

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

VOLUME I - YEAR 2008 REVISIONS

| Drawing Number | Title | Nature of Change | Effective Date |
|-----------------------|--|--|-----------------------|
| 234.2 | "Typical Bus Stop Passenger Loading and Shelter Pads" | Revision to Accommodate the New Bus Shelter Design and Improve Americans with Disabilities Act (ADA) Requirements. | 07/01/08 |
| 234.2A | "Typical Double Bus Stop Passenger Loading and Shelter Pads" | Revision to Accommodate the New Bus Shelter Design and Improve Americans with Disabilities Act (ADA) Requirements. | 07/01/08 |
| 235 (Sheet 1 of 4) | "Sidewalk Ramp Case I" | Revision to add 'Paired Ramp in Curb Return' to comply with current American with Disabilities Act (ADA) requirements. | 01/01/08 |
| 256.4 | "Shared Use Path Crossing 6 Lane Roadway" | Revision to reduce the 504 feet of restricted crossing to 100 feet for improved access management. | 01/01/08 |
| 303 | "Streetlight Locations at Intersections 100' or Greater/60' Right-of-Way (Except Clark County)" | Revision to correct editorial error in the pole location table. | 01/01/08 |
| 304 | "Streetlight Locations at Intersections 100' or Greater/51' or Less Right-of-Way (Except Clark County)" | Revision to correct editorial error in the pole location table. | 01/01/08 |
| 500AL | "0 To 5 Years Pavement Restoration Longitudinal Cut" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 500AT | "0 To 5 Years Pavement Restoration Transverse Cut" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 500BL1 | "Over 5 Years Pavement Restoration Longitudinal Cut – Greater Than 60'R/W" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 500BL2 | "Over 5 Years Pavement Restoration Longitudinal Cut – Greater Than 60'R/W or Less" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 500BT | "Over 5 Years Pavement Restoration Transverse All R/W Widths" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 502 | "Method B For Rigid And Flexible Pipe Trench Backfill – Unpaved Areas And Areas Outside Existing or Future Proposed Street Right-Of-Way" | Deleted | 07/01/08 |
| 502A | "Method A For Flexible Pipe Trench Backfill – Unpaved Areas And Areas Outside Existing or Future Proposed Street Right-Of-Way" | Deleted | 07/01/08 |

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

VOLUME I - YEAR 2008 REVISIONS

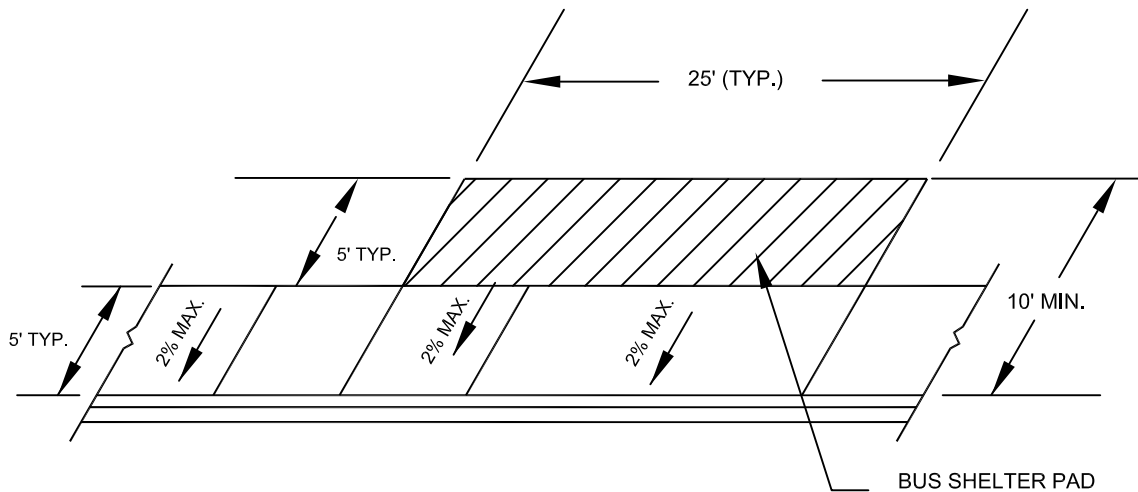
| | | | |
|-------|--|--|----------|
| 502B | "Method A For Rigid Pipe Trench Backfill – Unpaved Areas And Areas Outside Existing or Future Proposed Street Right-Of-Way" | Deleted | 07/01/08 |
| 503 | "Method B For Rigid And Flexible Pipe Trench Backfill – Paved Areas And Areas Outside Existing or Future Proposed Street Right-Of-Way" | Deleted | 07/01/08 |
| 503A | "Method For Flexible Pipe Trench Backfill – Paved Areas" | Revision to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 503B | "Method For Rigid And Flexible Pipe Trench Backfill – Paved Areas" | Revision to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 503AB | "Method For Rigid Pipe Trench Backfill – Paved Areas" | Addition to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |
| 504 | "Trench Backfill with Controlled Low Strength Material (CLSM) Paved Areas (Streets Greater than 60' R/W)" | Revision to clarify the cross section of the permanent asphalt patch. | 01/01/08 |
| 504 | "Trench Backfill with Controlled Low Strength Material (CLSM) Above Pipe Zone In Paved Areas (Streets Greater than 60' R/W)" | Revision to incorporate new Standards from the Recommendations of the recent pavements cut study and trench working group. | 07/01/08 |

Sign up for the-mail notification of Uniform Standard Specifications and Drawings updates at:

<http://www.rtcsonthernnevada.com/mpo/streets/>

Click on the icon:



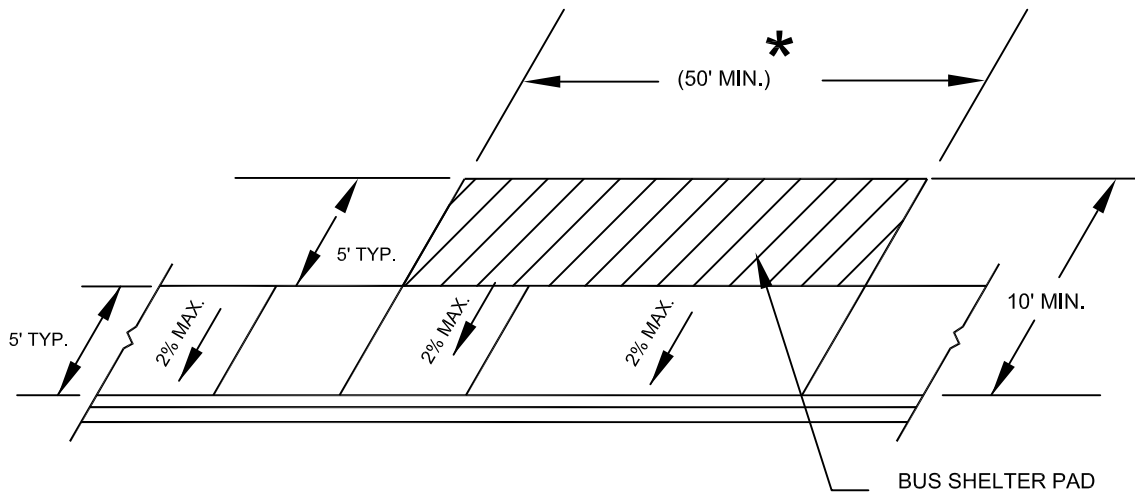


ADDITIONAL AREA REQUIRED BEHIND
TYPICAL 5 FT. SIDEWALK FOR
PASSENGER LOADING PAD

NOTES:

1. SIDEWALK RAMP MAY BE REQUIRED TO BE CONSTRUCTED IN THOSE LOCATIONS WHERE THE BUS STOP WOULD OTHERWISE BE INACCESSIBLE AS DEFINED BY THE AMERICANS WITH DISABILITIES ACT. SEE DRAWING NO. 235, SHEET 4 OF 5 FOR SIDEWALK RAMP DETAILS.
2. ADDITIONAL RIGHT-OF-WAY OR EASEMENT IS REQUIRED FOR LOADING PAD AND VARIABLE HEIGHT CURB AT BACK OF SIDEWALK RAMP AND SHALL BE DEDICATED TO THE REGIONAL TRANSPORTATION COMMISSION OF SOUTHERN NEVADA.
3. AGGREGATE BASE AND CONCRETE FOR LOADING PAD SHALL BE THE SAME AS REQUIRED FOR SIDEWALK. SEE DRAWING NO. 234.
4. LOADING PAD CONNECTION TO DETACHED SIDEWALK CONDITION SHALL BE DETERMINED BY THE ENTITIES.

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
|-------------------------|---------------------|--|----------------|
| 302 | AGGREGATE BASE | TYPICAL BUS STOP PASSENGER LOADING AND SHELTER PADS | |
| 501 | CONCRETE | | |
| 502 | CONCRETE STRUCTURES | | |
| | | | |
| | | DATE 3-13-08 | DWG. NO. 234.2 |



