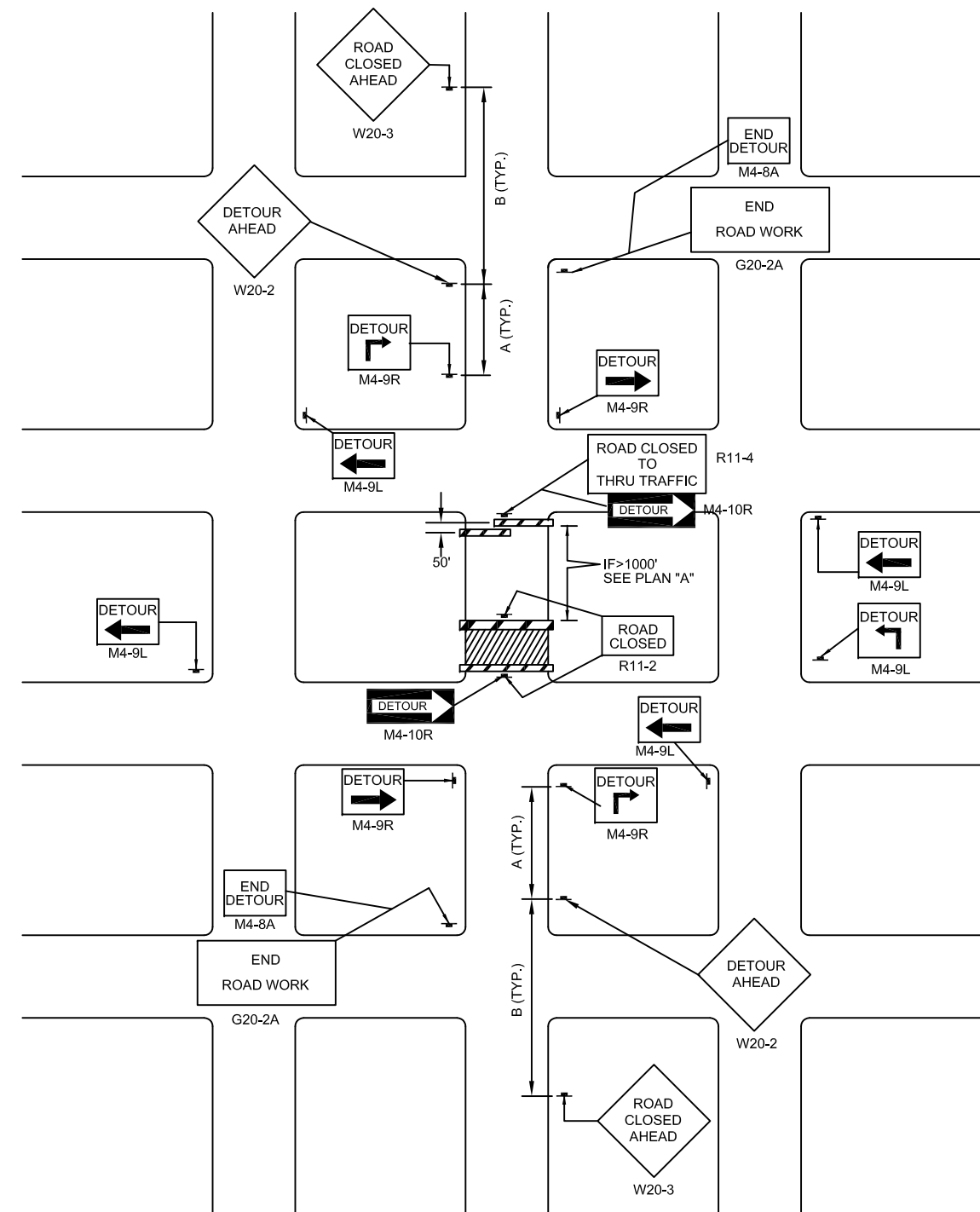
1. ANY ROAD CLOSURE MUST BE EXPRESSLY PERMITTED IN WRITING BY THE ADMINISTERING ENTITY'S TRAFFIC MANAGEMENT DIVISION MANAGER OR THE DIRECTOR OF ITS PUBLIC WORKS DEPARTMENT.
2. ALL WARNING SIGNS SHALL HAVE BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND. ALL SIGNS HAVING AN ORANGE COLOR SHALL BE MADE OF MATERIALS CONFORMING TO SECTION 716.03.01 OF THE UNIFORM STANDARD SPECIFICATIONS.
3. REGULATORY TRAFFIC CONTROL DEVICES TO BE MODIFIED AS NEEDED FOR THE DURATION OF THE DETOUR.
4. WARNING LIGHTS MAY BE USED TO MARK BARRICADES AT NIGHT AS NEEDED.
5. STREET NAMES MAY BE USED WHEN DESIRABLE FOR DIRECTING DETOURED TRAFFIC. LETTERS USED FOR STREET NAMES SHALL A MINIMUM OF 6" IN HEIGHT.
6. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.