ADDITIONAL AREA REQUIRED BEHIND
TYPICAL 5 FT. SIDEWALK FOR
PASSENGER LOADING PAD

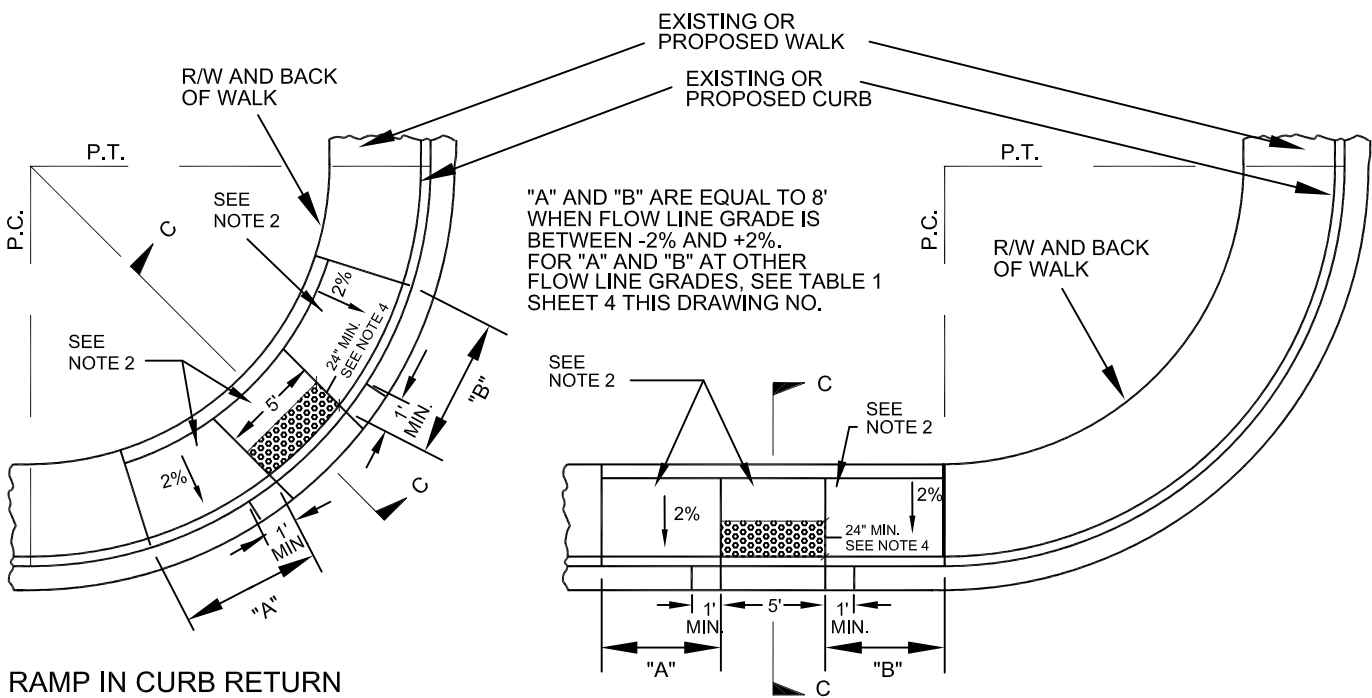


ADDITIONAL 25 FEET MAY BE
REQUIRED BY RTC.

NOTES:

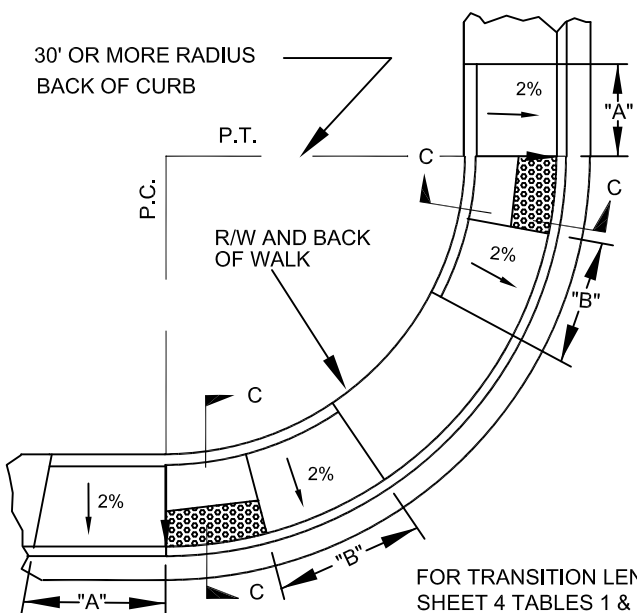
1. SIDEWALK RAMP MAY BE REQUIRED TO BE CONSTRUCTED IN THOSE LOCATIONS WHERE THE BUS STOP WOULD OTHERWISE BE INACCESSIBLE AS DEFINED BY THE AMERICANS WITH DISABILITIES ACT. SEE DRAWING NO. 235, SHEET 4 OF 5 FOR SIDEWALK RAMP DETAILS.
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3. AGGREGATE BASE AND CONCRETE FOR LOADING PAD SHALL BE THE SAME AS REQUIRED FOR SIDEWALK. SEE DRAWING NO. 234.
4. LOADING PAD CONNECTION TO DETACHED SIDEWALK CONDITION SHALL BE DETERMINED BY THE ENTITIES.

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
|-------------------------|---------------------|---|-----------------|
| 302 | AGGREGATE BASE | TYPICAL DOUBLE BUS STOP PASSENGER LOADING AND SHELTER PADS | |
| 501 | CONCRETE | | |
| 502 | CONCRETE STRUCTURES | | |
| | | | |
| | | DATE 3-13-08 | DWG. NO. 234.2A |



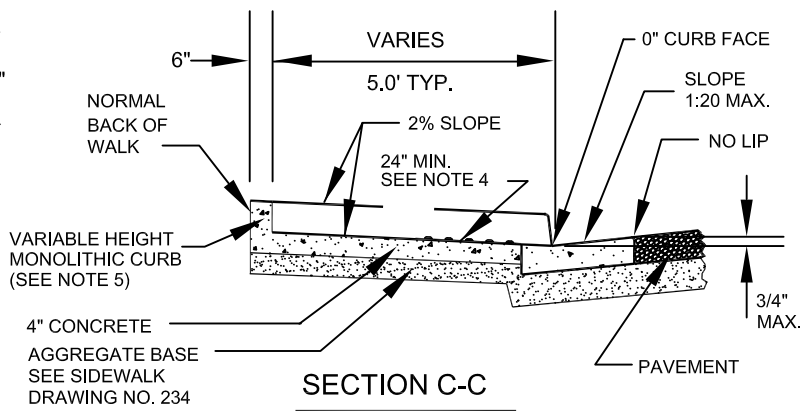
RAMP IN CURB RETURN

RAMP OUTSIDE CURB RETURN



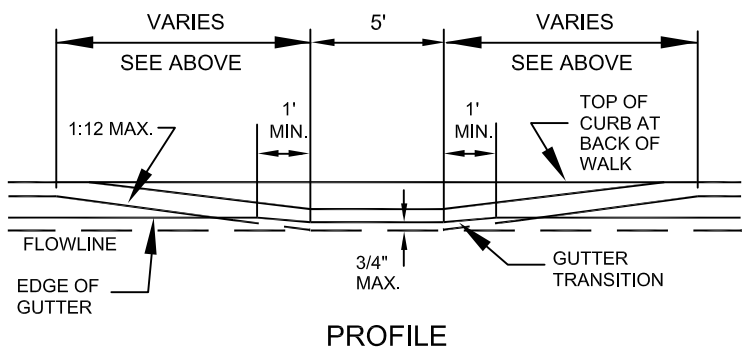
PAIRED RAMP IN CURB RETURN

FOR TRANSITION LENGTHS, SEE SHEET 4 TABLES 1 & 2



NOTES:

1. SIDEWALK RAMPS OUTSIDE OF THE CURB RETURN SHALL BE LOCATED ADJACENT TO THE RETURN UNLESS OTHERWISE APPROVED.
2. RAMPS SHALL BE CONSTRUCTED WITH A ROUGH BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
3. WHEN CONSTRUCTING RAMP WHERE CURB & GUTTER EXISTS, COMPLETELY REMOVE INTERFERING PORTIONS OF EXISTING CURB & GUTTER.
4. DETECTABLE WARNING CONSISTING OF RAISED TRUNCATED DOMES WHICH COMPLY WITH DETAILS ON SHEET 4 OF THIS DRAWING NO. AND CONTRASTING VISUALLY WITH ADJOINING SURFACES SHALL BE PLACED ON BOTTOM PORTION OF RAMP EXTENDING THE FULL WIDTH OF THE RAMP AND TO A MINIMUM DEPTH OF 24 INCHES. PAVER BLOCKS PERMITTED ONLY IN THE CITY OF BOULDER CITY FOR DETECTABLE WARNING AREAS.
5. CURB MAY BE PLACED AND IS PREFERRED BEHIND BACK OF WALK IF SUFFICIENT RIGHT-OF-WAY OR EASEMENTS EXIST AND AS APPROVED BY THE ENGINEER.

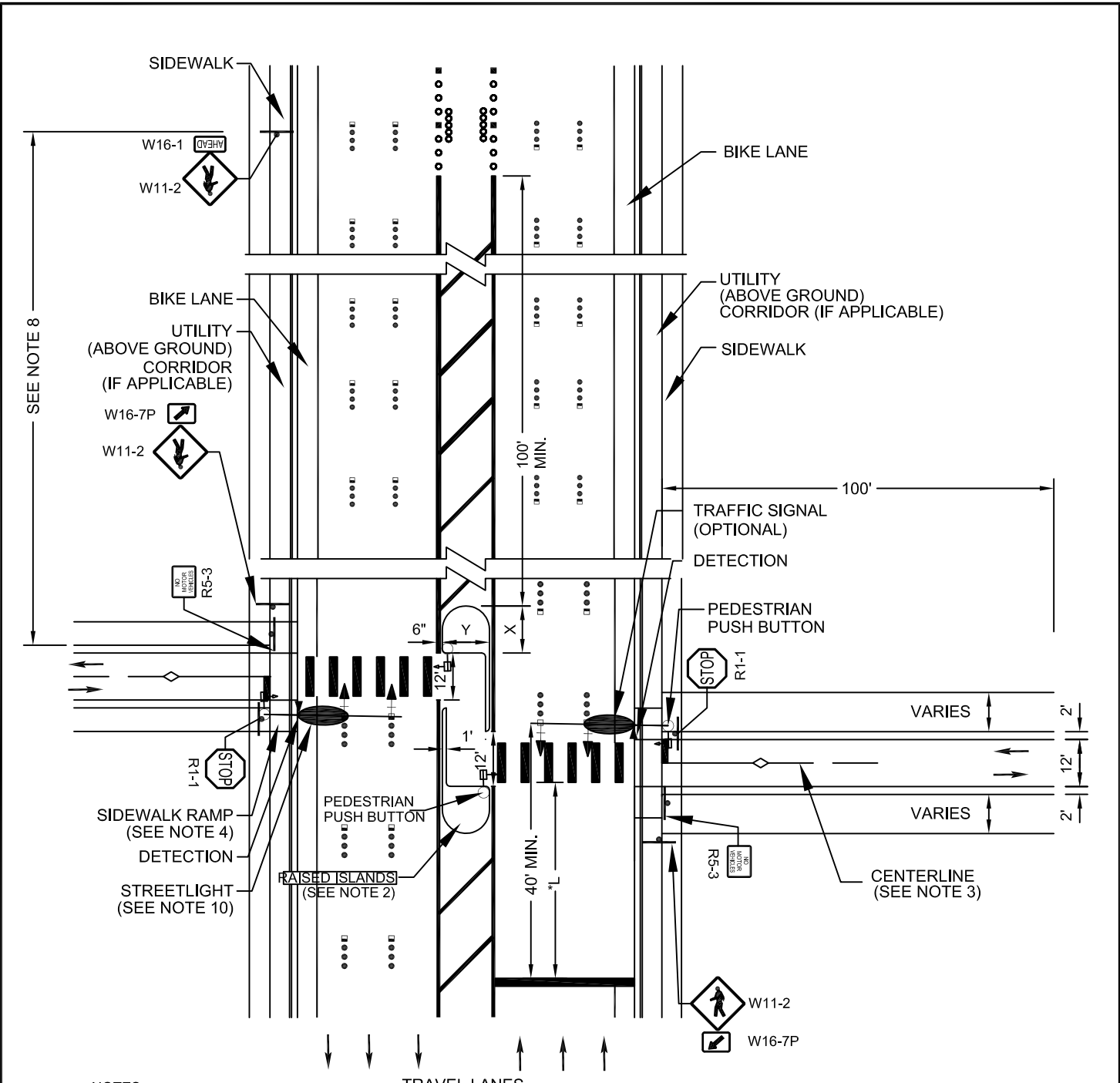


| SPECIFICATION REFERENCE | |
|-------------------------|---------------------|
| 302 | AGGREGATE BASE |
| 501 | CONCRETE |
| 502 | CONCRETE STRUCTURES |
| | |
| | |