BOULDER CITY	293-9200	MESQUITE	346-5295
CLARK COUNTY	455-6100	NORTH LAS VEGAS	642-2462
HENDERSON	565-2140		
LAS VEGAS	229-6331		
7. TYPE "B" HIGH INTENSITY FLASHING WARNING LIGHTS MAY BE INSTALLED ABOVE EACH WORK ZONE CONSTRUCTION SIGN FOR USE DURING HOURS OF DARKNESS.

SYMBOLS

-  WORK AREA
-  TYPE III BARRICADES, PLACED END-TO-END FOR WIDTH OF ROADWAY DEPICTED.
-  SIGN ON PORTABLE OR PERMANENT SUPPORT

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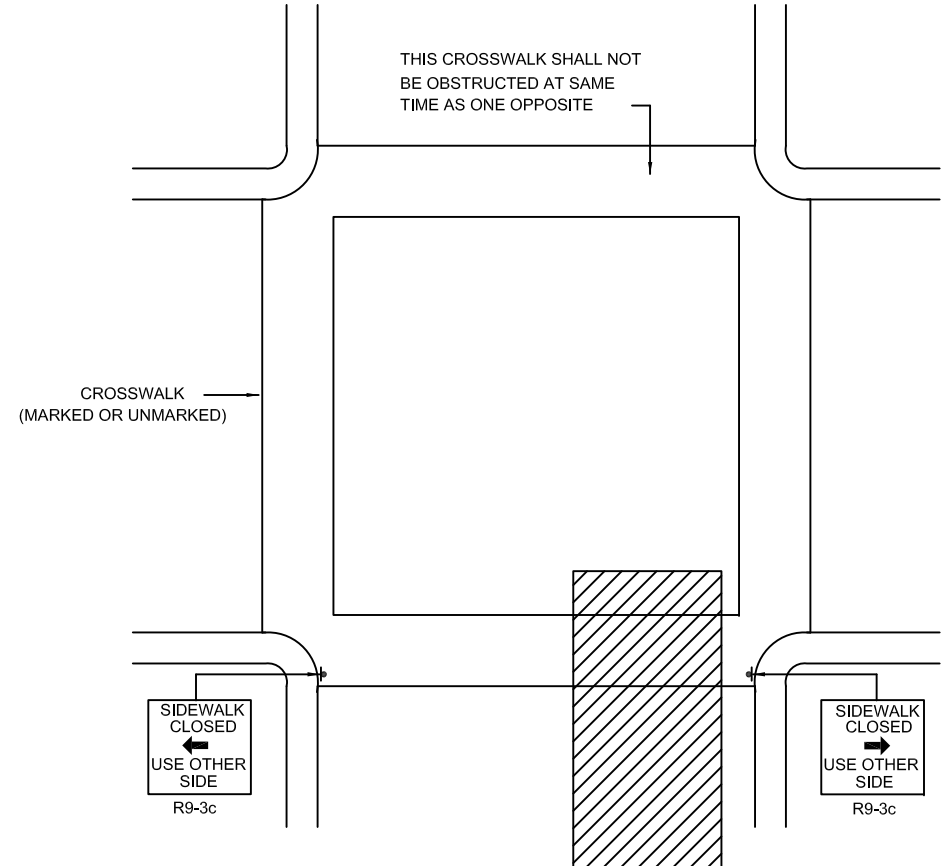
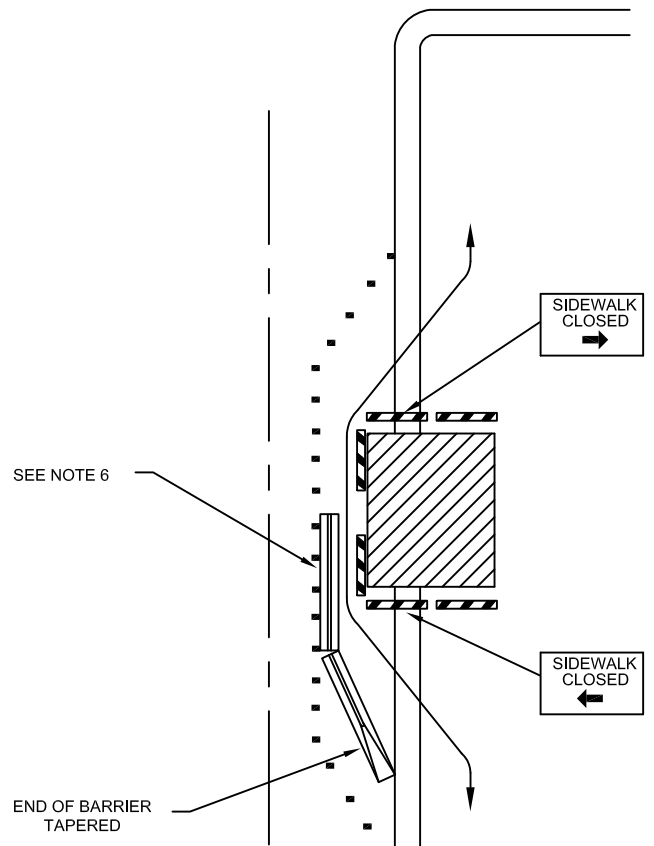
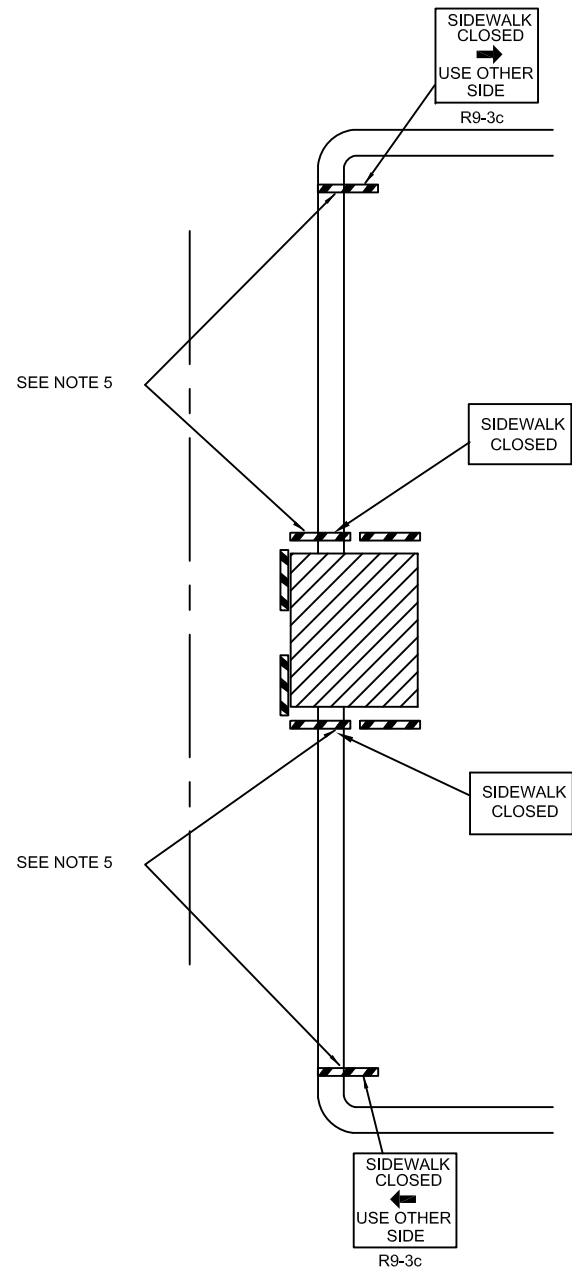


B. TYPICAL APPLICATION-DETOUR SIGNING FOR ROAD CONSTRUCTION PROJECT IN A STREET GRID.

8. M4-9 DETOUR SIGNS MAY BE LOCATED ON THE FAR SIDE OF THE INTERSECTIONS.
9. TABLE FOR SPACING OF ADVANCE WARNING SIGNS

TABLE FOR SPACING OF ADVANCE WARNING SIGNS			
ROAD TYPE	DISTANCE BETWEEN SIGNS (FT)		
	A	B	C
URBAN (LESS THAN 35 MPH)	200	200	200
URBAN (35 MPH OR GREATER)	350	350	350
RURAL	500	500	500
EXPRESSWAY/FREEWAY	1000	1600	2600

10. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.



- SYMBOLS**
- CHANNELIZING DEVICES
 - ▩ BARRICADES
 - ▨ WORK AREA
 - ▮ CONCRETE BARRIER RAIL
 - ↕ ↗ TRAFFIC DIRECTION

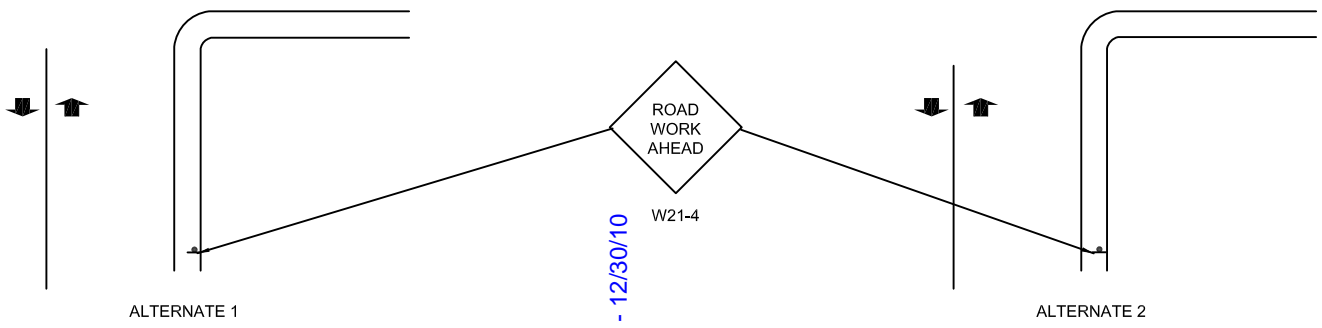
AT INTERSECTION

7. IF WORKING AT OR NEAR A TRAFFIC SIGNAL, CONTACT LVACTS AT 229-6611 AND LOCAL ENTITY AT APPROPRIATE NUMBERS LISTED BELOW AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK.
- | | | | |
|--------------|----------|-----------------|----------|
| BOULDER CITY | 293-9200 | MESQUITE | 346-5295 |
| CLARK COUNTY | 455-6100 | NORTH LAS VEGAS | 642-2462 |
| HENDERSON | 565-2140 | | |
| LAS VEGAS | 229-6331 | | |

GENERAL NOTES:

1. ADDITIONAL ADVANCE WARNING MAY BE NECESSARY.
2. CONTROLS FOR PEDESTRIANS ONLY ARE SHOWN. VEHICULAR TRAFFIC CONTROLS SHALL COMPLY WITH APPROPRIATE STANDARD DRAWINGS.
3. STREET LIGHTING SHOULD BE CONSIDERED.
4. WARNING LIGHTS MAY BE USED ON BARRIER RAILS.
5. IF THERE EXIST ANY SOURCE OF PEDESTRIAN MOVEMENTS IN THIS AREA, SUCH THAT THE PEDESTRIAN APPROACHING THE WORK AREA COULD NOT SEE THE R9-3C SIGN, THEN ALTERNATES MUST BE USED TO INSURE THAT THIS IS VISIBLE.
6. CONCRETE BARRIER RAIL SHALL CONFORM TO STANDARD DRAWING NO. 602. WHEN PEDESTRIAN ROUTE IS DIVERTED TO TRAVEL LANE TO AVOID WORK AREA AND/OR HIGH SPEEDS ARE ANTICIPATED, BARRIER RAIL SHALL BE USED TO SEPARATE TEMPORARY WALKWAY FROM TRAFFIC.

8. PEDESTRIANS SHOULD BE DIVERTED TO A SAFE AREA. DIVERSIONS SHALL BE AN ACCESSIBLE ROUTE AS DEFINED BY THE AMERICANS WITH DISABILITIES ACT (ADA).
9. FOR NIGHT-TIME CLOSURES, TYPE A FLASHING LIGHTS MAY BE USED ON BARRICADES SUPPORTING SIGNS AND CLOSING WALKWAYS. TYPE C STEADY-BURN LIGHTS SHALL BE USED ON CHANNELIZING DEVICES SEPARATING THE TEMPORARY WALKWAY FROM VEHICULAR TRAFFIC.
10. ACCESS FOR CAT TRANSIT SERVICE, PEDESTRIANS AND BICYCLES SHALL BE MAINTAINED THROUGHOUT DURATION OF CONSTRUCTION. IF RE-ROUTING OF ACCESS IS NECESSARY, THE CONTRACTOR SHALL PROVIDE THE ENTITY'S TRAFFIC ENGINEER WITH A MAP SHOWING THE PROPOSED RE-ROUTES FOR APPROVAL. IF CONSTRUCTION OPERATIONS AFFECT CAT BUS STOPS OR FACILITIES, THE CONTRACTOR SHALL NOTIFY THE REGIONAL TRANSPORTATION COMMISSION AT 455-4481 AT LEAST 3 NORMAL WORKING DAYS PRIOR TO BEGINNING SUCH OPERATIONS.