**UNIFORM STANDARD DRAWINGS
CLARK COUNTY AREA**

**SIDEWALK RAMP
CASE I**

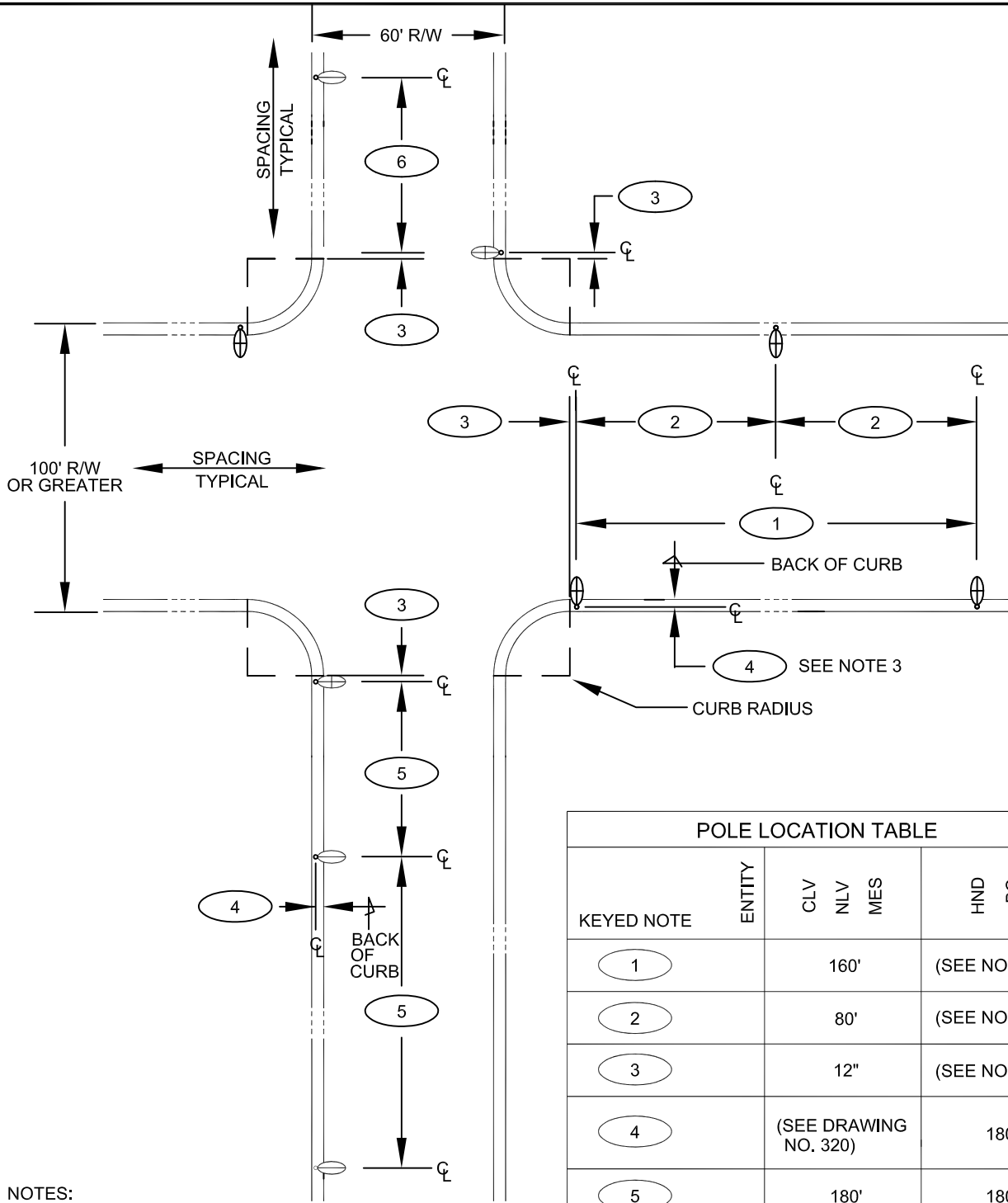
| | | | |
|--------------|--------------|----------|--|
| DATE 11-8-07 | DWG. NO. 235 | (1 OF 4) | |
|--------------|--------------|----------|--|



NOTES:

1. USE ENGINEERING JUDGEMENT TO APPLY THIS DETAIL TO SIMILAR SCENARIOS.
 2. SEE DRAWING NO. 218, 248, AND 256.2 FOR MEDIAN ISLAND.
 3. SEE DRAWING NO. 255.4 FOR BOLLARDS AND CENTERLINE STRIPING.
 4. SEE DRAWING NO. 235, CASE III, FOR SIDEWALK RAMPS (USE 12 FEET INSTEAD OF 5 FEET).
 5. SEE DRAWING NO. 254 AND 254A FOR CROSSWALKS.
 6. SEE DRAWING NO. 255.3 FOR SIGN SIZES FOR SHARED USE PATHS.
 7. SEE DRAWING NO. 345 (2 OF 3) FOR DELINEATION IN TRANSITION SECTIONS.
 8. SEE TABLE 2C-4 IN MUTCD 2000 FOR ADVANCE PLACEMENT OF WARNING SIGNS.
 9. SEE PAGE 654 TO 680 IN AASHTO HIGHWAYS AND STREETS 2001 FOR SIGHT VISIBILITY ZONES (SIGHT TRIANGLES).
 10. SEE STREET LIGHTING SECTION.
 11. CONTACT AGENCY'S TRAFFIC ENGINEER TO VERIFY IF AGENCY PREFERENCES TO USE A W11-1 (BICYCLE) SIGN IN PLACE OF THE W11-1 SIGN.
- * L=SPEED LIMIT

| | | | | | |
|-------------------------|---------------------------|--|-------|----------|-------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | | | |
| 628 | PAINTING TRAFFIC STRIPING | SHARED USE PATH CROSSING 6 LANE ROADWAY | | | |
| 633 | PAVEMENT MARKERS | | | | |
| | | | | | |
| | | | | | |
| DATE | 7-12-07 | DWG. NO. | 256.4 | PAGE NO. | 59.25 |



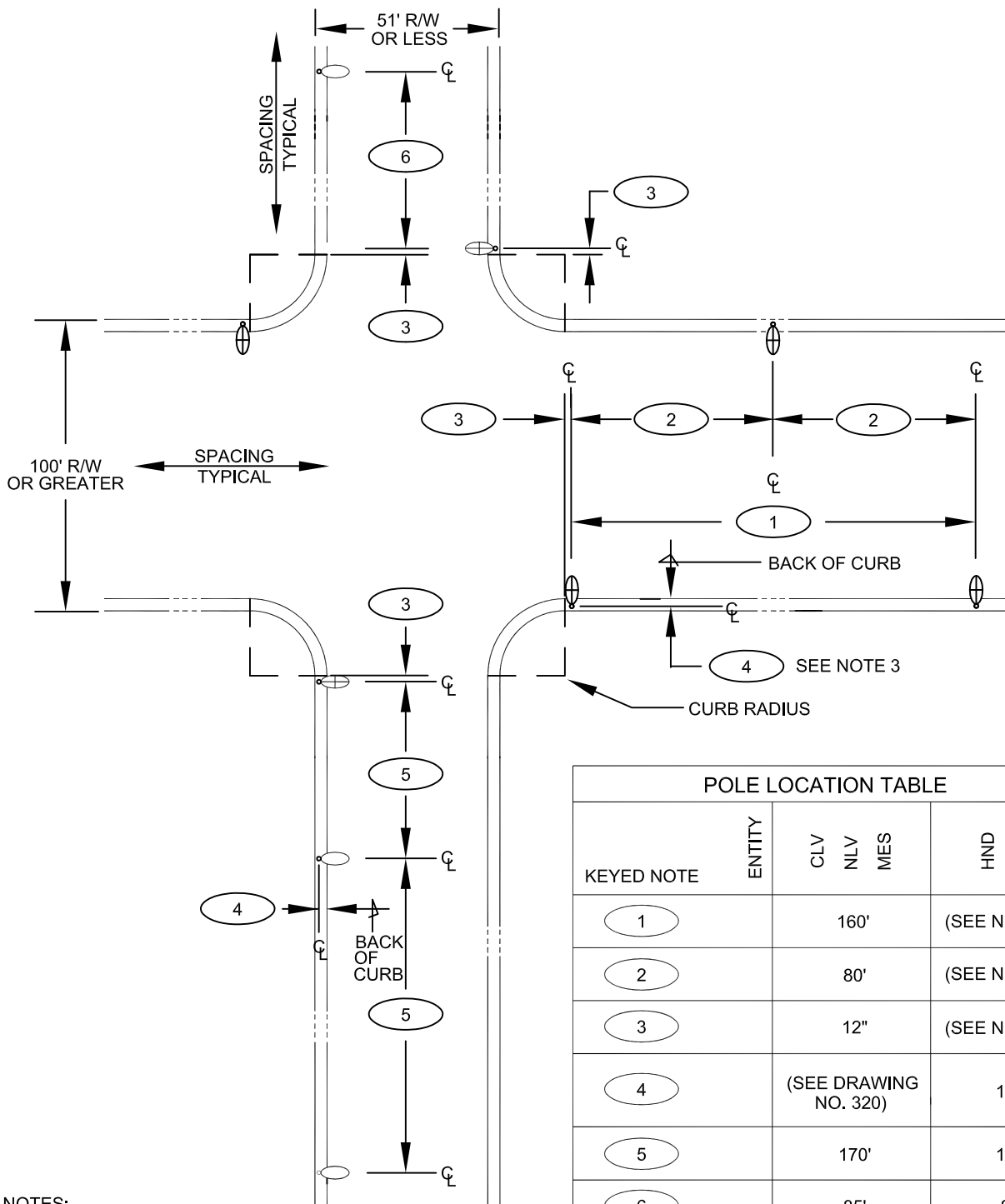
NOTES:

1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

| POLE LOCATION TABLE | | | | | |
|---------------------|--------|-----------------------|-----|--------------|--------|
| KEYED NOTE | ENTITY | CLV | | | HND BC |
| | | NLV | MES | | |
| 1 | | 160' | | (SEE NOTE 3) | |
| 2 | | 80' | | (SEE NOTE 3) | |
| 3 | | 12" | | (SEE NOTE 3) | |
| 4 | | (SEE DRAWING NO. 320) | | 180' | |
| 5 | | 180' | | 180' | |
| 6 | | 90' | | 90' | |

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | | | |
|-------------------------|----------------------------------|--|---------|----------|-----|
| 623 | TRAFFIC SIGNALS & STREETLIGHTING | STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/60' RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON) | | | |
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| | | | | | |
| | | DATE | 2-08-07 | DWG. NO. | 303 |



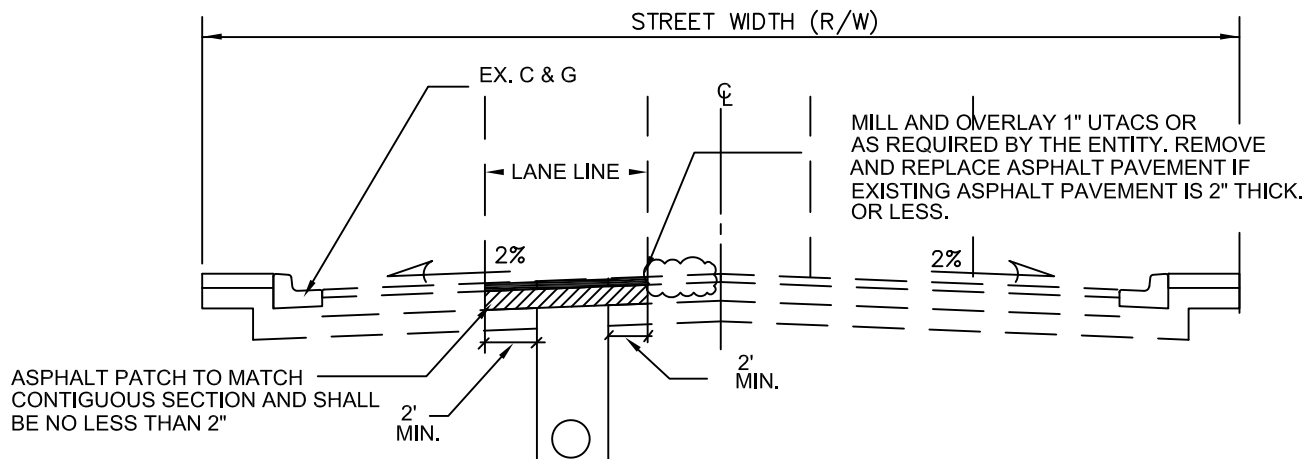
| POLE LOCATION TABLE | | | |
|---------------------|--------|-----------------------|--------------|
| KEYED NOTE | ENTITY | CLV NLV MES | HND BC |
| 1 | | 160' | (SEE NOTE 3) |
| 2 | | 80' | (SEE NOTE 3) |
| 3 | | 12" | (SEE NOTE 3) |
| 4 | | (SEE DRAWING NO. 320) | 180' |
| 5 | | 170' | 170' |
| 6 | | 85' | 85' |

NOTES:

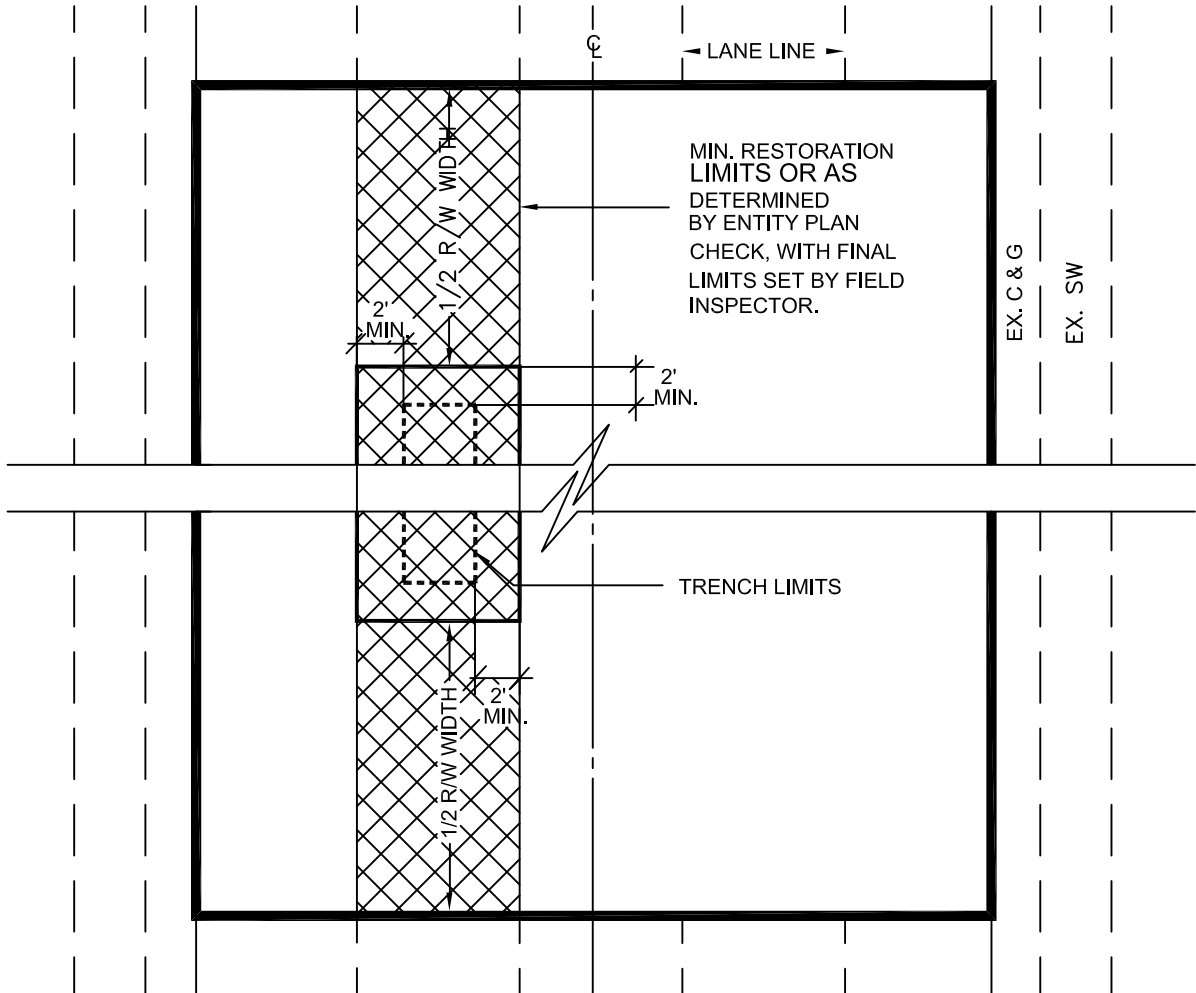
1. SEE GENERAL NOTES STANDARD DRAWING NO. 300.
2. AN APPROVED LIGHTING STUDY PER NOTE 2, STANDARD DRAWING NO. 300 IS REQUIRED FOR RIGHT-OF-WAY GREATER THAN 100 FEET. ADEQUATE INTERSECTION LIGHTING SHALL ALSO BE ADDRESSED IN THE LIGHTING STUDY.
3. CITY OF HENDERSON AND BOULDER CITY REQUIRE STREETLIGHTING IN THE MEDIAN FOR RIGHTS-OF-WAY 100 FEET OR GREATER. SEE STANDARD DRAWING NO. 312. IN THE ABSENCE OF A MEDIAN, STREETLIGHT LOCATION SHALL BE THE SAME AS THE OTHER ENTITIES.

* PROFESSIONAL ELECTRICAL ENGINEER STAMP ON FILE.

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
|-------------------------|----------------------------------|--|--------------|
| 623 | TRAFFIC SIGNALS & STREETLIGHTING | STREETLIGHT LOCATIONS AT INTERSECTIONS 100' OR GREATER/51' OR LESS RIGHT-OF-WAY (EXCEPT CLARK COUNTY & HENDERSON) | |
| | | | |
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| | | DATE 2-08-07 | DWG. NO. 304 |



LONGITUDINAL CUT RESTORATION



PLAN VIEW

NOTES:

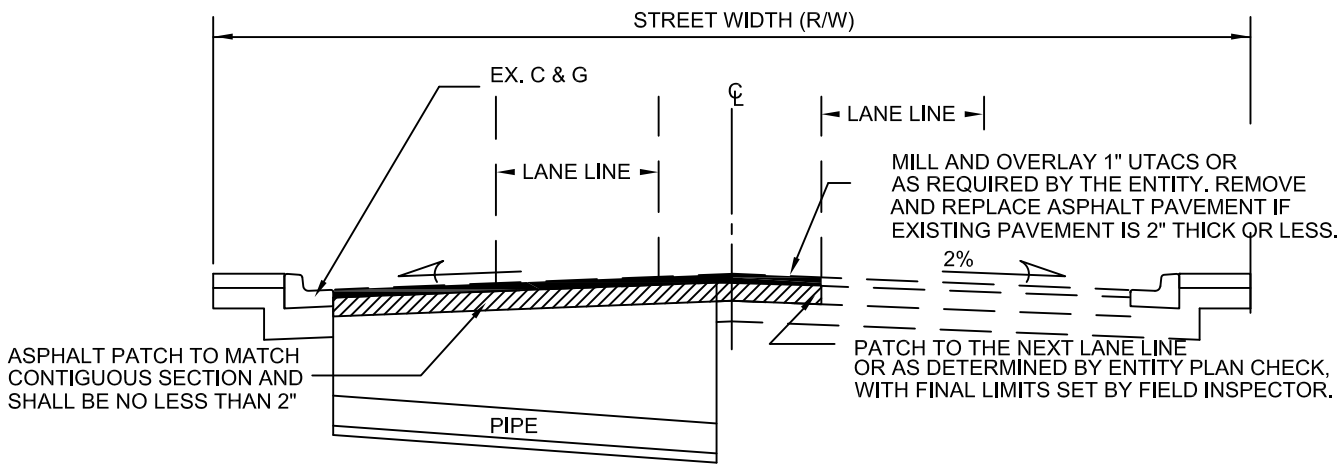
SEE DWG. 500AL SHEET 2 OF 2

| | | | | |
|-------------------------|---------------------|---|----------------|--------------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | | |
| 302 | AGGREGATE BASE | 0 TO 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT | | |
| 401 | BITUMINOUS PAVEMENT | | | |
| 406 | PRIME COAT | | | |
| 407 | FOG SEAL | | | |
| 501 | CONCRETE | | | |
| | | DATE 6-12-08 | DWG. NO. 500AL | SHEET 1 OF 2 |