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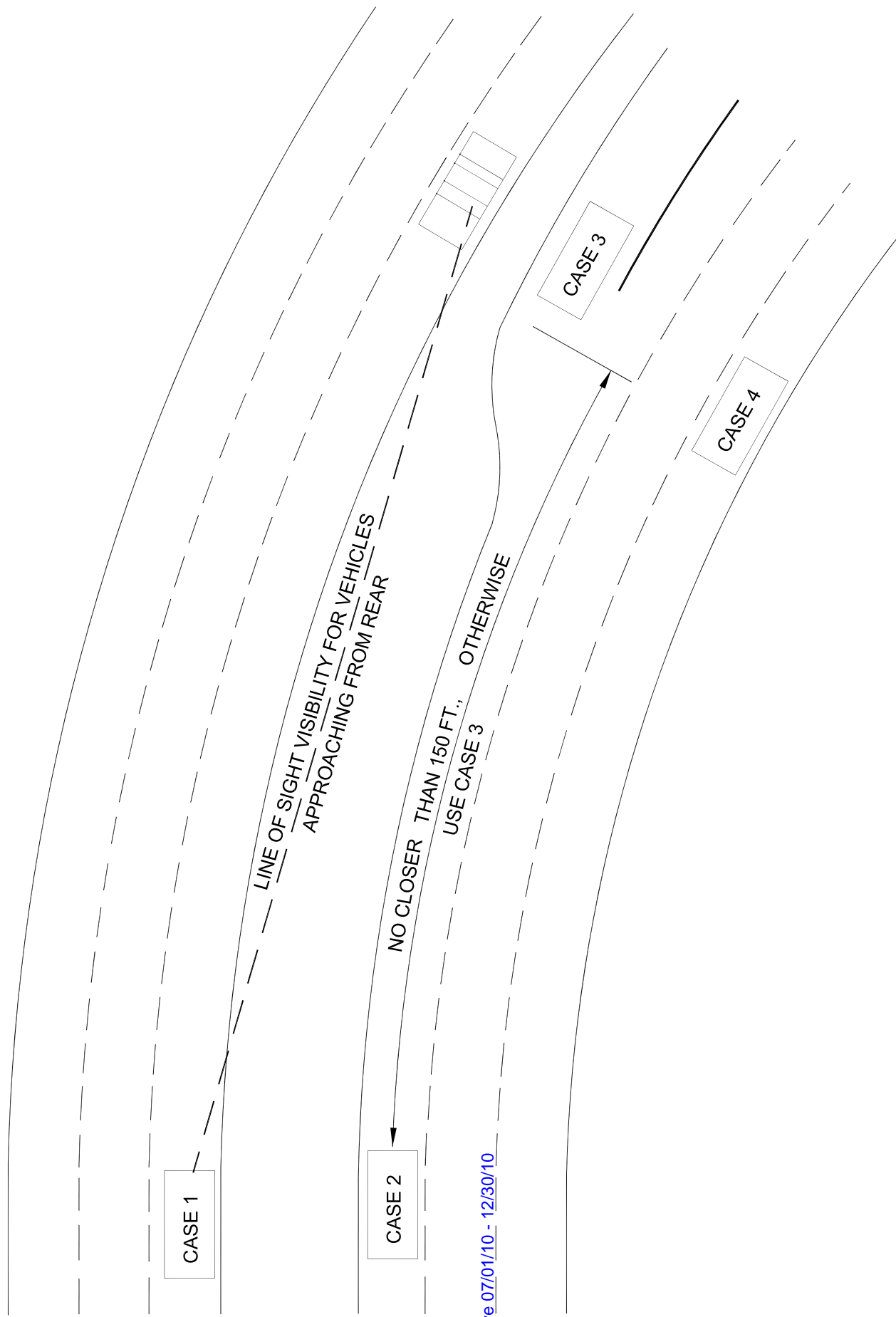
TRAFFIC CONTROL PLAN
FOR
HIGHWAY WORK ZONE