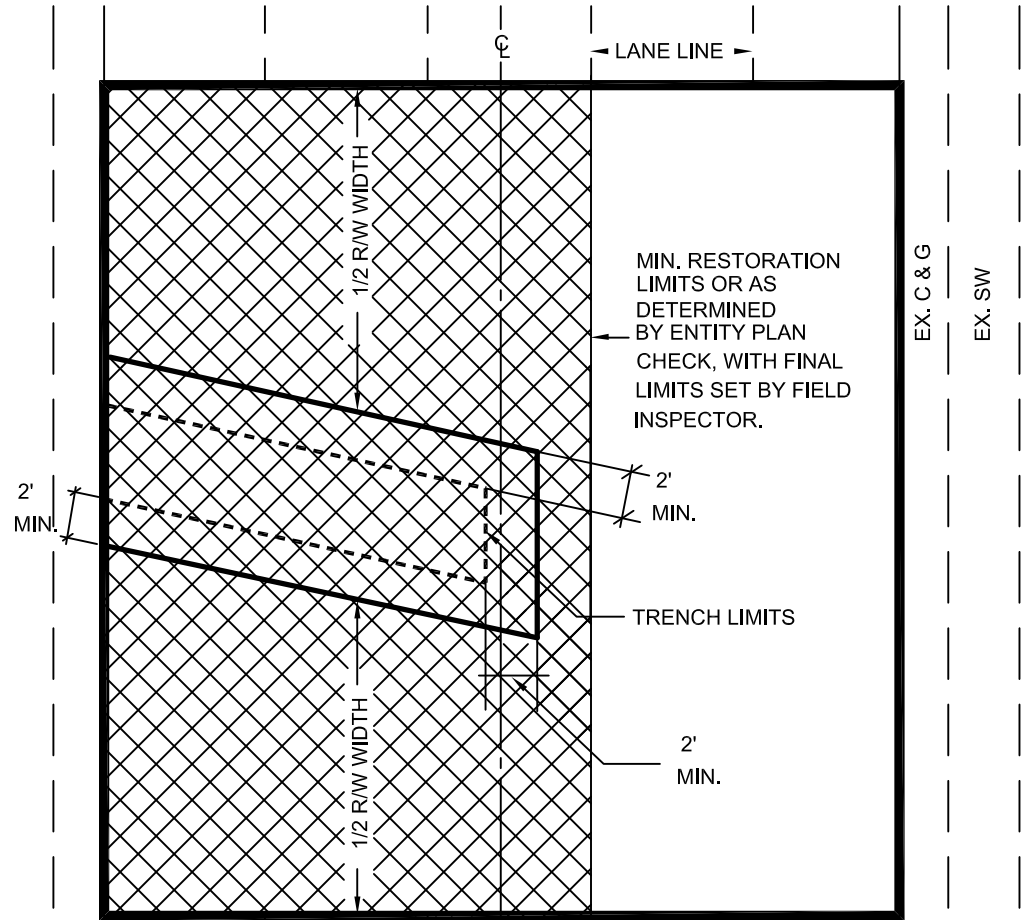
NOTES:

1. IF THERE IS A MEDIAN, RESTORATION MAY BE LIMITED TO THE AREA BETWEEN C & G AND THE MEDIAN CURB.
2. WHEN EXISTING PAVEMENT IS 2" THICK OR LESS, PAVEMENT WITHIN THE RESTORATION AREA SHALL BE REMOVED AND REPLACED IN KIND AS REQUIRED BY THE ENTITY.
3. IF SAWCUT LINE IS WITHIN FIVE FEET OF EDGE OF EXISTING ASPHALT CONCRETE SURFACE OR EXISTING SAWCUT LINE, MILL AND OVERLAY OR REPLACE TO THAT EDGE.
4. PAVEMENT RESTORATION AREA SAWCUT LINES SHALL NOT FALL WITHIN STREET INTERSECTION.
5. IF CUT IS WITHIN A LANE , PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | | |
|-------------------------|---------------------|---|----------------|--------------|
| 302 | AGGREGATE BASE | 0 TO 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT | | |
| 401 | BITUMINOUS PAVEMENT | | | |
| 406 | PRIME COAT | | | |
| 407 | FOG SEAL | | | |
| 501 | CONCRETE | | | |
| | | DATE 6-12-08 | DWG. NO. 500AL | SHEET 2 OF 2 |



TRANSVERSE CUT RESTORATION

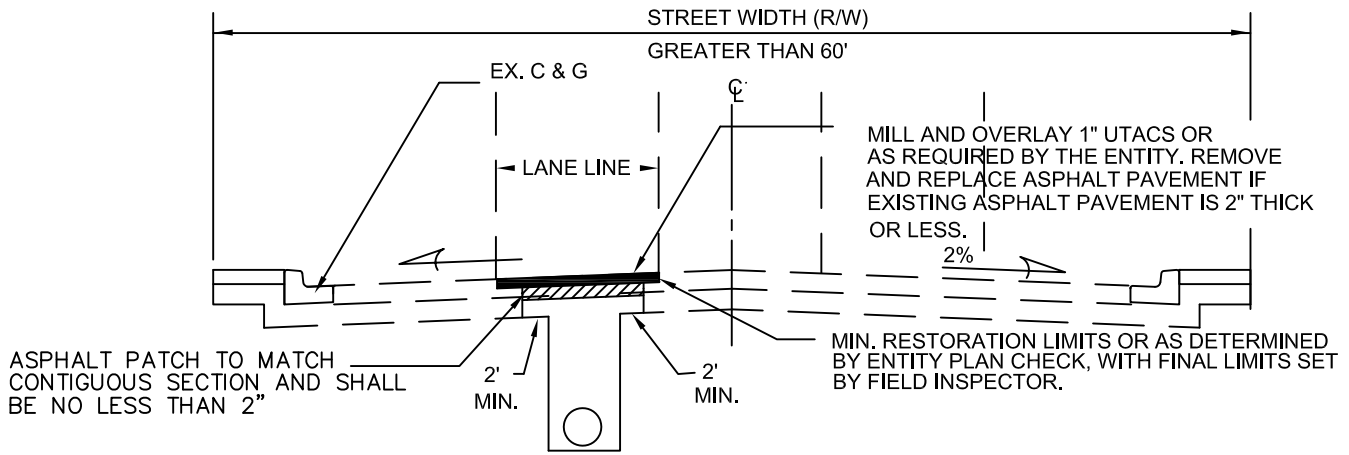


PLAN VIEW

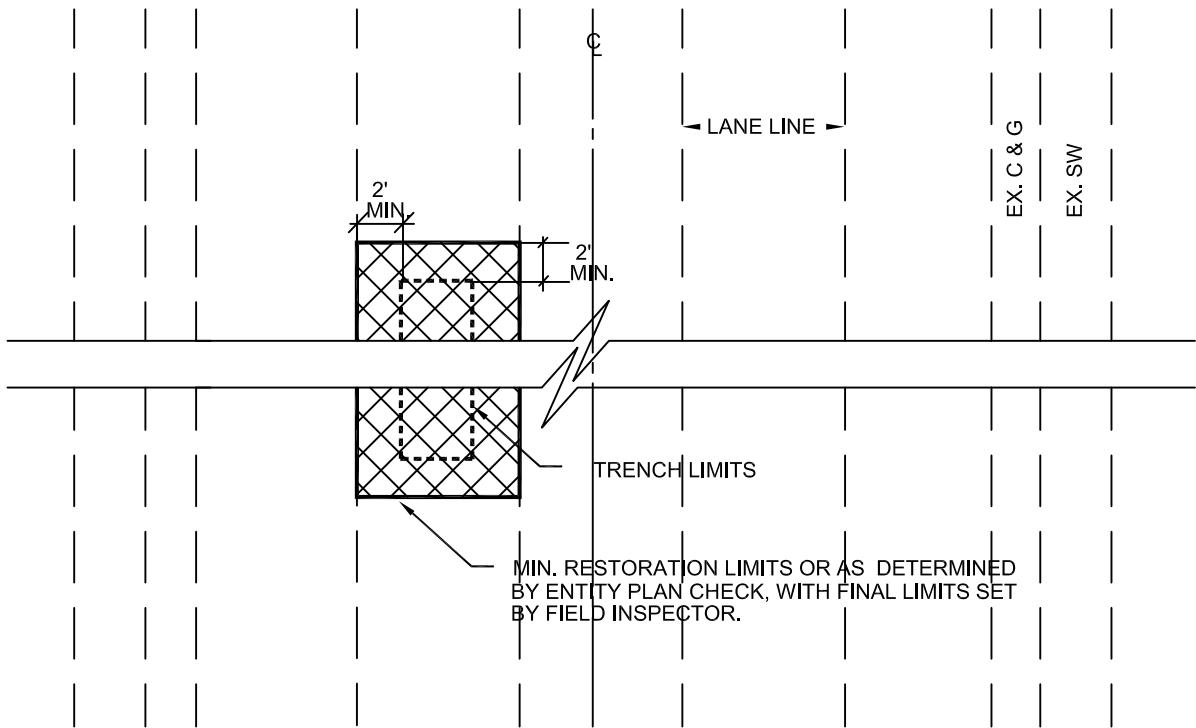
NOTES:

1. IF THERE IS MEDIAN, RESTORATION MAY BE LIMITED TO THE AREA BETWEEN C&G TO CURB OF MEDIAN.
2. IF SAWCUT LINE IS WITHIN FIVE FEET OF EDGE OF EXISTING ASPHALT CONCRETE SURFACE OR EXISTING SAWCUT LINE, MILL AND OVERLAY OR REPLACE TO THAT EDGE.
3. WHEN EXISTING PAVEMENT IS LESS 2" THICK OR LESS, PAVEMENT WITHIN THE RESTORATION AREA SHALL BE REMOVED AND REPLACED IN KIND AS REQUIRED BY THE ENTITY.
4. PAVEMENT RESTORATION AREA SAWCUT LINES SHALL NOT FALL WITHIN STREET INTERSECTION.
5. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.

| | | | |
|-------------------------|---------------------|---|-----------------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 302 | AGGREGATE BASE | 0 TO 5 YEARS PAVEMENT RESTORATION TRANSVERSE CUT | |
| 401 | BITUMINOUS PAVEMENT | | |
| 406 | PRIME COAT | | |
| 407 | FOG SEAL | | |
| 501 | CONCRETE | | |
| | | DATE 6-12-08 | DWG. NO. 500 AT |



LONGITUDINAL CUT RESTORATION

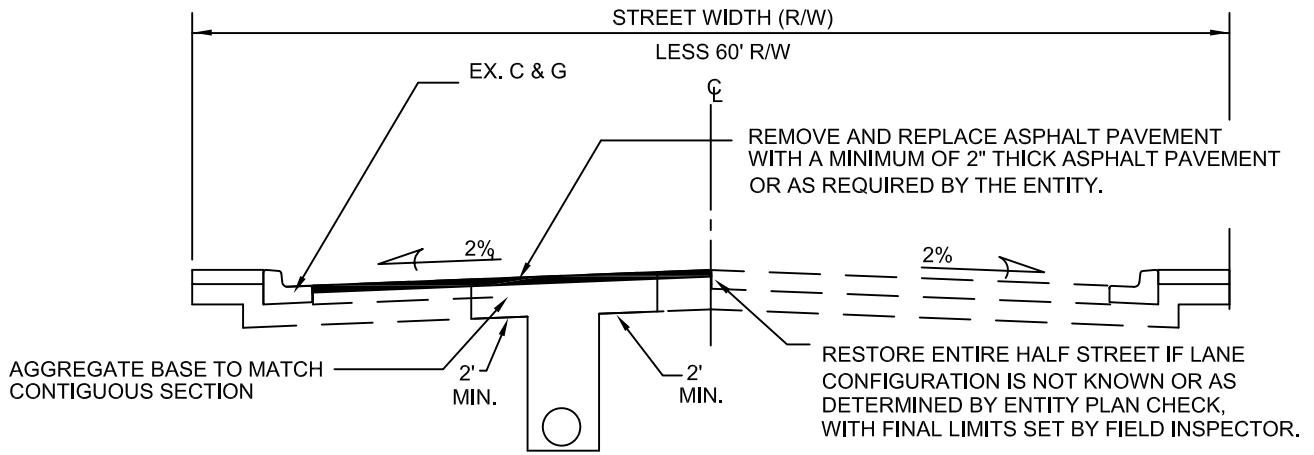


PLAN VIEW

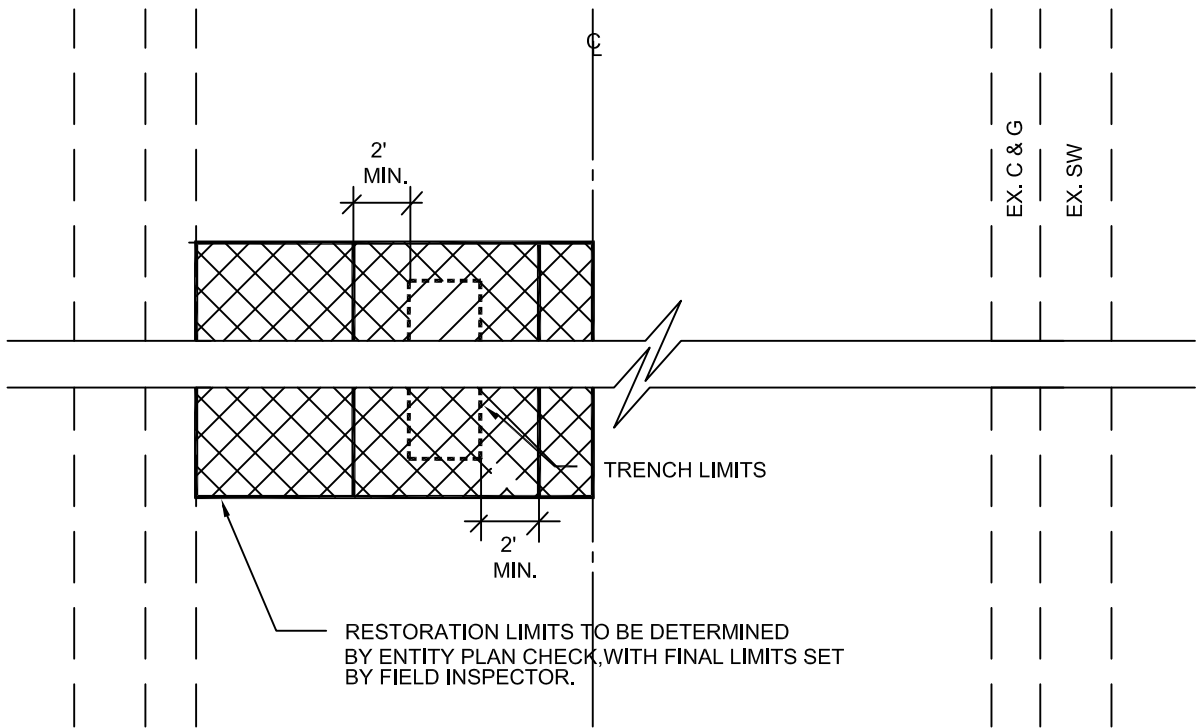
NOTES:

- 1. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.

| | | | |
|-------------------------|---------------------|--|-----------------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 302 | AGGREGATE BASE | OVER 5 YEARS PAVEMENT RESTORATION - LONGITUDINAL CUT- GREATER THAN 60' R/W | |
| 401 | BITUMINOUS PAVEMENT | | |
| 406 | PRIME COAT | | |
| 407 | FOG SEAL | | |
| 501 | CONCRETE | | |
| | | DATE 6-12-08 | DWG. NO. 500BL1 |



LONGITUDINAL CUT RESTORATION

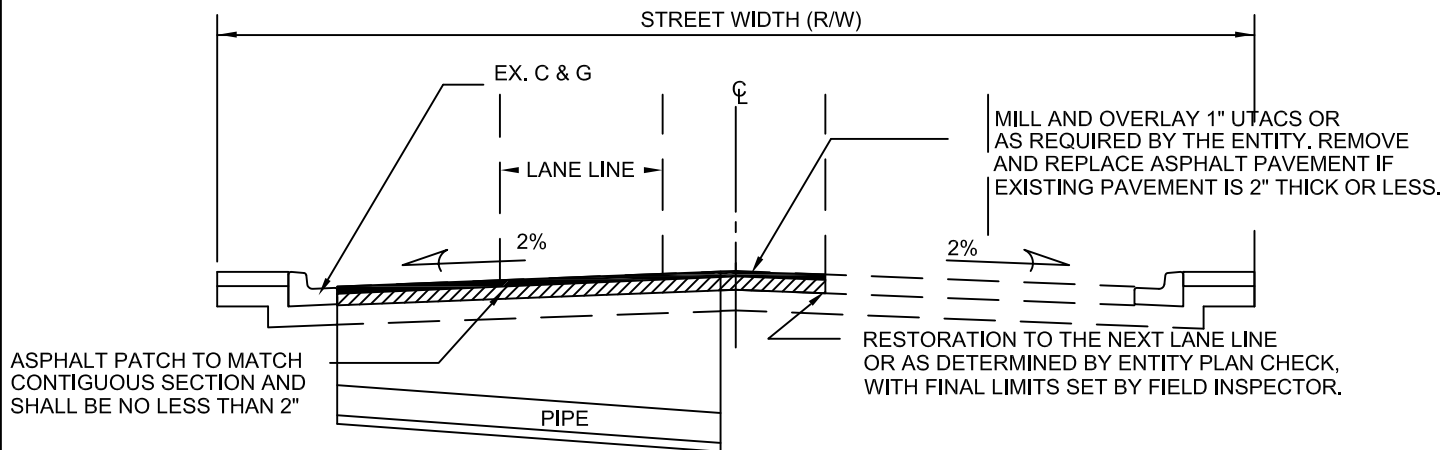


PLAN VIEW

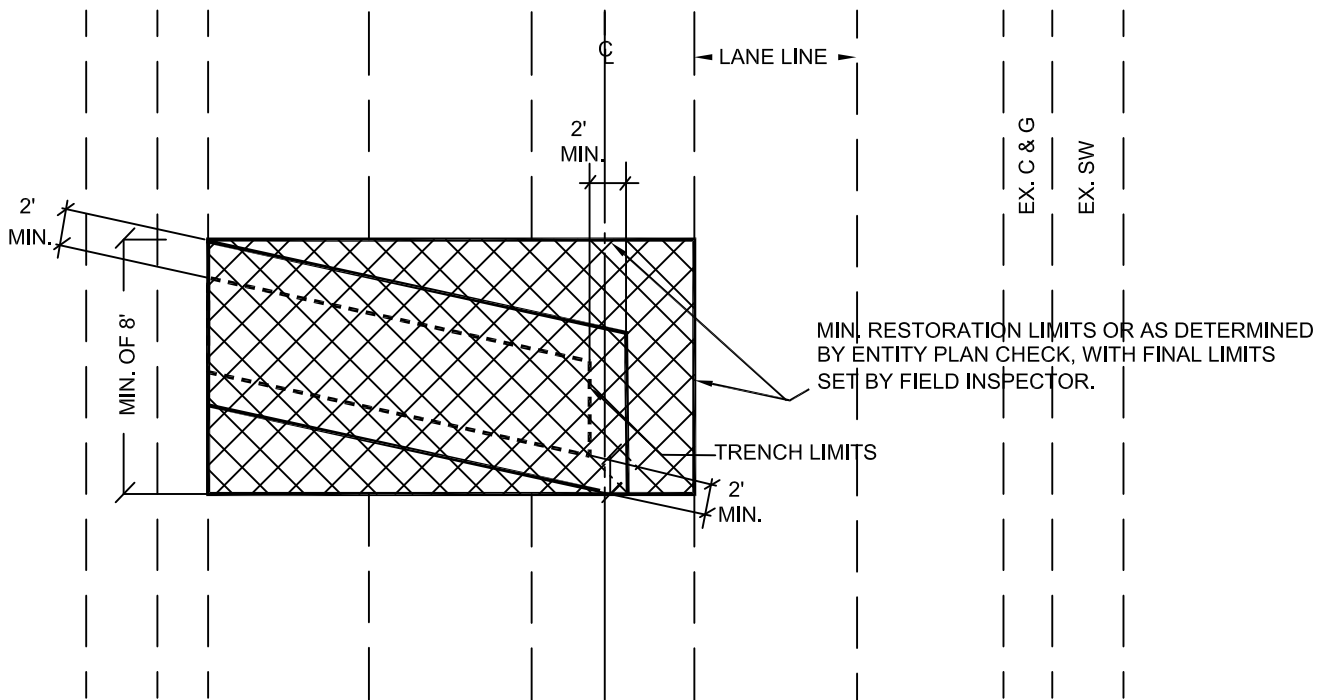
NOTES:

1. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.

| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
|-------------------------|---------------------|---|-----------------|
| 302 | AGGREGATE BASE | OVER 5 YEARS PAVEMENT RESTORATION LONGITUDINAL CUT - 60' R/W OR LESS | |
| 401 | BITUMINOUS PAVEMENT | | |
| 406 | PRIME COAT | | |
| 407 | FOG SEAL | | |
| 501 | CONCRETE | | |
| | | DATE 6-12-08 | DWG. NO. 500BL2 |



TRANSVERSE CUT RESTORATION

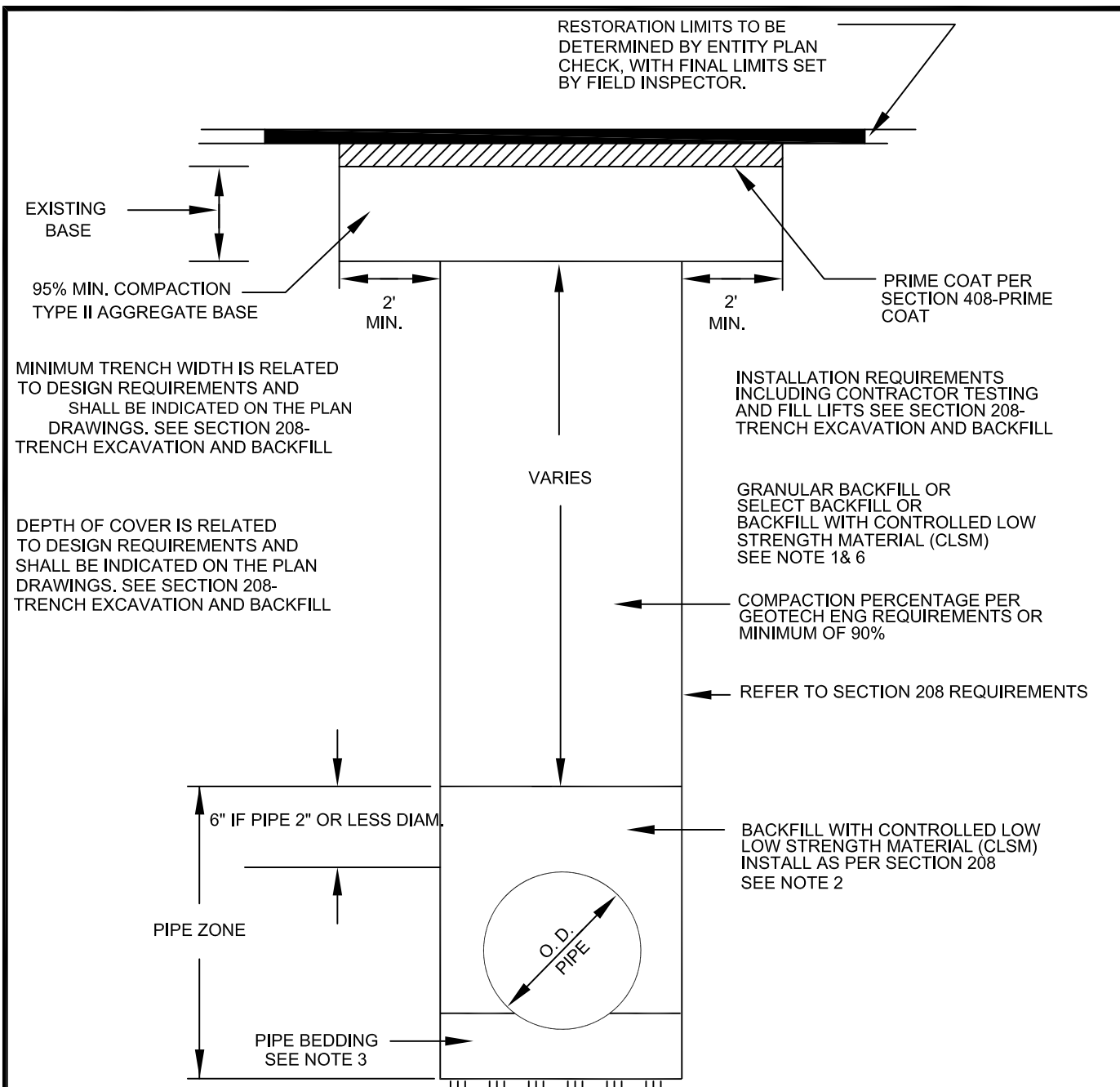


PLAN VIEW

NOTES:

1. IF CUT IS WITHIN A LANE, PAVEMENT RESTORATION MUST EXTEND TO THE NEXT LANE LINE.

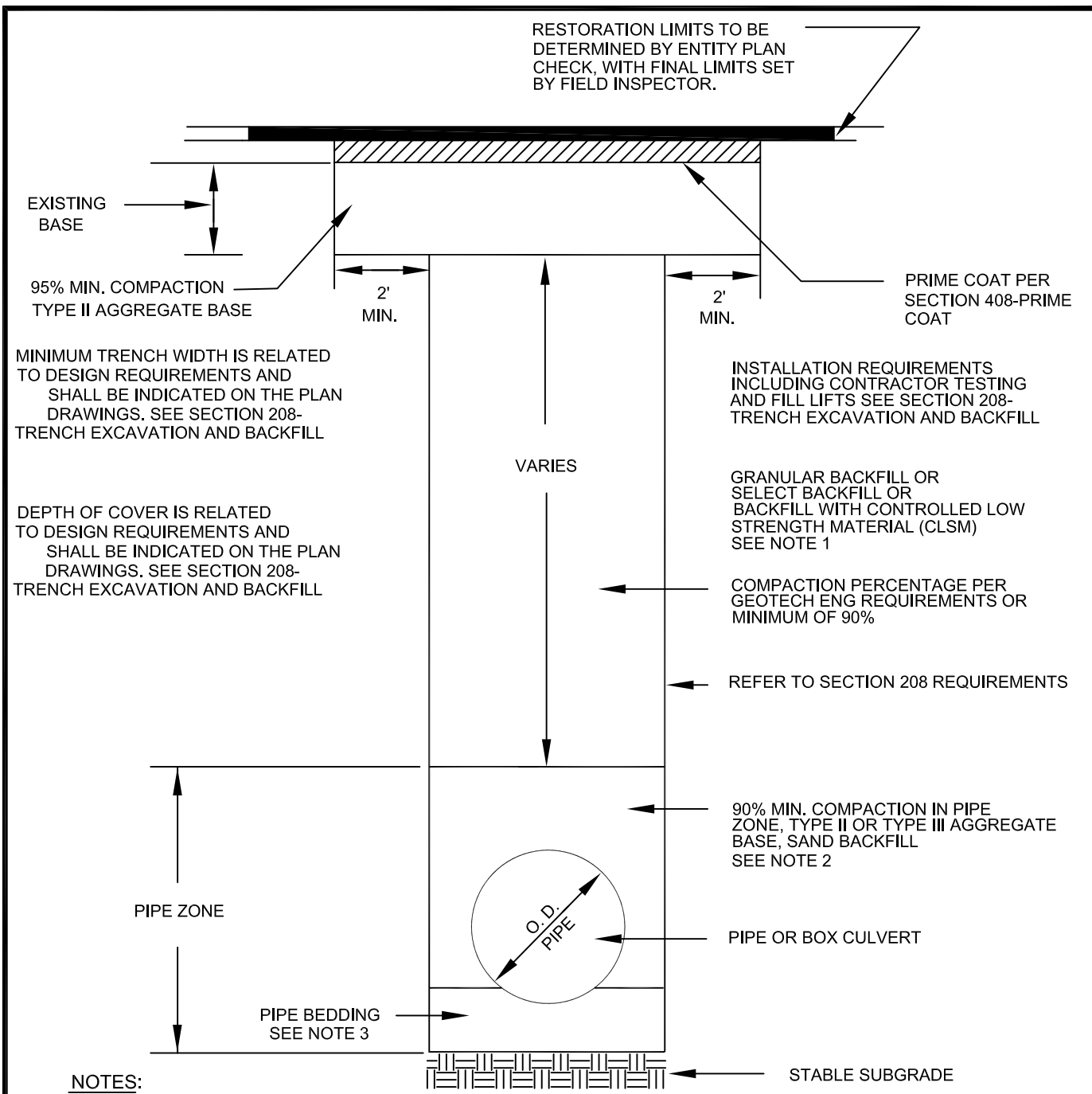
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| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 302 | AGGREGATE BASE | OVER 5 YEARS PAVEMENT RESTORATION TRANSVERSE CUT ALL R/W WIDTHS | |
| 401 | BITUMINOUS PAVEMENT | | |
| 406 | PRIME COAT | | |
| 407 | FOG SEAL | | |
| 501 | CONCRETE | | |
| | | DATE 6-12-08 | DWG. NO. 500BT |



NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.
5. A ONE INCH MAXIMUM LEVELING COURSE IS PERMITTED WHEN APPROVED BY THE ENGINEER.
6. CONTROLLED LOW STRENGTH MATERIALS (CLSM) SHALL BE USED IN THE UPPER 24" WITH RIGHT-OF-WAYS 80 FEET OR GREATER. TRENCH WIDTHS 24" OR LESS SHALL BE BACKFILLED WITH CLSM IN THE TRENCH ZONE.