SPECIFICATION REFERENCE

TYPICAL APPLICATION FOR
CONTROLLING PEDESTRIAN TRAFFIC

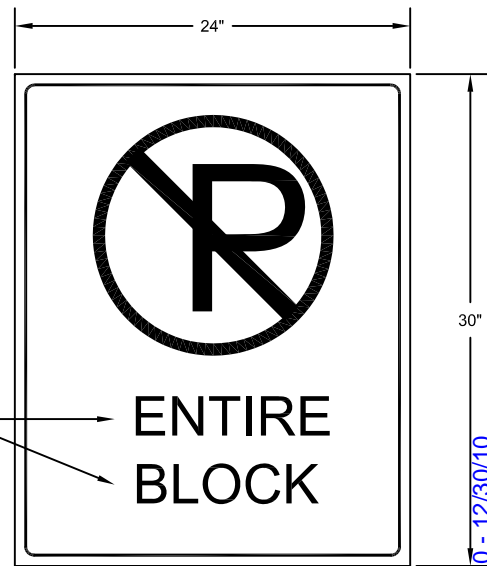
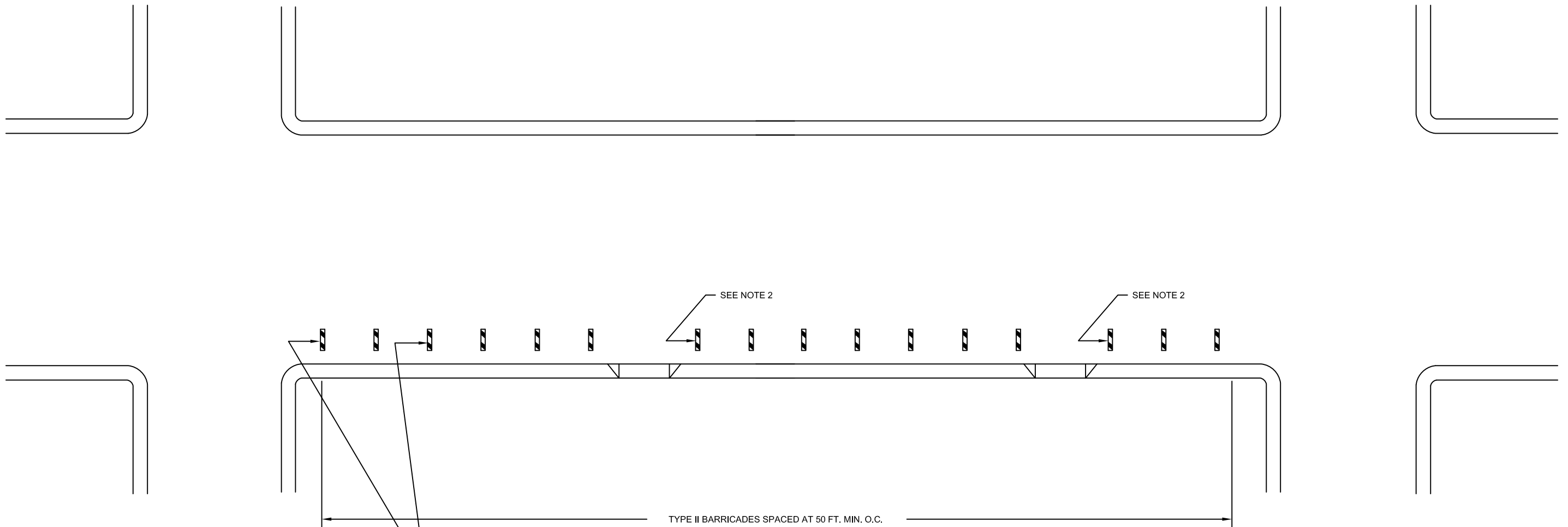
UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA

STANDARD PROCEDURE & CONDITIONS WHICH, WHEN MET, ELIMINATE THE NEED FOR INDIVIDUAL TRAFFIC CONTROL PLAN AND/OR PERMIT



DEVICE OR PARAMETER	SITUATION/CASE #			
	1	2	3	4
A. MINIMUM 60 IN. WIDE FLASHER BAR ATOP VEHICLE, WITH GREATER THAN 4 LIGHT ELEMENTS VISIBLE TO APPROACHING TRAFFIC	✓	✓	✓	✓
B. CONES SET OUT BEHIND VEHICLE	3, ACROSS BLOCKED LANE	3, ACROSS BLOCKED LANE	5, ACROSS BLOCKED LANE	NONE
C. TURN ON VEHICLE'S EMERGENCY HAZARD FLASHERS	✓	✓	✓	NOT REQUIRED
D. ALL PERSONNEL WEAR ORANGE VESTS OR SHIRTS WHEN OUTSIDE OF VEHICLE	ALWAYS	ALWAYS	ALWAYS	ALWAYS
E. O.K. FOR NIGHTTIME DEPLOYMENT?	NO	ONLY WHEN SPEED LIMIT ≤ 35 MPH	O.K., BUT USE REFLECTIVE VESTS	O.K., BUT USE REFLECTIVE VESTS
F. WATER-FILLED CRASH CUSHION, OR EQUIVALENT; TRUNK OR TRAILER-MOUNTED IMPACT ATTENUATORS	RECOMMENDED, BUT MANDATORY WHEN SPEED LIMIT EXCEEDS 45 MPH		NO	NO
G. NO STOPPING UNLESS STOPPED VEHICLE IS VISIBLE TO APPROACHING TRAFFIC GREATER THAN 10 SECONDS AT SPEED LIMIT	YES, APPLY THIS RULE	N/A - ON STRAIGHT-AWAY	NOT REQUIRED	DESIRED, BUT NOT REQUIRED
H. O.K. TO SET UP DURING PEAK TRAVEL HOURS: 7-9 AM, 4-6 PM	YES, BUT ONLY FOR EMERGENCY-TYPE REPAIR ACTIVITIES		O.K.	NOT RECOMMENDED

NOTE: TYPICAL APPLICATION IS FOR LANDSCAPE OR UTILITY ACTIVITIES.