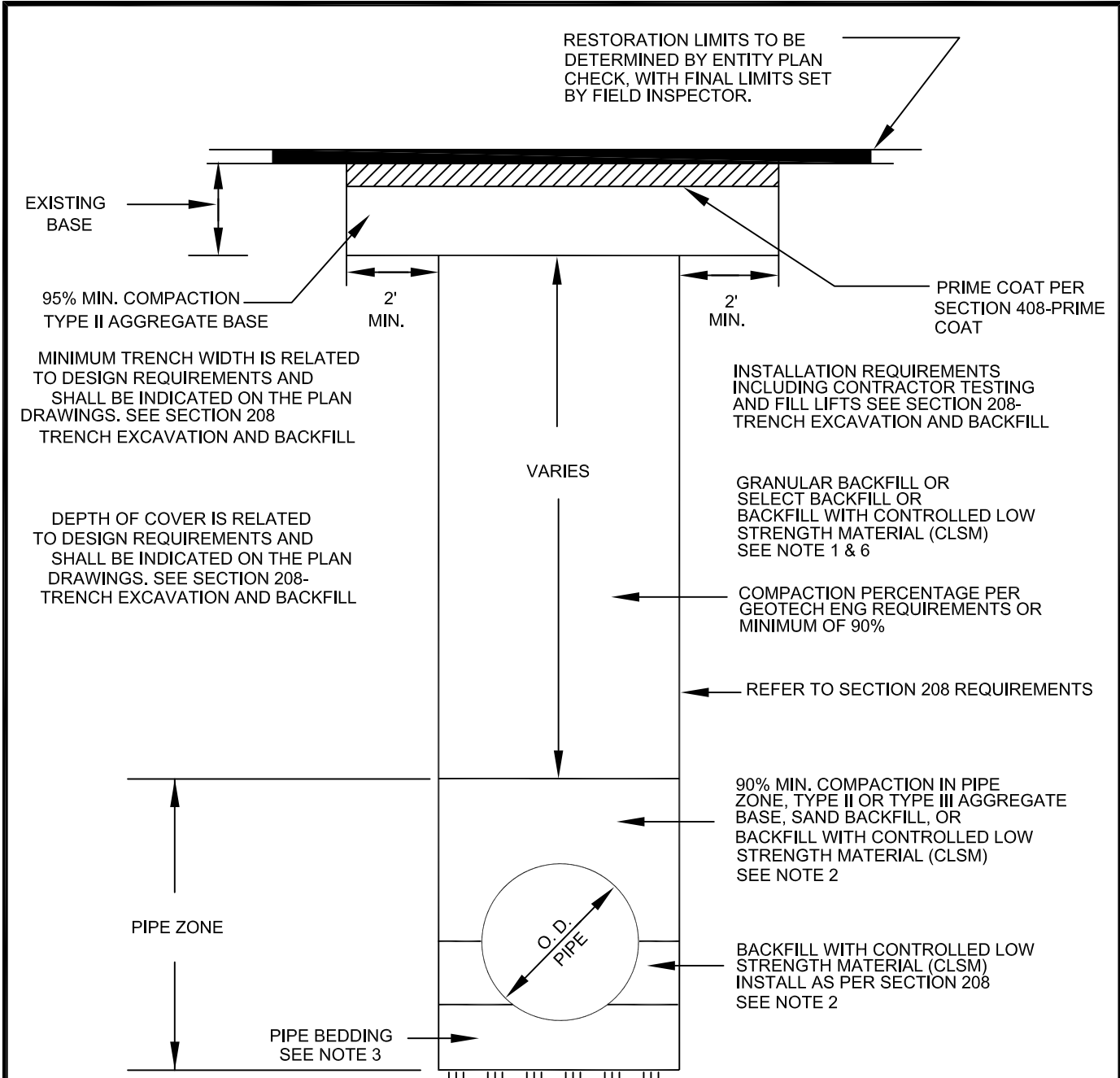
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|-------------------------|------------------------------|---|---------------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 208 | TRENCH EXCAVATION & BACKFILL | | |
| 302 | AGGREGATE BASE COURSES | METHOD A FOR FLEXIBLE PIPE TRENCH BACKFILL - PAVED AREAS | |
| | | | |
| | | | |
| | | | |
| | | DATE 6-12-08 | DWG. NO. 503A |



NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
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4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.
5. A ONE INCH MAXIMUM LEVELING COURSE IS PERMITTED WHEN APPROVED BY THE ENGINEER.

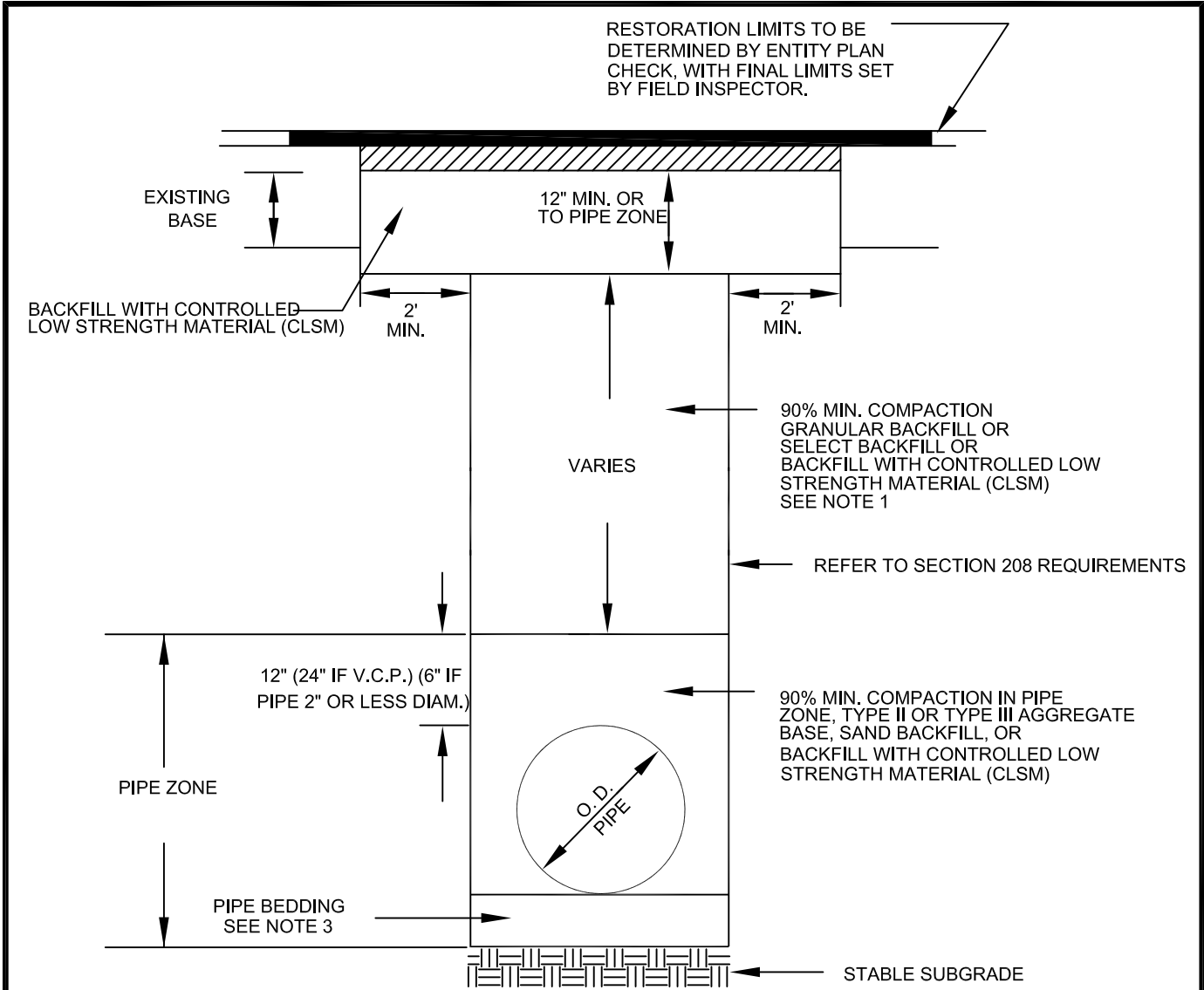
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|-------------------------|------------------------------|---|---------------|
| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 208 | TRENCH EXCAVATION & BACKFILL | | |
| 302 | AGGREGATE BASE COURSES | METHOD B FOR RIGID AND FLEXIBLE PIPE TRENCH BACKFILL - PAVED AREAS | |
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| | | | |
| | | DATE 6-12-08 | DWG. NO. 503B |



NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. TRENCH WIDTH, BEDDING, SUBGRADE AND PIPE ZONE REQUIREMENTS FOR UTILITY INSTALLATIONS SHALL CONFORM TO THE RESPECTIVE ENTITY REQUIREMENTS.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING AGENCY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.
5. A ONE INCH MAXIMUM LEVELING COURSE IS PERMITTED WHEN APPROVED BY THE ENGINEER.
6. CONTROLLED LOW STRENGTH MATERIALS (CLSM) SHALL BE USED IN THE UPPER 12" WITH RIGHT-OF-WAYS 60' FEET OR GREATER.

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| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 208 | TRENCH EXCAVATION & BACKFILL | METHOD A/B FOR RIGID PIPE TRENCH BACKFILL – PAVED AREAS | |
| 302 | AGGREGATE BASE COURSES | | |
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| | | DATE 6-12-08 | DWG. NO. 503AB |



NOTES:

1. NO STONES OR LUMPS GREATER THAN 3" PERMITTED IN TRENCH 2' OR LESS IN WIDTH.
2. IF SAWCUT IS WITHIN THREE FEET OF EDGE OF EXISTING ASPHALT CONCRETE SURFACE OR OTHER PATCH, REMOVE EXISTING PAVEMENT TO THAT EDGE AND REPLACE ENTIRE SECTION.
3. CRUSHED ROCK MAY BE USED FOR PIPE BEDDING ONLY IF MATERIAL USE HAS BEEN SPECIFICALLY APPROVED BY THE GOVERNING ENTITY. SEE STANDARD DRAWING NO. 505 FOR PIPE BEDDING METHODS.
4. LAS VEGAS VALLEY WATER DISTRICT REQUIRES PIPE BEDDING AND BACKFILL WITHIN THE PIPE ZONE TO BE OF THE SAME MATERIAL.

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| SPECIFICATION REFERENCE | | UNIFORM STANDARD DRAWINGS CLARK COUNTY AREA | |
| 208 | TRENCH EXCAVATION & BACKFILL | TRENCH BACKFILL WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM) ABOVE PIPE ZONE IN PAVED AREAS (STREETS GREATER THAN 60' R/W) | |
| 302 | AGGREGATE BASE COURSES | | |
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| | | DATE 6-12-08 | DWG. NO. 504 |