"NO PARKING" SIGN DETAIL

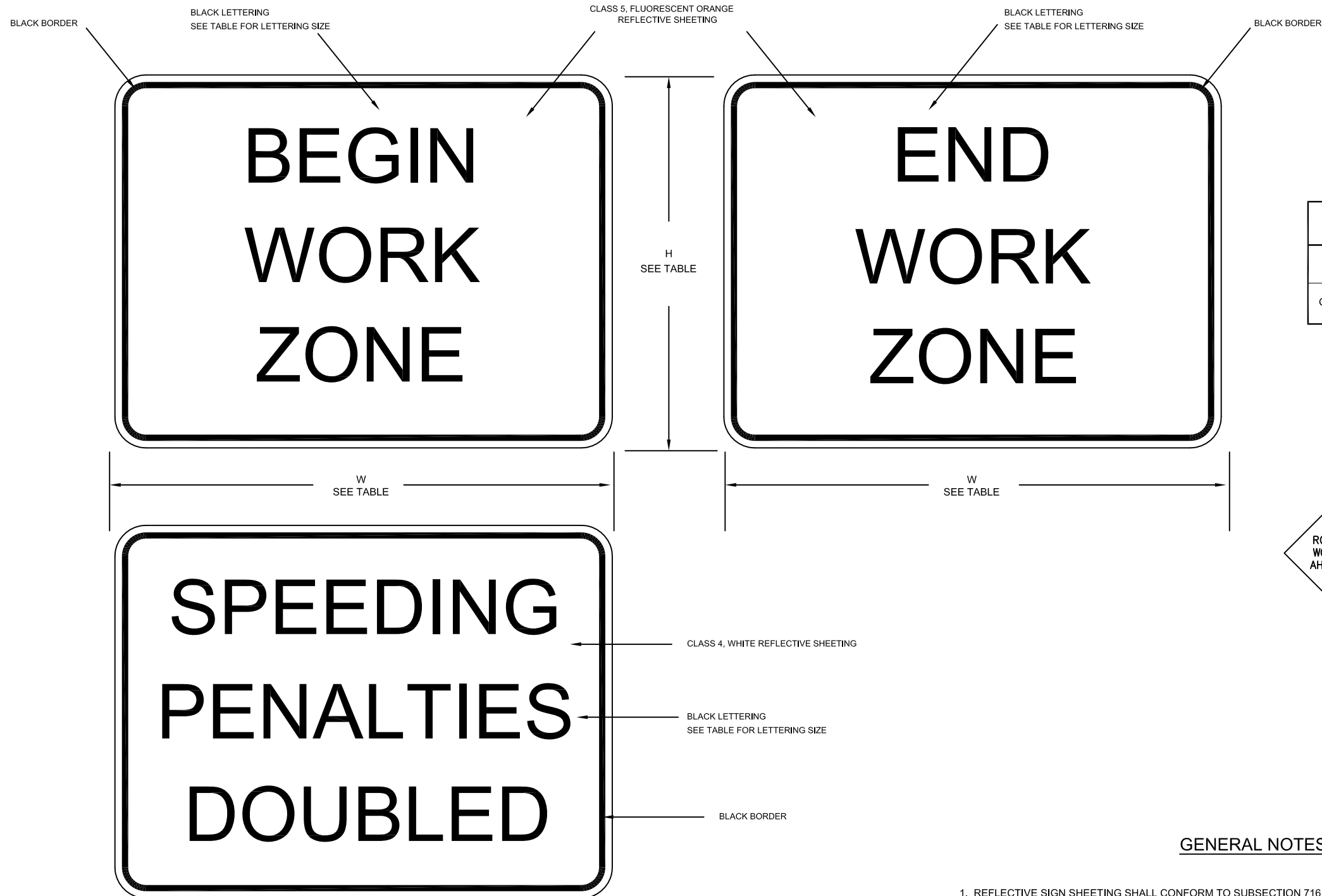
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GENERAL NOTES:

1. SPECIAL "NO PARKING" SIGN SHALL BE PLACED ON FIRST BARRICADE AND ON EVERY OTHER BARRICADE THEREAFTER.
2. BARRICADES SHALL NOT BLOCK DRIVEWAYS OR ACCESSES PRIOR TO MAINTENANCE OPERATION. SPECIAL "NO PARKING" SIGN SHALL BE PLACED ON FIRST BARRICADE FOLLOWING SPACE PROVIDED FOR ACCESS.
3. BARRICADES MAY BE PLACED ON PAVEMENT OR ON SIDEWALK AT THE DISCRETION OF THE ENGINEER. "NO PARKING" SIGNS PLACED ON SIDEWALK SHALL NOT BE SET AT AN ANGLE NO GREATER THAN 30 DEGREES WITH THE LINE OF TRAFFIC FLOW TO BE VISIBLE TO APPROACHING TRAFFIC. A MINIMUM OF 36" CLEAR SPACE ON SIDEWALK SHALL BE MAINTAINED IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT WHEN BARRICADES ARE PLACED ON SIDEWALKS.
4. "NO PARKING" SIGNS AND BARRICADES SHOULD BE PLACED IN AREA OF REHABILITATION AT LEAST 72 HOURS IN ADVANCE OF WORK BEGINNING. NOTIFICATION OF PERSONS AFFECTED BY STREET WORK SHALL BE PERFORMED AS REQUIRED BY RESPECTIVE ENTITY AND NEVADA REVISED STATUTES.
5. ALL BARRICADES AND "NO PARKING" SIGNS SHALL BE REMOVED AS SOON AS IMPROVED SURFACE IS READY FOR TRAFFIC AS DETERMINED BY THE ENGINEER.

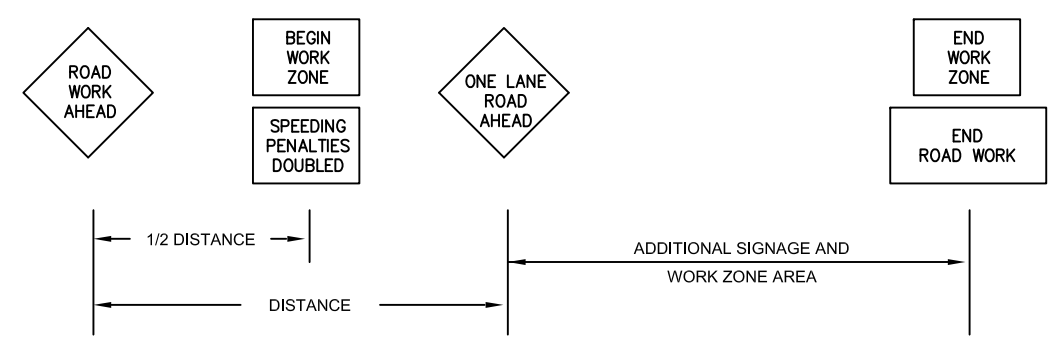
SYMBOLS:





SIGN AND LETTERING SIZE TABLE

SPEED LIMIT	"H" (IN.)	"W" (IN.)	LETTERING "SPEEDING..." SIGN	LETTERING "BEGIN/END..." SIGN
35 MPH OR LESS	18	24	4 INCH SERIES "C"	4 INCH SERIES "D"
GREATER THAN 35 MPH	30	36	6 INCH SERIES "C"	6 INCH SERIES "D"



TYPICAL SIGN PLACEMENT

GENERAL NOTES:

1. REFLECTIVE SIGN SHEETING SHALL CONFORM TO SUBSECTION 716.03.01, LATEST REVISION, OF THE UNIFORM STANDARD SPECIFICATIONS.
2. SIGN LEGENDS AND BORDERS SHALL COMPLY WITH THE REQUIREMENTS OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
3. SIGNS SHALL BE MOUNTED IN SAME MANNER AS OTHER CONSTRUCTION SIGNS IN THE WORK ZONE.
4. "BEGIN WORK ZONE" AND "SPEEDING PENALTIES DOUBLED" SIGNS SHALL BE MOUNTED TOGETHER ON SAME DEVICE OR POST AND SHALL BE LOCATED AFTER THE FIRST SIGN IN THE CONSTRUCTION SIGN SERIES, TYPICALLY THE "ROAD WORK AHEAD" SIGN, OR AS DIRECTED BY THE TRAFFIC ENGINEER.
5. "END WORK ZONE" SIGN SHALL BE MOUNTED AT THE END OF THE WORK ZONE WITH THE "END ROAD WORK" SIGN, IF APPLICABLE, ON THE SAME DEVICE OR POST, OR AS DIRECTED BY THE TRAFFIC ENGINEER.

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